

# PUBLIC HEARING BINDER

3428-3464 Mt. Seymour Parkway

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AGENDA INFORMATION	
<input checked="" type="checkbox"/> Regular Meeting	Date: <u>May 28, 2018</u>
<input type="checkbox"/> Other:	Date: _____



**The District of North Vancouver  
REPORT TO COUNCIL**

May 16, 2018  
File: 08.3060.20/033.17

**AUTHOR:** Erik Wilhelm, Development Planner

**SUBJECT:** Rezoning Bylaw 8275 and Housing Agreement Bylaw 8276 - 29 Unit Townhouse Development at 3428-3464 Mt. Seymour Pkwy.

**RECOMMENDATION:**

THAT "District of North Vancouver Rezoning Bylaw 1366 (Bylaw 8275)" to rezone the properties at 3428-3464 Mount Seymour Parkway from "Single-Family Residential One Acre Zone" (RS1) to "Comprehensive Development Zone 114" (CD114) be given FIRST reading;

AND THAT "Housing Agreement Bylaw 8276, 2017 (3428 - 3464 Mount Seymour Parkway)" be given FIRST reading;

AND THAT Bylaw 8275 be referred to a Public Hearing.

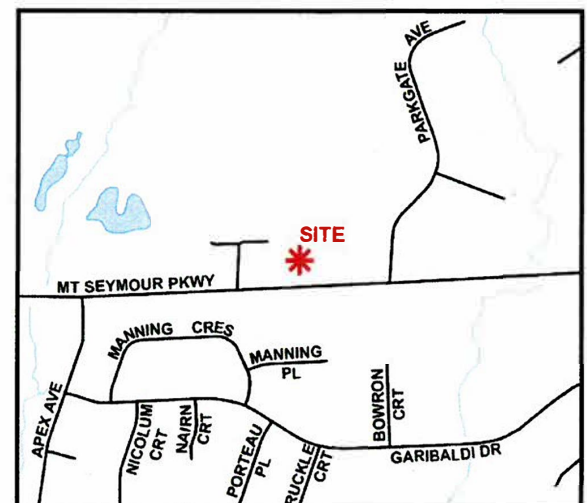
**REASON FOR REPORT:**

The proposed project requires Council's consideration of:

- Bylaw 8275 to rezone the subject properties; and,
- Bylaw 8276 to authorize a housing agreement to ensure all future owners are eligible to rent their units.

**SUMMARY:**

The applicant proposes to redevelop four residential lots located at 3428-3464 Mt. Seymour Parkway to allow a 29 unit townhouse development with a Floor Space Ratio (FSR) of 1.2.

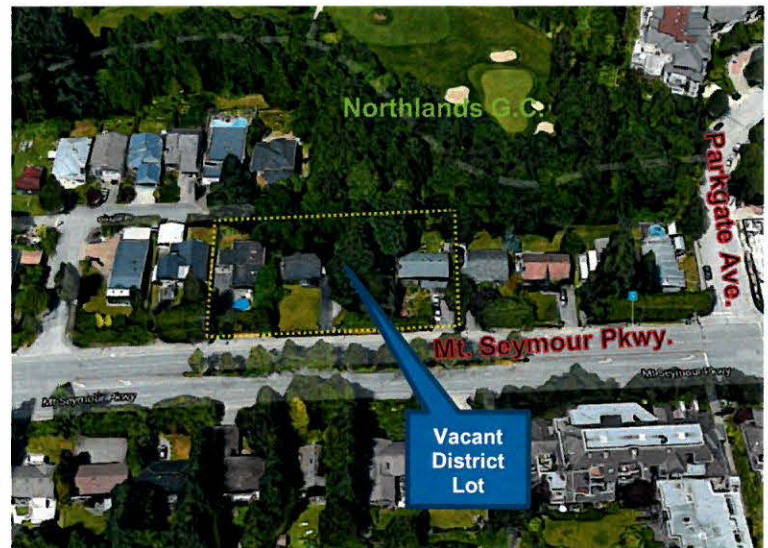




Implementation of the project requires a rezoning (Bylaw 8275), and a Housing Agreement (Bylaw 8276). The proposal is in keeping with the Official Community Plan and is consistent with Land Use provisions of the Seymour Local Plan (reference policy document). The rezoning bylaw is recommended to be referred to a Public Hearing (the housing agreement bylaw does not require a Public Hearing).

### **SUBJECT PROPERTY:**

The development site is approximately 0.27 hectares (0.66 acres) in area and includes 3 single-family lots and a District owned lot which is currently vacant (see adjacent image). The site is located on the north side of Mount Seymour Parkway west of Parkgate Avenue. Surrounding uses includes unconstructed lane and Northlands Golf Course to the north; single family homes to the west; single family homes (across Mt. Seymour Parkway). Single family homes are located east of the site but a 27 unit townhouse proposal is anticipated to be considered by Council on the four adjacent lots east of the development site.



### **EXISTING POLICY:**

#### Official Community Plan

The adjacent map indicates the surrounding OCP designations in the area.

The site is designated “Residential Level 4: Transition Multi Family” in the Official Community Plan. This designation envisions a mix of townhouse and apartment developments in close proximity to centres and corridors with a density of up to approximately 1.2 FSR. The proposed density of 1.2 FSR is in keeping with the OCP density provisions for the site.



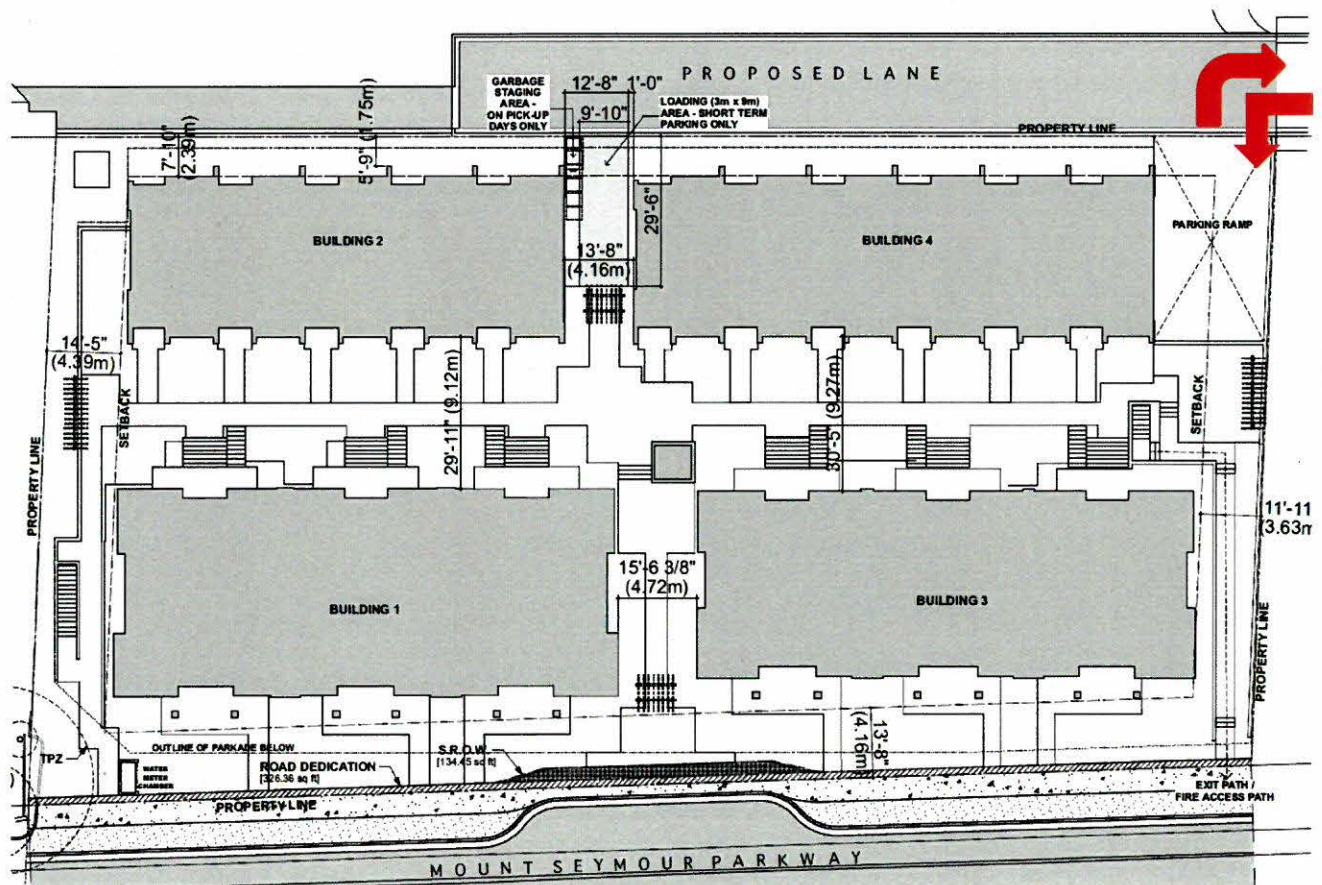
The proposal is intended to provide housing to meet the needs of downsizers, the missing middle and families.



**ANALYSIS:**

Site Plan and Project Description

The project as proposed consists of 29 townhouse units in four separate buildings (see site plan below) with an approximate Floor Space Ratio of 1.2. The secured underground parking area (providing a total of 57 stalls) is proposed to be accessed from the rear lane to the west of Parkgate Avenue. This new lane within the existing dedicated lane area will be constructed by the developer.



All buildings proposed are three storeys in height and all buildings include rooftop decks. The development will provide for twenty-three 3-bedroom townhouse units and six 2-bedroom townhouse units.

The image on the next page depicts the general architectural expression being pursued. The buildings have an orderly presence along the street frontages with a west coast modern architectural character not currently seen along Mt. Seymour Pkwy. All of the buildings incorporate similar colours, materials and building forms yet each building provides for subtle variation. The architectural drawings are attached as Attachment 1.



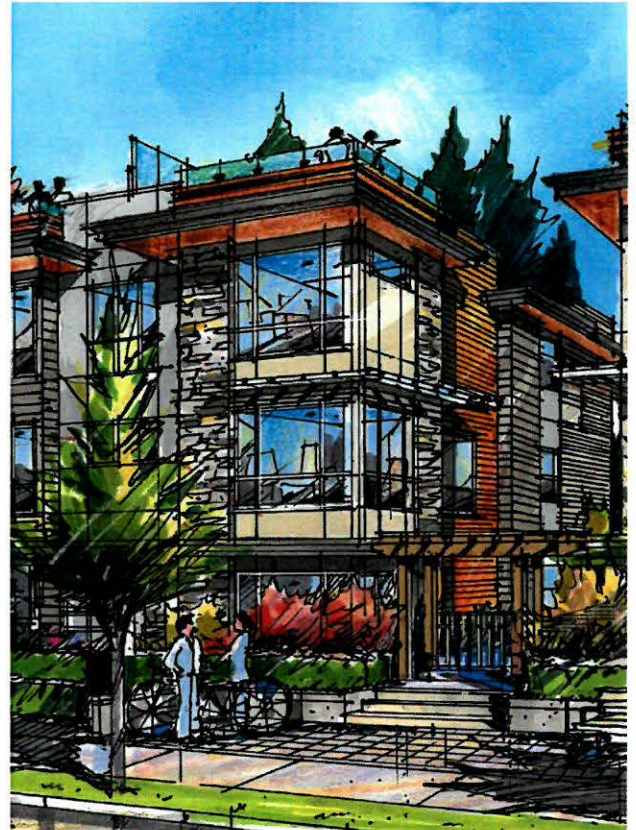
Development Permits – Development Site

The development site is designated within Development Permit Areas for (1) Form and Character; and (2) Energy and Water Conservation and GHG Emission Reduction.

a) Form and Character:

The proposal complies with the “Design Guidelines for Ground-Oriented Housing” as outlined within the OCP. Further details outlining the project’s compliance with the Form and Character Design Guidelines will be provided for Council’s consideration at the Development Permit stage should the rezoning bylaw proceed.

As noted previously, a 27 unit townhouse proposal is anticipated to be considered by Council on the four adjacent lots east of the development site. The image below provides an indication of the buildings fronting Mt. Seymour Parkway (south elevation). The two developments differ in architectural form and detailing. Both projects were designed to provide front door entrances onto the Parkway and front yard landscaping while still providing building breaks to ensure a continuous street wall is not created.



b) Energy and Water Conservation and GHG Emission Reduction:

In April 2017, the Province adopted the BC Energy Step Code (“Step Code”) which provides an incremental and consistent approach to achieving more energy-efficient buildings beyond the requirements of the base BC Building Code. The “Step Code” has been included with the District’s new Construction Bylaw and mandatory compliance in effect as of July 1, 2018. The development proposal will need to comply with the “Step 3” of the Step Code as the building permit for this project will likely be submitted after July 1, 2018.



Further details outlining the project's compliance with the Energy and Water Conservation and Greenhouse Gas Emission Reduction DPA will be provided for Council's consideration at the Development Permit stage should the rezoning bylaw proceed.

#### Development Permits – Off Site (Northlands Golf Course)

The lands north of the development site, within Northlands Golf Course, are designated within Development Permit Areas for (1) Wildfire Hazard; (2) Streamside Protection; and (3) Protection of the Natural Environment. The development proposal necessitates construction of the rear lane. The lane construction activities will prompt compliance with the previously noted Development Permit Areas and the associated guidelines. Removal of trees and restoration works, necessary for lane construction, within Northlands Golf Course is proposed and will be further identified later in this report.

#### Advisory Design Panel

The application was considered by the Advisory Design Panel (ADP) on October 11, 2017 and the Panel recommended approval of the project subject to addressing Panel comments. The applicant has addressed the Panel's comments by improving the architecture to provide more open space between buildings, an enlarged central gathering area with new elevator location/arrival point, improved children's play area on the eastern side of the site, provision of a rear lane loading space, modified parkade and landscaping on the western edge of the site to provide tree preservation for the adjacent neighbour.

Further design information, responding to the Advisory Design Panel comments and Development Permit Area design guidelines, will be provided when Council considers the required Development Permit should the application proceed through the rezoning process.

#### Accessibility

As part of the development process, the applicant will submit a checklist which identifies how the development fulfils the requirements of the District's "Accessible Design Policy for Multi-Family Housing". In accordance with the policy, 6 ground floor units will provide "basic accessible design elements" and 4 of those units (14%) will also include "enhanced accessible design elements", to provide a higher level of accessibility. The approach to accessibility exceeds the targets in the District's policy.

#### Zoning

The site is currently zoned Residential Single Family One Acre (RS1). To accommodate the development, the site must be rezoned to permit ground-oriented housing and Bylaw 8275 (Attachment 2) creates a new Comprehensive Development Zone 114 (CD114) for this purpose. This new zone establishes the following requirements:



- establishes townhouses as a permitted use;
- limits the number of units to 29 townhouses;
- Limits the floor space area to a maximum of 3,408 m<sup>2</sup> (36,683 sq. ft) which equates to 1.2 FSR;
- establishes a community amenity contribution of \$521,274;
- establishes a maximum building height of 12.04 m (39.5 ft);
- sets building coverage at a maximum of 50%;
- sets site coverage at a maximum of 55%;
- requires the provision of a total of 57 off-street vehicle parking stalls, including 7 visitor stalls, and 1 loading/drop-off stall at the rear lane;
- requires a secure shared bicycle storage room (for at least 18 bicycles);
- requires secure individual bicycle storage areas (for at least 22 bicycles); and
- establishes building setbacks.

#### Strata Rental Protection Policy

Corporate Policy 8-3300-2 "Strata Rental Protection Policy" applies to this project as the rezoning application would permit development of more than five residential units. The policy requires a Housing Agreement to ensure that future strata bylaws do not prevent owners from renting their units. Bylaw 8256 (Attachment 3) authorizes a Housing Agreement to implement this policy.

#### Traffic Generation & Parking

##### Traffic:

The developer's transportation consultant, in conjunction with the development applicant to the east, has submitted a coordinated traffic impact assessment (TIA) report which identifies the potential traffic generated from both development proposals and provides a comprehensive review of the traffic movements in the immediate area. The TIA report reviews existing conditions and forecasts future traffic conditions utilizing projected development as outlined in the OCP. Nearby intersections were reviewed and vehicular movements are expected to function within acceptable parameters and experience limited congestion at peak travel times along Mt. Seymour Parkway or along Parkgate Avenue.

The District's Transportation Engineering staff has reviewed the submitted TIA report and finds that the proposed development will not unduly affect traffic along Mt. Seymour Parkway or along Parkgate Avenue.

##### Parking:

Given concerns raised at the Public Information Meeting, the developer was required to provide a comprehensive review of on-street parking conditions along Parkgate Avenue. Despite anecdotal testimonials, Parkgate Avenue was found to have ample on-street parking available throughout the day; however, had increased use during sports and church events taking place further north along



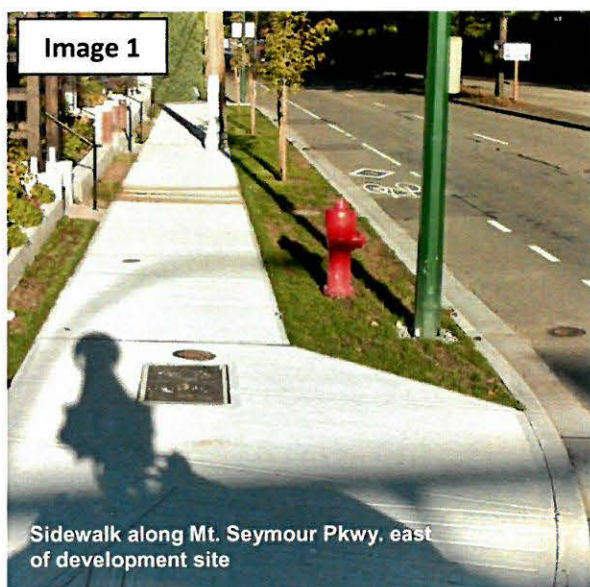
Parkgate Avenue. The applicant proposes at least 57 parking stalls which includes a minimum of 7 visitor stalls and one at grade stall directly accessing from the lane (for quick drop-offs or temporary loading).

The District's Transportation Engineering staff has reviewed the proposed parking on-site and find that the development will not adversely effect on-street parking along Parkgate Avenue.

### **Off-site Improvements:**

#### *Roads:*

The application includes upgrades to asphalt, sidewalks, boulevard, curb, gutter, and lighting along the Mt. Seymour Parkway frontage. The roadway design will provide for a planted boulevard separating the sidewalk from the curb on Mt. Seymour Parkway (Image 1). A new pull-out (similar to the pull-out shown in Image 2) will be provided on Mt. Seymour Parkway which will be time duration parking for drop-offs or temporary loading.

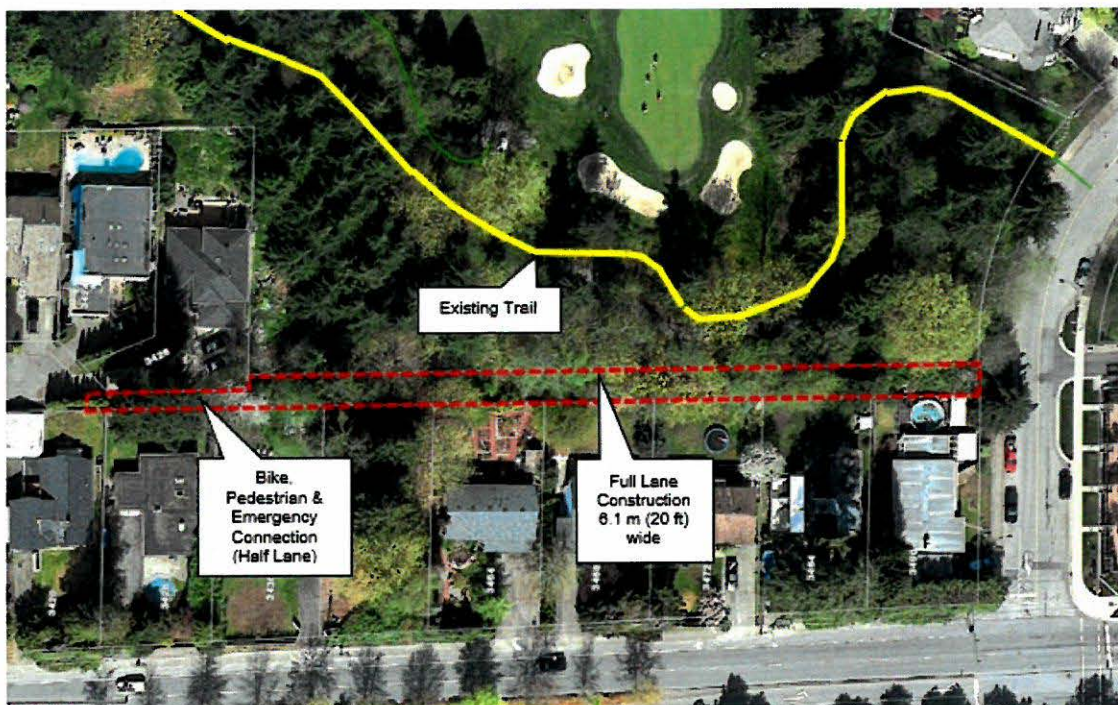


#### *Lane:*

As originally contemplated within the Seymour Local Plan, the applicant (in conjunction with the applicant to the east) will be required to pave the rear lane to the extent seen below. This lane is eventually slated to link Parkgate Avenue and Gaspe Place to the west; however, the western edge of the lane will have bollards installed and will remain with only a bike/pedestrian/emergency connection in order to minimize traffic disruptions on Gaspe Place. Upon potential redevelopment of the balance of the block and/or Gaspe Place, it would be expected that the lane would be widened to provide full vehicle access. In the interim, the lane will serve only the subject development site and the adjacent townhouse development to the east. The lane egress to Gaspe Place may be used at



times during construction if required to minimize impacts to Parkgate Avenue and Mt. Seymour Parkway.



*Tree removal off-site:*

Lane construction will necessitate removal of trees within the existing dedicated/unconstructed lane and on the southern edge of Northlands Golf Course. In August of 1996, the golf course lands were dedicated as “park” upon adoption of Bylaw 6904 (Northlands Golf Course Park Dedication Bylaw). Development Engineering and Planning staff have reviewed the lane design and have determined that lane construction is necessary to service the development and to reduce interruptions to traffic, cycling and transit movements on Mt. Seymour Parkway.

The applicant was required to submit an arborist report and environmental review of the area given the potential environmental impact to Northlands Golf Course. The environmental review identified that no significant environmental features are present in the area needed for lane construction activities and further outlined a rehabilitation and restoration planting plan for an area north of lane (with Northlands Golf Course). Parks, Environmental and Development Engineering staff have reviewed the reports submitted by the applicant and concur that 16 trees will need to be removed within the unconstructed lane and 1 tree removed within Northlands Golf Course lands. Additional tree removal may be necessary depending on final tree health and wind firming review closer to construction (should the rezoning bylaw proceed).

Although tree removal is not normally encouraged on public land this development proposal is consistent with the OCP and Seymour Local Plan. Furthermore, the developer has provided all necessary information to understand the tree loss and has provided preliminary restoration plans to



encourage environmental rehabilitation in the area of disturbance. The developer will be required to restore the lands as part of the rezoning process and install an “environmental fence” to discourage disturbance of the restored area north of the lane within Northland Golf Course lands. The restoration works to areas north of the lane will be required prior to start of construction and will be protected with environmental fencing. This will be ensured through covenant in order to limit the time needed to provide the restoration works.

Although this portion of lane construction necessitates less disturbance of Northlands Golf Course lands, golf course staff have identified that tree loss in this area will not adversely affect the golf course operations and tree removal may actually allow more winter time sunlight infiltration to encourage growth and drainage of the nearby green (13<sup>th</sup> hole).

Further details outlining the project’s compliance with the Development Permit Areas for (1) Wildfire Hazard; (2) Streamside Protection; and (3) Protection of the Natural Environment will be provided for Council’s consideration at the Development Permit stage should the rezoning bylaw proceed.

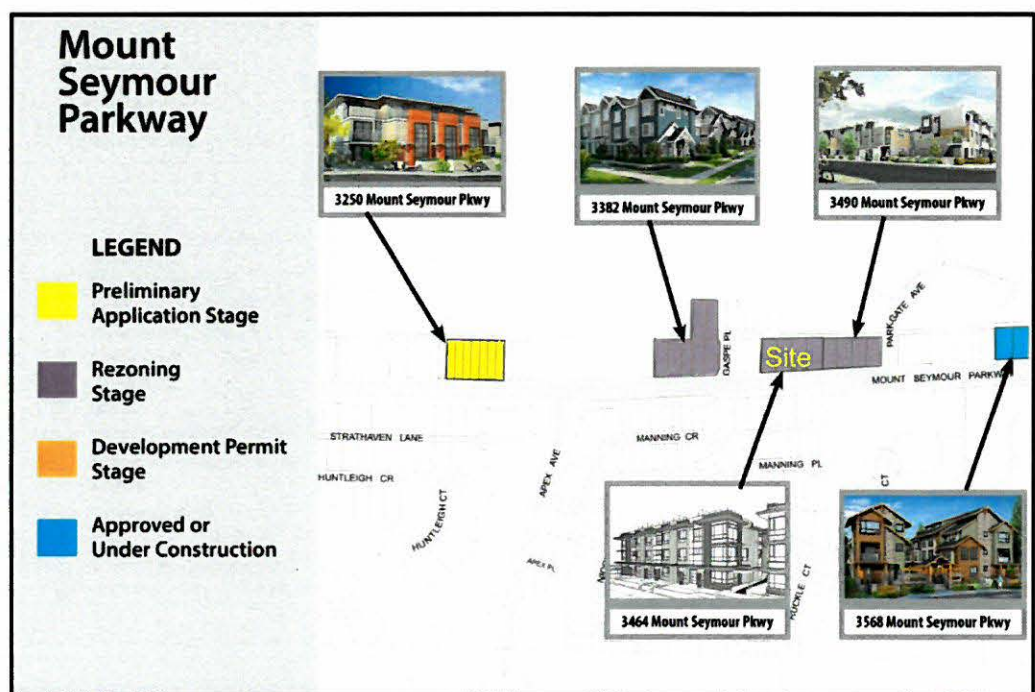
### On-site Landscaping

Landscaping on-site is designed to be low-maintenance and to feature native plantings. The courtyards between the buildings will provide a shared walkway and private outdoor amenity areas for each unit. Each unit with rooftop decks will be provided landscaping within planters to improve the spaces and help to create privacy between the roof top decks. A copy of a preliminary landscape plan is provided within Attachment 1.

### Construction Traffic Management Plan:

The applicant, in conjunction with the adjacent development applicant to the east, has submitted comprehensive and coordinated Construction Traffic Management Plans (CTMP) in order to reduce the development’s impact on pedestrian and vehicular movements in the area.

The adjacent image shows the site in relation to





other applications in the area along Mt. Seymour Parkway.

Should the rezoning bylaw proceed, in conjunction with the potential development to the east, both applicants will be required to coordinate their construction activities to limit disruption to neighbours. This will be compulsory through registration of a "Construction Traffic Management Covenant" which both developers have agreed to. The covenant will require simultaneous excavation, shoring, concrete installation, and civil works by one contractor. Should one development proceed before the other then the second development will be delayed until the excavation, shoring and concrete installation is completed on the first development in order to limit heavy truck movement conflicts in the area. A final CTMP is required to be approved by the District prior to issuance of a building permit. Further information related to construction traffic management will be provided upon consideration of the Development Permit should the rezoning bylaw proceed.

In particular, the Construction Traffic Management Plan must:

1. Provide safe passage for pedestrians, cyclists, and vehicle traffic;
2. Outline roadway efficiencies (i.e. location of traffic management signs and flaggers);
3. Make provisions for trade vehicle parking which is acceptable to the District and minimizes impacts to neighbourhoods;
4. Provide a point of contact for all calls and concerns;
5. Provide a sequence and schedule of construction activities;
6. Identify methods of sharing construction schedule with other developments in the area;
7. Ascertain a location for truck marshalling;
8. Address silt/dust control and cleaning up from adjacent streets;
9. Provide a plan for litter clean-up and street sweeping adjacent to site; and,
10. Include a communication plan to notify surrounding businesses and residents.

With respect to this specific development site, there should be limited disruption for neighbours given that the rear lane (to be constructed by the developer) will be primarily used for vehicle staging, concrete pours, marshalling and trades parking. Use of the rear lane area for staging activities was successfully implemented on the townhouse project constructed east of the site (at 3508 Parkgate Avenue).

#### Public Input

##### a) Public Information Meeting:

The applicant held a facilitated Public Information Meeting (PIM) on September 20, 2017 and the meeting was attended by approximately 18 members of the public. A copy of the PIM "summary report" from the meeting's facilitator is attached as Attachment 4. Input received includes written submissions focused on construction traffic management, parking, traffic, community amenity



contributions (CACs), pathways, parks, cost of units, tree removal, access, density, architectural design and privacy.

**Seymour Local Plan (2003) Compliance:**

The development site was designated "Multi-Family Residential" in the Seymour Local Plan reference policy document. The Seymour Local Plan supported density of between 0.6 and 1.2 FSR along the north side of Mount Seymour Parkway (3200 to 3500 blocks) provided that:

- *The housing form is predominantly townhouses*
- *A minimum of 15,000 square feet (1400 m) is assembled for townhouse development and no "locked in" lots are created*
- *The development addresses issues of traffic, access, safety, acoustical standards, view and backdrop considerations, community acceptance and design standards*
- *The development contributes to community objectives*

The above noted points have been generally achieved by the development proposal. The Public Hearing will provide an opportunity for the public to comment on the overall "community acceptance" of the development proposal (providing the rezoning application proceeds to a Public Hearing).

**COMMUNITY AMENITY CONTRIBUTIONS AND DEVELOPMENT COST CHARGES:**

As the subject property requires rezoning, a Community Amenity Contribution (CAC) has been calculated in the amount of \$521,274 in accordance with District CAC policy. The CD114 zone specifies this amount in order to achieve the maximum density of 1.2 FSR and outlines projects to which the CAC may be applied, including park, trail, environmental, public art or other public realm improvements, as well as municipal or recreation service or facility improvements, and/or affordable housing. Trail upgrades north of the development site, within Northlands Golf Course, was noted by the Parks Department as a potential nearby CAC priority.

The developer will be required to pay Development Cost Charges prior to building permit issuance in accordance the Development Cost Charge Bylaw applicable at that time.

**RENTAL AND AFFORDABLE HOUSING STRATEGY:**

In response to the District's Rental and Affordable Housing Strategy, the applicant has noted that the development will expand the supply and diversity of housing within the Parkgate Area. As stated within the strategy: "Increased supply of housing in centres will add diverse multi-family housing choices (type, tenure, unit sizes etc.) for District residents, and encourage competitive pricing for homes." The homes proposed in the subject development will be suitable for families and provide a more affordable alternative to detached single-family homes.



**CONCURRENCE:**

Staff:

The project has been reviewed by the following District Departments: Building, Parks, Environment, Legal, Development Engineering, Urban Design, Community Planning, Real Estate and Properties, Public Art, and Fire Prevention. Recommendations from individual departments have been incorporated to improve the development project and to advance public and District interests.

**CONCLUSION:**

This project assists in implementation of the District's Official Community Plan objectives. Primarily, the townhouse proposal creates a choice of housing suitable for families. The rezoning proposal is now ready for Council's consideration.

**OPTIONS:**

The following options are available for Council's consideration:

1. Introduce Bylaws 8275 and 8276 and refer Bylaw 8275 to a Public Hearing (staff recommendation); or,
2. Defeat the Bylaws at First Reading.

Erik Wilhelm, MCIP, RPP  
Development Planner

**Attachments:**

1. Architectural and Landscape Plans
2. Bylaw 8275 – Rezoning Bylaw
3. Bylaw 8276 – Housing Agreement Bylaw
4. Public Information Meeting Facilitator Summary Report



REVIEWED WITH:		
<input type="checkbox"/> Sustainable Community Dev.	_____	<input type="checkbox"/> Clerk's Office
<input type="checkbox"/> Development Services	_____	<input type="checkbox"/> Communications
<input type="checkbox"/> Utilities	_____	<input type="checkbox"/> Finance
<input type="checkbox"/> Engineering Operations	_____	<input type="checkbox"/> Fire Services
<input type="checkbox"/> Parks	_____	<input type="checkbox"/> ITS
<input type="checkbox"/> Environment	_____	<input type="checkbox"/> Solicitor
<input type="checkbox"/> Facilities	_____	<input type="checkbox"/> GIS
<input type="checkbox"/> Human Resources	_____	<input type="checkbox"/> Real Estate
		External Agencies:
		<input type="checkbox"/> Library Board
		<input type="checkbox"/> NS Health
		<input type="checkbox"/> RCMP
		<input type="checkbox"/> NVRC
		<input type="checkbox"/> Museum & Arch.
		<input type="checkbox"/> Other:





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**ALLAIRE**  
HEADWATER PARTNERS

EXHIBIT 1001

ALLAIRE GROUP/  
HEADWATER PROJECTS

MOUNT SEYMOUR  
TOWNHOMES

3425 - 3464 Mount Seymour Parkway  
North Vancouver, BC

1/10/15

**SITE COVER**

1/10/15

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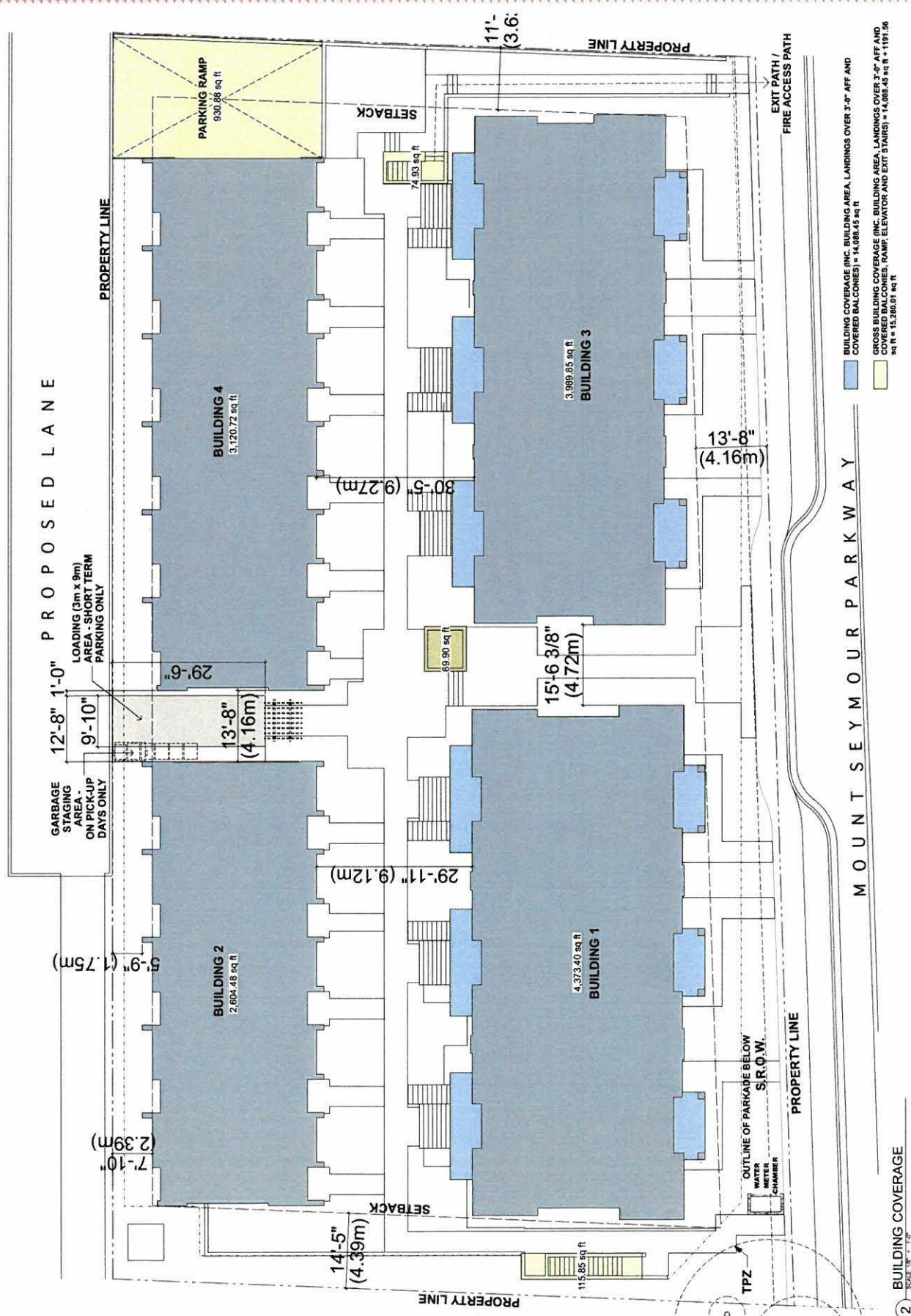
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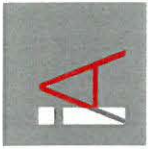
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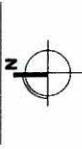




1 Perspective Looking North-West from Mount Seymour Parkway  
NOT TO SCALE



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E-mail: info@integra-arch.com



**ALLAIRE**  
Headwater LLP

DATE: 14 FEB 2011

ALLAIRE GROUP/  
HEADWATER PROJECTS

MOUNT SEYMOUR  
TOWNHOMES  
1025 - 1044 Mount Seymour Hwy  
North Vancouver, BC

1:50 (A1)

Perspective View

1:50 (A1)

1:50 (A1)

Wednesday, February 14, 2011

4.0 Development Permit

1:50 (A1)

**A-8.01**





**ARMED AND DANGEROUS**

**ALLAIRE GROUP/  
HEADWATER PROJECTS**

**MOUNT SEYMOUR  
TOWNHOMES**

1995

### Perspective View

.....

1850

Wednesday, February 14, 2018<sup>(1)</sup>

**Permitt**  
(14.003)

(1994, 1995)

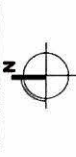
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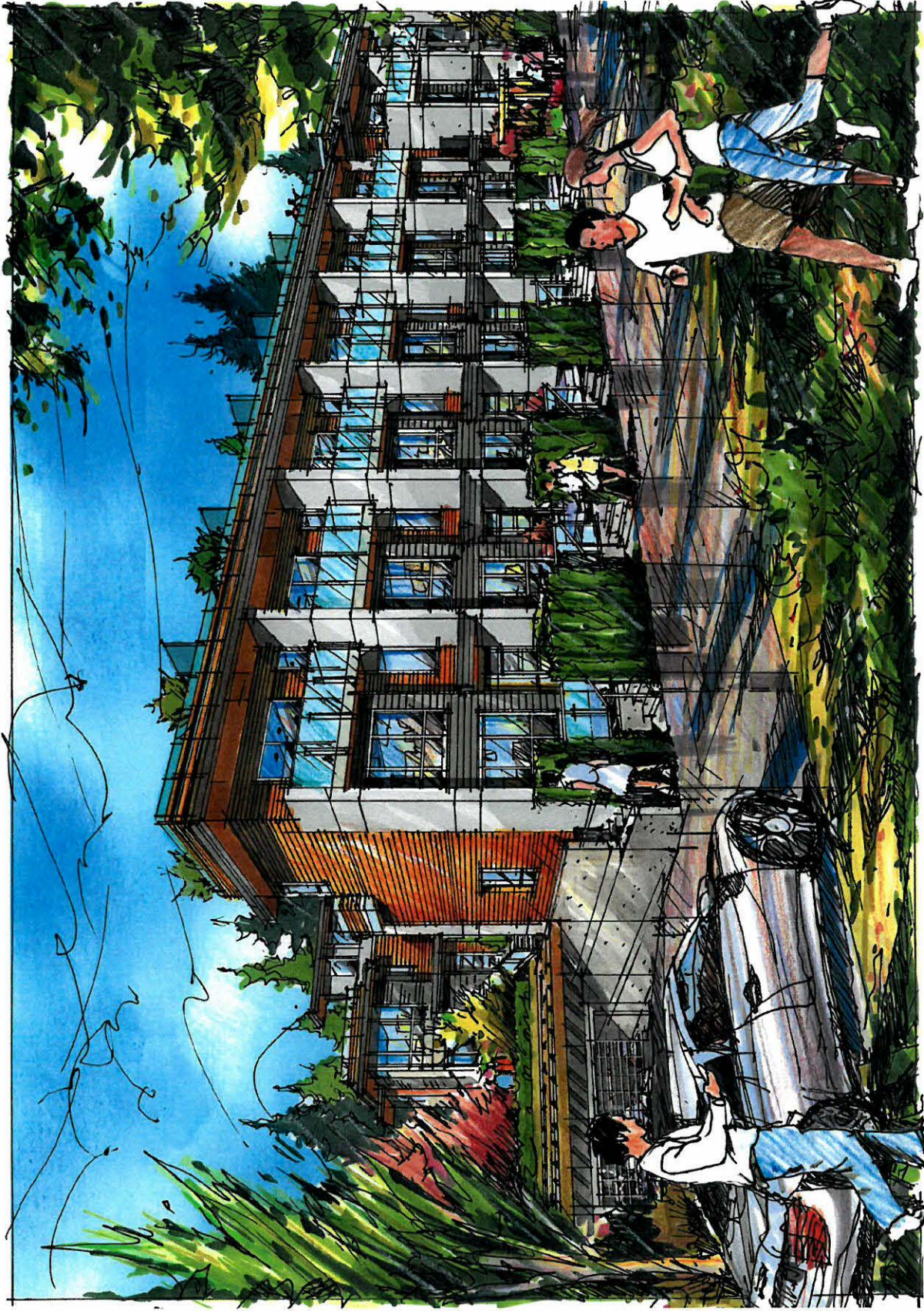


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**ALLAIRE**  
Headwater Living



1 Lane Perspective Looking South-West

NOT TO SCALE

DATE: 02/14/2011

ALLAIRE GROUP/  
HEADWATER PROJECTS

1/14/2011

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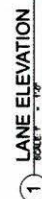


1 COURTYARD ELEVATION LOOKING SOUTH  
SCALE 1" = 10'



2 COURTYARD ELEVATION LOOKING NORTH  
SCALE 1" = 10'





LABORATORY DATA

**ALLAIRE GROUP/  
HEADWATER PROJECTS**

**MOUNT SEYMOUR  
TOWNHOMES**  
3428 - 3464 Mount Seymour Pkwy  
North Vancouver, B.C.

1000

### Street Elevation Renderings

1.800/940-1111  
[www.merck.com/medwatch](http://www.merck.com/medwatch)

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118

May 18, 2016<sup>2016</sup>

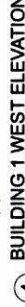
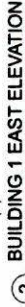
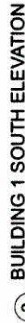
**Permit**  
10-6-03

1

(10 June 1991)

**A-4.06**





Colour	Manufacturer	Colour to match... Reference No.	Elements
Light Grey	Starline Vinyl Windows & Doors	Standard Silver	Double Glazed Vinyl Windows & Doors
Grey	Parged Concrete	Grey	Exposed Concrete Foundations / Columns
Wine	Main Metals Ltd	colour to match adjacent material	Gutters + Downpipes Pre-finished Steel Fixings
Light Grey	Engineered Aluminium Synthetic	To match Anodized aluminium	Pre-finished Aluminium sunbeds
Cherry	Composite wood siding	Teugrain	Glass panel exterior cladding @

5 Colour & Material Legend

**ALLAIRE GROUP/**  
**HEADWATER PROJECTS**

---

**MOUNT SEYMOUR**  
**TOWNHOMES**

3428 - 3444 Mount Seymour Place  
 North Vancouver, BC

---

**BUILDING 1**  
**ELEVATIONS**

---

16396	16397	16398
18" x 14"	14" x 14"	14" x 14"
Wednesday, February 16, 2011	Thursday, February 17, 2011	Friday, February 18, 2011
4.4 Development Permit		

100-20-100-0000

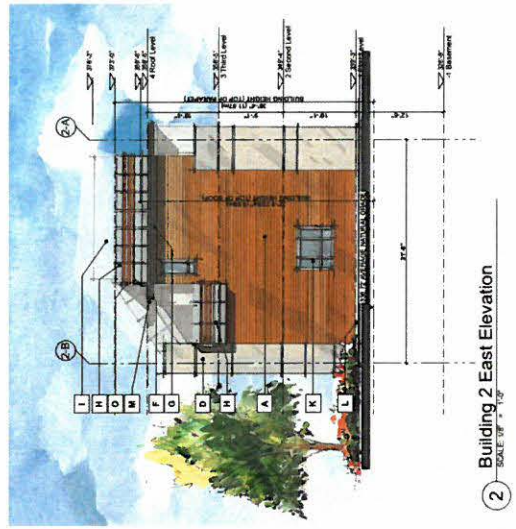




### MATERIAL & COLOUR LEGEND

Colour	Manufacturer	Elements	Colour to match... Reference No.
Dark Brown	Woodstone	Accent Siding	Old Cherry
Dark Brown	HardiePlank Lap Siding	@ Exterior Walls	
Dark Brown	HardiePlank Lap Siding	@ Exterior walls, Selected locations	
Gray	HardiePlank Lap Siding	@ Exterior walls, Selected locations	
Gray	HardiePlank Lap Siding	@ Exterior walls, Selected locations	
Gray	HardiePlank Lap Siding	@ Exterior walls, Selected locations	
Gray	HardiePlank Lap Siding	@ Exterior walls, Selected locations	
Dark Gray	HardiePlank Lap Siding	@ Exterior walls, Selected locations	
Dark Gray	HardiePlank Lap Siding	@ Exterior walls, Selected locations	
Dark Gray	HardiePlank Lap Siding	@ Exterior walls, Selected locations	

### Colour & Material Legend



Colour	Manufacturer	Elements	Colour to match... Reference No.
Light Gray	Starline Vinyl Windows & Doors	Double Glazed Vinyl Windows & Doors	Standard Silver
Gray	Parged Concrete	Exposed Concrete Foundations / Columns	
Gray	Parged Concrete	Exposed Concrete Foundations / Columns	
Gray	Parged Concrete	Exposed Concrete Foundations / Columns	
Gray	Parged Concrete	Exposed Concrete Foundations / Columns	
Gray	Parged Concrete	Exposed Concrete Foundations / Columns	
Gray	Parged Concrete	Exposed Concrete Foundations / Columns	
Gray	Parged Concrete	Exposed Concrete Foundations / Columns	
Gray	Parged Concrete	Exposed Concrete Foundations / Columns	
Gray	Parged Concrete	Exposed Concrete Foundations / Columns	






### Colour & Material Legend





MATERIAL & COLOUR LEGEND		
Colour	Manufacturer	Colour to match Reference No.
A	Woodstone Floor Concrete Plank Siding	Old Cherry
B	Dark Brown ColourPlus HardiePlank, Luan Siding	Rich Espresso
C	Gray HardiePlank Siding Smooth Texture	Plaint Grey
D	Beige ColourPlus HardiePlank Siding	Cobble Stone
E	Dark Gray Cultured Stone	Cabotized "Echo Ridge"

Colour	Manufacturer	Colour to match..... Reference No.	Elements
Dark Gray	Wood Trim Boards	Benjamin Moore 212-10 Gray	Saloon & Roof Fascias
Cedar	Woodstone Fiber Cement Plank Siding	Old Cherry	Roof overhangs & balconies soffits
Light Gray	Engineered Aluminum Railings	To match Anodized aluminum	Pre-finished Aluminum Railing on Clear Safety Glass
Light Gray	Engineered Aluminum Privacy Screens	To match Anodized aluminum	Pre-finished Aluminum Frame on Obscure Safety Glass
White	Neat Doors		@ Unit Entrance

Colour	Manufacturer	Reference No.	Colour to match...	Elements
 Light Gray	Starline Vinyl Windows & Doors	Standard Silver		Double Glazed Vinyl Windows & Doors
 Gray	Parged Concrete	Gray		Exposed Concrete Foundations / Columns
 Waves	Makin Metals Ltd	colour to match adjacent material		Gutters - Downspouts, Pre-finished Steel Flashings
 Light Gray	Engineered Aluminum Surrogate	To match Anodized aluminum		Pre-finished Aluminum surfacing
 Cherry Brown	Composite wood siding	Tugrien Walnut		Glass guard exterior cladding @ various locations

5 Colour & Material Legend

**A-4.03**

**ALLAIRE GROUP/  
HEADWATER PROJECTS**

**MOUNT SEYMOUR  
TOWNHOMES**

3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

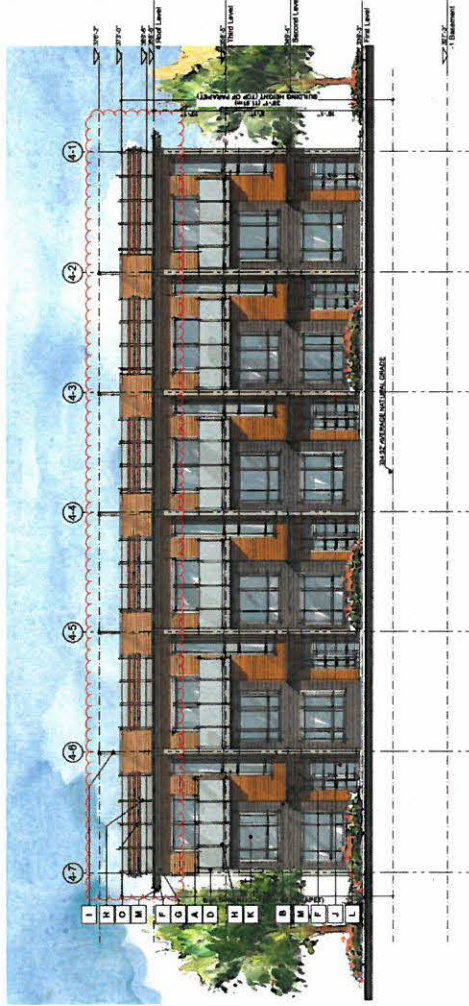
### BUILDING 3 ELEVATIONS

16399

1/8" = 1'-0"

A-403

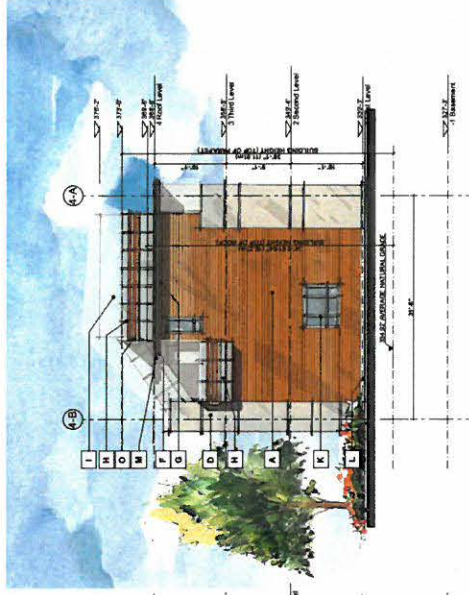




1 Building 4 North Elevation  
 SCALE 1/8" = 1'-0"



3 Building 4 South Elevation  
 SCALE 1/8" = 1'-0"



2 Building 4 East Elevation  
 SCALE 1/8" = 1'-0"



4 Building 4 West Elevation  
 SCALE 1/8" = 1'-0"

**MATERIAL & COLOUR LEGEND**

Colour	Manufacturer	Elements	Colour to match... Reference No.
Dark Brown	Woodstone	Accent Siding @ Exterior walls, Selected locations	Benjamin Moore 2121-10 Gray
Gray	HardiePlank Lap Siding	HardiePlank Lap Siding	Old Cherry
Gray	HardiePlank Lap Siding	HardiePlank Lap Siding	Old Cherry
Gray	HardiePlank Lap Siding	HardiePlank Lap Siding	Old Cherry
Gray	HardiePlank Lap Siding	HardiePlank Lap Siding	Old Cherry
Gray	HardiePlank Lap Siding	HardiePlank Lap Siding	Old Cherry
Gray	HardiePlank Lap Siding	HardiePlank Lap Siding	Old Cherry
Gray	HardiePlank Lap Siding	HardiePlank Lap Siding	Old Cherry
Gray	HardiePlank Lap Siding	HardiePlank Lap Siding	Old Cherry
Gray	HardiePlank Lap Siding	HardiePlank Lap Siding	Old Cherry

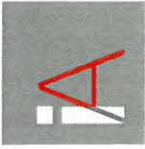
Colour	Manufacturer	Elements	Colour to match... Reference No.
Dark Gray	Wood Trim Boards	Balcony & Roof Fascias	Benjamin Moore 2121-10 Gray
Dark Gray	Wood Trim Boards	Balcony & Roof Fascias	Benjamin Moore 2121-10 Gray
Dark Gray	Wood Trim Boards	Balcony & Roof Fascias	Benjamin Moore 2121-10 Gray
Dark Gray	Wood Trim Boards	Balcony & Roof Fascias	Benjamin Moore 2121-10 Gray
Dark Gray	Wood Trim Boards	Balcony & Roof Fascias	Benjamin Moore 2121-10 Gray
Dark Gray	Wood Trim Boards	Balcony & Roof Fascias	Benjamin Moore 2121-10 Gray
Dark Gray	Wood Trim Boards	Balcony & Roof Fascias	Benjamin Moore 2121-10 Gray
Dark Gray	Wood Trim Boards	Balcony & Roof Fascias	Benjamin Moore 2121-10 Gray
Dark Gray	Wood Trim Boards	Balcony & Roof Fascias	Benjamin Moore 2121-10 Gray
Dark Gray	Wood Trim Boards	Balcony & Roof Fascias	Benjamin Moore 2121-10 Gray

Colour	Manufacturer	Elements	Colour to match... Reference No.
Light Gray	Starline	Vinyl Windows & Doors	Standard Silver
Gray	Starline	Vinyl Windows & Doors	Standard Silver
Gray	Starline	Vinyl Windows & Doors	Standard Silver
Gray	Starline	Vinyl Windows & Doors	Standard Silver
Gray	Starline	Vinyl Windows & Doors	Standard Silver
Gray	Starline	Vinyl Windows & Doors	Standard Silver
Gray	Starline	Vinyl Windows & Doors	Standard Silver
Gray	Starline	Vinyl Windows & Doors	Standard Silver
Gray	Starline	Vinyl Windows & Doors	Standard Silver
Gray	Starline	Vinyl Windows & Doors	Standard Silver

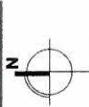
**Colour & Material Legend**

SCALE 1/8" = 1'-0"

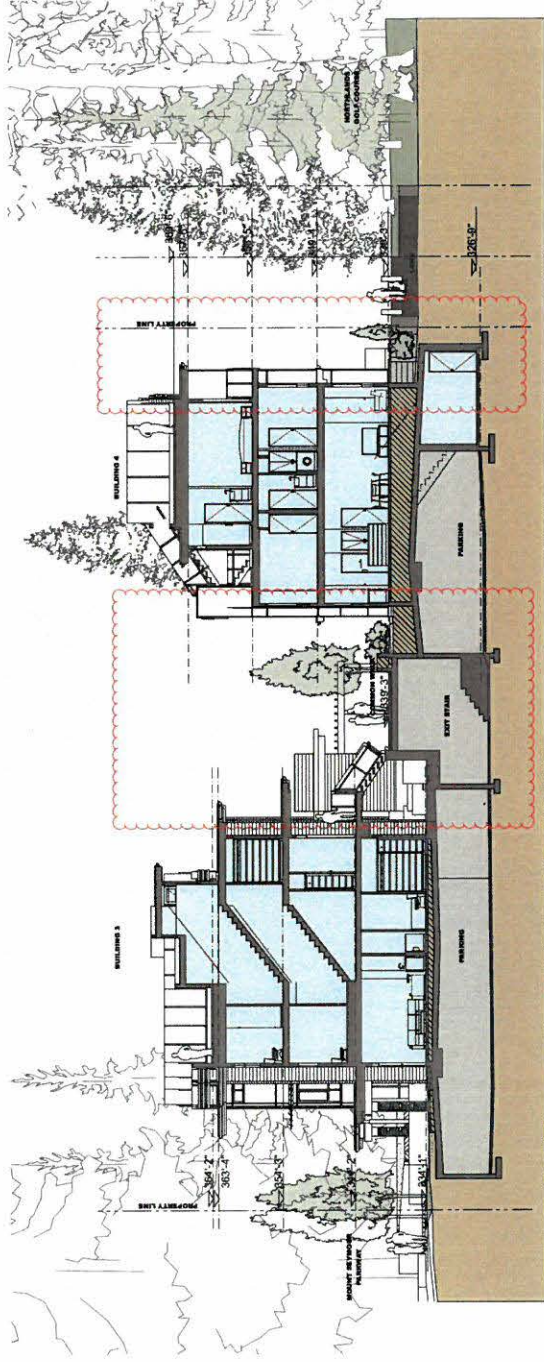




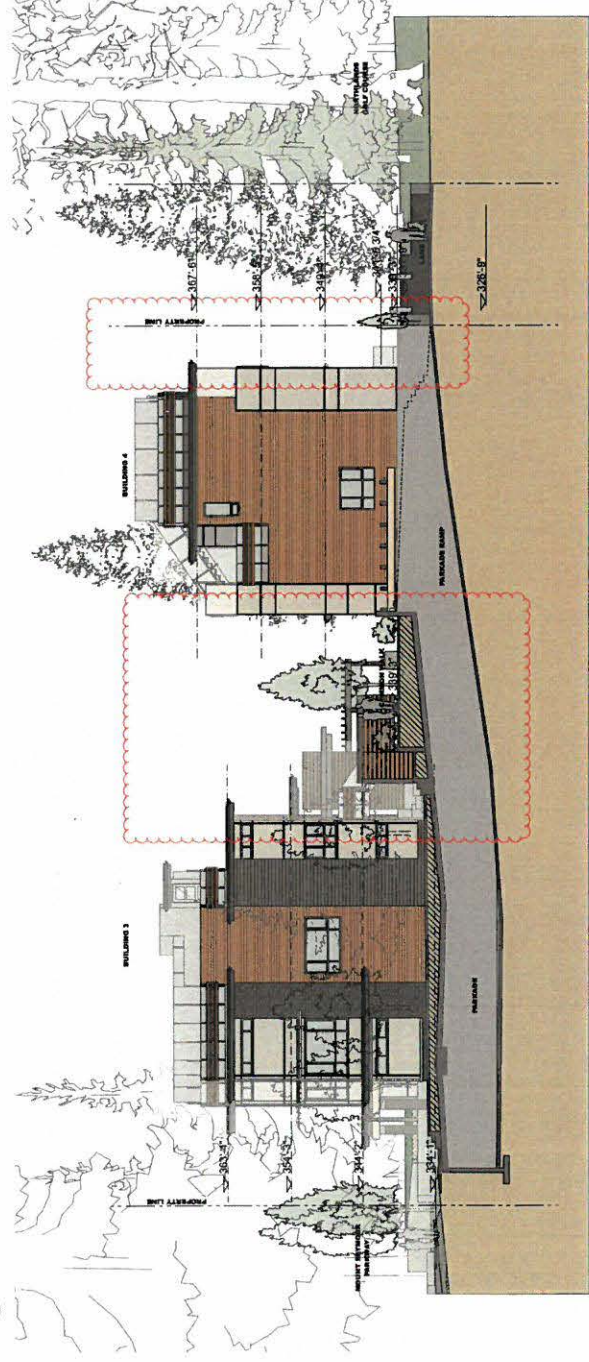
**Integra**  
ARCHITECTURE INC.  
2330-200 Granville Street  
Vancouver, BC, V6C 1S4  
www.integra-arch.com  
Telephone: 604 688 4220



**ALLAIRE**  
HEADWATER LIVING



1 SECTION  
SCALE 1/8" = 1'-0"



2 SECTION  
SCALE 1/8" = 1'-0"

DATE: 02/21/2013

CLIENT  
ALLAIRE GROUP/  
HEADWATER PROJECTS  
PROJECT  
MOUNT SEYMOUR  
TOWNHOMES  
3425 - 3464 Mount Seymour Place  
North Vancouver, BC

DATE: 02/21/2013

SECTIONS

16399  
1/8" = 1'-0"  
Wednesday, February 21, 2013  
4.0 Development Permit

A-5.01



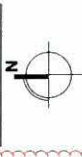


# Integra

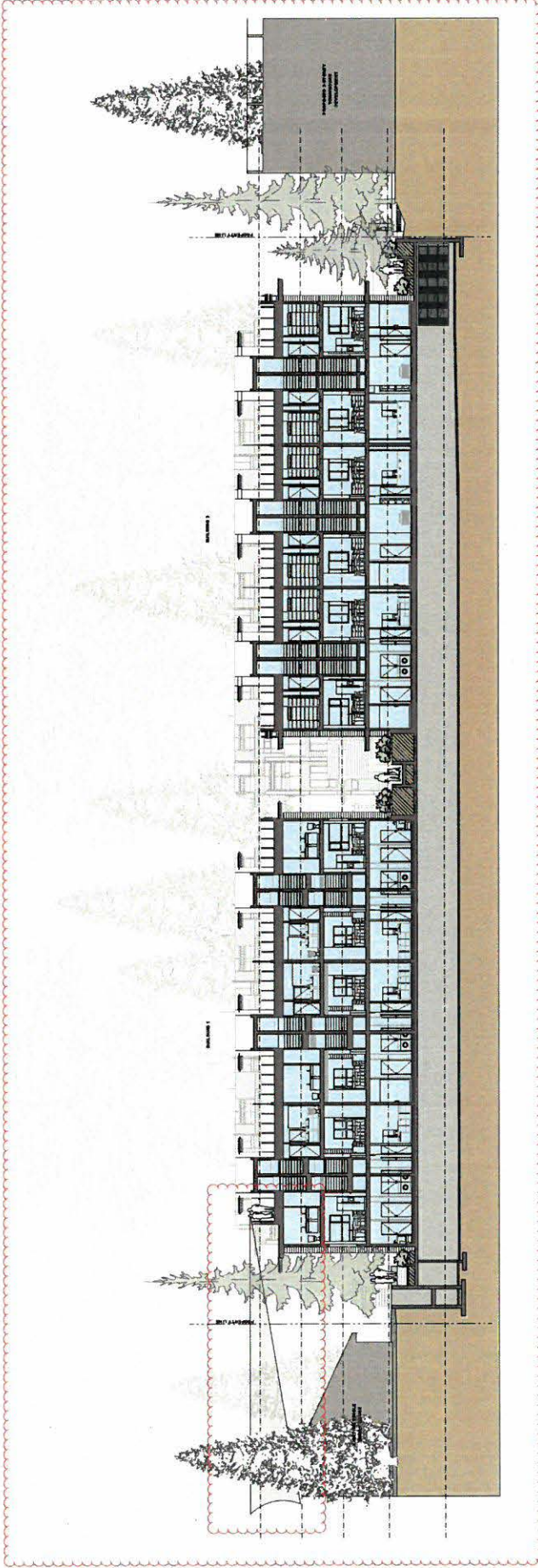
ARCHITECTURE INC.

2230-200 Granville Street  
Vancouver, BC, V6C 1S4  
www.integra-arch.com  
tel: 604 685 4220  
fax: 604 685 4220

Integra Architecture Inc. is a registered professional corporation under the laws of the Province of British Columbia. The company is a member of the Canadian Council of Architectural Firms (CCAF).



**ALLAIRE**  
Headwater Living



SECTION 1  
SCALE 3/8" = 1'-0"

(SEE SHEET 20-01)

ALLAIRE GROUP/  
HEADWATER PROJECTS

(SEE SHEET 20-01)

MOUNT SEYMOUR  
TOWNHOMES

3458 - 3464 Mount Seymour Hwy  
North Vancouver, BC

(SEE SHEET 20-01)

## SECTIONS

15.000 (SEE SHEET 20-01)

3/22" = 1'-0" (SEE SHEET 20-01)

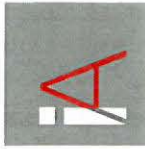
Wednesday, February 21, 2018

4.8 Development Permit (SEE SHEET 20-01)

(SEE SHEET 20-01)

A-5.02





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ARCHITECTURE INC.  
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Vancouver, BC, V6C 1S4  
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Telephone: 604 688 4220



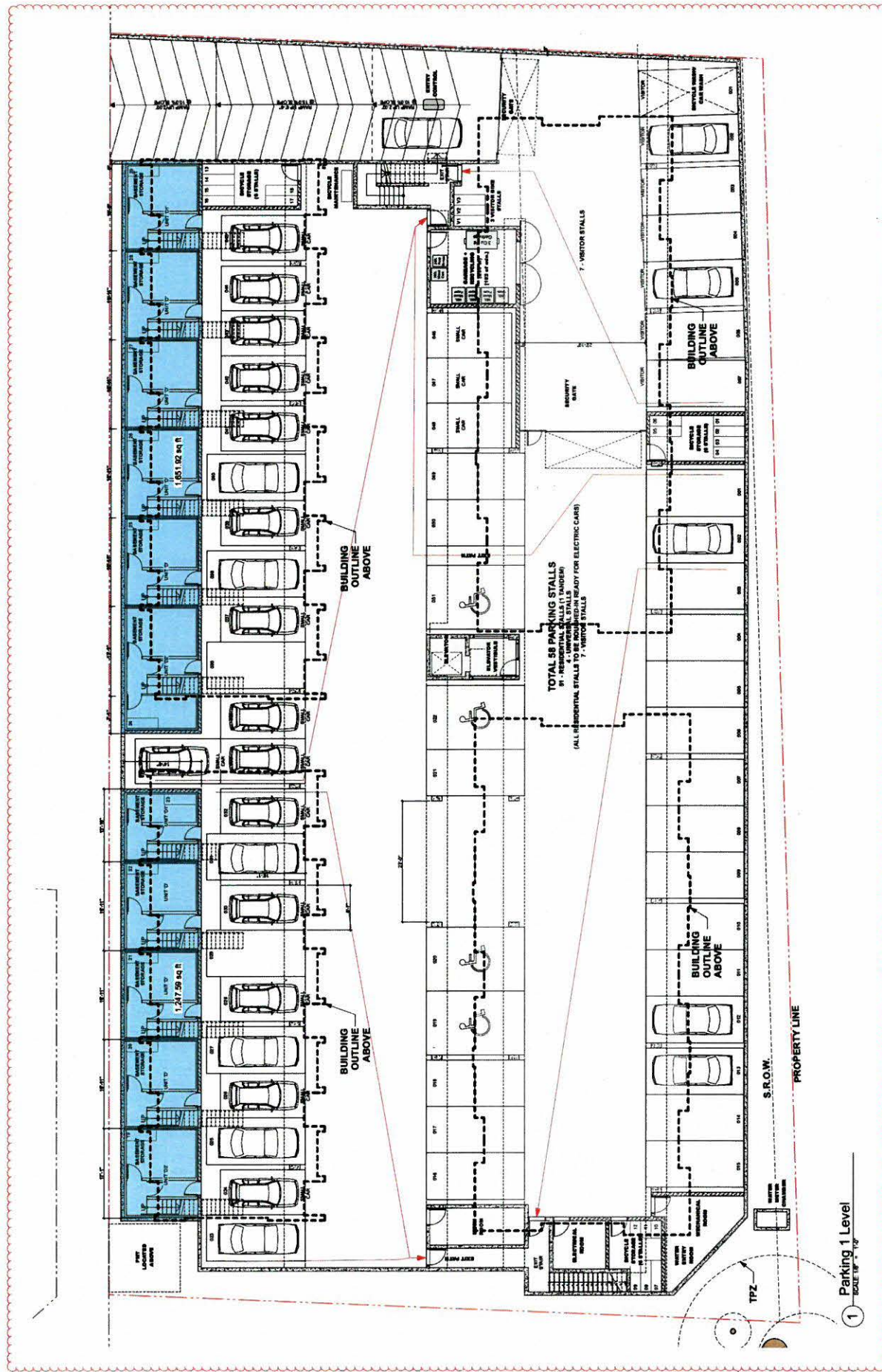
INSPIRED BY

ALLAIRE GROUP/  
HEADWATER PROJECTS  
MOUNT SEYMOUR  
TOWNHOMES  
3425 - 3464 Mount Seymour Hwy  
North Vancouver, BC

PARKADE

15299  
1/8" = 1'-0"  
Thursday, February 13, 2014  
4.8 Development Permit

A-1.03



RESIDENTIAL AREA (BELOW GRADE STORAGE)  
EXEMPT FROM PSR - 2899.51 SQFT

1 Parking 1 Level  
SCALE 1/8" = 1'-0"



# EXTERIOR MATERIALS & COLOURS



**A/G:**  
**ACCENT SIDING/SOFFITS**  
WOODTONE  
FIBRE CEMENT LAP SIDING  
RUSTIC SERIES  
'OLD CHERRY'



**B:**  
**SIDING**  
HARDI-PANK LAP SIDING  
CEDAR MILL TEXTURE  
'RICH ESPRESSO'



**C:**  
**SIDING**  
HARDI-PANEL SIDING  
SMOOTH TEXTURE  
'PEARL GRAY'



**D:**  
**SIDING**  
HARDI-PANEL SIDING  
SMOOTH TEXTURE  
'COBBLE STONE'



**E:**  
**ACCENT SIDING**  
CULTURED STONE  
COBBLEFIELD  
'ECHO RIDGE'



**F:**  
**WOOD TRIM BOARDS**  
BENJAMIN MOORE  
2121-10 GRAY



**H:**  
**ENGINEERED ALUMINUM**  
RAILINGS  
ANODIZED ALUMINUM FINISH  
C/W CLEAR SAFETY GLASS



**I:**  
**ENGINEERED ALUMINUM PRIVACY**  
SCREENS  
ANODIZED ALUMINUM FINISH  
C/W OBSCURE SAFETY GLASS



**J:**  
**METAL UNIT ENTRANCE DOORS**  
BENJAMIN MOORE  
OC17 'WHITE DOVE'



**K:**  
**DOUBLE GLAZED**  
VINYL DOORS & WINDOWS  
STARLINE WINDOWS  
'STANDARD SILVER'



**L:**  
**EXPOSED CONCRETE**  
FOUNDATIONS, COLUMNS  
PARGED CONCRETE  
GRAY



**M:**  
**GUTTERS, DOWNSPOUTS & METAL**  
FLASHINGS  
MAKIN METALS  
COLOUR TO MATCH ADJACENT  
MATERIAL



**N:**  
**ENGINEERED ALUMINUM SUNSHADE**  
ANODIZED ALUMINUM



**O:**  
**GLASS GUARD EXTERIOR CLADDING**  
LONGBOARD TUBE  
WOOD GRAIN FINISH  
'LIGHT CHERRY'



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Telephone: 604 688 4220  
Fax: 604 688 4221  
1000-1000 Street, Suite 1000  
Vancouver, BC, V6C 1S4



**ALLAIRE**  
Headwater, INC.

(ARCHITECT SEAL)

ALLAIRE GROUP/  
HEADWATER PROJECTS

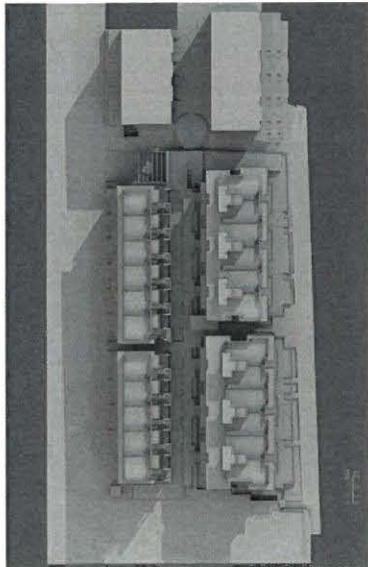
MOUNT SEYMOUR  
TOWNHOMES  
3425 - 3464 Mount Seymour Park  
North Vancouver, BC

**Materials &  
Colours**

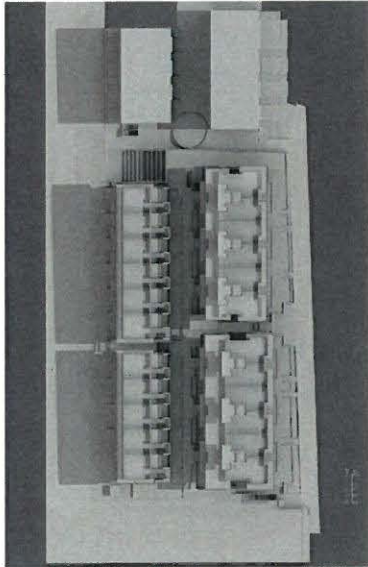
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Wednesday, February 14, 2019  
4:4 Development Permit

**A-8.04**

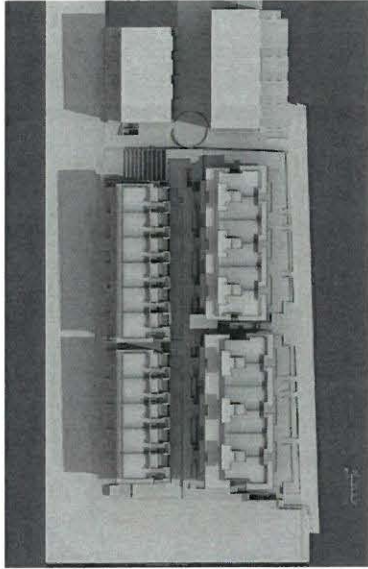




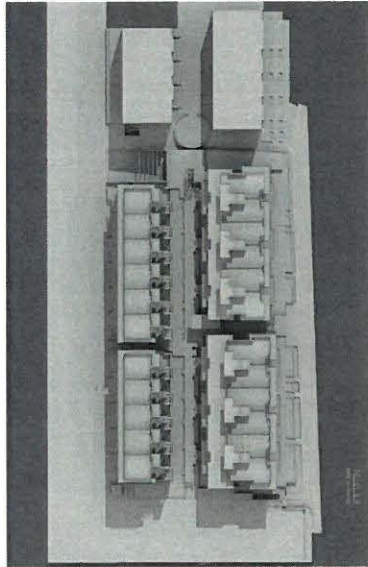
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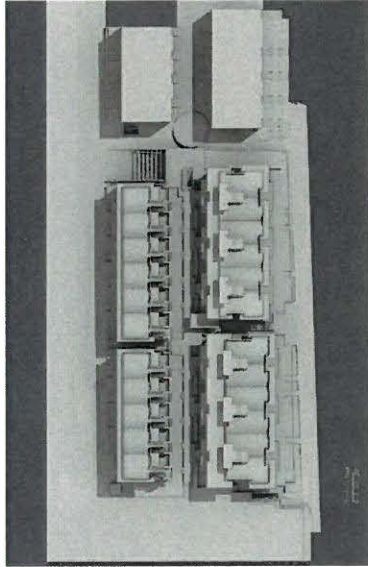
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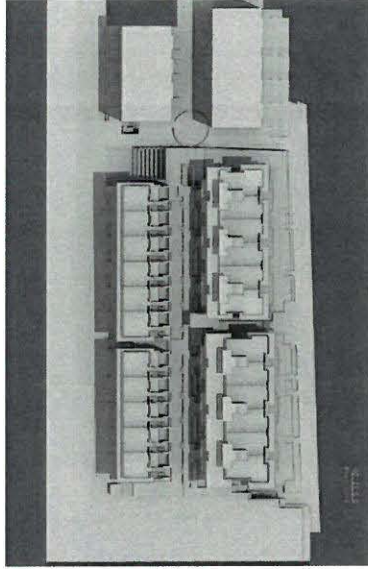
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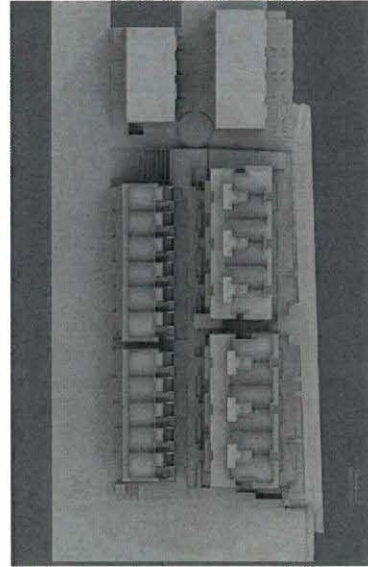
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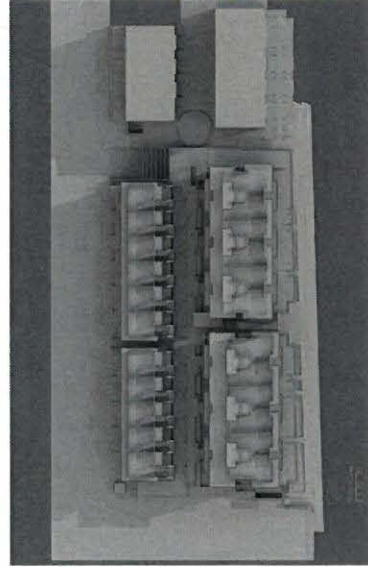
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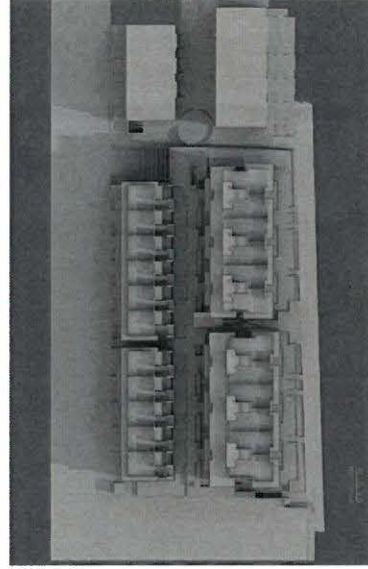
JUNE 21 2:00PM



DECEMBER 21 10:00AM



DECEMBER 21 12:00PM



DECEMBER 21 2:00PM















**The Corporation of the District of North Vancouver****Bylaw 8275**

A bylaw to amend District of North Vancouver Zoning Bylaw 3210, 1965

---

The Council for The Corporation of the District of North Vancouver enacts as follows:

**1. Citation**

This bylaw may be cited as "District of North Vancouver Rezoning Bylaw 1366 (Bylaw 8275)".

**2. Amendments**

2.1 District of North Vancouver Zoning Bylaw 3210, 1965 is amended as follows:

Section 301(2) by inserting the following zoning designation in numeric sequence:

"Comprehensive Development Zone CD114"

2.2 Part 4B by inserting the following:

**"4B114 Comprehensive Development Zone 114 (CD114)"**

**4B114-1 Intent:**

The purpose of the CD114 zone is to establish specific land use and development regulations for a 29 unit townhouse development.

**4B114-2 Uses:**

The following *principal uses* are permitted in the Comprehensive Development 114 Zone:

**(a) Uses Permitted without Conditions:**

Not applicable

**(b) Conditional Uses:**

(i) Residential building, multifamily townhouse

For the purposes of this CD114 Zone, "Residential building, multifamily townhouse" means a building having not more than three residential storeys



with private rooftop deck and consisting of two or more dwelling units with individual, exterior access to grade, all above an underground parkade.

**4B114-3 Conditions of Use:**

- (a) Balcony enclosures are not permitted;
- (b) Trellises, pergolas, or similar structures anchored to a building on the rooftop level are not permitted; and
- (c) Providing the site is developed in accordance with density provisions permitted in Section "4B114-5 Amenities", the following outlines the minimum number of unit typologies that must be constructed:

Unit Description	Number of Units
2 bedroom	6
3 Bedroom	23

**4B114-4 Accessory Use:**

- (a) *Accessory uses* are permitted and are limited to:
  - (i) Home occupations in accordance with the regulations in Section 405 of this Bylaw.

**4B114-5 Density:**

- (a) The maximum permitted density in the CD114 Zone is limited to a floor space ratio (FSR) of 0.45 and a maximum number of 3 dwelling units, inclusive of any density bonus for energy performance; and
- (b) For the purposes of calculating floor space ratio, the following areas are exempted:
  - (i) underground parkades, including: drive aisles, electrical/mechanical rooms, garbage and recycling collection areas, bicycle storage areas, and basement areas on the parkade level; and
  - (ii) unenclosed balcony areas and rooftop deck areas.

**4B114-6 Amenities:**

Despite subsection 4B114-5, density in the CD114 Zone is increased to a maximum floor space of 3,408 m<sup>2</sup> (36,683 sq. ft.) and a maximum number of 29 townhouse units, inclusive of any density bonus for energy performance, if the owner:



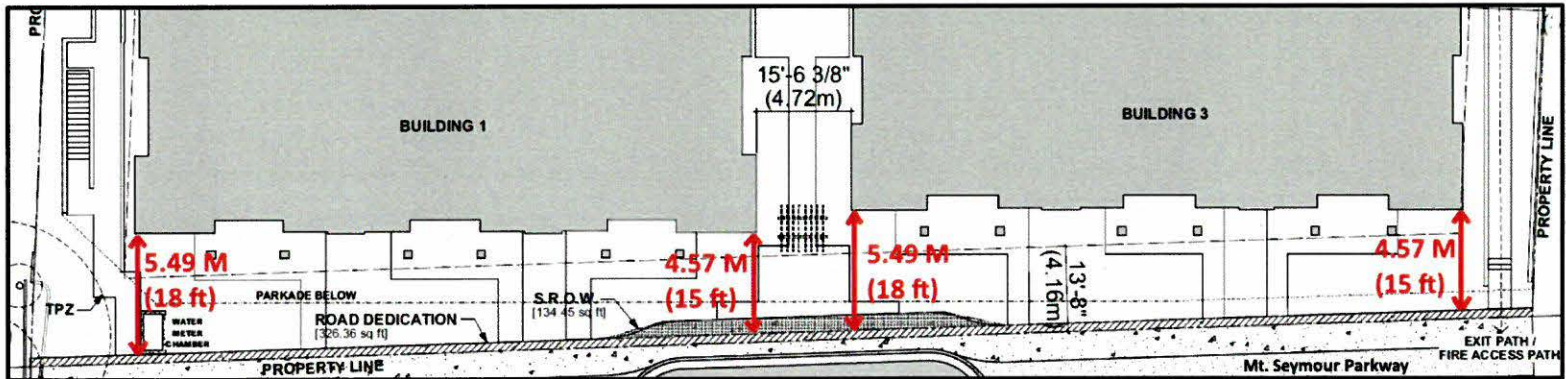
1. Contributes \$521,274.00 to the municipality to be used for any or all of the following amenities (with allocation to be determined by the municipality in its sole discretion): public art, park, trail, environmental or other public realm improvements; municipal or recreation service or facility improvements and/or affordable housing; and
2. Enters into a Housing Agreement requiring a rental disclosure statement to be filed and prohibiting any strata bylaw or regulation establishing rental restrictions.

#### **4B114-7 Setbacks:**

- (a) Buildings must be set back from property lines to the closest building face, excluding bay windows and small encroachments not to exceed 0.6 m (2.0 ft) in depth, in accordance with the following regulations:

Location	Minimum Required Setback
North Lot Line	2.39 m (7.84 ft)
East Lot Line	3 m (9.84 ft)
South Lot Line	per setbacks within section 4B114-7(b)
West Lot Line	4.39 m (14.4 ft)

- (b) Notwithstanding setbacks stipulated in section 4B114-7 (a), buildings must be set back from property lines to the closest building face in accordance with the following plan, due to road dedication requirements:



- (c) The north and south faces of buildings, excluding bay windows and small encroachments not to exceed 0.6 m (2.0 ft) in depth, must be separated by a minimum distance of 9 m (29.5 ft).

#### **4B114-8 Coverage:**

- a) Maximum permitted Building Coverage is 50%; and
- b) Maximum permitted Site Coverage is 55%.



#### **4B114-9 Height:**

- a) Maximum permitted height is 12.04 meters (39.5 ft) from finished grade.

#### **4B114-10 Landscaping:**

- a) All land areas not occupied by buildings, structures, parking spaces, loading spaces, driveways, manoeuvring aisles and sidewalks shall be landscaped or finished in accordance with an approved landscape plan; and
- b) All electrical kiosks and garbage and recycling container pads not located underground or within a building shall be screened with landscaping or fencing in accordance with an approved landscape plan.

#### **4B114-11 Subdivision Requirements**

Within the CD114 zone, the minimum lot area for the purposes of subdivision is 2,508 m<sup>2</sup> (27,000 sq. ft.).

#### **4B114-12 Motor Vehicle Parking, Bicycle Parking and Storage Regulations:**

Parking, Loading and Drive Aisle regulations within Part 10 of the Zoning Bylaw are applicable to CD114 lands except that:

- (a) A minimum of 57 motor vehicle parking spaces shall be provided in an underground parking garage inclusive of visitor parking spaces, small car spaces, and accessible parking spaces;
- (b) A minimum of 7 motor vehicle parking spaces shall be provided for the use of visitors;
- (c) A maximum of 17 small car spaces shall be permitted;
- (d) A minimum of one motor vehicle parking space, with a time duration parking restriction, shall be provided at grade with direct access from the lane;
- (e) A minimum of 40 Class 1 bicycle parking spaces (within a secured bike storage area) shall be provided; and
- (f) Each dwelling unit shall be provided a minimum of 1 Class 2 bicycle parking space (secure bicycle rack, bollard or post) at ground level within the townhouse complex."



2.3 The Zoning Map is amended in the case of the lands in Schedule A, by rezoning the land outlined and noted as "site" to Comprehensive Development 114 Zone (CD114).

**READ** a first time

**PUBLIC HEARING** held

**READ** a second time

**READ** a third time

**ADOPTED**

---

Mayor

---

Municipal Clerk

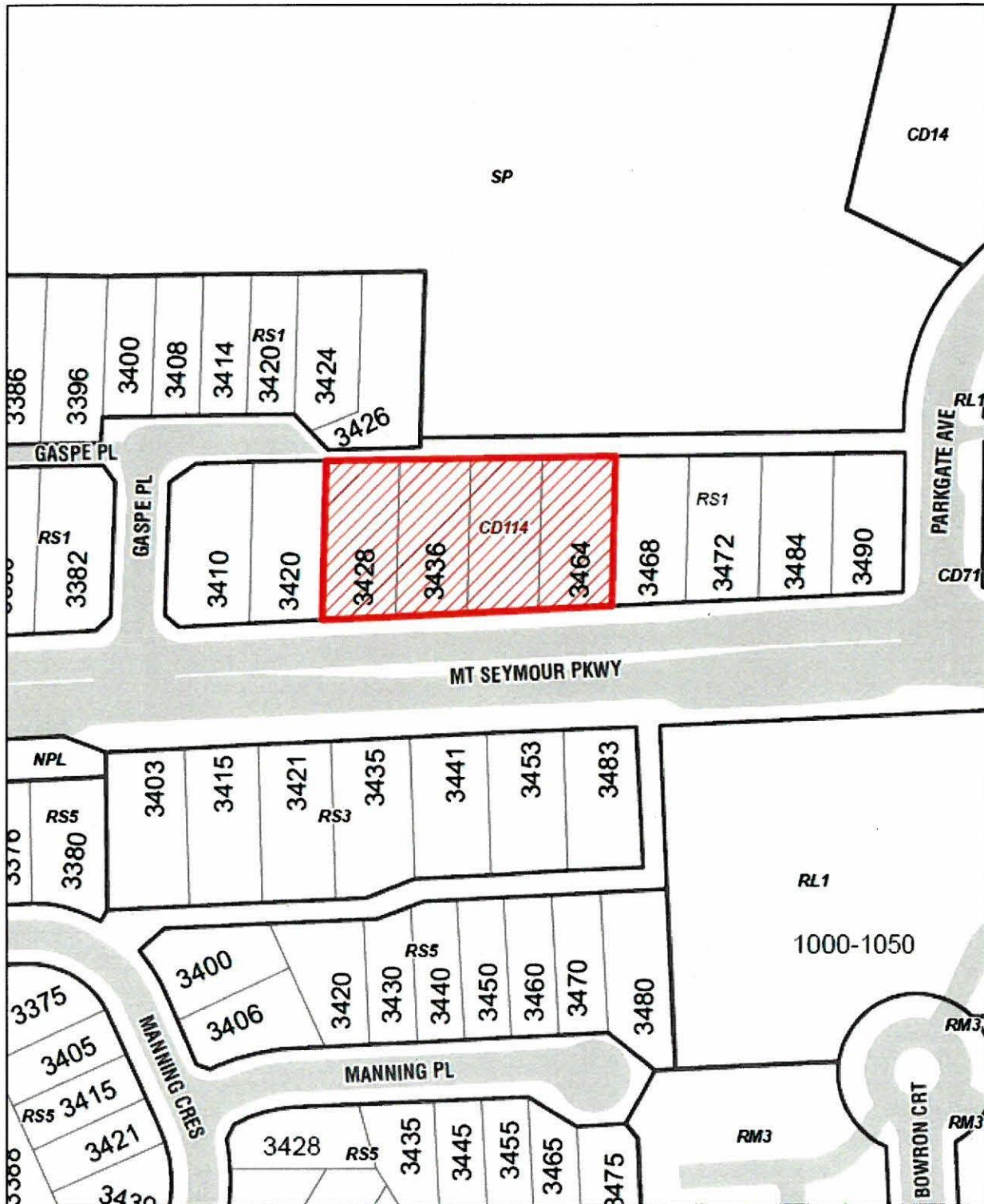
Certified a true copy

---

Municipal Clerk



# Schedule A to Bylaw 8275



The Zoning Map is amended in the case of the lands in Schedule A, by rezoning the land outlined and noted as "site" from Single-Family Residential One Acre Zone (RS1) to Comprehensive Development 114 Zone (CD114)."





**The Corporation of the District of North Vancouver****Bylaw 8276**

A bylaw to enter into a Housing Agreement  
(3428 - 3464 Mount Seymour Parkway)

---

The Council for The Corporation of the District of North Vancouver enacts as follows:

**1. Citation**

This bylaw may be cited as "Housing Agreement Bylaw 8276, 2017 (3428 - 3464 Mount Seymour Parkway)".

**2. Authorization to Enter into Agreement**

2.1 The Council hereby authorizes a housing agreement between The Corporation of the District of North Vancouver and Allaire Headwater (Seymour) Residences Ltd. (Inc. No. BC1108300) substantially in the form attached to this Bylaw as Schedule "A" with respect to the following lands:

- |                    |  |
|--------------------|--|
| a) PID 012-602-591 | Lot 15 of Lot 6 Blocks 1 to 4 DL 622 Plan 2866 |
| b) PID 013-370-073 | Lot 16 of Lot 6 Blocks 1 to 4 DL 622 Plan 2866 |
| c) PID 006-788-467 | Lot 17 of Lot 6 Blocks 1 to 4 DL 622 Plan 2866 |
| d) PID 013-370-090 | Lot 18 of Lot 6 Blocks 1 to 4 DL 622 Plan 2866 |

**3. Execution of Documents**

The Mayor and Municipal Clerk are authorized to execute any documents required to give effect to the Housing Agreement.

**READ** a first time

**READ** a second time

**READ** a third time

**ADOPTED**

---

Mayor

---

Municipal Clerk



Certified a true copy

---

Municipal Clerk



## **Schedule A to Bylaw 8276**

### **SECTION 219 COVENANT – HOUSING AGREEMENT**

THIS AGREEMENT is dated for reference the \_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_

BETWEEN:

**ALLAIRE HEADWATER (SEYMOUR) RESIDENCES LTD.** (Inc. No. BC1108300) a company incorporated under the laws of the Province of British Columbia having an office at 245 – 9600 Cameron Street, Burnaby, BC V3J 7N3

(the “Developer”)

AND:

**THE CORPORATION OF THE DISTRICT OF NORTH VANCOUVER**, a municipality incorporated under the *Local Government Act*, RSBC 2015, c.1 and having its office at 355 West Queens Road, North Vancouver, BC V7N 4N5

(the “District”)

WHEREAS:

1. The Developer is the registered owner of the Lands (as hereinafter defined);
2. The Developer wishes to obtain development permissions with respect to the Lands and wishes to create a condominium development which will contain residential strata units on the Lands;
3. Section 483 of the *Local Government Act* authorises the District, by bylaw, to enter into a housing agreement to provide for the prevention of rental restrictions on housing, and provides for the contents of the agreement; and
4. Section 219 of the *Land Title Act* (British Columbia) permits the registration in favour of the District of a covenant of a negative or positive nature relating to the use of land or a building thereon, or providing that land is to be built on in accordance with the covenant, or providing that land is not to be built on except in accordance with the covenant, or providing that land is not to be subdivided except in accordance with the covenant;

NOW THEREFORE in consideration of the mutual promises contained in it, and in consideration of the payment of \$1.00 by the District to the Developer (the receipt and sufficiency of which are hereby acknowledged by the Developer), the parties covenant and agree with each other as follows, as a housing agreement under Section 483 of the *Local Government Act*, as a contract and a deed under seal between the parties, and as a covenant under Section 219 of the *Land Title Act*, and the Developer hereby further covenants and agrees that neither the Lands nor any building constructed thereon shall be used or built on except in accordance with this Agreement:



## **1. DEFINITIONS**

### **1.01 Definitions**

In this agreement:

- (a) "*Development Permit*" means development permit No. 33.17 issued by the District;
- (b) "*Lands*" means land described in Item 2 of the *Land Title Act* Form C to which this agreement is attached;
- (c) "*Owner*" means the Developer and any other person or persons registered in the Lower Mainland Land Title Office as owner of the Lands from time to time, or of any parcel into which the Lands are consolidated or subdivided, whether in that person's own right or in a representative capacity or otherwise;
- (d) "*Proposed Development*" means the proposed development containing not more than 29 units to be constructed on the Lands in accordance with the Development Permit;
- (e) "*Short Term Rentals*" means any rental of a Unit for any period less than 30 days;
- (f) "*Strata Corporation*" means the strata corporation formed upon the deposit of a plan to strata subdivide the Proposed Development pursuant to the *Strata Property Act*;
- (g) "*Unit*" means a residential dwelling strata unit in the Proposed Development; and
- (h) "*Unit Owner*" means the registered owner of a Dwelling Unit in the Proposed Development.

## **2. TERM**

This Agreement will commence upon adoption by District Council of Bylaw 8276 and remain in effect until terminated by the District as set out in this Agreement.

## **3. RENTAL ACCOMODATION**

### **3.01 Rental Disclosure Statement**

No Unit in the Proposed Development may be occupied unless the Owner has:

- (a) before the first Unit is offered for sale, or conveyed to a purchaser without being offered for sale, filed with the Superintendent of Real Estate a rental disclosure statement in the prescribed form (the "*Rental Disclosure Statement*") designating all of the Units as rental strata lots and imposing at least a 99 year rental period in relation to all of the Units pursuant to the *Strata Property Act* (or any successor or replacement legislation), except in relation to Short Term Rentals and, for greater certainty, stipulating specifically that the 99 year rental restriction does not apply to a Strata Corporation bylaw prohibiting or restricting Short Term Rentals; and



- (b) given a copy of the Rental Disclosure Statement to each prospective purchaser of any Unit before the prospective purchaser enters into an agreement to purchase in respect of the Unit. For the purposes of this paragraph 3.01(b), the Owner is deemed to have given a copy of the Rental Disclosure Statement to each prospective purchaser of any Unit in the building if the Owner has included the Rental Disclosure Statement as an exhibit to the disclosure statement for the Proposed Development prepared by the Owner pursuant to the *Real Estate Development Marketing Act*.

3.02 Rental Accommodation

The Units constructed on the Lands from time to time may always be used to provide rental accommodation as the Owner or a Unit Owner may choose from time to time, except that this section 3.02 does not apply to Short Term Rentals which may be restricted by the Strata Corporation to the full extent permitted by law.

3.03 Binding on Strata Corporation

This agreement shall be binding upon all Strata Corporations created by the subdivision of the Lands or any part thereof (including the Units) pursuant to the *Strata Property Act*, and upon all Unit Owners.

3.04 Strata Bylaw Invalid

Any Strata Corporation bylaw which prevents, restricts or abridges the right to use any of the Units as rental accommodations (other than Short Term Rentals) shall have no force or effect.

3.05 No Bylaw

The Strata Corporation shall not pass any bylaws preventing, restricting or abridging the use of the Lands, the Proposed Development or the Units contained therein from time to time as rental accommodation (other than Short Term Rentals).

3.06 Vote

No Unit Owner, nor any tenant or mortgagee thereof, shall vote for any Strata Corporation bylaw purporting to prevent, restrict or abridge the use of the Lands, the Proposed Development or the Units contained therein from time to time as rental accommodation (other than Short Term Rentals).

3.07 Notice

The Owner will provide notice of this Agreement to any person or persons intending to purchase a Unit prior to any such person entering into an agreement of purchase and sale, agreement for sale, or option or similar right to purchase as part of the disclosure statement for any part of the Proposed Development prepared by the Owner pursuant to the *Real Estate Development Marketing Act*.



3.08 Release of Covenant [optional clause]

The District agrees that if the District of North Vancouver Rezoning Bylaw 1366 (Bylaw 8275), is not adopted by the District's Council before [date], the Owner is entitled to require the District to execute and deliver to the Owner a discharge, in registrable form, of this Agreement from title to the Land. The Owner is responsible for the preparation of the discharge under this section and for the cost of registration at the Land Title Office.

4. **DEFAULT AND REMEDIES**

4.01 Notice of Default

The District may, acting reasonably, give to the Owner written notice to cure a default under this Agreement within 30 days of delivery of the notice. The notice must specify the nature of the default. The Owner must act with diligence to correct the default within the time specified.

4.02 Costs

The Owner will pay to the District upon demand all the District's costs of exercising its rights or remedies under this Agreement, on a full indemnity basis.

4.03 Damages an Inadequate Remedy

The Owner acknowledges and agrees that in the case of a breach of this Agreement which is not fully remediable by the mere payment of money and promptly so remedied, the harm sustained by the District and to the public interest will be irreparable and not susceptible of adequate monetary compensation.

4.04 Equitable Remedies

Each party to this Agreement, in addition to its rights under this Agreement or at law, will be entitled to all equitable remedies including specific performance, injunction and declaratory relief, or any of them, to enforce its rights under this Agreement.

4.05 No Penalty or Forfeiture

The Owner acknowledges and agrees that it is entering into this Agreement to benefit the public interest in providing rental accommodation, and that the District's rights and remedies under this Agreement are necessary to ensure that this purpose is carried out, and the District's rights and remedies under this Agreement are fair and reasonable and ought not to be construed as a penalty or forfeiture.

4.06 Cumulative Remedies

No reference to nor exercise of any specific right or remedy under this Agreement or at law or at equity by any party will prejudice, limit or preclude that party from exercising any other right or remedy. No right or remedy will be exclusive or dependent upon any other right to remedy, but any party, from time to time, may exercise any one or more of such rights or remedies independently, successively, or in combination. The Owner acknowledges that specific



performance, injunctive relief (mandatory or otherwise) or other equitable relief may be the only adequate remedy for a default by the Owner under this Agreement.

**5. LIABILITY**

**5.01 Indemnity**

Except if arising directly from the negligence of the District or its employees, agents or contractors, the Owner will indemnify and save harmless each of the District and its board members, officers, directors, employees, agents, and elected or appointed officials,, and their heirs, executors, administrators, personal representatives, successors and assigns, from and against all claims, demands, actions, loss, damage, costs and liabilities that all or any of them will or may be liable for or suffer or incur or be put to any act or omission by the Owner or its officers, directors, employees, agents, contractors, or other persons for whom the Owner is at law responsible, or by reason of or arising out of the Owner's ownership, operation, management or financing of the Proposed Development or any part thereof.

**5.02 Release**

The Owner hereby releases and forever discharges the District, its elected officials, board members, officers, directors, employees and agents, and its and their heirs, executors, administrators, personal representatives, successors and assigns from and against all claims, demands, damages, actions or causes of action by reason of or arising out of advice or direction respecting the ownership, operation or management of the Proposed Development or any part thereof which has been or hereafter may be given to the Owner by all or any of them.

**5.03 Survival**

The covenants of the Owner set out in Sections 5.01 and 5.02 will survive termination of this Agreement and continue to apply to any breach of the Agreement or claim arising under this Agreement during the ownership by the Owner of the Lands or any Unit therein, as applicable.

**6. GENERAL PROVISIONS**

**6.01 District's Power Unaffected**

Nothing in this Agreement:

- (a) affects or limits any discretion, rights, powers, duties or obligations of the District under any enactment or at common law, including in relation to the use or subdivision of land;
- (b) affects or limits any enactment relating to the use of the Lands or any condition contained in any approval including any development permit concerning the development of the Lands; or
- (c) relieves the Owner from complying with any enactment, including the District's bylaws in relation to the use of the Lands.



6.02 Agreement for Benefit of District Only

The Owner and District agree that:

- (a) this Agreement is entered into only for the benefit of the District:
- (b) this Agreement is not intended to protect the interests of the Owner, any Unit Owner, any occupant of any Unit or any future owner, occupier or user of any part of the Proposed Development, including any Unit, or the interests of any third party, and the District has no obligation to anyone to enforce the terms of this Agreement; and
- (c) The District may at any time terminate this Agreement, in whole or in part, and execute a release and discharge of this Agreement in respect of the Proposed Development or any Unit therein, without liability to anyone for doing so.

6.03 Agreement Runs With the Lands

This Agreement burdens and runs with the Lands and any part into which any of them may be subdivided or consolidated, by strata plan or otherwise. All of the covenants and agreements contained in this Agreement are made by the Owner for itself, its successors and assigns, and all persons who acquire an interest in the Lands or in any Unit after the date of this Agreement.

6.04 Release

The covenants and agreements on the part of the Owner and any Unit Owner and herein set forth in this Agreement have been made by the Owner and any Unit Owner as contractual obligations as well as being made pursuant to Section 483 of the *Local Government Act* (British Columbia) and as such will be binding on the Owner and any Unit Owner, except that neither the Owner nor any Unit Owner shall be liable for any default in the performance or observance of this Agreement occurring after such party ceases to own the Lands or a Unit as the case may be.

6.05 Priority of This Agreement

The Owner will, at its expense, do or cause to be done all acts reasonably necessary to ensure this Agreement is registered against the title to each Unit in the Proposed Development, including any amendments to this Agreement as may be required by the Land Title Office or the District to effect such registration.

6.06 Agreement to Have Effect as Deed

The District and the Owner each intend by execution and delivery of this Agreement to create both a contract and a deed under seal.

6.07 Waiver

An alleged waiver by a party of any breach by another party of its obligations under this Agreement will be effective only if it is an express waiver of the breach in writing. No waiver of a breach of this Agreement is deemed or construed to be a consent or waiver of any other breach of this Agreement.



6.08 Time

Time is of the essence in this Agreement. If any party waives this requirement, that party may reinstate it by delivering notice to another party.

6.09 Validity of Provisions

If a Court of competent jurisdiction finds that any part of this Agreement is invalid, illegal, or unenforceable, that part is to be considered to have been severed from the rest of this Agreement and the rest of this Agreement remains in force unaffected by that holding or by the severance of that part.

6.10 Extent of Obligations and Costs

Every obligation of a party which is set out in this Agreement will extend throughout the Term and, to the extent that any obligation ought to have been observed or performed prior to or upon the expiry or earlier termination of the Term, such obligation will survive the expiry or earlier termination of the Term until it has been observed or performed.

6.11 Notices

All notices, demands, or requests of any kind, which a party may be required or permitted to serve on another in connection with this Agreement, must be in writing and may be served on the other parties by registered mail or by personal service, to the following address for each party:

If to the District:

District Municipal Hall  
355 West Queens Road  
North Vancouver, BC V7N 4N5

Attention: Planning Department

If to the Owner:

Allaire Headwater (Seymour) Residences Ltd.  
245 – 9600 Cameron Street  
Burnaby, BC V3J 7N3

If to the Unit Owner:

The address of the registered owner which appears on title to the Unit at the time of notice.

Service of any such notice, demand, or request will be deemed complete, if made by registered mail, 72 hours after the date and hour of mailing, except where there is a postal service disruption during such period, in which case service will be deemed to be complete only upon actual delivery of the notice, demand or request and if made by personal service, upon personal service being effected. Any party, from time to time, by notice in writing served upon the other parties, may



designate a different address or different or additional persons to which all notices, demands, or requests are to be addressed.

6.12 Further Assurances

Upon request by the District, the Owner will promptly do such acts and execute such documents as may be reasonably necessary, in the opinion of the District, to give effect to this Agreement.

6.13 Enuring Effect

This Agreement will enure to the benefit of and be binding upon each of the parties and their successors and permitted assigns.

7. INTERPRETATION

7.01 References

Gender specific terms include both genders and include corporations. Words in the singular include the plural, and words in the plural include the singular.

7.02 Construction

The division of this Agreement into sections and the use of headings are for convenience of reference only and are not intended to govern, limit or aid in the construction of any provision. In all cases, the language in this Agreement is to be construed simply according to its fair meaning, and not strictly for or against either party.

7.03 No Limitation

The word "including" when following any general statement or term is not to be construed to limit the general statement or term to the specific items which immediately follow the general statement or term similar items whether or not words such as "without limitation" or "but not limited to" are used, but rather the general statement or term is to be construed to refer to all other items that could reasonably fall within the broadest possible scope of the general statement or term.

7.04 Terms Mandatory

The words "must" and "will" and "shall" are to be construed as imperative.

7.05 Statutes

Any reference in this Agreement to any statute or bylaw includes any subsequent amendment, re-enactment, or replacement of that statute or bylaw.

7.06 Entire Agreement

(d) This is the entire agreement between the District and the Owner concerning its subject, and there are no warranties, representations, conditions or collateral agreements relating to this Agreement, except as included in this Agreement.



- (e) This Agreement may be amended only by a document executed by the parties to this Agreement and by bylaw, such amendment to be effective only upon adoption by District Council of a bylaw to amend Bylaw 8276.

7.07 Governing Law

This Agreement is to be governed by and construed and enforced in accordance with the laws of British Columbia.

As evidence of their agreement to be bound by the terms of this instrument, the parties hereto have executed the *Land Title Act* Form C that is attached hereto and forms part of this Agreement.



### GRANT OF PRIORITY

WHEREAS \_\_\_\_\_ (the "**Chargeholder**") is the holder of the following charge which is registered in the Land Title Office:

(a) \_\_\_\_\_ (the "**Charge**");

AND WHEREAS the Chargeholder agrees to allow the Section 219 Covenant herein to have priority over the Charge;

THIS PRIORITY AGREEMENT is evidence that in consideration of the sum of \$1.00 paid by THE CORPORATION OF THE DISTRICT OF NORTH VANCOUVER (the "**District**") to the Chargeholder, the receipt and sufficiency of which are hereby acknowledged, the Chargeholder covenants and agrees to subordinate and postpone all its rights, title and interest in and to the lands described in the Form C to which this Agreement is attached (the "**Lands**") with the intent and with the effect that the interests of the District rank ahead of the Charge as though the Section 219 Covenant herein had been executed, delivered and registered against title to the Lands before registration of the Charge.

As evidence of its Agreement to be bound by the above terms, as a contract and as a deed executed and delivered under seal, the Chargeholder has executed the Form C to which this Agreement is attached and which forms part of this Agreement.



## 3428 - 3464 Mount Seymour Townhomes Development Application

### Public Information Meeting Summary Report

**Event Date:** September 20, 2017  
**Time:** 7:00pm – 8:30pm  
**Location:** Parkgate Community Centre  
**Attendance:** 18 members of the public signed in.  
**Comments:** 4 comment sheets

**Meeting Purpose:**

- 1) To present development application materials to neighbours
- 2) To provide an opportunity for the public to ask questions about the development
- 3) To provide an opportunity for neighbours to comment on the proposal.

**Notification:**

In accordance with District of North Vancouver policies:

Invitation Brochures

Invitations and informational packages were delivered to 207 addresses within a 100m radius from the site, meeting District requirements. Appendix A includes a copy of this package and a map of the distribution area.

Newspaper Ad

A newspaper ad was placed in the North Shore News on Wednesday, September 13 and Friday, September 15, 2017. A copy of the ad is included in Appendix A: Notification.

**Attendance:**

18 members of the public signed in for the meeting. Copies of the sign-in sheets are included in Appendix B.

The following District staff and project team members were in attendance:

District of North Vancouver:

- Erik Wilhelm, Planner

Project Team presenters included:

- Steve Watt, Integra Architecture
- Alyssa Semczyszyn, Jonathan Losee Landscape Architecture

Facilitators:

- Steven Petersson, Petersson Planning Consulting
- Kathleen Heggie, Petersson Planning Consulting



**Overview:**

The meeting was held in an Open House format. Meeting participants could browse the display boards and engage with the project team and the District Planner directly. The facilitator listened for questions and comments and noted them on a flip chart for all to see.

A planned presentation and facilitated question and answer period took place part way through the meeting. Many participants chose to spend significant time at the meeting to review and discuss the project. The facilitator heard many questions and comments about the project.

The participants were invited to submit written comments to the facilitator or to the municipal planner. Comment sheets are attached in Appendix C.

The key themes of the evening were traffic, privacy, protection of green space and existing local character, construction impacts, as well as District policies and processes.

**Public Dialogue:**

(Q = Question, A = Answer, C=Comment, and the number is to track the dialogue)

- C1** We're totally against this. I've lived here for a long time, and I'm worried about traffic, noise, and crime. There are already way too many cars, and all of this will have negative impacts on the existing population.
- C2** Looking at the other nearby projects too, it seems like there is no limit to density. This concerns me with the number of cars and units in this area.
- A2** (EW) The Official Community Plan sets the maximum density. This area has a maximum FSR of 1.2, which this project complies with.
- Q3** While I'm glad this isn't a high-rise, we need more development so young people can live here. Your proposed landscaping should be coordinated with neighbouring projects.
- Q4** How many other proposals active development proposals are nearby?
- A4** (EW) Currently two.
- Q5** I'm mostly concerned with traffic on Mt. Seymour Parkway. What will happen with the very large trees to the north of the site, by the golf course?
- A5** Some trees will be lost for the proposed laneway.
- A5** (EW) All nearby developers are required to help fund pathway renewal, invasive species removal, and tree planting.
- C6** My Dad built 4 side-by-side homes in this area, and we're not selling. We want to protect our heritage homes and green spaces. The District has contradicted its own policies and is allowing over-development. This is too much, too fast. It will worsen traffic. Infrastructure is already insufficient. All other nearby projects on Mt. Seymour Parkway will bring the units to double what was originally allowed. Plus, we're not considering all of the other nearby development such as the towers in Lower Capilano, and what's happening in the City of North Vancouver and



West Vancouver. The community can't absorb that much. I strongly object to this project and the DNV's policies and procedures (e.g. meeting during holidays). My family has been negatively affected by the unethical practices of realtors and the DNV's lack of development management. Old growth trees will be lost, some of which are on District lots that should be held in trust. I don't support the sale of DNV land.

**Q7** We need an audience to listen to the critiques of District procedures. Shouldn't the DNV planning staff and council hear us, just as the developers of this project have to?

**A7** A DNV staff member is here, and a report of this meeting will go to DNV staff and Council. The public can also speak at Council meetings and, should Council refer it to Public Hearing, at the Public Hearing.

**C7** We all need opportunities to have our voices heard. This project is a microcosm of larger issues.

**C8** I live on Gaspé Place. I hope neighbours' privacy is protected from over-looking from the proposed rooftop patios.

**A8** Privacy issues can be mitigated by pulling the patios back from the edge of the roof, as well as possibly the parkade and decks.

**Q8** Will there be restrictive covenants to stop tenants from partying on the roofs?

**A8** The strata corporation would regulate this.

**C9** I'm happy that a walkway will connect to Gaspé Place.

**A9** (EW) If Gaspé Place is eventually redeveloped, there will likely be a road connection to Parkgate Avenue.

**Q10** Will any green building measures be included?

**A10** Yes, we're aiming for gold standards. Wiring will be available for electric vehicle charging stations.

**Q11** How much devastation to greenery will this cause?

**A11** Trees in the lane right-of-way will have to be removed, and the arborist will work to minimize tree removal in the adjacent greenway. For any one tree removed, the developer will have to plant one new one.

**C11** Removing trees and their root systems can lead to flooding and affect nearby homes.

**Q12** Does this accommodate people with accessibility challenges?

**A12** Yes, the parking is accessible, as are some of the units.

**C13** I live on the other side of Parkgate Avenue. How will construction traffic be managed, with two projects underway simultaneously?

**A13** (EW) Both projects have submitted traffic reports and preliminary construction traffic plans. The DNV will strive to ensure that all workers park in the laneway and not on the roads, especially not on Mt. Seymour Parkway.

**C14** Our kids won't be able to afford these homes – they won't be affordable. It has been these same types of projects that have wiped out heritage homes and farms that were here.



- C15** There will be a traffic issue on Parkgate. We had a terrible time with construction worker parking when an earlier development was going up. I can't imagine that all workers will be able to fit into the laneway. Why should we have to suffer from that?
- C16** I live in one of those new condos on Parkgate, and if it wasn't for this type of multi-family development, we wouldn't be able to afford to live there. Affordable units are needed.
- C17** This will be absolutely crippling. It's totally unacceptable that the DNV allows this type of development and uses this process.
- Q18** Why have these two projects side-by-side, when the 3500 block of Mt. Seymour Parkway hasn't been touched yet? We have to honour the plan.
- C19** It would be nice to see local amenities improved before this kind of development, which will bring many more families (e.g. daycare). Are there any requirements for this?
- A19** Development Cost Charges are set by bylaw, which mainly fund things like roads, parks, sewer. Community Amenity Contributions (CACs) are a tool for the DNV to obtain money from developers for local amenities.
- Q19** How is it decided what CAC money is spent on?
- A19** (EW) We can sometimes try to direct this money towards local projects, based on community input. The CAC hasn't been agreed upon yet for this project, but the rezoning won't go through without it.
- Q20** How big will the setbacks be, so the units don't have privacy issues, as well as impact neighbours to the west?
- A20** We try to limit windows on walls facing bigger roads. We'll also use plantings as a buffer.
- C20** Mature trees are already there, though, and are being removed for this project. They should be used for privacy.
- C21** People are talking like this project is already happening when it's still up in the air. We'll do whatever we can to stop it.
- Q22** What about the intrinsic value of the mature trees? Can't they be repurposed somehow if not protected?
- Q23** What is the unit size range?
- A23** The smallest is 2-BR at 1,130 sq.ft., and the largest is 1,570 sq.ft.

#### **Comment Sheet and Email Summary**

Participants were invited to submit comments for a two-week response period after the meeting. Four comment sheets were submitted.



One respondent was concerned about the impacts of construction on the neighbourhood, such as parking for labourers and construction traffic. A second respondent opposed density increases on Mount Seymour Parkway until further investments in public transit infrastructure are made. A third respondent opposed any density increase in the neighbourhood. The fourth respondent suggested that each unit be provided with wiring to charge electric vehicles.

### **Conclusion**

The purpose of this public meeting was to present to neighbours the proposed development concept, and provide them with an opportunity to ask clarifying questions and comment on the proposal. 207 invitations were mailed to the community, and eighteen community members signed in. Two newspaper ads notified the community of the meeting, and a sign was posted on the property.

The public could participate in this process in three ways:

- browsing boards
- talking to the project team and District Planner
- submitting written comments.

The meeting length and format was sufficient to provide all participants an opportunity to learn more, ask questions, and make the comments they wished to provide that evening. Participants asked the development team and District planner a variety of specific questions, mostly related to traffic, privacy, protection of green space and existing local character, construction impacts, as well as District policies and processes. There was fulsome discussion and the community was given ample opportunity to express their views of the proposal.







FRIDAY, SEPTEMBER 15, 2017 north shore news nsnews.com

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**PUBLIC INFORMATION MEETING**

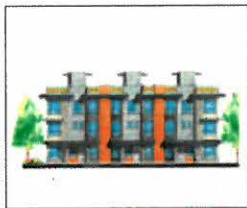
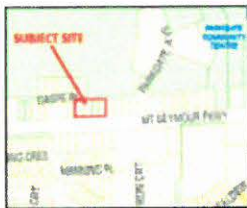
A redevelopment is being proposed for 3428-3464 Mt. Seymour Parkway, to construct a 29 unit townhouse project. You are invited to a meeting to review and discuss the project.

Date: Wednesday, September 20<sup>th</sup>, 2017

Time: 7:00 – 8:30pm

Location of Meeting: Parkgate Community Centre – 3625 Banff Court, North Vancouver, BC

The applicant proposes to rezone the site from a single family zoning to a comprehensive development zone to permit a 29 unit ground oriented townhouse project. The project is made up of both 3 bedroom and 2 bedroom and den units that range between approximately 1,100 and 1,400 square feet in size and includes 52 residential parking stalls and 7 visitor stalls accessed through a new laneway at the rear of the development.



Information packages are being distributed to residents within a 100 meter radius of the site. If you would like to receive a copy or if you would like more information, please contact Erik Wilhelm of the Community Planning Department at 604.990.2360 or Marc Allaire, Applicant Representative from Allaire Headwater Residences at 604.422.8718 or bring your questions and comments to the meeting.

\*This is not a public hearing. District of North Vancouver Council will receive a report from staff on the issues raised at the meeting and will formally consider the proposal at a later date.

**GET A JOB!****JOB FIND GUIDE****Looking for employees?**

We have a special "pop-up" section running in the North Shore News that is designed to pull out and get the attention of our readers who are looking for work. (Perhaps even enticing them to switch to a new job?)

We'll help design your ad that gets results... one that outlines perks and reasons why someone should apply with **you**.

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BOOKING DEADLINE: FRIDAY, SEPT 22  
ISSUE DATE: FRIDAY, SEPT 29

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\*From \$39.11 per month (including taxes and stadium fees) for 12 months beginning September 2017. Subject to annual \$10 processing fee per account.

**Sept.  
14 - 25**



Notification Sign





Notification Flyers

Meeting Agenda:

Doors Open: 7:00pm

Open House: 7:00 - 7:30pm

Presentation: 7:30pm – 7:45pm

Question and Answer: 7:45 - 8:30pm

For further information please contact:

Marc Allaire  
604.422.8718

Allaire Headwater Residences

Erik Wilhelm  
604.990.2360

District of North Vancouver,  
Planning Department

**Notice of a Public  
Information Meeting in Your  
Neighborhood**

Allaire Headwater Residences is hosting a Public Information Meeting to present the development proposal for a 29 unit town house project at 3428-3464 Mt. Seymour Pkwy.

The information package is being distributed to the owners and occupants within 100 meters of the proposed development site in accordance with the District of North Vancouver policy.

**Meeting time and Location:**

**Wednesday, September 20<sup>th</sup>, 2017**

**7:00pm – 8:30pm**

**Parkgate Community Centre**

**3625 Banff Court**

**North Vancouver, BC**



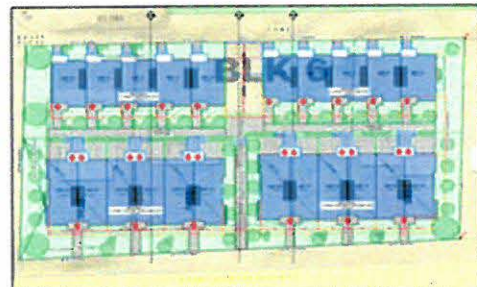
## The Proposal:

Allaire Headwater Residences proposes to construct a three storey town house development at 3428-3464 Mt. Seymour Parkway between Gaspé Place and Parkgate Avenue.

The proposal is for 29 townhouse units which will include 23 three bedroom units and 6 two bedroom and den units. The units will be built within 4 separate buildings oriented around an internal landscaped courtyard with internal/external walkways.

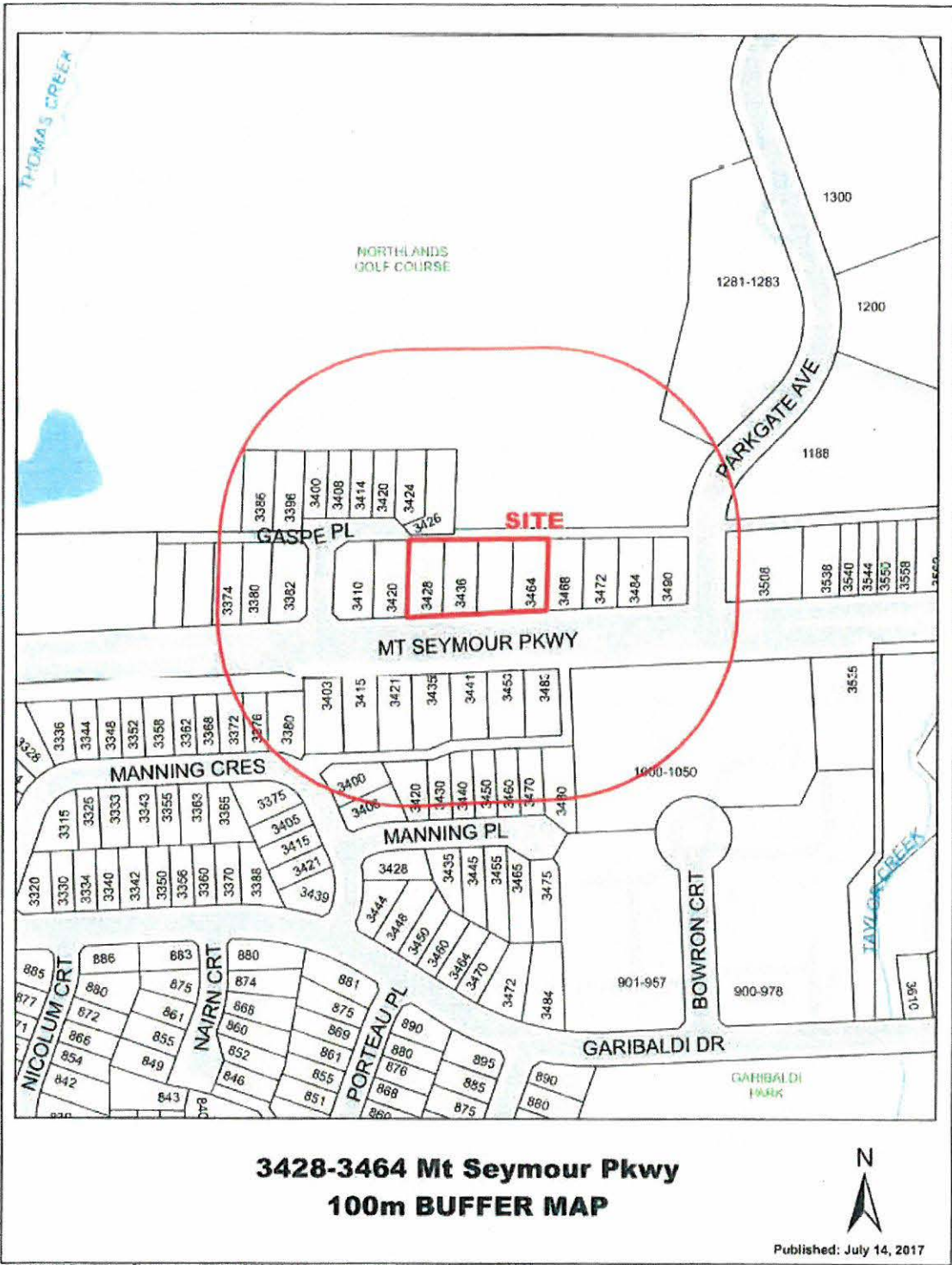
The site will be accessed from a rear driveway off of Parkgate Avenue. Parking will be located in the underground garage. 52 parking spaces will be provided for the residents with 7 visitor parking spaces.

This proposal also includes upgrades to the adjacent public path, where a new trail will be provided as this projects community amenity contribution.





Notification Area Map





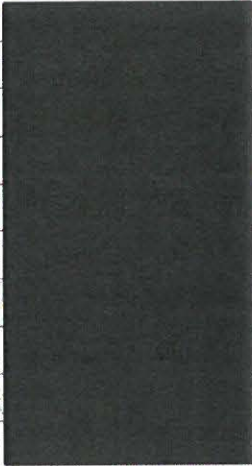
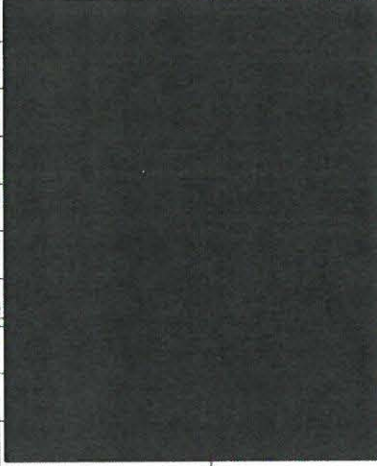




Sign-In Sheet

3428-3464 Mt Seymour Parkway Development Application

Public Information Meeting September 20, 2017

NAME	ADDRESS	EMAIL (optional)	PHONE (optional)
	1281 Parkway		
	1281 Parkway		
	1281 Parkway Ave		
	2400 Comp Hwy		
	1281 Parkway		
	Parkway Terrace		
	1281 Parkway		
X			

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Appendix C: Public Comments: Written Submissions

DISTRICT OF  
NORTH  
VANCOUVER

COMMENT SHEET  
District of North Vancouver

PROPOSAL: 29 Unit Townhouse Development on Mt. Seymour Pkwy.  
PROPOSAL ADDRESS: 3428-3464 Mount Seymour Parkway  
DEVELOPER: Allaire Headwater Residences

Please provide us with any input you have on this project and feel free to attach additional sheets to help the District of North Vancouver understand neighbourhood concerns and views:

As owners we're very concerned about the construction process itself. It is possible that there could be two developments underway at the same time with the possibility of many many workers who have to park their vehicles - where else are they going to park but in Parkgate Ave? The last time we had the townhouse development built on the corner, there were days we could hardly get out of our driveway and if we managed to just get dirty looks. With the trucks construction vehicles, contractors' delivery trucks etc. coming and going we are going to be living with adaptation for however long it takes to complete this project (perhaps two). We are tax payers, too. What consideration to us as homeowners not to mention the people who need to drop off their children at the daycare at the church or the trains of young people parents and coaches who use the park at the end of the street.

Your Name

Street Address 103-1281 Parkgate Ave.

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Please insert your comment sheet into the comment box provided at the Public Information Meeting; or alternatively, mail or email your comment sheet (no later than October 6, 2017) to:

c/o Erik Wilhelm, Development Planner  
District of North Vancouver - Community Planning Department  
355 West Queens Road, North Vancouver, BC V7N 4N5

Email: [ewilhelm@dnv.org](mailto:ewilhelm@dnv.org)

Document: 3334330



DISTRICT OF  
**NORTH  
VANCOUVER**

**COMMENT SHEET**  
District of North Vancouver

**PROPOSAL:** 29 Unit Townhouse Development on Mt. Seymour Pkwy.  
**PROPOSAL ADDRESS:** 3428-3464 Mount Seymour Parkway  
**DEVELOPER:** Allaire Headwater Residences

Please provide us with any input you have on this project and feel free to attach additional sheets to help the District of North Vancouver understand neighbourhood concerns and views:

I am very opposed to the continued densification of Mt. Seymour Parkway while needed infrastructure in toward transit has not been provided for at least 10 years, we have been promised an improved Public Exchange nothing! Three years ago we asked for a direct bus to the Seabus. Nothing! Meanwhile we encounter increased gridlock on Highway 1 and the road routes in and out of Seymour. Stop adding density without infrastructure!

Your Name

Street Address

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DISTRICT OF  
**NORTH  
VANCOUVER**

**COMMENT SHEET**  
*District of North Vancouver*

**PROPOSAL:** 29 Unit Townhouse Development on Mt. Seymour Pkwy.  
**PROPOSAL ADDRESS:** 3428-3464 Mount Seymour Parkway  
**DEVELOPER:** Allaire Headwater Residences

Please provide us with any input you have on this project and feel free to attach additional sheets to help the District of North Vancouver understand neighbourhood concerns and views:

I am completely against anyone development  
in the Seymour area. We have moved at this  
address since 1961 and there has been a  
large increase in traffic, noise, crime, to become  
a bad place to live.  
I have fears as to what they are not  
able to do. As leave no remainder  
without the stress of being hit by  
an incoming traffic. Things I will take a  
harsh against the District to show my  
disapproval if they continue to proceed!

Your Name

Street Address

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DISTRICT OF  
**NORTH  
VANCOUVER**

**COMMENT SHEET**  
*District of North Vancouver*

**PROPOSAL:** 29 Unit Townhouse Development on Mt. Seymour Pkwy.  
**PROPOSAL ADDRESS:** 3428-3464 Mount Seymour Parkway  
**DEVELOPER:** Allaire Headwater Residences

*Please provide us with any input you have on this project and feel free to attach additional sheets to help the District of North Vancouver understand neighbourhood concerns and views:*

EACH UNIT SHOULD HAVE POWER AND FULL ELECTRIC  
VEHICLE CHARGING

Your Name

Street Address 3428 Mt. Seymour Parkway

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Document 3334330



October 20, 2017

Dear Mr. Wilhelm:

Please accept the attached pages signed by owners at 1281/1283 Parkgate Place with regard to the 29 Unit Townhouse Development proposal at 3428-3464 Mount Seymour Parkway.

We know that this is past the due date but we are hopeful that you can add our voices and comments to your presentation to the Council when it comes time to approve this proposal. As you can see by the comments we are very concerned about the lack of consideration of the current infrastructure and the ability to handle the construction and owner traffic for this and any future proposals for this part of the Seymour Parkway.

Thank you for your consideration.



**COMMENT SHEET**  
District of North Vancouver

**PROPOSAL:** 29 Unit Townhouse Development on Mt. Seymour Pkwy.  
**PROPOSAL ADDRESS:** 3428-3464 Mount Seymour Parkway  
**DEVELOPER:** Allaire Headwater Residences

We, the undersigned owners at Parkgate Place, 1281-1283 Parkgate Avenue, North Vancouver, do NOT approve of the proposal to develop the 3428-3464 Mt. Seymour Parkway lots into a complex of 29 townhouses.

Name	Comments
Judith Cochrane	- concerned about lack of infrastructure to
Nancy Platt	accommodate the additional traffic
Barclay Ismer	I agree with the above comment.
Nathan von Willigen	" "
Kerissa Slade	" "
John Hayes	Traffic already crowded & busy
BOB WILSON	" "
Bill & Helen Kelly	" "
Bret Slade	" "
BRAD GARD	" "
Oliver Spink	Increased traffic a big concern
Coel Dill	" "
Caroline Stewart	" "
ANNE JANSEN	" "
Mark McPuffin	" "
Grace Gillis	" "
Your Name Parkgate Place Owners	Street Address 1281-1283 Parkgate Ave.

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355 West Queens Road, North Vancouver, BC V7N 4N5

Email: [ewilhelm@dnv.org](mailto:ewilhelm@dnv.org)

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**COMMENT SHEET**  
District of North Vancouver

**PROPOSAL:** 29 Unit Townhouse Development on Mt. Seymour Pkwy.  
**PROPOSAL ADDRESS:** 3428-3464 Mount Seymour Parkway  
**DEVELOPER:** Allaire Headwater Residences

We, the undersigned owners at Parkgate Place, 1281-1283 Parkgate Avenue,  
North Vancouver, do NOT approve of the proposal to develop the 3428-3464 Mt.  
Seymour Parkway lots into a complex of 29 townhouses.

Name	Comments
Diane Winram	- Lack of infrastructure to support development
Gord Ellwyn	- too much traffic; need to manage density
Rula Davison	- too many units not enough parking? - sub 2 parking spots per unit - HOPKINS PARKING SPOTS
Annemarie Cathcart EUNICE FINDLAY	- no support for future infrastructure way out for new town houses should be for cars & gas place Mt Seymour Pkwy is getting too busy in North Vancouver
Ileen DUNCAN	- NO! enough is enough! What happens to our walking trail?
Christian & Jane Proctor RITA & STEPHAN GILL MARY BROOKS	NO. Lack of parking & infrastructure!

Your Name \_\_\_\_\_ Street Address \_\_\_\_\_

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**COMMENT SHEET**  
*District of North Vancouver*

**PROPOSAL:** 29 Unit Townhouse Development on Mt. Seymour Pkwy.  
**PROPOSAL ADDRESS:** 3428-3464 Mount Seymour Parkway  
**DEVELOPER:** Allaire Headwater Residences

*Please provide us with any input you have on this project and feel free to attach additional sheets to help the District of North Vancouver understand neighbourhood concerns and views:*

RITANA CHURCH

Your Name \_\_\_\_\_ Street Address \_\_\_\_\_

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**Email: [ewilhelm@dnv.org](mailto:ewilhelm@dnv.org)**

Document: 3334330



# **The Corporation of the District of North Vancouver**

## **Bylaw 8275**

A bylaw to amend District of North Vancouver Zoning Bylaw 3210, 1965

---

The Council for The Corporation of the District of North Vancouver enacts as follows:

### **1. Citation**

This bylaw may be cited as “District of North Vancouver Rezoning Bylaw 1366 (Bylaw 8275)”.

### **2. Amendments**

2.1 District of North Vancouver Zoning Bylaw 3210, 1965 is amended as follows:

Section 301(2) by inserting the following zoning designation in numeric sequence:

“Comprehensive Development Zone CD114”

2.2 Part 4B by inserting the following:

**“4B114 Comprehensive Development Zone 114 (CD114)”**

#### **4B114-1 Intent:**

The purpose of the CD114 zone is to establish specific land use and development regulations for a 29 unit townhouse development.

#### **4B114-2 Uses:**

The following *principal uses* are permitted in the Comprehensive Development 114 Zone:

##### **(a) Uses Permitted without Conditions:**

Not applicable

##### **(b) Conditional Uses:**

- (i) Residential building, multifamily townhouse

For the purposes of this CD114 Zone, “Residential building, multifamily townhouse” means a building having not more than three residential storeys



with private rooftop deck and consisting of two or more dwelling units with individual, exterior access to grade, all above an underground parkade.

**4B114-3 Conditions of Use:**

- (a) Balcony enclosures are not permitted;
- (b) Trellises, pergolas, or similar structures anchored to a building on the rooftop level are not permitted; and
- (c) Providing the site is developed in accordance with density provisions permitted in Section “4B114-5 Amenities”, the following outlines the minimum number of unit typologies that must be constructed:

Unit Description	Number of Units
2 bedroom	6
3 Bedroom	23

**4B114-4 Accessory Use:**

- (a) *Accessory uses* are permitted and are limited to:
  - (i) Home occupations in accordance with the regulations in Section 405 of this Bylaw.

**4B114-5 Density:**

- (a) The maximum permitted density in the CD114 Zone is limited to a floor space ratio (FSR) of 0.45 and a maximum number of 3 dwelling units, inclusive of any density bonus for energy performance; and
- (b) For the purposes of calculating floor space ratio, the following areas are exempted:
  - (i) underground parkades, including: drive aisles, electrical/mechanical rooms, garbage and recycling collection areas, bicycle storage areas, and basement areas on the parkade level; and
  - (ii) unenclosed balcony areas and rooftop deck areas.

**4B114-6 Amenities:**

Despite subsection 4B114-5, density in the CD114 Zone is increased to a maximum floor space of 3,408 m<sup>2</sup> (36,683 sq. ft.) and a maximum number of 29 townhouse units, inclusive of any density bonus for energy performance, if the owner:



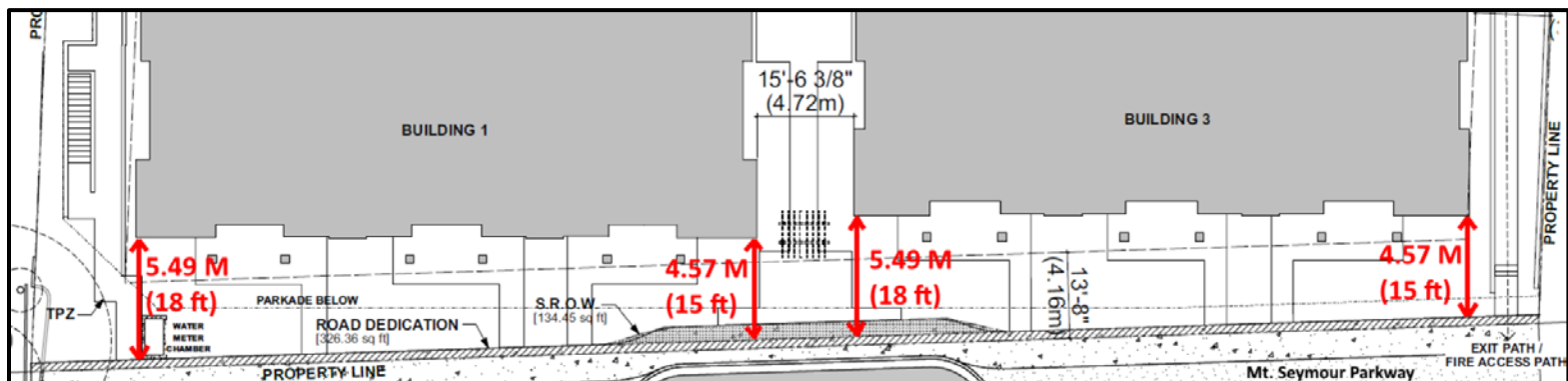
1. Contributes \$521,274.00 to the municipality to be used for any or all of the following amenities (with allocation to be determined by the municipality in its sole discretion): public art, park, trail, environmental or other public realm improvements; municipal or recreation service or facility improvements and/or affordable housing; and
2. Enters into a Housing Agreement requiring a rental disclosure statement to be filed and prohibiting any strata bylaw or regulation establishing rental restrictions.

#### **4B114-7 Setbacks:**

- (a) Buildings must be set back from property lines to the closest building face, excluding bay windows and small encroachments not to exceed 0.6 m (2.0 ft) in depth, in accordance with the following regulations:

Location	Minimum Required Setback
North Lot Line	2.39 m (7.84 ft)
East Lot Line	3 m (9.84 ft)
South Lot Line	per setbacks within section 4B114-7(b)
West Lot Line	4.39 m (14.4 ft)

- (b) Notwithstanding setbacks stipulated in section 4B114-7 (a), buildings must be set back from property lines to the closest building face in accordance with the following plan, due to road dedication requirements:



- (c) The north and south faces of buildings, excluding bay windows and small encroachments not to exceed 0.6 m (2.0 ft) in depth, must be separated by a minimum distance of 9 m (29.5 ft).

#### **4B114-8 Coverage:**

- a) Maximum permitted Building Coverage is 50%; and
- b) Maximum permitted Site Coverage is 55%.



#### **4B114-9 Height:**

- a) Maximum permitted height is 12.04 meters (39.5 ft) from finished grade.

#### **4B114-10 Landscaping:**

- a) All land areas not occupied by buildings, structures, parking spaces, loading spaces, driveways, manoeuvring aisles and sidewalks shall be landscaped or finished in accordance with an approved landscape plan; and
- b) All electrical kiosks and garbage and recycling container pads not located underground or within a building shall be screened with landscaping or fencing in accordance with an approved landscape plan.

#### **4B114-11 Subdivision Requirements**

Within the CD114 zone, the minimum lot area for the purposes of subdivision is 2,508 m<sup>2</sup> (27,000 sq. ft.).

#### **4B114-12 Motor Vehicle Parking, Bicycle Parking and Storage Regulations:**

Parking, Loading and Drive Aisle regulations within Part 10 of the Zoning Bylaw are applicable to CD114 lands except that:

- (a) A minimum of 57 motor vehicle parking spaces shall be provided in an underground parking garage inclusive of visitor parking spaces, small car spaces, and accessible parking spaces;
- (b) A minimum of 7 motor vehicle parking spaces shall be provided for the use of visitors;
- (c) A maximum of 17 small car spaces shall be permitted;
- (d) A minimum of one motor vehicle parking space, with a time duration parking restriction, shall be provided at grade with direct access from the lane;
- (e) A minimum of 40 Class 1 bicycle parking spaces (within a secured bike storage area) shall be provided; and
- (f) Each dwelling unit shall be provided a minimum of 1 Class 2 bicycle parking space (secure bicycle rack, bollard or post) at ground level within the townhouse complex.”



2.3 The Zoning Map is amended in the case of the lands in Schedule A, by rezoning the land outlined and noted as "site" to Comprehensive Development 114 Zone (CD114).

**READ** a first time May 28<sup>th</sup>, 2018

**PUBLIC HEARING** held

**READ** a second time

**READ** a third time

**ADOPTED**

---

Mayor

---

Municipal Clerk

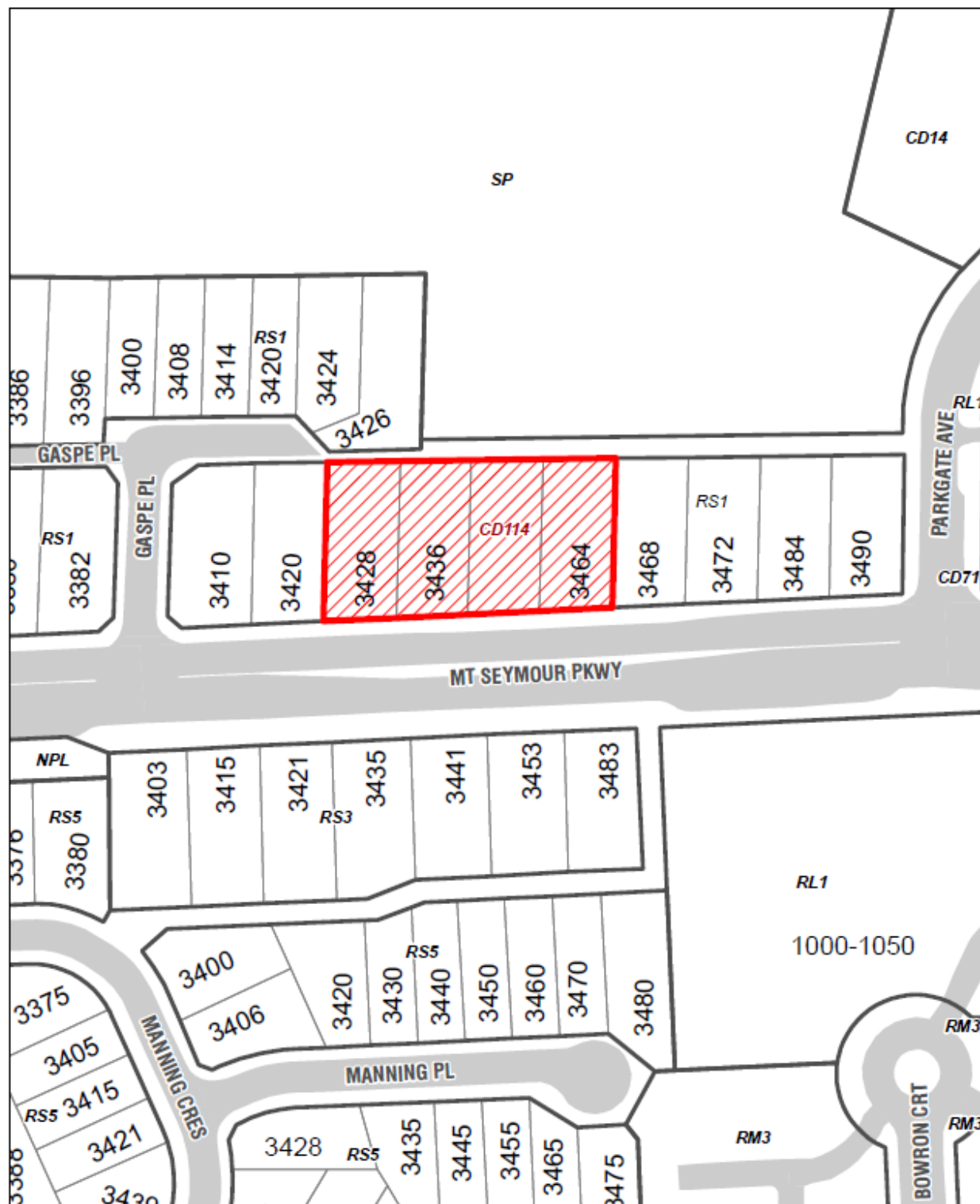
Certified a true copy

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Municipal Clerk



# Schedule A to Bylaw 8275



The Zoning Map is amended in the case of the lands in Schedule A, by rezoning the land outlined and noted as "site" from Single-Family Residential One Acre Zone (RS1) to Comprehensive Development 114 Zone (CD114)."





# **The Corporation of the District of North Vancouver**

## **Bylaw 8276**

A bylaw to enter into a Housing Agreement  
(3428 - 3464 Mount Seymour Parkway)

---

The Council for The Corporation of the District of North Vancouver enacts as follows:

### **1. Citation**

This bylaw may be cited as "Housing Agreement Bylaw 8276, 2017 (3428 - 3464 Mount Seymour Parkway)".

### **2. Authorization to Enter into Agreement**

2.1 The Council hereby authorizes a housing agreement between The Corporation of the District of North Vancouver and Allaire Headwater (Seymour) Residences Ltd. (Inc. No. BC1108300) substantially in the form attached to this Bylaw as Schedule "A" with respect to the following lands:

- |                    |  |
|--------------------|--|
| a) PID 012-602-591 | Lot 15 of Lot 6 Blocks 1 to 4 DL 622 Plan 2866 |
| b) PID 013-370-073 | Lot 16 of Lot 6 Blocks 1 to 4 DL 622 Plan 2866 |
| c) PID 006-788-467 | Lot 17 of Lot 6 Blocks 1 to 4 DL 622 Plan 2866 |
| d) PID 013-370-090 | Lot 18 of Lot 6 Blocks 1 to 4 DL 622 Plan 2866 |

### **3. Execution of Documents**

The Mayor and Municipal Clerk are authorized to execute any documents required to give effect to the Housing Agreement.

**READ** a first time May 28<sup>th</sup>, 2018

**READ** a second time

**READ** a third time

**ADOPTED**

---

Mayor

---

Municipal Clerk



Certified a true copy

---

Municipal Clerk



## Schedule A to Bylaw 8276

### SECTION 219 COVENANT – HOUSING AGREEMENT

THIS AGREEMENT is dated for reference the \_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_

BETWEEN:

**ALLAIRE HEADWATER (SEYMOUR) RESIDENCES LTD.** (Inc. No. BC1108300) a company incorporated under the laws of the Province of British Columbia having an office at 245 – 9600 Cameron Street, Burnaby, BC V3J 7N3

(the “Developer”)

AND:

**THE CORPORATION OF THE DISTRICT OF NORTH VANCOUVER**, a municipality incorporated under the *Local Government Act*, RSBC 2015, c.1 and having its office at 355 West Queens Road, North Vancouver, BC V7N 4N5

(the “District”)

WHEREAS:

1. The Developer is the registered owner of the Lands (as hereinafter defined);
2. The Developer wishes to obtain development permissions with respect to the Lands and wishes to create a condominium development which will contain residential strata units on the Lands;
3. Section 483 of the *Local Government Act* authorises the District, by bylaw, to enter into a housing agreement to provide for the prevention of rental restrictions on housing, and provides for the contents of the agreement; and
4. Section 219 of the *Land Title Act* (British Columbia) permits the registration in favour of the District of a covenant of a negative or positive nature relating to the use of land or a building thereon, or providing that land is to be built on in accordance with the covenant, or providing that land is not to be built on except in accordance with the covenant, or providing that land is not to be subdivided except in accordance with the covenant;

NOW THEREFORE in consideration of the mutual promises contained in it, and in consideration of the payment of \$1.00 by the District to the Developer (the receipt and sufficiency of which are hereby acknowledged by the Developer), the parties covenant and agree with each other as follows, as a housing agreement under Section 483 of the *Local Government Act*, as a contract and a deed under seal between the parties, and as a covenant under Section 219 of the *Land Title Act*, and the Developer hereby further covenants and agrees that neither the Lands nor any building constructed thereon shall be used or built on except in accordance with this Agreement:



## **1. DEFINITIONS**

### **1.01 Definitions**

In this agreement:

- (a) *"Development Permit"* means development permit No. 33.17 issued by the District;
- (b) *"Lands"* means land described in Item 2 of the *Land Title Act* Form C to which this agreement is attached;
- (c) *"Owner"* means the Developer and any other person or persons registered in the Lower Mainland Land Title Office as owner of the Lands from time to time, or of any parcel into which the Lands are consolidated or subdivided, whether in that person's own right or in a representative capacity or otherwise;
- (d) *"Proposed Development"* means the proposed development containing not more than 29 units to be constructed on the Lands in accordance with the Development Permit;
- (e) *"Short Term Rentals"* means any rental of a Unit for any period less than 30 days;
- (f) *"Strata Corporation"* means the strata corporation formed upon the deposit of a plan to strata subdivide the Proposed Development pursuant to the *Strata Property Act*;
- (g) *"Unit"* means a residential dwelling strata unit in the Proposed Development; and
- (h) *"Unit Owner"* means the registered owner of a Dwelling Unit in the Proposed Development.

## **2. TERM**

This Agreement will commence upon adoption by District Council of Bylaw 8276 and remain in effect until terminated by the District as set out in this Agreement.

## **3. RENTAL ACCOMODATION**

### **3.01 Rental Disclosure Statement**

No Unit in the Proposed Development may be occupied unless the Owner has:

- (a) before the first Unit is offered for sale, or conveyed to a purchaser without being offered for sale, filed with the Superintendent of Real Estate a rental disclosure statement in the prescribed form (the "Rental Disclosure Statement") designating all of the Units as rental strata lots and imposing at least a 99 year rental period in relation to all of the Units pursuant to the *Strata Property Act* (or any successor or replacement legislation), except in relation to Short Term Rentals and, for greater certainty, stipulating specifically that the 99 year rental restriction does not apply to a Strata Corporation bylaw prohibiting or restricting Short Term Rentals; and



- (b) given a copy of the Rental Disclosure Statement to each prospective purchaser of any Unit before the prospective purchaser enters into an agreement to purchase in respect of the Unit. For the purposes of this paragraph 3.01(b), the Owner is deemed to have given a copy of the Rental Disclosure Statement to each prospective purchaser of any Unit in the building if the Owner has included the Rental Disclosure Statement as an exhibit to the disclosure statement for the Proposed Development prepared by the Owner pursuant to the *Real Estate Development Marketing Act*.

3.02 Rental Accommodation

The Units constructed on the Lands from time to time may always be used to provide rental accommodation as the Owner or a Unit Owner may choose from time to time, except that this section 3.02 does not apply to Short Term Rentals which may be restricted by the Strata Corporation to the full extent permitted by law.

3.03 Binding on Strata Corporation

This agreement shall be binding upon all Strata Corporations created by the subdivision of the Lands or any part thereof (including the Units) pursuant to the *Strata Property Act*, and upon all Unit Owners.

3.04 Strata Bylaw Invalid

Any Strata Corporation bylaw which prevents, restricts or abridges the right to use any of the Units as rental accommodations (other than Short Term Rentals) shall have no force or effect.

3.05 No Bylaw

The Strata Corporation shall not pass any bylaws preventing, restricting or abridging the use of the Lands, the Proposed Development or the Units contained therein from time to time as rental accommodation (other than Short Term Rentals).

3.06 Vote

No Unit Owner, nor any tenant or mortgagee thereof, shall vote for any Strata Corporation bylaw purporting to prevent, restrict or abridge the use of the Lands, the Proposed Development or the Units contained therein from time to time as rental accommodation (other than Short Term Rentals).

3.07 Notice

The Owner will provide notice of this Agreement to any person or persons intending to purchase a Unit prior to any such person entering into an agreement of purchase and sale, agreement for sale, or option or similar right to purchase as part of the disclosure statement for any part of the Proposed Development prepared by the Owner pursuant to the *Real Estate Development Marketing Act*.



3.08 Release of Covenant [optional clause]

The District agrees that if the District of North Vancouver Rezoning Bylaw 1366 (Bylaw 8275), is not adopted by the District's Council before [date], the Owner is entitled to require the District to execute and deliver to the Owner a discharge, in registrable form, of this Agreement from title to the Land. The Owner is responsible for the preparation of the discharge under this section and for the cost of registration at the Land Title Office.

**4. DEFAULT AND REMEDIES**

4.01 Notice of Default

The District may, acting reasonably, give to the Owner written notice to cure a default under this Agreement within 30 days of delivery of the notice. The notice must specify the nature of the default. The Owner must act with diligence to correct the default within the time specified.

4.02 Costs

The Owner will pay to the District upon demand all the District's costs of exercising its rights or remedies under this Agreement, on a full indemnity basis.

4.03 Damages an Inadequate Remedy

The Owner acknowledges and agrees that in the case of a breach of this Agreement which is not fully remediable by the mere payment of money and promptly so remedied, the harm sustained by the District and to the public interest will be irreparable and not susceptible of adequate monetary compensation.

4.04 Equitable Remedies

Each party to this Agreement, in addition to its rights under this Agreement or at law, will be entitled to all equitable remedies including specific performance, injunction and declaratory relief, or any of them, to enforce its rights under this Agreement.

4.05 No Penalty or Forfeiture

The Owner acknowledges and agrees that it is entering into this Agreement to benefit the public interest in providing rental accommodation, and that the District's rights and remedies under this Agreement are necessary to ensure that this purpose is carried out, and the District's rights and remedies under this Agreement are fair and reasonable and ought not to be construed as a penalty or forfeiture.

4.06 Cumulative Remedies

No reference to nor exercise of any specific right or remedy under this Agreement or at law or at equity by any party will prejudice, limit or preclude that party from exercising any other right or remedy. No right or remedy will be exclusive or dependent upon any other right to remedy, but any party, from time to time, may exercise any one or more of such rights or remedies independently, successively, or in combination. The Owner acknowledges that specific



performance, injunctive relief (mandatory or otherwise) or other equitable relief may be the only adequate remedy for a default by the Owner under this Agreement.

## **5. LIABILITY**

### **5.01 Indemnity**

Except if arising directly from the negligence of the District or its employees, agents or contractors, the Owner will indemnify and save harmless each of the District and its board members, officers, directors, employees, agents, and elected or appointed officials,, and their heirs, executors, administrators, personal representatives, successors and assigns, from and against all claims, demands, actions, loss, damage, costs and liabilities that all or any of them will or may be liable for or suffer or incur or be put to any act or omission by the Owner or its officers, directors, employees, agents, contractors, or other persons for whom the Owner is at law responsible, or by reason of or arising out of the Owner's ownership, operation, management or financing of the Proposed Development or any part thereof.

### **5.02 Release**

The Owner hereby releases and forever discharges the District, its elected officials, board members, officers, directors, employees and agents, and its and their heirs, executors, administrators, personal representatives, successors and assigns from and against all claims, demands, damages, actions or causes of action by reason of or arising out of advice or direction respecting the ownership, operation or management of the Proposed Development or any part thereof which has been or hereafter may be given to the Owner by all or any of them.

### **5.03 Survival**

The covenants of the Owner set out in Sections 5.01 and 5.02 will survive termination of this Agreement and continue to apply to any breach of the Agreement or claim arising under this Agreement during the ownership by the Owner of the Lands or any Unit therein, as applicable.

## **6. GENERAL PROVISIONS**

### **6.01 District's Power Unaffected**

Nothing in this Agreement:

- (a) affects or limits any discretion, rights, powers, duties or obligations of the District under any enactment or at common law, including in relation to the use or subdivision of land;
- (b) affects or limits any enactment relating to the use of the Lands or any condition contained in any approval including any development permit concerning the development of the Lands; or
- (c) relieves the Owner from complying with any enactment, including the District's bylaws in relation to the use of the Lands.



6.02 Agreement for Benefit of District Only

The Owner and District agree that:

- (a) this Agreement is entered into only for the benefit of the District:
- (b) this Agreement is not intended to protect the interests of the Owner, any Unit Owner, any occupant of any Unit or any future owner, occupier or user of any part of the Proposed Development, including any Unit, or the interests of any third party, and the District has no obligation to anyone to enforce the terms of this Agreement; and
- (c) The District may at any time terminate this Agreement, in whole or in part, and execute a release and discharge of this Agreement in respect of the Proposed Development or any Unit therein, without liability to anyone for doing so.

6.03 Agreement Runs With the Lands

This Agreement burdens and runs with the Lands and any part into which any of them may be subdivided or consolidated, by strata plan or otherwise. All of the covenants and agreements contained in this Agreement are made by the Owner for itself, its successors and assigns, and all persons who acquire an interest in the Lands or in any Unit after the date of this Agreement.

6.04 Release

The covenants and agreements on the part of the Owner and any Unit Owner and herein set forth in this Agreement have been made by the Owner and any Unit Owner as contractual obligations as well as being made pursuant to Section 483 of the *Local Government Act* (British Columbia) and as such will be binding on the Owner and any Unit Owner, except that neither the Owner nor any Unit Owner shall be liable for any default in the performance or observance of this Agreement occurring after such party ceases to own the Lands or a Unit as the case may be.

6.05 Priority of This Agreement

The Owner will, at its expense, do or cause to be done all acts reasonably necessary to ensure this Agreement is registered against the title to each Unit in the Proposed Development, including any amendments to this Agreement as may be required by the Land Title Office or the District to effect such registration.

6.06 Agreement to Have Effect as Deed

The District and the Owner each intend by execution and delivery of this Agreement to create both a contract and a deed under seal.

6.07 Waiver

An alleged waiver by a party of any breach by another party of its obligations under this Agreement will be effective only if it is an express waiver of the breach in writing. No waiver of a breach of this Agreement is deemed or construed to be a consent or waiver of any other breach of this Agreement.



6.08 Time

Time is of the essence in this Agreement. If any party waives this requirement, that party may reinstate it by delivering notice to another party.

6.09 Validity of Provisions

If a Court of competent jurisdiction finds that any part of this Agreement is invalid, illegal, or unenforceable, that part is to be considered to have been severed from the rest of this Agreement and the rest of this Agreement remains in force unaffected by that holding or by the severance of that part.

6.10 Extent of Obligations and Costs

Every obligation of a party which is set out in this Agreement will extend throughout the Term and, to the extent that any obligation ought to have been observed or performed prior to or upon the expiry or earlier termination of the Term, such obligation will survive the expiry or earlier termination of the Term until it has been observed or performed.

6.11 Notices

All notices, demands, or requests of any kind, which a party may be required or permitted to serve on another in connection with this Agreement, must be in writing and may be served on the other parties by registered mail or by personal service, to the following address for each party:

If to the District:

District Municipal Hall  
355 West Queens Road  
North Vancouver, BC V7N 4N5

Attention: Planning Department

If to the Owner:

Allaire Headwater (Seymour) Residences Ltd.  
245 – 9600 Cameron Street  
Burnaby, BC V3J 7N3

If to the Unit Owner:

The address of the registered owner which appears on title to the Unit at the time of notice.

Service of any such notice, demand, or request will be deemed complete, if made by registered mail, 72 hours after the date and hour of mailing, except where there is a postal service disruption during such period, in which case service will be deemed to be complete only upon actual delivery of the notice, demand or request and if made by personal service, upon personal service being effected. Any party, from time to time, by notice in writing served upon the other parties, may



designate a different address or different or additional persons to which all notices, demands, or requests are to be addressed.

6.12 Further Assurances

Upon request by the District, the Owner will promptly do such acts and execute such documents as may be reasonably necessary, in the opinion of the District, to give effect to this Agreement.

6.13 Enuring Effect

This Agreement will enure to the benefit of and be binding upon each of the parties and their successors and permitted assigns.

**7. INTERPRETATION**

7.01 References

Gender specific terms include both genders and include corporations. Words in the singular include the plural, and words in the plural include the singular.

7.02 Construction

The division of this Agreement into sections and the use of headings are for convenience of reference only and are not intended to govern, limit or aid in the construction of any provision. In all cases, the language in this Agreement is to be construed simply according to its fair meaning, and not strictly for or against either party.

7.03 No Limitation

The word “including” when following any general statement or term is not to be construed to limit the general statement or term to the specific items which immediately follow the general statement or term similar items whether or not words such as “without limitation” or “but not limited to” are used, but rather the general statement or term is to be construed to refer to all other items that could reasonably fall within the broadest possible scope of the general statement or term.

7.04 Terms Mandatory

The words “must” and “will” and “shall” are to be construed as imperative.

7.05 Statutes

Any reference in this Agreement to any statute or bylaw includes any subsequent amendment, re-enactment, or replacement of that statute or bylaw.

7.06 Entire Agreement

(d) This is the entire agreement between the District and the Owner concerning its subject, and there are no warranties, representations, conditions or collateral agreements relating to this Agreement, except as included in this Agreement.



- (e) This Agreement may be amended only by a document executed by the parties to this Agreement and by bylaw, such amendment to be effective only upon adoption by District Council of a bylaw to amend Bylaw 8276.

7.07 Governing Law

This Agreement is to be governed by and construed and enforced in accordance with the laws of British Columbia.

As evidence of their agreement to be bound by the terms of this instrument, the parties hereto have executed the *Land Title Act* Form C that is attached hereto and forms part of this Agreement.



### GRANT OF PRIORITY

WHEREAS \_\_\_\_\_ (the "**Chargeholder**") is the holder of the following charge which is registered in the Land Title Office:

(a) \_\_\_\_\_ (the "**Charge**");

AND WHEREAS the Chargeholder agrees to allow the Section 219 Covenant herein to have priority over the Charge;

THIS PRIORITY AGREEMENT is evidence that in consideration of the sum of \$1.00 paid by THE CORPORATION OF THE DISTRICT OF NORTH VANCOUVER (the "**District**") to the Chargeholder, the receipt and sufficiency of which are hereby acknowledged, the Chargeholder covenants and agrees to subordinate and postpone all its rights, title and interest in and to the lands described in the Form C to which this Agreement is attached (the "**Lands**") with the intent and with the effect that the interests of the District rank ahead of the Charge as though the Section 219 Covenant herein had been executed, delivered and registered against title to the Lands before registration of the Charge.

As evidence of its Agreement to be bound by the above terms, as a contract and as a deed executed and delivered under seal, the Chargeholder has executed the Form C to which this Agreement is attached and which forms part of this Agreement.



# Public Hearings

**When:** 7 pm, Tuesday, June 26, 2018

**Where:** Council Chambers, District of North Vancouver Municipal Hall, 355 West Queens Road, North Vancouver, BC

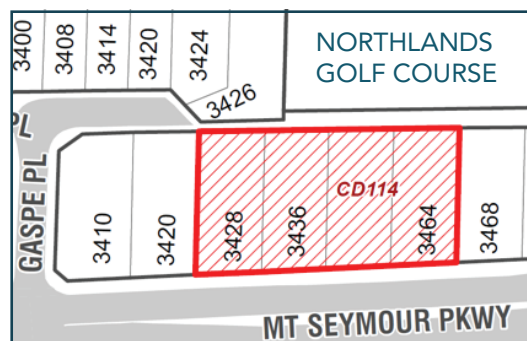
**Two public hearings will occur consecutively in the order noted below.**

## 3428-3464 Mt. Seymour Pkwy. 29 Unit Townhouse Project

**What:** A Public Hearing for Bylaw 8275, a proposed amendment to the Zoning Bylaw, to permit the development of a twenty-nine unit townhouse project.

### What changes?

Bylaw 8275 proposes to amend the District's Zoning



Proposed\*



\*Provided by applicant for illustrative purposes only.  
The actual development, if approved, may differ.

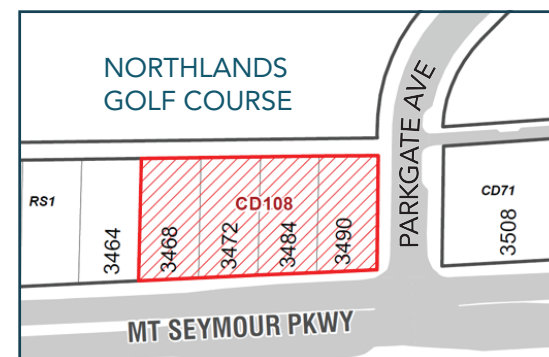
Bylaw by rezoning the subject site from Single-Family Residential One Acre Zone (RS1) to Comprehensive Development Zone 114 (CD114). The CD114 Zone addresses use and accessory use, density, amenities, setbacks, building and site coverage, height, landscaping, subdivision requirements and parking and storage requirements.

## 3468, 3472, 3484 & 3490 Mt. Seymour Pkwy. 27 Unit Townhouse Project

**What:** A Public Hearing for Bylaw 8254, a proposed amendment to the Zoning Bylaw, to permit the development of a twenty-seven unit townhouse project.

### What changes?

Bylaw 8254 proposes to amend the District's Zoning Bylaw by rezoning the subject site from Single-Family Residential



Proposed\*



\*Provided by applicant for illustrative purposes only.  
The actual development, if approved, may differ.

One Acre Zone (RS1) to Comprehensive Development Zone 108 (CD108). The CD108 Zone addresses use and accessory use, density, amenities, setbacks, building and site coverage, height, landscaping, subdivision requirements and parking and storage regulations.

## When can I provide input?

We welcome your input Tuesday, June 26, 2018, at 7 pm. You can speak in person by signing up at the hearing, or you can provide a written submission to the Municipal Clerk at [input@dnv.org](mailto:input@dnv.org) or by mail to Municipal Clerk, District of North Vancouver, 355 West Queens Road, North Vancouver, BC, V7N 4N5, before the conclusion of the hearing. *Please note that Council may not receive further submissions from the public concerning this application after the conclusion of the public hearing.*

## Need more info?

Relevant background material and copies of the bylaws are available for review at the Municipal Clerk's Office or online at [dnv.org/public\\_hearing](http://dnv.org/public_hearing) from June 12 to June 26. Office hours are Monday to Friday 8 am to 4:30 pm, except statutory holidays.



ATTACHMENT 5 to SCHEDULE "A", Bylaw 7347

**SCHEDULE C – LOCAL PLANS**  
**C.8 Seymour Local Plan**



## **C.8 SEYMOUR LOCAL PLAN**

### **1.0 The Seymour Local Plan**

The District Official Community Plan is a municipal bylaw adopted by Council to establish community level objectives, broad social, economic and environmental policies and development permit areas. The Seymour Local Plan addresses these issues in more detail and from a local perspective, for the local area indicated by *Chapter 1 Map*. The Seymour Local Plan forms a part of the District Official Community Plan.

The Seymour Local Plan is intended to guide decision-making and protect Seymour in accordance with the shared community vision developed through the Seymour Local Plan process. This process was unique. First, it was approved by District council to be designed and managed by Seymour residents and driven by public input, with staff's role being one of support. Second, the phasing strategy of the plan focuses on a ten year horizon, but looks beyond to 20 years or more, in order to provide its growth management perspective. Finally, while the neighbouring areas of Maplewood and the Tsleil-Waututh First Nation lie outside the boundary and authority of the Seymour Local Plan, it attempts to account for growth and development activities in these areas to comprehensively consider change and its impacts in the entire area east of the Seymour River.

The objectives and policies included in the Seymour Local Plan are supported by the rationale and detailed background information provided in *The Seymour Local Plan Background Planning Report*.

### **2.0 Plan Vision & Goals**

A local plan sets out the shared vision of the community and a strategy, in the form of goals and policies, to pursue it. The shared vision describes what the community wishes to retain or protect and the things it hopes to achieve. In Seymour, this means protecting the natural environment and maintaining the forested character of its attractive neighbourhoods for current and future residents.

Seymour's vision is of a community where urban development edges a scenic wilderness and skirts precious shoreline. The challenge and desire is to maintain the delicate balance between urban and natural lands, through a commitment to stewardship of the land. A more complete Seymour community will foster linkages between home, work and recreation, supporting opportunities based on local skills, knowledge and the natural environment. The community vision sees an accessible community with a variety of transportation options which are user and environmentally friendly, safe and socially equitable. The community will continue to be defined by the natural environment, local history and shared goals. Above all, Seymour's vision is of a community governed by '*stewardship of the land*.' The wish is to pass to future generations the natural and unique qualities of the present, through careful



management of growth and change. The vision will be promoted by pursuing the following broad goals:

*Seymour & Its Environment:* Recognize and consider the relationship between the Seymour community and its natural environment in all planning and decision making and promote the wise management and sustainable use of land, water, energy and other resources.

*Attractive Residential Community:* Preserve and enhance Seymour as an attractive residential community defined by and protective of its natural forest and waterfront setting.

*Managed Growth:* Manage limited and gradual growth, which balances environmental concerns and community aspirations, to enhance and sustain the Seymour community.

*Transportation & Access:* Achieve a flexible, efficient and responsive transportation system, which particularly addresses east-west movement, to enhance the liveability of Seymour.

*Housing:* While maintaining the predominantly single family character of Seymour, provide a diversity of housing choices to meet the community's needs as they change.

*Community Services:* Provide a level of community services in Seymour that supports the community's needs, is equitable and keeps pace with growth and change.

*Economic Growth & Development:* In keeping with the overall low-growth strategy, foster opportunities based on local skills, knowledge, the arts and the natural environment, to improve linkages between home, work and recreation within Seymour.

*Partnerships & Relationships:* Work with neighbours and partners, including District, First Nations, Regional, Provincial and Federal agencies, neighbouring communities and others to achieve Seymour's goals.

*Seymour Identity:* Establish and promote a 'Seymour Theme' in decision-making and to reflect Seymour's identity and sense of place. This theme should reflect the forest and waterfront setting, local history, neighbourhoods and respect for the environment.

### **3.0 Seymour's Natural Environment**

Seymour's residential neighbourhoods are nestled within a setting spanning mountain forests and ocean shores. Seymour River and numerous freshwater streams link these features and nourish both human and natural systems. The



forests and watercourses not only serve as a scenic backdrop to the District of North Vancouver's eastern neighbourhoods; but also as recreational assets, important wildlife habitats and vital links in a complex ecological system. The Seymour vision is of a high quality of life for current residents and future generations, shaped by a commitment to stewardship of the land. This makes protection of the environment a key theme of the Plan, which promotes District-wide environmental provisions already in place and enhances them with local knowledge and community priorities.

Fundamentally, the Plan does this by directing development to sites already developed, environmentally compromised or formerly used for other purposes, and by bypassing sites with environmental significance. In addition, the Plan adopts many of the recommendations of the Waterfront Task Force. Finally, the policies here and in Section 7.0, *Parks & Open Space*, promote the sustainability of Seymour's environment by seeking to balance use and enjoyment with preservation, enhancement and wise management of access to parks, trails, waterfront and other natural areas.

### **Goal**

***Preserve and enhance Seymour as an attractive residential community, defined by and protective of its natural forest and waterfront setting.***

**Objective 3.1    *To become a more environmentally sustainable community.***

**Policy 3.1.1**      Development and redevelopment will be regarded as an opportunity to promote community environmental goals such as obtaining more green space, reclaiming natural areas and encouraging transit use.

**Policy 3.1.2**      New development or redevelopment will employ environmentally friendly design and construction methods.

### **Implementation**

3.1.2.1            Develop guidelines for environmentally preferable design and construction methods, including attention to energy efficiency, water conservation, recycling of construction materials and safe disposal of construction waste.

3.1.2.2            Ensure adherence to the Land Development Guidelines for the Protection of Aquatic Environments and relevant regulations, including the Streamside Protection Regulations (SPR). In any case, the minimum setback for any new construction will be fifteen meters.

**Policy 3.1.3**      Explore opportunities to improve storm water management and promote alternative development standards such as reduced hard surfaces, innovative drainage methods, clustered development and water conserving landscaping, for example.



**Policy 3.1.4** Encourage the development of watershed management plans.

**Policy 3.1.5** Continue to investigate and document Seymour's environmental features, sensitive areas and other environmental issues as a reference for District departments, residents, businesses, developers, visitors and others.

**Policy 3.1.6** Create a positive reputation for parks, wilderness and environmental preservation in Seymour.

Implementation

3.1.6.1 Undertake a public education program to promote community awareness of and responsibility for energy and water use, waste reduction and environmental stewardship. E.g. storm sewer markings.

3.1.6.2 District to host environmental awareness and education workshops for residents, developers and others.

3.1.6.3 Work with District departments, volunteers, community groups and others to promote strategies for co-existing with nature. For example, work with North Shore Streamkeepers, The North Shore Black Bear Network and others to reduce stream degradation and human/wildlife conflicts.

**Policy 3.1.7** Support volunteer efforts to restore, maintain and enhance the natural community. E.g. North Shore Streamkeepers.

**Objective 3.2** ***To be a community that balances environmental protection and community uses.***

**Policy 3.2.1** Seek to maximize environmental benefit and reduce negative environmental impact in decision-making.

**Policy 3.2.2** Work with waterfront homeowners, businesses and others to identify effective ways of protecting shoreline properties while also protecting and restoring shoreline habitat.

**Policy 3.2.3** Promote environmental objectives by managing recreational access to and use of Seymour's trails, wilderness areas and waterfront.

Implementation

3.2.3.1 Ensure environmental studies are conducted prior to designing and establishing new waterfront or other trails.



- 3.2.3.2 Develop strategies to protect shoreline areas that have been identified as being sensitive to foot traffic<sup>1</sup>.
- 3.2.3.3 Encourage the development and use of shared moorage facilities for waterfront properties to reduce both the number of private wharves along the waterfront and the negative marine environmental impact of such structures.
- Policy 3.2.4** Develop and maintain different types and scales of greenways and green space to link and define neighbourhoods. For example, community gardens, green space between buildings, rooftop gardens.
- Policy 3.2.5** Identify, maintain and develop sufficient and appropriate wildlife corridors and linkages to connect natural areas throughout the community.
- Objective 3.3** *To preserve, restore and enhance natural areas, key habitats and other special areas.*
- Policy 3.3.1** Prioritize identification and rehabilitation of environmentally damaged and environmentally sensitive areas.
- Policy 3.3.2** Preserve and enhance wildlife habitat and corridors.
- Implementation
- 3.3.2.1 Provide a protected wildlife corridor from Mountain Forest to the Maplewood mudflats.
- 3.3.2.2 Work with residents, other District communities and agencies to identify and protect other wildlife habitats and corridors, through signage and other means.
- 3.3.2.3 Work with residents, District departments and other agencies to investigate the potential for a continuous corridor between Canyon Creek Park and Hogan's Pools Conservation area.
- 3.3.2.4 Promote natural landscaping ("naturescaping") among District departments, developers and private owners to restore and create neighbourhood and backyard wildlife habitat throughout the community.
- Policy 3.3.3** Protect the forested character of Seymour.
- Implementation
- 3.3.3.1 Protect, and allow to remain undisturbed, tracts of wilderness to preserve the overall forested character of Seymour and support present wildlife habitats.

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<sup>1</sup> Westmar Consultants Inc and G.L. Williams & Associates Ltd. *Waterfront Trail Prefeasibility Study*. March 5, 1999.



- 3.3.3.2 Initiate a planning study, including a biophysical inventory, of Mountain and Cove forests to determine appropriate use and management. (See also Section 7.3)
- 3.3.3.3 Preserve undeveloped forest areas of Mountain and Cove Forests as natural park.
- 3.3.3.4 Involve and educate the community regarding forest protection practices, including promoting proper management and maintenance of native trees, through publications and display materials
- 3.3.3.5 Developers and tree companies will conform to required standards for tree protection and preservation in accordance with the District Environmental Protection and Preservation Bylaw (EPPB).
- 3.3.3.6 Continue to ensure effective protection of trees during land development activities in accordance with the District Environmental Protection and Preservation Bylaw (EPPB).

**Policy 3.3.4** Protect wetlands, creeks, streams, rivers and waterfront as natural landscapes and identify restoration opportunities.

Implementation

- 3.3.4.1 Identify and protect marine habitats.
- 3.3.4.2 Work with Vancouver Port Authority to remove all encroachments onto the beach.
- 3.3.4.3 Intertidal zones should be left in a natural state or restored, where possible, to their natural state.
- 3.3.4.4 Restore and replant damaged or historically lost riparian areas.<sup>2</sup>
- 3.3.4.5 Continue to remove barriers to fish migration.
- 3.3.4.6 In accordance with District, Provincial and Federal regulations, creek crossings will be achieved with clear spans wherever possible and will be designed, in all cases, to minimize disturbance and maximize protection of the creek environment.
- 3.3.4.7 Investigate opportunities to daylight streams.<sup>3</sup>

**Policy 3.3.5** Ensure Development Permit Area designations for protection of the natural environment apply to all stream corridors in the Seymour Local Plan area.

Implementation

- 3.3.5.1 Initiate a comprehensive review of Seymour's streams to determine precise boundaries, conditions, guidelines and activities, prior to amending the District Official Community Plan.

**Objective 3.4** *To protect development from natural hazards.*

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<sup>2</sup> Riparian: relating to or living or located on the bank of a natural watercourse

<sup>3</sup> To daylight a stream: is to re-establish an open watercourse by removing whatever form of engineering enclosure (pipe, culvert, etc.) that was previously installed to carry the flow of water.



**Policy 3.4.1** Development Permit Area designations for protection from hazardous conditions will be expanded to account for debris flow/flood hazards from Seymour's creeks.

Implementation

3.4.1.1 Amend the District Official Community Plan Development Permit Areas (DPA) for protection from hazardous conditions to designate properties within debris flow/flood and fan areas for creeks in the Deep Cove Zone and Indian Arm Zone.<sup>4</sup>

3.4.1.2 Amend the District Official Community Plan to describe the creek characteristics, hazard, consequence and risk of debris flows and floods.

3.4.1.3 Amend the District Official Community Plan to specify guidelines for development permit activity within hazard areas.

3.4.1.4 Continue specific site analysis to determine the extent and impact of debris flow hazards.

**Policy 3.4.2** Work with other jurisdictions and owners to protect development near areas of steep slopes.

**Policy 3.4.3** Examine the feasibility of a DNV-Provincial Government land exchange agreement to achieve objectives relating to the protection of steep slopes along Riverside Drive.

**Objective 3.5** *To protect and improve the environmental health and safety of the community.*

**Policy 3.5.1** Work towards zero pollution.

Implementation

3.5.1.1 Promote cleaner air by identifying improvements to local transit, encouraging use of public transit and car-pooling and examining alternative transportation options such as park & ride, shuttle buses and marine links.

3.5.1.2 Establish pump out stations for boats at various sites on the District waterfront. Work with stakeholders and consider capital and operating costs, potential revenue, funding, private/public ownership and alternative park or marine sites.

3.5.1.3 Work to improve swimming quality of water at Deep Cove.

**Policy 3.5.2** Encourage the protection of the Seymour watershed as a publicly owned and operated system to maintain a safe water supply, and encourage the GVWD to continue to prohibit watershed logging.

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<sup>4</sup> As identified in: EBA Engineering Consultants Ltd & Kerr Wood Leidal Associates Ltd. *District of North Vancouver Overview Report on Debris Flow Hazards*. April 1999.



**Policy 3.5.3** Encourage the responsible management of sewage input and treatment in order to reduce sewage volume, increase treatment efficiency and promote community and environmental health.

**Policy 3.5.4** Identify contaminated sites and develop remediation strategies, with timelines.

Implementation

3.5.4.1 Document identified instances of contamination in Seymour and work with landowners to develop remediation plans.

3.5.4.2 Ensure site assessments and remediation plans are conducted in the succession planning for the Northlands site (formerly the Blair Rifle Range).

**Policy 3.5.5** Support and encourage the future transition from industrial use to mixed-use development with a strong public component on the Roche Point Waterfront.

**Policy 3.5.6** Ensure that a site assessment and a remediation plan are included in the succession planning for the McKenzie Barge/Noble Towing site. (*See also Sections 4.0, 7.0 & 9.0*)

**Policy 3.5.7** Promote awareness of and enforce District environmental standards.

Implementation

3.5.7.1 Continue to enforce District Environmental Protection & Preservation Bylaw (EPPB) and other standards with regard to waste disposal, pollution, soil and tree removal and noise bylaws, including enforcement of penalties for contravention of the bylaws.

3.5.7.2 Continue to employ the District Integrated Pest Management Program and investigate drafting a pesticide bylaw to restrict the use of pesticides for cosmetic purposes.

#### **4.0 Managed Growth & Development**

Within the context of the Greater Vancouver area and its Regional Growth Strategy, the District of North Vancouver, including Seymour, is a designated low growth area. Seymour's greater contribution to Regional objectives is the provision and guardianship of much of the Region's Green Zone. Still, as one of the last of Greater Vancouver's inner suburbs with some open land, Seymour does harbour potential for change.

Within this context, public input and deliberations throughout the plan process identified these key variables influencing potential development:

- Environmental stewardship



- Transportation and traffic systems
  - Growth parameters
  - Minimizing change in established neighbourhoods
  - Ensuring that community services & facilities keep pace with change
- Indeed, a primary factor in the growth phasing strategy outlined in this section is the “character” of the land being considered for development. That is, to manage growth in accordance with plan principles, the strategy directs development first to sites that are environmentally compromised, formerly used for other purposes, or currently developed. Undeveloped sites in a natural state, exhibiting significant environmental value, are not included in plan phasing.

It is because of the inherent potential for change within Seymour, coupled with the community’s priorities for environmental stewardship, transportation improvements and liveability, that the Plan adopts a phasing strategy that limits growth to an average 50 units per year, organized in 5-year phases and spanning a twenty-year (and beyond) time frame. Phasing is embedded in this longer plan horizon to guard against rapid growth that exceeds the functional capacity of Seymour. As well, it is expected that residential growth in Maplewood and on Tsleil-Waututh land in the next 5 to 10 years will tax transportation and other capacities of the area east of the Seymour River. Pressures of growth will limit access to bridges, highways, hospitals, schools and support services for all residents. While this situation is considered serious, it is possible that limited improvements will be realized in the coming ten years. Should this occur, managed residential growth and development may be possible. If improvements are made more rapidly than expected, the phasing could be advanced; whereas, if improvements are slower than expected, development would likewise proceed more slowly. These potential outcomes are recognized by the Plan and addressed through the managed growth and phasing strategy, which is applied to these sites (*Chapter 4 Map*):

*Anne Macdonald Way  
Northlands  
Mount Seymour Parkway  
Roche Point East  
Riverside Terrace*

The Plan provides an important overall strategy for growth management in Seymour, based upon public input at the time of its development. As the Plan is implemented, any specific proposal will be assessed against Plan criteria and be subject to all the usual development review processes, including public review. Over time, plan criteria will be assessed to ensure that the growth management and phasing strategy continues to reflect community objectives.

### **Goal**

***To manage limited and balanced growth to protect the environment, enhance liveability, recognize Seymour’s regional role in Greater Vancouver’s Green Zone and contribute to a more sustainable community.***



- Objective 4.1** *Increased stewardship of the environment and community sustainability.*
- Policy 4.1.1** Support and promote the protection of the Green Zone as a local and regional asset and responsibility.
- Policy 4.1.2** Encourage and support community-based or other efforts to restore, protect and maintain forests, streams, wetlands and natural habitats.
- Policy 4.1.3** Promote a sustainable community by considering the benefits and impacts of residential, economic and tourism growth comprehensively.
- Policy 4.1.4** Assess any development proposals in terms of impact on the land, transportation systems, use of resources, compatibility with the existing community and principles for neighbourhood pedestrian linkages, compact design, access to transit, facilities and services.
- Policy 4.1.5** Growth in Seymour should be managed concurrently with the timely and equitable provision of infrastructure, community services and facilities and transportation improvements.
- Objective 4.2** *Low residential growth that is managed to meet the needs of Seymour residents.*
- Policy 4.2.1** Residential development will maintain the predominantly single family character of Seymour, but will also include some housing options for residents, as needs change.
- Objective 4.3** *Annual Seymour Plan area residential growth that should not exceed 50 units per year on average, within a series of 5-year time frames over the life of the plan, to a total of 250 units per five-year period.*
- Objective 4.4** *Phase residential development of Seymour Plan area sites on the basis of an average of 50 units per year, considering the criteria outlined in Policy 4.4.1, below.*
- Policy 4.4.1** The phasing strategy, and subsequent specific development proposals will consider the following:
- a. Development in Maplewood and on Tsleil-Waututh lands will be considered in the 250 units per 5-year limit, for the purpose of determining the amount of development that is possible on Seymour Plan area sites.



- b. Character and availability of developable land
  - c. Scale and timing of the proposal
  - d. Environmental issues and constraints
  - e. Capacity of and impact on the transportation system
  - f. Capacity of and impact on community services, facilities, infrastructure, and community evacuation in the event of emergency
  - g. Investment made or required for infrastructure and community services
  - h. Compatibility with surrounding neighbourhood
  - i. Consistency with compact community principles for neighbourhood linkages, access to transit, services and amenities
  - j. Benefit contributed to Seymour
  - k. Other factors identified by the community and partners
- Policy 4.4.2** The phasing strategy will identify the location of potential development in Seymour. (*Chapter 4 Map*)
- Policy 4.4.3** The phasing strategy will outline key variables affecting development of particular sites and articulate conditions under which development may proceed. (*Table 4-1*)
- Implementation:
- 4.4.3.1 Should residential development in the area east of the Seymour River exceed 50 units per year, on average, the amount of permitted residential development on Seymour Plan area sites in subsequent 5-year phases will be reduced accordingly.
- 4.4.3.2 Should residential development in the area east of the Seymour River be less than 50 units per year, on average, the amount of permitted residential development on Seymour Plan area sites planned for subsequent 5-year phases may commence earlier than originally anticipated.
- Policy 4.4.4** Provided that identified conditions of development are addressed, the sequencing of development is flexible.
- Policy 4.4.5** Consult on an ongoing basis with Tsleil-Waututh representatives, to plan comprehensively for residential growth and its impacts on the Seymour area.
- Policy 4.4.6** The District Community Planning Department will consider Seymour-wide impact when planning for growth in the area east of the Seymour River.
- Policy 4.4.7** Residential land use planning in Seymour will consider changes in neighbouring communities, other District of North Vancouver plans



and policies within the context of the Liveable Region Strategic Plan.

**Objective 4.5**     ***Regular and ongoing review of the impacts of growth and Plan criteria.***

**Policy 4.5.1**     With Community Planning, community representatives and others, a Plan monitoring committee will be established, and a strategy and criteria will be developed, to ensure that development decisions are contributing to community goals. The strategy will include:

- a.         Regular review of the growth rate, with an initial assessment one year after Plan adoption and ongoing review thereafter as required, but within 5 years as a minimum
- b.         Review of plan phasing criteria
- c.         Review of progress on traffic improvements and the impact on growth parameters
- d.         Progress on improving community sustainability
- e.         Effective communication regarding plan criteria, subsequent development proposals, decisions made and evolving community needs and objectives

**Objective 4.6**     ***Increased recreational and tourism opportunities in Seymour, which benefit the Seymour community, regional residents and visitors, while maintaining the natural environment and residential quality of life.***

**Policy 4.6.1**     Include recreational/tourism growth as a potential component of a balanced community.

**Policy 4.6.2**     Explore partnership opportunities to increase recreation and tourism, including First Nations initiatives.

**Policy 4.6.3**     Identify and evaluate all proposals in consultation with the community to ensure such proposals minimize traffic impacts and contribute to Seymour community objectives.

**Policy 4.6.4**     Ensure that all existing or proposed recreational uses respect, preserve and enhance natural features, habitats and Seymour's attractive residential character.

Implementation

4.6.4.1         Investigate the need, design and phasing of buffer zones between residential and recreational uses.

**Objective 4.7**     ***Increased local opportunities for economic development and employment.***



- Polilcy 4.7.1** Identify and support community economic development opportunities in areas such as: professional services, recreation, tourism, film, television production, arts, culture, non-polluting and technology-based industries.
- Polilcy 4.7.2** Consider economic growth potential in neighbouring areas, including Maplewood and Tsleil-Waututh Nation, in planning for economic and other growth.
- Polilcy 4.7.3** Explore partnership opportunities to promote local community economic development.



**TABLE 4-1: PHASING VARIABLES & CONDITIONS OF DEVELOPMENT****Notes:**

- Adherence to the Plan growth rate of 50 units per year, on average over any five-year time frame, is a key variable in all cases and a change in rate of growth in one phase will affect the rate in subsequent phases. (See 4.4.3)
- 5 year phases over the life of the Plan (to 2022) are: Phase I – 2003-2007; Phase II – 2008-2012; Phase III – 2013 – 2017; Phase IV 2018-2022; Phase V – beyond 2022
- To determine the amount of development possible on the Seymour Plan area sites listed below, development in Maplewood & on Tsileil-Waututh lands will be considered in the 250 units per phase total

LOCATION	UNITS (Est.)	KEY VARIABLES	CONDITIONS
Anne Macdonald Way	29 (+ up to 8)	<ul style="list-style-type: none"> <li>• Market</li> <li>• Phasing</li> <li>• Existing CD14 zone</li> </ul>	<ul style="list-style-type: none"> <li>• District land</li> <li>• Process &amp; design guidelines established</li> </ul>
Northlands	300	<ul style="list-style-type: none"> <li>• Growth rate parameter</li> <li>• Multiphase</li> <li>• Environmental constraints (contamination, remediation, risk assessment, protection of creeks)</li> <li>• Impact on traffic</li> <li>• Readiness of landowner</li> <li>• Economic viability</li> </ul>	<ul style="list-style-type: none"> <li>• Community support</li> <li>• Early, open, frequent consultation to establish overall plan</li> <li>• Growth rate</li> <li>• Comprehensive phasing plan required at outset</li> <li>• Start small, start south end</li> <li>• Transportation study and plan to address east-west traffic impacts &amp; other linkages</li> <li>• Comprehensive environmental assessment &amp; plan</li> <li>• Assessment of impact on schools &amp; other community services</li> <li>• Buffer from Parkway</li> <li>• Integrate adjacent communities through multi-use trails, linkages.</li> </ul>
Mount Seymour Parkway <ul style="list-style-type: none"> <li>• South side</li> <li>• 3600 block</li> </ul>	25	<ul style="list-style-type: none"> <li>• Adherence to growth parameters</li> <li>• Financial feasibility</li> <li>• DNV/Private partnership</li> <li>• Community support</li> </ul>	<ul style="list-style-type: none"> <li>• Community support; design acceptable to immediate and broader community, i.e. resolve neighbours preference for higher building with overall desire for no high rises and low density</li> <li>• Private/DNV lots partnership to be achieved</li> <li>• Buffer to Parkway &amp; to existing townhouse neighbours</li> <li>• Mitigate noise</li> <li>• No access off Parkway</li> <li>• Traffic study &amp; plan</li> </ul>



**TABLE 4-1: PHASING VARIABLES & CONDITIONS OF DEVELOPMENT****Notes:**

- Adherence to the Plan growth rate of 50 units per year, on average over any five-year time frame, is a key variable in all cases and a change in rate of growth in one phase will affect the rate in subsequent phases. (See 4.4.3)
- 5 year phases over the life of the Plan (to 2022) are: Phase I – 2003-2007; Phase II – 2008-2012; Phase III – 2013 – 2017; Phase IV 2018-2022; Phase V – beyond 2022
- To determine the amount of development possible on the Seymour Plan area sites listed below, development in Maplewood & on Tsleil-Waututh lands will be considered in the 250 units per phase total

LOCATION	UNITS (Estimated)	KEY VARIABLES	CONDITIONS
Mount Seymour Parkway <ul style="list-style-type: none"> <li>• North side</li> <li>• Potential blocks 3200 to 3500</li> </ul>	<ul style="list-style-type: none"> <li>• 35-105</li> <li>• Depends on form</li> </ul>	<ul style="list-style-type: none"> <li>• Adherence to growth parameters</li> <li>• Traffic impacts</li> <li>• Environmental constraints (creek crossing)</li> <li>• Community support</li> <li>• DNV/Private partnership</li> <li>• Financial feasibility</li> </ul>	<ul style="list-style-type: none"> <li>• Community support</li> <li>• Phasing &amp; design to respect growth parameters</li> <li>• Traffic study &amp; plan</li> <li>• No access off parkway; shared access; lane preferred</li> <li>• Buffer from Parkway, mitigate noise, acoustical design</li> <li>• DNV/Private partnership</li> </ul>
Roche Point East	4	<ul style="list-style-type: none"> <li>• Environmental impact &amp; preservation</li> <li>• Consider holding for future study</li> </ul>	<ul style="list-style-type: none"> <li>• Community support</li> <li>• No connection of Roche Point Drive</li> <li>• Detailed environmental assessment, including top-of-bank</li> <li>• Retain green ways at end of cul-de-sac</li> </ul>
Riverside Terrace	<ul style="list-style-type: none"> <li>• Estimated up to 36</li> <li>• In keeping with surrounding single family</li> </ul>	<ul style="list-style-type: none"> <li>• Growth parameters</li> <li>• Environmental constraints especially related to Hogan's Pools</li> <li>• Geotechnical issues relating to steep banks</li> <li>• Traffic access &amp; impact, particularly related to Jordan St. accessing Riverside Drive</li> <li>• Consider holding for future study</li> </ul>	<ul style="list-style-type: none"> <li>• Community support</li> <li>• Traffic study &amp; plan</li> <li>• Retention of green space &amp; public access</li> <li>• Detailed environmental, Geotechnical and other studies leading to acceptable plan</li> </ul>



## **5.0    Transportation, Traffic & Utilities**

With traffic congestion a daily reality for commuters and residents, access and transportation are key determinants of the liveability in Seymour and a prime consideration in planning for the area. The current transportation system continues to be shaped by the Provincial highway system and Seymour's mountainside setting. The interchanges for Highway 1 and the Ironworkers Memorial Bridge determine traffic flow to, from and within the Seymour, Maplewood and Inter-River communities.

The solution to Seymour's traffic challenge must be multi-faceted; including adopting a rate of development consistent with the existing infrastructure, creating viable alternative means of accessing Seymour and promoting the use of these alternatives among commuters. The Plan therefore highlights the need for transportation improvements as a condition of development and to acknowledge the direct influence of traffic on Seymour's liveability. While recognizing the regional transportation context shaping Seymour, the Plan is aimed at encouraging transportation decisions that improve local conditions. As such, it underscores the need to communicate with surrounding communities and provincial ministries to achieve solutions to traffic congestion and improvements to east-west movement. This includes improving mobility for residents with destinations in North Vancouver City and western District neighbourhoods not wishing to access the bridge or provincial road system. It involves managing traffic to local and regional attractions, in ways that minimize local area impacts. It also includes a comprehensive examination of the need and alternatives for additional north-south connections.

Finally, reflecting Seymour's desire to promote community sustainability, plan statements also strongly encourage the development and improvement of alternative transportation options, including improved bus service, other forms of public transit (such as community mini-buses and future marine linkages) and an increased emphasis on cycle and pedestrian linkages.

### **Goal**

***A flexible, efficient and safe multi-modal transportation system, which particularly addresses east-west movement, to enhance the liveability of Seymour.***

**Objective 5.1    *Alternative access options and changes to the major road network, which improve local conditions.***

**Policy 5.1.1**    In co-operation with other governments and neighbouring communities, continue to develop a multi-modal transportation system, which emphasizes improvements to east-west routes.

**Policy 5.1.2**    Encourage the Province to improve access to the Ironworkers Memorial Second Narrows Bridge and to points East and West, from both Mount Seymour Parkway and Dollarton Highway. Improvements could include public transit alternatives, bridgehead access, ramps, or an east-west connector, for example.



- Policy 5.1.3** In consultation with the community, the Province and other partners, give priority to creating an alternative route for cross-municipal traffic between Seymour and other North Vancouver destinations.
- Policy 5.1.4** In co-operation with other governments, agencies, neighbouring communities and partners, actively pursue a commuter ferry or other marine linkages, which accommodate cyclists.
- Policy 5.1.5** In consultation with the Seymour neighbourhoods, adjacent communities, Tsleil-Waututh Nation and other possible partners, investigate and prioritize the need for additional north/south connections between Mount Seymour Parkway and Dollarton Highway. (E.g. along Apex Road or other points through IR#3).
- Policy 5.1.6** Roche Point Drive north and Roche Point Drive south will remain as cul-de-sacs, with Roche Point Drive north accessing only Mount Seymour Parkway and Roche Point south accessing Dollarton Highway.
- Policy 5.1.7** Identify and provide additional north-south pedestrian and cycle corridors – both on and off road – to link Seymour neighbourhoods and services.
- Policy 5.1.8** Work with other levels of governments, agencies and organizations to minimize environmental impact by facilitating a shift from cars to public transit and cycling, in part by developing cycle routes that are safe, direct and efficient.
- Policy 5.1.9** Encourage TransLink and the Province to prioritize projects that improve transit access to the Second Narrows Bridge and points beyond. These could include, for example: twinning of the Dollarton Highway Bridge, queue-jumping along Mount Seymour Parkway and improvements to the Ironworkers Memorial Second Narrows bridge and bridgehead ramps.
- Policy 5.1.10** Request that TransLink give priority to providing effective public transit service to educational institutions to reduce congestion during commuting periods.
- Policy 5.1.11** Improve local neighbourhood safety along major roads.

#### Implementation

- 5.1.11.1 Monitor traffic movements and capacities along arterial and collector roads to identify and prioritize the provision of traffic signals, pedestrian signals, and special cross walks or other intersection improvements.
- 5.1.11.2 Work with provincial ministries, Tsleil-Waututh Nation, the RCMP and others to improve pedestrian safety on Dollarton Highway, Mount Seymour



Parkway, Deep Cove Road, Berkley Road, Mount Seymour Road and Indian River Drive in part by:

- a) examining solutions to maintain posted speed limits
- b) providing a safer pedestrian route along Dollarton Highway and at specific crossing points e.g. Dollar Road, Riverside Drive
- c) providing clear signage to major destinations such as the Seymour Mountain Ski area

**Policy 5.1.12** Facilitate the movement of goods in to and out of the community.

Implementation

5.1.12.1 Consider the needs and impacts of truck and other commercial traffic in major road network decisions.

**Policy 5.1.13** Within the context of the comprehensive review of the District of North Vancouver Subdivision Control Bylaw by the Engineering and Planning departments, review and amend where necessary, Seymour's road classifications.

**Policy 5.1.14** Retain traffic and transit rights-of-way for current and future needs.

Implementation

5.1.14.1 Identify, document and reserve unopened road allowances for future pedestrian and cycle route improvements.

5.1.14.2 Integrate trail marker signage with sidewalks to foster pedestrian and cycle linkages within the community.

**Objective 5.2** ***A community-focused transportation system that is safe, equitable and inclusive for vehicles, cyclists, pedestrians and transit users.***

**Policy 5.2.1** Consider the needs of a range of users in designing and implementing transportation and transit policies.

**Policy 5.2.2** Continue to monitor and improve safety along local roads and routes by employing neighbourhood input, awareness, traffic calming and other approaches, for example.

**Policy 5.2.3** Continue to improve accessibility to public transit services by investigating alternative transit models.

**Policy 5.2.4** Work with TransLink and the community to improve the quality of service for all transit users -- peak-hour commuters, off-peak hour users, seniors, students, youth and people with disabilities.

Implementation



- 5.2.4.1 Review the need for express bus service from Deep Cove to downtown Vancouver.
- 5.2.4.2 Examine ways to improve service to downtown Vancouver and other employment areas on the North Shore, including augmenting the frequency of service.
- 5.2.4.3 Encourage TransLink to improve service between the Seymour and Inter-River communities.
- 5.2.4.4 Encourage TransLink to improve ridership and enhance service to neighbourhoods with the introduction of community mini buses
- 5.2.4.5 Provide bus shelters, benches and garbage bins at stops without these facilities now and ensure these do not impede pedestrians with disabilities
- 5.2.4.6 Expand park and ride facilities at Phibbs Exchange (or other suitable location) and enhance the safety and security of such sites
- 5.2.4.7 Continue to make bus stop landing areas wheel chair accessible
- 5.2.4.8 Integrate convenient, well-lit and safe access to transit services
- 5.2.4.9 Examine the possibility of developing queue-jumper facilities along Mount Seymour Parkway for transit accessing Phibbs Exchange. In the longer term consider queue-jumpers along any north/south connector roads.

**Policy 5.2.5** Continue to improve the circulation system and facilities for cyclists and pedestrians and specifically consider safety and security in any implementation. (e.g. lighting, appropriate landscaping).

**Policy 5.2.6** Implement the North Vancouver Bicycle Master Plan network in Seymour; namely, Dollarton Highway, Riverside Drive, Deep Cove Road/Gallant, Mount Seymour Parkway and bypass routes.

**Policy 5.2.7** Work with the Joint Bicycle Advisory Committee, Parks Department, Provincial ministries, adjacent communities and others to improve safety and convenience for cyclists.

#### Implementation

- 5.2.7.1 Improve paving and road markings on arterial and collector roads
- 5.2.7.2 Provide minimum standard cycling lanes (4.3. meters wide shared curb lane) wherever feasible.
- 5.2.7.3 Improve the bicycle friendliness of the Highway 1/Dollarton Highway interchange at the Second Narrows bridgehead
- 5.2.7.4 Incorporate provisions for cyclists in trail development e.g. V-grooves and cycle bridges.
- 5.2.7.5 Formalize a shared vehicle/cycle lane from Maplewood to Deep Cove Village
- 5.2.7.6 Investigate additional routes and facilities such as bike racks or lockers at major transit stops.

**Policy 5.2.8** Engineering and Planning departments will continue to review the sidewalk network and work with the community to prioritize expansion and



improvements aimed at enhancing safety, access and ease of mobility for all users. Particular attention will be paid to sidewalks, parking and safety issues in school areas and reference made to the Pedestrian Access Guidelines.

**Policy 5.2.9** Ensure that the circulation system for new developments includes vehicle, pedestrian, transit and cycle linkages, while minimizing negative impacts on the environment and existing neighbourhoods.

Implementation

- 5.2.9.1 Review all proposed new development for impacts on the environment, safety, existing capacity and for enhancements to the circulation system for vehicles, pedestrians and cyclists.
- 5.2.9.2 Any future development of Northlands (the former Blair Rifle Range) will require detailed transportation impact studies and transportation plan.
- 5.2.9.3 In future developments and redevelopments, limit the number of driveway accesses on arterial and collector roads, such as Mount Seymour Parkway, to reduce potential safety hazards and points of conflict.
- 5.2.9.4 Traffic impact studies and consultation with area residents will be required for any new development or redevelopment proposal, to mitigate negative impacts of additional traffic generated by such development.

**Objective 5.3** *Minimize the impact on local neighbourhoods of traffic destined for District and Regional facilities located in Seymour.*

**Policy 5.3.1** Work with the community, local business, District departments and others to study the non-resident use of on- street parking near commercial, recreational or other activity areas.

**Policy 5.3.2** Work with the community, business operators, District departments and others to monitor and assess weekend non-resident use of parking to access recreation sites such as Indian Arm, Cates Park, Deep Cove, Seymour Mountain, Cove/Mountain Forest and the Lower Seymour Conservation Reserve area. Undertake necessary studies and develop solutions to minimize the impact on local neighbourhoods. E.g. weekend shuttle services between popular destinations.

**Objective 5.4** *Maintain and improve the quality of utility networks and services.*

**Policy 5.4.1** Maintain services (water, hydro, gas, telephone, sanitary, storm, garbage collection, storm water control, recycling collection) at existing levels of quality and improve services where necessary and feasible.

**Policy 5.4.2** Ensure that services to new neighbourhood developments are within the existing service capacities, or that extension of capacity is established in advance of actual development.



**Policy 5.4.3** Ensure sufficient provision of public waste receptacles to promote health and attractiveness throughout the community.

**Policy 5.4.4** Services to any new development should be as unobtrusive as possible.

Implementation

5.4.4.1 Electrical services to new neighbourhood development will be placed underground.

5.4.4.2 Electrical transformers, connection boxes, gas meters, communication services or other utilities should be located and screened to minimize visibility to the neighbourhood.

**Policy 5.4.5** Consider environmental impacts and the concepts of sustainable communities in the provision and management of services.

**Policy 5.4.6** Review new energy services in light of the “Sustainable Community Principles” set out by the BC Energy Council.

**Policy 5.4.7** Review proposed developments for their impact on the environmental quality of the District’s storm water system.

## **6.0 Housing & Schools**

Although Seymour is increasingly a recreation destination, it will always be a residential community of family-oriented neighbourhoods. Nevertheless, general changes and trends among Seymour’s neighbourhoods, its residents, their housing needs and lifestyles are evident. Seymour’s population is aging, household sizes have dropped and the number of 2-person families has risen. While single-family housing continues to predominate, the number of multi-family units has increased. Home ownership is high in Seymour, average family income is higher than in the District as a whole, and the incidence of low income is lower.

Within this context, housing policies and recommendations reflect the clear community sentiment that housing development is dependent upon improvements to east-west traffic systems. This is especially significant in any future development of the Northlands area. Also inherent in the housing policies is the recognition of the community’s commitment to protect environmentally sensitive areas and to establish a Seymour waterfront that is clean, natural and accessible. The Plan also requires the continual evaluation of community service capacity as development proceeds, to ensure the liveability of the Seymour community.

Overall, the Plan’s housing policies seek to minimize change while providing some alternative housing choices to promote community sustainability and support residents as their needs change. Seymour seniors, in particular, have identified the need for a greater



range of housing options, including the ability to downsize to manageable homes and to access supportive housing services and congregate care facilities. In terms of housing type, a range of single-family housing sizes should be encouraged. Other ground-oriented choices with yard area should also be considered. In general, multi family developments should be low scale (not exceeding 4 storeys), as the community does not support high-rise residential buildings.

**Goal**

***To maintain the predominantly single family character of Seymour while providing increased diversity in housing choices to meet residents' changing needs.***

**Objective 6.1**    ***To maintain and protect existing, stable residential neighbourhoods and housing stock.***

**Policy 6.1.1**    Retain existing zoning in established residential neighbourhoods unless otherwise determined by the neighbourhood itself; i.e. Riverside East, Blueridge, Seymour Heights, McCartney Woods, Windsor Park, Dollarton, Cove Cliff (*Chapter 6 Map*)

**Policy 6.1.2**    Any new development in established neighbourhoods, permitted within existing zoning and subdivision regulations, must be in keeping with the scale and character of the area.

**Policy 6.1.3**    Consider area specific neighbourhood zoning to ensure that any new single family housing is compatible with the character of the neighbourhood in terms of size, shape, height, setbacks, garage locations and other siting factors.

**Policy 6.1.4**    Retain existing multifamily rental housing stock by discouraging changes in tenure that reduce rental housing and by considering redevelopment of multifamily rental units only if a comparable number of rental units will be provided.

**Policy 6.1.5**    Any redevelopment of ageing multifamily units should provide the same number of units as those being replaced. Increases in numbers of units may be considered where a benefit to the community is identified and can be realized.

**Policy 6.1.6**    Provide opportunity for neighbours to be involved in redevelopment in early stages.

**Implementation**

**6.1.6.1**    Standard operating procedures, which incorporate consultation with local community associations in the pre-application stage for OCP amendments,



rezoning, development permits and some subdivisions are considered important and are supported.<sup>1</sup>

**Objective 6.2** *To meet housing needs of residents as families and life styles change, in ways that make neighbourhoods sustainable.*

**Policy 6.2.1** New residential development is to be phased and designated to provide single family and multifamily sites in accordance with the Plan. (*Chapter 6 Map & Table 6-1*)

**Policy 6.2.2** New single-family development will consist primarily of standard single-family lots, but could also include smaller single-family lots, duplexes, and other ground-oriented choices with yard areas, where appropriate.

**Policy 6.2.3** Future development of remaining Anne Macdonald Way single-family lots will be in keeping with established Design Guidelines.

**Policy 6.2.4** Any future development of Northlands should incorporate a range of single-family lot sizes, including traditional Seymour lot sizes (RS3, RS4 and RS5 equivalents), as well as a variety of small lots, detached homes and creative design layouts. (See *Table 6-1*)

**Policy 6.2.5** Future development of Roche Point Drive East single-family lots should be in keeping with the scale and design of the existing homes in the neighbourhood. (See *Table 6-1*)

**Policy 6.2.6** Future development of Riverside Terrace should be in keeping with the scale and character of the existing, surrounding single-family neighbourhood. (See *Table 6-1*)

**Policy 6.2.7** New multi-family developments will not exceed four storeys in height.

**Policy 6.2.8** New multi-family developments should be located in proximity to transportation, services, community facilities and open space.

**Policy 6.2.9** Multi family development within the context of a comprehensive plan for Northlands should be located in proximity to existing and planned transit, community facilities and services. Any development will be in accordance with the conditions and principles outlined in *Table 6-1* and must contribute to community objectives.

**Policy 6.2.10** Future redevelopment of single family to multifamily housing along the north side of Mount Seymour Parkway (3200 to 3500 blocks) will be permitted to a density of between 0.6 and 1.2 Floor Space Ratio (FSR) provided that:

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<sup>1</sup> *District of North Vancouver, Community Planning Process Improvement Project Final Report, June 2001.*



- The housing form is predominantly townhouses
- A minimum of 15,000 square feet (1400 m<sup>2</sup>) is assembled for townhouse development and no “locked in” lots are created
- The development addresses issues of traffic, access, safety, acoustical standards, view and backdrop considerations, community acceptance and design standards, in accordance with *Table 6-1*.
- The development contributes to community objectives

**Policy 6.2.11** Future redevelopment of single family to multifamily in the 3600 block of the south side of Mount Seymour Parkway should address issues of access, noise, safety, compatibility with neighbouring developments, and be of a scale and design acceptable to the immediate and general community. (*Table 6-1*)

**Policy 6.2.12** The District Official Community Plan Development Permit Areas for Commercial, Industrial, Multifamily Development will be amended to include the 3200 to 3500 north side blocks and 3600 south side blocks of Mount Seymour Parkway, so as to regulate redevelopment on these sites in accordance with Schedule ‘B,’ Section 4.0 of the District Official Community Plan.

**Policy 6.2.13** Support and facilitate the choice of families and seniors to stay in their current homes and neighbourhood as long as possible.

#### Implementation

- 6.2.13.1 Identify suitable sites for supportive housing and other facilities for seniors in the community, considering issues such as: proximity to transit, commercial, recreation, health and other services, a safe, level pedestrian setting and avoiding isolation.
- 6.2.13.2 Encourage adaptable housing approaches in new developments, including the use of Adaptable Design Guidelines.
- 6.2.13.3 Encourage other levels of government and appropriate agencies and authorities to provide support and funding for services and for adaptable design homes and facilities.
- 6.2.13.4 Explore means to protect apartment renters from displacement and/or drastic rent increases in the event of redevelopment.

**Policy 6.2.14** The approval process for new development projects will ensure that Plan and District objectives relating to housing type, tenure, environmental protection, transportation improvements, services, infrastructure and community amenities are met.

**Policy 6.2.15** Secondary suites are recognized as a means of increasing options for the homeowner and providing affordable accommodation for renters in the community.



**Policy 6.2.16** Through design of housing, neighbourhoods and amenities, promote healthy neighbourhoods in which residents live and interact.

**Objective 6.3 *Safeguard lands, natural and built assets for future generations.***

**Policy 6.3.1** Designate District land, suitable sites and other resources for long term housing needs of an aging population and future generations.

**Policy 6.3.2** Given their significant environmental value, Roche Point West and the Mystery Crescent area in the Indian River Neighbourhood (*Chapter 6 Map*) will be designated as Parks, Recreation & Wilderness. Further comprehensive study will precede any future change in designation.

**Policy 6.3.3** District –owned properties on Summerside Lane and Badger Place will not be developed for residential purposes over the life of this plan. Further comprehensive study will precede any future development proposals.

**Policy 6.3.4** Medium and high-density residential development is not to be permitted along Seymour’s waterfront.

**Policy 6.3.5** Any future re-development of the McKenzie Barge/Noble Towing site west of Cates Park should be treated as a comprehensive development focusing on the arts, culture, economic and tourism opportunities for the site. Such future comprehensive development could include 25 or fewer residential units, to be incorporated into the conceptual phasing strategy detailed in Section 4.0 <sup>2</sup>

**Policy 6.3.6** Consult with Tsleil-Waututh neighbours on issues of shared interest concerning development, planning and the impacts of growth and change.

Implementation

6.3.6.1 Explore opportunities for joint acquisition and management of sites of shared strategic or cultural significance to promote common objectives. E.g. McKenzie Barge/Noble Towing properties.

**Policy 6.3.7** Ensure new construction is of high quality and design appropriate to Seymour’s built and natural environment. (*See Table 6-1*)

**Policy 6.3.8** The Community Planning Department, in consultation with other departments, agencies and community members will monitor demographic and social trends along with the impacts of development, to ensure community needs are planned for now and in the future.

**Objective 6.4 *Secure, comfortable and invigorating schools in which Seymour’s students can thrive.***

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<sup>2</sup> See preferred re-development details in Section 9.0, Policy 9.2.5



- Policy 6.4.1** Liase with School District #44 regarding the Capital Planning Program and to communicate priorities and co-ordinate school and neighbourhood planning. (*See also 8.2.1*)
- Policy 6.4.2** Continue to develop joint use agreements between the District and School District #44 to increase community use of schools and fields. (*See also Sections 7.0 and 8.0*)



**TABLE 6-1: PHASING VARIABLES & CONDITIONS OF DEVELOPMENT****Notes:**

- Adherence to the Plan growth rate of 50 units per year, on average over any five-year time frame, is a key variable in all cases and a change in rate of growth in one phase will affect the rate in subsequent phases. (See 4.4.3)
- 5 year phases over the life of the Plan (to 2022) are: Phase I – 2003-2007; Phase II – 2008-2012; Phase III – 2013 – 2017; Phase IV 2018-2022; Phase V – beyond 2022
- To determine the amount of development possible on the Seymour Plan area sites listed below, development in Maplewood & on Tsleil-Waututh lands will be considered in the 250 units per phase total

LOCATION	UNITS (Est.)	KEY VARIABLES	CONDITIONS	TYPE (Recommended Form & Character)
Anne Macdonald Way	29 (+ up to 8)	<ul style="list-style-type: none"> <li>• Market</li> <li>• Phasing</li> <li>• Existing CD14 zone</li> </ul>	<ul style="list-style-type: none"> <li>• Process &amp; design guidelines established</li> </ul>	<ul style="list-style-type: none"> <li>• Single Family</li> </ul>
Northlands	300	<ul style="list-style-type: none"> <li>• Growth rate parameter</li> <li>• Multiphase</li> <li>• Environmental constraints (contamination, remediation, risk assessment, protection of creeks)</li> <li>• Impact on traffic</li> <li>• Readiness of landowner</li> <li>• Economic viability</li> </ul>	<ul style="list-style-type: none"> <li>• Community support</li> <li>• Early, open, frequent consultation to establish overall plan</li> <li>• Growth rate</li> <li>• Comprehensive phasing plan required at outset</li> <li>• Start small, start south end</li> <li>• Transportation study and plan to address east-west traffic impacts &amp; other linkages</li> <li>• Comprehensive environmental assessment &amp; plan</li> <li>• Assessment of impact on schools &amp; other community services</li> <li>• Buffer from Parkway</li> <li>• Integrate adjacent communities through multi-use trails, linkages</li> </ul>	<ul style="list-style-type: none"> <li>• Predominantly single family; minimal multi-family (e.g.80%/20%)</li> <li>• Single family in range of sizes &amp; creative formats</li> <li>• Multifamily, which could include seniors or supportive housing, in proximity to existing &amp; planned transit, facilities &amp; services,</li> <li>• Design guidelines reflecting "Seymour theme"</li> <li>• Include community amenities such as: playing fields, track, live theatre, community space, staging areas for bike and mountain activities</li> </ul>



**TABLE 6-1: PHASING VARIABLES & CONDITIONS OF DEVELOPMENT****Notes:**

- Adherence to the Plan growth rate of 50 units per year, on average over any five-year time frame, is a key variable in all cases and a change in rate of growth in one phase will affect the rate in subsequent phases. (See 4.4.3)
- 5 year phases over the life of the Plan (to 2022) are: Phase I – 2003-2007; Phase II – 2008-2012; Phase III – 2013 – 2017; Phase IV 2018-2022; Phase V – beyond 2022
- To determine the amount of development possible on the Seymour Plan area sites listed below, development in Maplewood & on Tsleil-Waututh lands will be considered in the 250 units per phase total

LOCATION	UNITS (Est.)	KEY VARIABLES	CONDITIONS	TYPE (Recommended Form & Character)
Mount Seymour Parkway <ul style="list-style-type: none"> <li>• South side</li> <li>▪ 3600 block</li> </ul>	25	<ul style="list-style-type: none"> <li>• Adherence to growth parameters</li> <li>• Financial feasibility</li> <li>• DNV/Private partnership</li> <li>▪ Community support</li> </ul>	<ul style="list-style-type: none"> <li>• Community support; design acceptable to immediate and broader community, i.e. resolve neighbours preference for higher building with overall desire for no high rises and low density</li> <li>• Private/DNV lots partnership to be achieved</li> <li>• Buffer to Parkway&amp; to existing townhouse neighbours</li> <li>• Mitigate noise</li> <li>• No access off Parkway</li> <li>▪ Traffic study &amp; plan</li> </ul>	<ul style="list-style-type: none"> <li>• Multi-family</li> <li>• Small building – 3 storeys</li> <li>• Small units benefiting from proximity to transit &amp; services</li> <li>• Could include seniors, based on community need</li> <li>• Compatible with surroundings</li> <li>• Design acceptable to community &amp; reflecting “Seymour theme”</li> <li>▪ Underground parking</li> </ul>
Mount Seymour Parkway <ul style="list-style-type: none"> <li>• North side</li> <li>▪ Potential blocks 3200 to 3500</li> </ul>	<ul style="list-style-type: none"> <li>• 35-105</li> </ul> Depends on form	<ul style="list-style-type: none"> <li>• Adherence to growth parameters</li> <li>• Traffic impacts</li> <li>• Environmental constraints (creek crossing)</li> <li>• Community support</li> <li>• DNV/Private partnership</li> <li>▪ Financial feasibility</li> </ul>	<ul style="list-style-type: none"> <li>• Community support</li> <li>• Phasing &amp; design to respect growth parameters</li> <li>• Traffic study &amp; plan</li> <li>• No access off parkway; shared access; lane preferred</li> <li>• Buffer from Parkway, mitigate noise, acoustical design</li> <li>• DNV/Private partnership</li> <li>▪ Development permit area</li> </ul>	<ul style="list-style-type: none"> <li>• Multi-family; could include seniors, based on need</li> <li>• Specific guidelines to be developed, but will generally reflect the following:</li> <li>• Predominantly townhouses, but with variety to avoid “wall” effect</li> <li>• Take advantage of golf course backdrop &amp; views</li> <li>• Density at 0.6 FSR with possibility to achieve maximum 1.2 FSR depending on:</li> <li>• Community benefit provided;</li> <li>• design acceptable to community &amp; with “Seymour theme”</li> <li>• Minimum 15,000 Sq ft. (1400m<sup>2</sup>) &amp; no “locked in” lots</li> </ul>



**TABLE 6-1: PHASING VARIABLES & CONDITIONS OF DEVELOPMENT****Notes:**

- Adherence to the Plan growth rate of 50 units per year, on average over any five-year time frame, is a key variable in all cases and a change in rate of growth in one phase will affect the rate in subsequent phases. (See 4.4.3)
- 5 year phases over the life of the Plan (to 2022) are: Phase I – 2003-2007; Phase II – 2008-2012; Phase III – 2013 – 2017; Phase IV 2018-2022; Phase V – beyond 2022
- To determine the amount of development possible on the Seymour Plan area sites listed below, development in Maplewood & on Tsleil-Waututh lands will be considered in the 250 units per phase total

LOCATION	UNITS (Est.)	KEY VARIABLES	CONDITIONS	TYPE (Recommended Form & Character)
Roche Point East	4	<ul style="list-style-type: none"> <li>• Environmental impact &amp; preservation</li> <li>• Consider holding for future study</li> </ul>	<ul style="list-style-type: none"> <li>• Community support</li> <li>• No connection of Roche Point Drive</li> <li>• Detailed environmental assessment, including top-of-bank</li> <li>• Retain green ways at end of cul-de-sac</li> </ul>	<ul style="list-style-type: none"> <li>• Single family in keeping with character of existing neighbourhood</li> </ul>
Riverside Terrace	<ul style="list-style-type: none"> <li>• Estimated up to 36</li> <li>• In keeping with surrounding single family</li> </ul>	<ul style="list-style-type: none"> <li>• Growth parameters</li> <li>• Environmental constraints especially related to Hogan's Pools</li> <li>• Geotechnical issues relating to steep banks</li> <li>• Traffic access &amp; impact, particularly related to Jordan St. accessing Riverside Drive</li> <li>• Consider holding for future study</li> </ul>	<ul style="list-style-type: none"> <li>• Community support</li> <li>• Traffic study &amp; plan</li> <li>• Retention of green space &amp; public access</li> <li>• Detailed environmental , Geotechnical and other studies leading to acceptable plan</li> </ul>	<ul style="list-style-type: none"> <li>• Single family in keeping with character of existing, surrounding neighbourhood</li> </ul>



## **7.0 Parks & Open Space**

Seymour's natural features are complemented by numerous parks, trails, open spaces and other recreational assets. While protecting the natural environment is a key goal of the community, so too is enhancing opportunities for recreation and healthy living within Seymour's neighbourhoods and along its unique waterfront. Although environmental and parks strategies are presented separately, there is a strong inter-relationship between environmental protection and parks and open space provision and management. Complementing the policies in Section 3.0, this section further recognizes this inter-relationship here. In general, it emphasizes the need for environmental study to precede detailed design of new park spaces and before changes or additions to existing facilities. Within this context, it specifically addresses the community desire to improve and augment parks, trails and other linkages between Seymour's neighbourhoods -- including the potential future neighbourhood at Northlands -- for pedestrians, cyclists and wildlife. The policies also address the challenge of providing recreational access to Seymour's trails, parks and waterfront, while minimizing negative effects on the environment and local neighbourhoods.

Effective management of Seymour's natural and recreational assets requires partnership and co-operation across boundaries and among partners and neighbours. The Plan highlights the importance of initiating the Cates Park Management Plan in consultation with the Tsleil-Waututh Nation and working co-operatively with School District #44, the Greater Vancouver Regional District, Provincial and Federal governments and other agencies. Finally, like Section 3.0, this section adopts many of the recommendations of the Waterfront Task Force.

### **Goal**

***While protecting the environment, create, enhance and manage parks, trails and open spaces for the enjoyment of Seymour's residents and visitors.***

**Objective 7.1** ***Identify and protect areas having biophysical, environmental, recreational or historic value.***

**Policy 7.1.1** Preserve and protect significant undeveloped natural forest, wilderness, wetland and waterfront areas.

**Policy 7.1.2** Promote environmental protection through the responsible management of recreational access to Seymour's trails, wilderness and waterfront.

**Policy 7.1.3** In consultation with community groups, other District departments, regional, provincial, First Nations and other partners, produce an inventory of environmentally sensitive sites requiring protection and managed access.



**Policy 7.1.4** Acknowledge the environmental importance and sensitivity of Hogan's Pools as a natural area and limit access through designated trails as described in the Hogan's Pools Park Environmental Inventory and Park Concept Plan<sup>1</sup>.

**Policy 7.1.5** Ensure detailed planning and environmental studies are conducted prior to new park or trail development.

**Objective 7.2** *Create linkages between neighbourhoods, parks, community/recreation centres, schools and shopping areas through interconnected parks, waterways and green space.*

**Policy 7.2.1** While respecting environmental considerations and regulations, provide connections necessary to complete existing trail systems.

Implementation

- 7.2.1.1 Identify existing pedestrian and bike linkages in the community, including connections to streets and through street ends, and produce a map illustrating these.
- 7.2.1.2 Continue to work with the GVRD, BC Parks and other agencies to implement and expand the North Shore Sector Recreational Greenway Plan in support of the regional Green Zone Strategy and Liveable Region Strategic Plan.
- 7.2.1.3 Upgrade the existing Baden-Powell Trail connecting Deep Cove to Grouse Mountain and continue to implement the guidelines of the Baden Powell Trail Study. DNV Parks to consult with trail users to identify priority sections for improvement on a regular basis. (e.g. the portion between Hyannis and the 'Mushroom Bus Stop' and moving the section currently along the Woodlands road into the adjacent forest.)
- 7.2.1.4 Subject to environmental review and detailed design, undertake the following:
  - a. Complete the multi-purpose Northlands Golf Course perimeter trail system
  - b. Establish a new trail south of the McCartney Creek Park sports fields along McCartney Creek to Mount Seymour Parkway
  - c. Provide a trail connection from Anne Macdonald Way south to connect into the existing trail along Taylor Creek at Parkgate Community Park
  - d. Provide a multi-purpose trail from Parkgate Community Park south along Taylor Creek to Mount Seymour Parkway
  - e. Provide an east-west connection through the eastern part of Parkgate Park to connect with Mount Seymour Road
  - f. Formalize an interpretive trail through the Roche Point forest to recognize the conservation and wildlife value of the area
  - g. Formalize the waterfront trail within Deep Cove/Panorama Park to connect Deep Cove Boathouse to Parkside Lane and continue through to Lockhaven Road and Wickenden Park.
  - h. Identify, and formalize through signage, a continuous route connecting existing trails, sidewalks, roadways and other linkages, from Cates Park to Panorama Park

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<sup>1</sup> Coast River Environmental Services Ltd. *Hogan's Pools Park Environmental Inventory & Park Concept Plan*.



- i. Establish a north-south multi-purpose trail from Roche Point Drive south through the Seymour Golf Course to Dollarton Highway and Cates Park
- 7.2.1.5 Work with GVRD, BC Parks, the Federal Government, and other agencies and users to maintain major, continuous natural trails across boundaries in Seymour's wilderness area
- 7.2.1.6 Upgrade the Bridle Path trail east of Hyannis trail.
- 7.2.1.7 Upgrade the steep sections and investigate lighting of the trail connecting Rivergrove Place to Berkley.
- 7.2.1.8 Retain District road allowances and lanes to enhance linkages within neighbourhoods and with other elements of pedestrian and cycle trail systems.

**Policy 7.2.2** In any new development, or redevelopment, ensure the provision of public pedestrian/cycle linkages.

**Policy 7.2.3** Ensure that trail connections between the McCartney Creek Trail System and the Northlands Golf Course Trail System are provided through any future planning of the CMHC-owned Northlands property.

**Policy 7.2.4** In any future redevelopment along the north side of Mount Seymour Parkway, provide pedestrian linkages to existing footpaths, trails and surrounding community services.

**Objective 7.3** *Maintain and enhance existing park facilities and amenities and plan for new ones to meet current and future park and open space needs.*

**Policy 7.3.1** Maintain and upgrade Seymour's existing parks, trails and open spaces to continue to meet users' needs.

Implementation

- 7.3.1.1 Maintain all existing park facilities to ensure public health, safety and enjoyment.
- 7.3.1.2 Establish and maintain information regarding each park's major role and future direction for parks improvement. (*Table 7-1*)
- 7.3.1.3 Unopened road and lane allowances are not to be consolidated with adjacent lots for subdivision or other purposes, nor contain structural encroachments. Applications for permits to occupy these will be considered within the context of District policy regarding occupation and encroachment upon municipal lands and unopened highways. In all cases, surrounding neighbours will be consulted and the broader community interest considered.
- 7.3.1.4 Identify and review local areas, natural space, creeks and street end areas currently being used for park purposes and consider providing a legal basis for their use as park through rezoning to PRO. E.g. McCartney Creek north of Larkhall (currently RS1), McCartney Creek north of Dollarton (RS4) and other opened and unopened pathway allowances.
- 7.3.1.5 Maintain existing park facilities in Myrtle Park appropriate to its role as a primary community-level park.



- 7.3.1.6 Upgrade the existing Strathcona public wharf facility for better recreational access to the water.
- 7.3.1.7 Identify opportunities to use public art to enhance District parks in Seymour.
- 7.3.1.8 Expand the playground at Indian River Park.

**Policy 7.3.2** Identify and prioritize opportunities for future park, trail or open space development and enhancement, while respecting environmental concerns and regulations.

Implementation

- 7.3.2.1 Initiate a park planning study, including a biophysical inventory, for the Mountain Forest/Cove Forest area and examine the feasibility of specific activities, which could include trail development, mountain biking or staging areas.
- 7.3.2.2 Examine, with the biking, equestrian and hiking community, the development of a system of trail routes to meet recreational needs while ensuring environmental and safety issues are addressed, including a review of unauthorized trail construction.
- 7.3.2.3 In any future planning for the Northlands area, explore possibilities to provide community level park facilities, which may include passive uses, but which would emphasize active park facilities such as sports fields, skateboard area, playgrounds, a running track, mountain bike, staging or BMX areas.
- 7.3.2.4 Conduct an environmental assessment of the forested area east of Taylor Creek in Parkgate Community Park prior to any other review for future park development.
- 7.3.2.5 Construct a permanent field house with public wash and change rooms on the west side of Taylor Creek in Parkgate Community Park, subject to environmental study and detailed design.
- 7.3.2.6 Continue to develop Deep Cove/Panorama Park as a major water-based activity centre. Examine the feasibility of providing more facilities for scuba diving, kayaks, canoes, rowboats, sailing dinghies, pedal boats and a designated water area for teaching.
- 7.3.2.7 Promote the compatibility of various activities in Deep Cove, in part by minimizing the use and speed of motorized water vessels in Deep Cove.
- 7.3.2.8 Retain Wickenden Park as a natural park, while upgrading existing trails.
- 7.3.2.9 Provide a major waterfront viewing point at Eastridge with a trail connection to Wickenden Park.
- 7.3.2.10 Investigate the demand for and possible future location of a new public marina outside of the Deep Cove area. E.g. McKenzie Barge site.
- 7.3.2.11 Investigate land suitable for a playground in the Riverside Drive area.

**Policy 7.3.3** In consultation with the Tsleil-Waututh Nation, initiate the Cates Park Management Plan to establish long term goals, objectives, capital and operation strategies appropriate to its status as a District park and to reflect its cultural, environmental and historic significance.



**Policy 7.3.4** Within the context of a Cates Park Management Plan and in reference to the Cates Park/Whey-ah-Wichen Protocol/Cultural Agreement and archaeological assessments, consult with the Tsleil-Waututh Nation, local historical groups, advisory groups and residents to continue to develop strategies for the preservation and interpretation of the cultural heritage of Cates Park.

**Policy 7.3.5** Within the context of a Cates Park Management Plan and in reference to the Cates Park/Whey-ah-Wichen Protocol/Cultural Agreement and archaeological assessments, consider the following as priorities:

- Upgrade of the existing boat launch
- Review of current and future water-based recreational activities and related support facilities. E.g. scuba diving, kayaks, canoes, rowboats, pedal boats
- Upgrading of existing concession, washroom and changeroom facilities
- Signage and upgraded interpretative objects recognizing First Nations and settlers history
- Examination of the extent and impacts of fishing, illegal fishing and crabbing
- Establish a foreshore erosion protection plan

**Policy 7.3.6** Extend and upgrade the natural waterfront trail from the northeast corner of Cates Park ("Little Cates") to the western border of Cates Park to include the recent addition to the park.

**Policy 7.3.7** In all decision making regarding Cates Park/Whey-ah-Wichen, recognize and protect the primary value and role of the park as an environmental, recreational, cultural and historic resource.

**Policy 7.3.8** Within the context of a Cates Park Management Plan and in reference to the Cates Park/Whey-ah-Wichen Protocol/Cultural Agreement and archaeological assessments, large scale commercial activities will not be considered suitable within the park.

**Objective 7.4** *Improve access and enjoyment for trail and park users while minimizing impacts on local areas.*

**Policy 7.4.1** Make trails, parks and open spaces accessible for a range of users.  
Implementation

- 7.4.1.1 Examine opportunities to increase access for seniors, individuals with physical limitations, and others. Consider providing park facilities for children with physical disabilities and pathways for use by people in wheelchairs, preferably in central locations.
- 7.4.1.2 For all existing and new trails, instigate a comprehensive trail signage system and trail mapping system for public information, education, safety and enjoyment. Consider signage appropriate for users with impaired vision.



- 7.4.1.3 Identify opportunities for additional off-leash areas for dogs and owners, including access to waterfront dog swimming areas, while continuing to enforce existing regulations.

**Policy 7.4.2** Work with the community, business operators, District departments and others to monitor and assess non-resident use of parking to access recreations sites such as Indian Arm, Cates Park, Deep Cove, Seymour Mountain, Fromme Mountain, Cove/Mountain Forest and the Lower Seymour Conservation Reserve area. Undertake necessary studies and develop solutions to minimize the impact on local neighbourhoods.

**Policy 7.4.3** Provide sufficient and appropriate amenities to meet park and trail users' needs and protect the surrounding neighbourhoods.

Implementation

- 7.4.3.1 Maintain & enhance parks, trails and trailheads through general maintenance, provision of refuse containers and drinking water. Consider providing bags and receptacles for dog waste.
- 7.4.3.2 Subject to environmental studies, consultation and detailed design identify and establish potential staging areas for use of the wilderness area to include parking and amenities such as washrooms, change rooms and fresh water. E.g. in Mountain Forest, McCartney Creek Park, Northlands.
- 7.4.3.3 Examine the impacts of and possible solutions (e.g. parking, staging, and services) to increasing use of Indian Arm by commuter, tourist and recreational users.
- 7.4.3.4 Examine the impacts of visitors to Deep Cove on existing waterfront parks, trails, parking, residential and business areas.
- 7.4.3.5 With the community, District departments, private operators and others, investigate solutions for parking and amenities such as washrooms and fresh water near popular recreational areas.

**Objective 7.5** *Work with the community, other agencies and private landowners to achieve parks, wilderness and environmental goals.*

**Policy 7.5.1** Promote a positive reputation for parks and environmental preservation within the Seymour community and beyond through inter-departmental co-operation, education and public awareness programs.

Implementation

- 7.5.1.1 Develop interpretive signage at Cates Park, Strathcona Park, Panorama Park and other key locations to educate the public about marine life in local waters and to encourage environmental stewardship.
- 7.5.1.2 Develop interpretive signage that relates solid waste and non-point pollution to the natural systems in the community by outlining impacts and solutions.
- 7.5.1.3 Work with volunteers, community groups, District departments and others to provide public education and signage about co-existing with wildlife. Examples of topics include feeding wildlife, garbage disposal and impacts of dogs on watercourses and riparian areas.



**Policy 7.5.2** Support community and volunteer based organizations contributing to park, trail and wilderness maintenance and enhancement through funding or administrative support.

Implementation

7.5.2.1 Promote and support parks volunteer projects such as nest boxes, garbage pick-up, clean up and planting.

**Policy 7.5.3** Pursue partnerships or other agreements with the federal government, provincial government, Tsleil-Waututh Nation, the Greater Vancouver Regional District (& Water District) to realize parks objectives.

**Policy 7.5.4** Formalize partnership agreements with Tsleil-Waututh Nation, BC Parks, GVWD Seymour Watershed and GVRD Parks for the cross boundary use and development of walking, hiking and biking trails.

**Policy 7.5.5** Film industry activities in Seymour's parks should benefit Seymour community parks.

**Policy 7.5.6** Work in co-operation with School District #44 to promote the joint use of school facilities and shared stewardship of local parks and trails.

Implementation

7.5.6.1 Through joint use agreements and other means, work co-operatively with School District # 44 and local residents to upgrade school fields and playgrounds and explore means to extend hours of facility and field use.

7.5.6.2 Encourage appropriate use and stewardship of trails and parks by school user groups conducting extra-curricular and sports events.

**Objective 7.6    *Increase public access to the waterfront.***

**Policy 7.6.1** Provide public access to the waterfront at strategic locations.

Implementation

7.6.1.1 Identify the least environmentally sensitive shoreline areas suitable for public access.

7.6.1.2 Wherever feasible, incorporate wheelchair accessibility, appropriate signage and tactile surfaces to aid navigation to the waterfront and along public piers.

7.6.1.3 Open up street ends for public access to the waterfront.

7.6.1.4 Develop a policy for the use and management of the Deep Cove government wharf

7.6.1.5 Develop a Deep Cove theme and beach signage system to increase public awareness of and access to the waterfront.

7.6.1.6 Work with local groups to provide heritage signs along the waterfront, in waterfront parks and trails to illustrate the history of Deep Cove

7.6.1.7 Any future redevelopment of the McKenzie Barge/Noble Towing site will provide for waterfront access, views between Dollarton Highway and the



waterfront and linkages to Cates Park and other neighbourhood destinations.  
(See also 9.2.5)

**Policy 7.6.2** Rehabilitation and preservation of natural systems will be emphasized in all public access initiatives, including the removal of existing encroachments.

Implementation

- 7.6.2.1 Work with the Vancouver Port Authority to remove encroachments onto the beach.
- 7.6.2.2 Remove encroachments along street ends.



**TABLE 7-1: SEYMOUR PARKS****Notes**

- *Reference should be made to the complete policy statements noted under “Future Directions/Plan Policies”*

<b>PARK</b>	<b>LOCATION</b>	<b>AREA (HA)</b>	<b>DESCRIPTION</b>	<b>FACILITIES</b>	<b>FUTURE DIRECTIONS/ PLAN POLICIES</b>
Northlands Golf Course	Off Anne Macdonald Way	58.55	<ul style="list-style-type: none"> <li>• District Park</li> <li>• 18 hole public golf course</li> <li>• 95% natural parkland</li> <li>• 5% urban parkland</li> </ul>	<ul style="list-style-type: none"> <li>• Clubhouse</li> <li>• Turf care centre</li> <li>• Public trail</li> <li>• Parking</li> </ul>	<ul style="list-style-type: none"> <li>• Complete the perimeter trail system (7.2.1.4 a.)</li> <li>• Ensure that trail connections between the McCartney Creek trail system and Northlands Golf course system are provided through any future planning of the CMHC-owned Northlands site (7.2.3)</li> </ul>
McCartney Creek Park	Access south of Larkhall Crescent, west of Northlands Drive	18.34	<ul style="list-style-type: none"> <li>• Community Park</li> <li>• 60% natural parkland</li> <li>• 40% urban parkland</li> </ul>	<ul style="list-style-type: none"> <li>• 1 soccer field</li> <li>• 4 baseball diamonds</li> <li>• McCartney Creek forest trail</li> <li>• Parking</li> <li>• Field house</li> <li>• Washrooms</li> <li>• Playground</li> <li>• 2 tennis courts</li> </ul>	<ul style="list-style-type: none"> <li>• Establish new trail south of the sports fields, along creek to Mount Seymour Parkway (7.2.1.4 b.)</li> <li>• Ensure that trail connections between the McCartney Creek trail system and Northlands Golf course system are provided through any future planning of the CMHC-owned Northlands site (7.2.3)</li> </ul>
Parkgate Community Park	North end of Parkgate Avenue	13.87 (5.7 west of Taylor Creek; 7.17 east of Taylor Creek)	<ul style="list-style-type: none"> <li>• Community Park</li> <li>• West of Taylor Creek: 10% natural parkland; 90% urban parkland</li> <li>• East of Taylor Creek currently 100% natural parkland</li> </ul>	<ul style="list-style-type: none"> <li>• 1 soccer field</li> <li>• 2 baseball diamonds</li> <li>• Taylor Creek forest trail</li> <li>• Parking</li> <li>• Washrooms</li> </ul>	<ul style="list-style-type: none"> <li>• Provide trail connection from Anne Macdonald Way south to connect into existing trail along Taylor Creek at Parkgate Community Park (7.2.1.4 c.)</li> <li>• Provide multi-purpose trail from Parkgate Community Park south along creek to Mount Seymour parkway (7.2.1.4 d.)</li> <li>• Provide east-west connection through eastern part of park to connect to Mount Seymour Road (7.2.1.4 e.)</li> <li>• Conduct an environmental assessment of the forested area east of Taylor Creek prior to any future park development (7.3.2.4)</li> <li>• Construct a permanent field house with public wash and change rooms on the west side of Taylor Creek (7.3.2.5)</li> </ul>
Hogan's Pools	East of Riverside Drive; north of Mount Seymour Parkway	9.85	<ul style="list-style-type: none"> <li>• Neighbourhood Park</li> <li>• 100% natural parkland</li> </ul>	<ul style="list-style-type: none"> <li>• Marsh area with informal forest trail</li> </ul>	<ul style="list-style-type: none"> <li>• Acknowledge the environmental importance; limit access as per Hogan's Pools Park Environmental Inventory &amp; Concept Plan (7.1.4)</li> </ul>



**TABLE 7-1: SEYMOUR PARKS****Notes**

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<b>PARK</b>	<b>LOCATION</b>	<b>AREA (HA)</b>	<b>DESCRIPTION</b>	<b>FACILITIES</b>	<b>FUTURE DIRECTIONS/ PLAN POLICIES</b>
Blueridge Park	2400 Block Berkley Avenue	1.70	<ul style="list-style-type: none"> <li>• Neighbourhood Park</li> <li>• 90% urban parkland</li> <li>• 10% natural parkland</li> </ul>	<ul style="list-style-type: none"> <li>• 1 baseball diamond</li> <li>• 1 junior soccer field</li> <li>• 1 playground</li> <li>• Open grassy area</li> </ul>	<ul style="list-style-type: none"> <li>• No change anticipated</li> </ul>
Byron Park	East of Blueridge Elementary School	0.49	<ul style="list-style-type: none"> <li>• Neighbourhood Park</li> <li>• 100% urban parkland</li> </ul>	<ul style="list-style-type: none"> <li>• 2 tennis courts</li> <li>• 1 playground</li> </ul>	<ul style="list-style-type: none"> <li>• No change anticipated</li> </ul>
Garibaldi Park	Off 3600 Block Garibaldi Drive	2.52	<ul style="list-style-type: none"> <li>• Neighbourhood Park</li> <li>• 85% urban parkland</li> <li>• 15% natural parkland</li> </ul>	<ul style="list-style-type: none"> <li>• 2 tennis courts</li> <li>• 2 playgrounds</li> <li>• 1 multi-purpose sports court</li> <li>• Parking</li> <li>• Open grassy areas</li> <li>• Trails in natural areas</li> </ul>	<ul style="list-style-type: none"> <li>• No change anticipated</li> </ul>
Trillium Park	Access off Trillium Place; east of Seymour Heights School	1.4	<ul style="list-style-type: none"> <li>• Neighbourhood Park</li> <li>• 90% natural parkland</li> <li>• 10% urban parkland</li> </ul>	<ul style="list-style-type: none"> <li>• 1 playground</li> <li>• Trails in forested greenbelt</li> </ul>	<ul style="list-style-type: none"> <li>• No change anticipated</li> </ul>
Northlands Neighbourhood Park	West of Northlands golf course; east of CMHC lands	1.60	<ul style="list-style-type: none"> <li>• Neighbourhood Park</li> <li>• 100% natural parkland</li> </ul>	<ul style="list-style-type: none"> <li>• Forested trail</li> <li>• Range Creek</li> </ul>	<ul style="list-style-type: none"> <li>• Upgrade perimeter trail sections in park</li> </ul>
Windsor Park	Access at 900 Block of Tollcross Road, off Plymouth Drive	1.47	<ul style="list-style-type: none"> <li>• Neighbourhood Park</li> <li>• 90% natural parkland</li> <li>• 10% urban parkland</li> </ul>	<ul style="list-style-type: none"> <li>• 1 playground</li> <li>• Trails</li> </ul>	<ul style="list-style-type: none"> <li>• No change anticipated</li> </ul>
Strathaven Park	Access at 3100 Block Huntleigh & 800 Block Strathaven Drive	3.51	<ul style="list-style-type: none"> <li>• Neighbourhood Park</li> <li>• 70% urban parkland</li> <li>• 30% natural parkland</li> </ul>	<ul style="list-style-type: none"> <li>• 1 playground</li> <li>• Open grassy area</li> <li>• Forested area</li> </ul>	<ul style="list-style-type: none"> <li>• No change anticipated</li> </ul>



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<b>PARK</b>	<b>LOCATION</b>	<b>AREA (HA)</b>	<b>DESCRIPTION</b>	<b>FACILITIES</b>	<b>FUTURE DIRECTIONS/ PLAN POLICIES</b>
Panorama Park	Off 2200 Block Panorama Drive near corner of Gallant Avenue	2.7	<ul style="list-style-type: none"> <li>• District Park</li> <li>• 100% urban parkland</li> </ul>	<ul style="list-style-type: none"> <li>• Waterfront trails</li> <li>• Swimming beach</li> <li>• Picnic Shelter</li> <li>• Washrooms</li> <li>• Change rooms</li> <li>• 1 playground</li> <li>• Open grassy area</li> <li>• Parking</li> <li>• Seating</li> <li>• plaza/fountain</li> <li>• Small boat access</li> </ul>	<ul style="list-style-type: none"> <li>• Formalize the waterfront trail within Deep Cove/Panorama Park to connect Deep Cove Boathouse to Parkside Avenue (7.2.1.4 g.)</li> <li>• Identify and formalize a continuous route connecting Cates Park to Panorama Park (7.2.1.4 h.)</li> <li>• Continue to develop Deep Cove/Panorama Park as a major water-based activity centre (7.3.2.6)</li> <li>• Develop interpretative signage at Cates Park, Strathcona Park, Panorama Park and other locations to educate the public about marine life in local waters and encourage stewardship (7.5.1.1)</li> </ul>
Deep Cove Park	Off 4900 Block Gallant Avenue; Banbury Road & Rockcliff Road	4.9	<ul style="list-style-type: none"> <li>• District Park</li> <li>• 60% natural parkland</li> <li>• 40% urban parkland</li> </ul>	<ul style="list-style-type: none"> <li>• Deep Cove Boathouse</li> <li>• Boat rentals</li> <li>• Boat launch</li> <li>• Waterfront trails</li> <li>• Picnic shelter</li> <li>• Forested greenbelt</li> </ul>	<ul style="list-style-type: none"> <li>• Formalize the waterfront trail within Deep Cove/Panorama Park to connect Deep Cove Boathouse to Parkside Avenue (7.2.1.4 g.)</li> <li>• Continue to develop Deep Cove/Panorama Park as a major water-based activity centre (7.3.2.6)</li> </ul>
Cates Park	Off 200 Block Dollarton Highway	22.30	<ul style="list-style-type: none"> <li>• District Park</li> <li>• 60% natural parkland</li> <li>• 40% urban parkland</li> <li>• Shared significance to District and Tsleil-Waututh First Nation</li> <li>• Contains archaeological features</li> </ul>	<ul style="list-style-type: none"> <li>• 4 tennis courts</li> <li>• 2 playgrounds</li> <li>• Boat launch</li> <li>• Concession</li> <li>• Wash rooms</li> <li>• Picnic shelter</li> <li>• Open grassy area</li> <li>• First Nation Canoe</li> <li>• Parking</li> <li>• Waterfront trail</li> </ul>	<ul style="list-style-type: none"> <li>• Development of Cates Park Management Plan to establish long term goals and strategies, in consultation with Tsleil-Waututh First Nation and in reference to Cates Park/Whey-ah-Wichen Protocol Agreement and archaeological assessments (7.3.3)</li> <li>• Continue to develop strategies for the preservation and interpretation of the cultural heritage of Cates Park (7.3.4)</li> </ul>



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PARK	LOCATION	AREA (HA)	DESCRIPTION	FACILITIES	FUTURE DIRECTIONS/ PLAN POLICIES
Cates Park (cont'd)					<ul style="list-style-type: none"> <li>• Priorities to include: upgrade of boat launch, review of current &amp; future water-based recreational activities, upgrade of concession, change rooms, signage and interpretive objects, examination of extent of fishing &amp; crabbing, foreshore erosion protection (7.3.5)</li> <li>• Continue the natural waterfront trail from Little Cates to western border of Cates Park (7.3.6)</li> <li>• Recognize primary role and character of Cates as significant parkland in all future decision making, including commercial activity decisions (7.3.8)</li> <li>• Identify and formalize a continuous route connecting Cates Park to Panorama Park (7.2.1.4 h.)</li> <li>• Establish a north-south multi-purpose trail from Roche Point Drive, south through the Seymour Golf Course to Dollarton Highway and Cates Park (7.2.1.4 i.)</li> <li>• Develop interpretative signage at Cates Park, Strathcona Park Panorama Park and other locations to educate the public about marine life in local waters and encourage stewardship (7.5.1.1)</li> </ul>
Roche Point Park	Access 4000 Block of Dollar Road Access from north end Roche Point Drive (south)	5.77	<ul style="list-style-type: none"> <li>• Community Park</li> <li>• Currently 50% natural parkland; 50% urban parkland</li> </ul>	<ul style="list-style-type: none"> <li>• Currently 1 soccer field and trails in forest greenbelt</li> <li>• Parking (at soccer field)</li> </ul>	



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PARK	LOCATION	AREA (HA)	DESCRIPTION	FACILITIES	FUTURE DIRECTIONS/ PLAN POLICIES
Myrtle Park	Off 1500 Block Deep Cove Road; 4400 Block Banbury Road	14.12	<ul style="list-style-type: none"> <li>• Community Park</li> <li>• 60% natural parkland</li> <li>• 40% urban parkland</li> </ul>	<ul style="list-style-type: none"> <li>• 2 soccer fields</li> <li>• 3 baseball diamonds</li> <li>• 4 tennis courts</li> <li>• 2 playgrounds</li> <li>• 1 lacrosse box</li> <li>• 1 spray pool</li> <li>• Fitness circuit</li> <li>• Wash rooms</li> <li>• Parking</li> <li>• Trail in forested greenbelt</li> <li>• Field house</li> </ul>	<ul style="list-style-type: none"> <li>• Maintain existing park facilities in Myrtle Park, appropriate to its role as a primary community-level park (7.3.1.5)</li> </ul>
Indian River Park	End of Inlet Crescent (3900 Block)	6.34	<ul style="list-style-type: none"> <li>• Neighbourhood Park</li> <li>• 70% natural parkland</li> <li>• 30% urban parkland</li> </ul>	<ul style="list-style-type: none"> <li>• Lit trails in forest greenbelt</li> <li>• Open grassy area</li> <li>• 2 tennis courts</li> <li>• 2 playgrounds</li> </ul>	<ul style="list-style-type: none"> <li>• Expansion of playground (7.3.1.8)</li> </ul>
Sherwood Park	800 Block Friar Crescent	0.37	<ul style="list-style-type: none"> <li>• Neighbourhood Park</li> <li>• 100% urban parkland</li> </ul>	<ul style="list-style-type: none"> <li>• Open grassy area</li> <li>• trail</li> <li>• 1 playground</li> </ul>	<ul style="list-style-type: none"> <li>• No change anticipated</li> </ul>
Wickenden Park	Access off Lockhaven Road, Eastridge Road, Cove cliff Road & Cardinal Crescent	9.40	<ul style="list-style-type: none"> <li>• Neighbourhood Park</li> <li>• 100% natural parkland</li> </ul>	<ul style="list-style-type: none"> <li>• Informal trails in forest</li> </ul>	<ul style="list-style-type: none"> <li>• Retain Wickenden as a natural park, while upgrading trails (7.3.2.8)</li> <li>• Provide a viewing point at Eastridge with trail connection to Wickenden Park (7.3.2.9)</li> </ul>
Cove Cliff Park	Off 4400 Block Cove Cliff Road, adjacent to Cove Cliff School	0.10	<ul style="list-style-type: none"> <li>• Neighbourhood Park</li> <li>• 100% natural parkland</li> </ul>	<ul style="list-style-type: none"> <li>• Creek trail</li> <li>• Interpretive platform</li> </ul>	<ul style="list-style-type: none"> <li>• No change anticipated</li> </ul>
Burns Park	Between Caledonia & Burns; access off Caledonia & Strathcona	0.59	<ul style="list-style-type: none"> <li>• Neighbourhood Park</li> <li>• 100% natural parkland</li> </ul>	<ul style="list-style-type: none"> <li>• Trail</li> </ul>	<ul style="list-style-type: none"> <li>• No change anticipated</li> </ul>



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PARK	LOCATION	AREA (HA)	DESCRIPTION	FACILITIES	FUTURE DIRECTIONS/ PLAN POLICIES
Russell Park	Off 4100 Block Russell Court	0.14	<ul style="list-style-type: none"> <li>• Neighbourhood Park</li> <li>• 100% urban parkland</li> </ul>	<ul style="list-style-type: none"> <li>• 1 playground</li> <li>• Seating</li> <li>• Open grassy area</li> <li>• Trail</li> </ul>	<ul style="list-style-type: none"> <li>• No change anticipated</li> </ul>
Strathcona Park	Off 4600 Block Strathcona Road	0.18	<ul style="list-style-type: none"> <li>• Neighbourhood Park</li> <li>• 100% urban parkland</li> </ul>	<ul style="list-style-type: none"> <li>• Open grassy area</li> <li>• Waterfront Access</li> <li>• Trails</li> <li>• Boat launch for small boats</li> </ul>	<ul style="list-style-type: none"> <li>• Develop interpretative signage at Cates Park, Strathcona Park panorama Park and other locations to educate the public about marine life in local waters and encourage stewardship (7.5.1.1)</li> </ul>
Strathcona Wharf					<ul style="list-style-type: none"> <li>• Upgrade the existing public wharf facility (7.3.1.6)</li> </ul>
Fairway Park	Off Loach Place & 4200 Block Fairway Place	0.44	<ul style="list-style-type: none"> <li>• Neighbourhood Park</li> <li>• 50% natural parkland</li> <li>• 50% urban parkland</li> </ul>	<ul style="list-style-type: none"> <li>• 1 playground</li> <li>• Trail</li> </ul>	<ul style="list-style-type: none"> <li>• No change anticipated</li> </ul>



## **8.0 Community Services**

Seymour's community vision of a high quality of life for current residents and future generations includes the notion of maintaining an attractive community that supports residents as their needs change. Therefore, the Plan also recognizes that the social needs of residents must be supported in order to maintain a truly sustainable, healthy community over time. In the Plan, "community services" describes a range of activities and facilities that contribute to the health and well being of Seymour, including recreation, health, social, safety, education or spiritual services.

Currently, Seymour's residents have access to a wide range of community services and recreational facilities and programs. Because of the residential growth experienced in recent decades and the subsequent "catching up" of community services that was required, there is a persistent concern that the provision of community services keeps pace with any future development. In the low-growth context of this Plan, major additions to community services are not foreseen. Rather, the approach emphasizes maintaining and augmenting the investment in facilities and community capacity that already exists in Seymour. This could include more fully utilizing community assets, improving delivery through partnerships and community governance, and monitoring trends, needs and desires so that residents continue to benefit from high quality services over time.

### **Goal**

***To provide a level of community services in Seymour that is equitable, accessible, keeps pace with community growth and responds to changing needs.***

### **Objective 8.1 *Maintain and fully utilize existing public and institutional facilities.***

- Policy 8.1.1** Properly maintain facilities through the allocation of sufficient municipal funds for the maintenance of existing District and RecCommission public facilities.
- Policy 8.1.2** Continue to develop and implement joint use agreements between the District and School District #44 to increase community use of schools and fields.
- Policy 8.1.3** Contribute to the maintenance of facilities that fall within Joint Use or other agreements, such as the Seycove Community Centre.
- Policy 8.1.4** Investigate the opportunity to coordinate school closures with other potential community uses.
- Policy 8.1.5** Generally support the retention of existing institutional and public assembly uses within Seymour.



- Policy 8.1.6** Generally support the use of District properties by non-profit organizations providing benefit to Seymour and its residents, provided that principles of fairness, accountability and review are satisfied.
- Policy 8.1.7** Work with community groups and partners to encourage continued and increased community access to meeting space and other facilities and resources in Seymour.
- Policy 8.1.8** Increase accessibility to public facilities for all Seymour residents by ensuring District facilities are barrier free and by working with other institutions and groups to reduce barriers to accessibility. E.g. through awareness, signage, navigation or assistive listening devices.

**Objective 8.2 *Maintain an adequate supply of land and provide sufficient community facilities and programs to meet future needs.***

- Policy 8.2.1** Provide elementary and secondary school sites in appropriate locations for future school construction, determined in consultation with School District #44 and other partners, and considering potential new development, demographic changes and projected enrolments. (See also Section 4.0 *Housing and Schools*)

Implementation

- 8.2.1.1 Retain the designated secondary school site south of Anne Macdonald Way.
- 8.2.1.2 Retain the designated school site in Northlands and work with School District #44 to determine its necessity based on potential new development.
- 8.2.1.3 Work with Tsleil-Waututh Nation to develop a shared elementary school site.
- 8.2.1.4 Continue to work co-operatively with School District #44 to facilitate renovations and upgrading of existing school facilities.
- Policy 8.2.2** Examine the potential for Seymour to accommodate additional recreational facilities and resources to serve local and District community needs, such as a running track, additional playing fields or other active uses. (See also 7.3.2)
- Policy 8.2.3** Continue to consult with the RecCommission, School District #44, the Arts & Culture Commission of North Vancouver and Tsleil-Waututh Nation for long-range facility planning in Seymour.
- Policy 8.2.4** In consultation with the community, developers, RecCommission, District Parks and other departments, consider Northlands as an appropriate location for the provision of active recreational space.



- Policy 8.2.5** Work with owners, developers and community partners to increase community facility space in any future development on public assembly or comprehensive development lands.
- Policy 8.2.6** Ensure that any new or re-developed community buildings respect the physical and cultural character of the surrounding community.
- Objective 8.3** ***Flexible, responsive programming and community services that respond to the needs of Seymour's residents.***
- Policy 8.3.1** Continue to fund and otherwise support youth outreach services and youth programming in Seymour.
- Policy 8.3.2** Encourage and support community groups, non-profit agencies and others providing services and programming for seniors.
- Policy 8.3.3** Support the provision of a continuum of childcare services in Seymour, for children aged 6 months to 12 years.
- Policy 8.3.4** Examine the need for and encourage services supporting families with older children.
- Policy 8.3.5** In partnership between Community Planning, North Shore Coast Garibaldi Health Region<sup>1</sup> community-based service providers, Tsleil-Waututh Nation, Seymour residents and others, regularly assess community social and demographic changes, trends, other changes, and their impact on community services.
- Policy 8.3.6** Work with all community partners to design, adapt and implement recreational, educational, health and social services and programs to meet current and changing needs.
- Implementation
- 8.3.6.1** Review library services, hours of operation and meeting room facilities to enhance access to these services.
- Objective 8.4** ***Achieve a high level of safety, security and accessibility to community services in Seymour.***
- Policy 8.4.1** Continue to provide emergency services, including police, fire and ambulance, at a level that ensures the safety and security of Seymour residents.
- Policy 8.4.2** Establish a Community Policing Centre in Seymour.

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<sup>1</sup> Full title is: North Shore/Coast Garibaldi Health Services Delivery Area, Vancouver Coastal Health Authority



- Policy 8.4.3** Encourage a high level of emergency preparedness for the Seymour community.
- Policy 8.4.4** Community Planning to work with North Shore Emergency Management Office, local schools, businesses, institutions, agencies, and residents to encourage involvement and promote the development of emergency response plans that emphasize personal and neighbourhood emergency preparedness.
- Policy 8.4.5** Promote public awareness of what services and facilities exist in Seymour and how to access them.
- Policy 8.4.6** Ensure that community services and facilities are safe, secure and accessible to all Seymour residents – physically, financially, by various transportation modes and by being welcoming.
- Implementation
- 8.4.6.1 Work with seniors, youth, families, facility staff, RecCommission, North Shore Coast Garibaldi Health Region and service agencies to promote safe and welcoming environments.
- 8.4.6.2 Promote awareness of the recreation voucher program and other programs enabling access to services.
- 8.4.6.3 Work with user groups, service providers, Advisory Committee on Disability Issues, District departments, TransLink and others to assess community facilities for accessibility by car, bus, bike on foot, or with assistance devices.
- Objective 8.5** ***Work in partnerships to deliver programs and services in cost-effective ways which best meet the community's needs.***
- Policy 8.5.1** Explore opportunities for innovative, community-based service delivery.
- Policy 8.5.2** Explore partnerships, encourage volunteerism and fundraising to enhance educational, recreational, health, social and other service opportunities in Seymour.
- Policy 8.5.3** Identify specific programs or projects based on local need and attributes, suitable for corporate or other sponsorship.
- Policy 8.5.4** Monitor community service delivery and community trends to ensure that residents' needs are being effectively met.

## **9.0 Economic Development**

The community vision includes linkages between home, work and recreation, supporting opportunities based on local skills, knowledge and the natural environment.



Because the Plan limits population growth, it also limits economic activity directly related to population, while encouraging modest growth in areas such as arts, culture, tourism and home-based businesses. Therefore, the Plan calls for retaining existing neighbourhood-scaled commercial and retail services (Parkgate Centre, Dollar Shopping Mall, Deep Cove's village centre and individual grocery or convenience store) as vibrant local shopping areas important to sustaining the community. However, based as they are on local population, expansion of these services is not foreseen. Currently, Seymour has approximately 202, 052 square feet of commercial (retail, service) space and minimal industrial activity on the McKenzie Barge-Noble Towing site on Dollarton Highway (approximately 4.2 net acres).<sup>1</sup> The Plan anticipates that any future commercial needs can be met on this site with redevelopment from industrial to mixed commercial use, with a strong public component and minimal residential use. (See *Policy 9.2.5*)

The Plan encourages current waterfront and wilderness recreation and tourism activities such as rentals, lessons and services and also supports a comprehensive tourism strategy, which balances opportunities with local area impacts. Plan policies also support growth in home-based business for its benefits in utilizing local skills and reducing commuting, while acknowledging the need to mitigate negative impacts on neighbours.

Finally, since greater potential for economic growth exists in neighbouring areas, including Maplewood and First Nations lands, the Plan supports co-operation across planning boundaries in order to achieve a healthy balance between opportunity and local area impacts.

### **Goal**

***In keeping with a low-growth strategy, foster economic opportunities based on local skills, knowledge, aspirations and the natural environment, to contribute to the quality of life of Seymour's residents.***

**Objective 9.1**     ***An attractive and dynamic climate, which supports existing local businesses.***

**Policy 9.1.1**     Retain and enhance existing commercial and retail areas to support local neighbourhood needs, in preference to creating additional new ones.

**Policy 9.1.2**     Encourage small, owner-managed shops consistent with the existing culture in Deep Cove and Dollarton.

**Policy 9.1.3**     Continue to allow local neighbourhood convenience stores, groceries and service stations.

**Policy 9.1.4**     Maintain Parkgate as a community focal point for a wider range of commercial services adjacent to key community recreational, health, library and other services.

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<sup>1</sup> Urban Systems. *District of North Vancouver Economic Development Strategy Study Final Report*. March 1997.



**Policy 9.1.5** Work with District departments, other agencies, governments and organizations to promote an improved transportation system as a vital part of quality of life for businesses and residents.

**Policy 9.1.6** To improve access to local centres, encourage improved transportation options, including transit, bike and pedestrian paths, safety improvements, shuttles, Park-and-Ride and other initiatives consistent with the policies in Section 5.0.

**Policy 9.1.7** Support local education, training, information exchange and other professional, technical, artistic and business development efforts.

**Objective 9.2** ***Create economic opportunities shaped by local skills, knowledge, culture and the natural environment.***

**Policy 9.2.1** Encourage business development that relies upon Seymour's assets, local knowledge and skills related to the natural environment, the outdoors, recreation, arts, culture, management, education, science and technology.

**Policy 9.2.2** Encourage economic activities that are quiet, non-polluting, safe and complementary to the residential character of Seymour, with minimal intrusion and negative impact on the environment.

**Policy 9.2.3** Support home-based businesses to promote home-work linkages and reduce commuting, while ensuring that such businesses are responsive to neighbourhood concerns.

Implementation

9.2.3.1 Encourage District planning and licensing staff to work with home-based business operators and community representatives to develop home-based business guidelines.

9.2.3.2 Provide information, support and complaint resolution assistance at the District level to ensure the interests of the home-based business operator and neighbours are protected.

**Policy 9.2.4** Encourage initiatives relating to the cultural economy, including arts education, training, performance and film industry activities. (See also Section 10.3)

Implementation

9.2.4.1 Within the context of the North Shore Cultural Plan and the District Tourism Strategy, and in consultation with the community, develop targeted marketing strategies to promote Seymour's natural, historic, cultural and recreational opportunities.

9.2.4.2 With School District #44, Capilano College, industry representatives and other partners, explore arts education, job training and performance opportunities in the Seymour area.



9.2.4.3 Continue to support the appropriate use of Seymour venues for filming and identify opportunities to further support this activity, while ensuring minimal negative impact on local neighbourhoods.

**Policy 9.2.5** Support the future re-development of Cates Landing from waterfront industrial use to a viable alternative consistent with the themes and directions of the Plan, such as outdoor recreation, tourism, arts and culture and as detailed in sub sections 9.2.5.1 to 9.2.5.8. (*See also Sections 3.0, 4.0, 6.0 and 7.0*)

Implementation

- 9.2.5.1 Detailed environmental, traffic and design studies will be required prior to any re-development of the site to a non-industrial use. Remediation is required before any redevelopment and detailed environmental plans will address creek and natural area protection and restoration.
- 9.2.5.2 In any re-development to a non-industrial use, the site should be treated as a comprehensive development and as an opportunity to develop a destination site featuring arts, culture, commercial and tourism elements.
- 9.2.5.3 Any future redevelopment will provide for waterfront access, views between Dollarton Highway and the waterfront and linkages to Cates Park and other neighbourhood destinations.
- 9.2.5.4 New uses will include a strong public component, which could include markets, a publicly accessible marina, pier, interpretive components relating to local and First Nations history, ferry services or other activities and services with a marine theme.
- 9.2.5.5 New uses will be compatible with the directions of the Cates Park Management Plan (*See Section 7.0*)
- 9.2.5.6 Any residential component will be minimal and not exceed 25 units, which must be incorporated within the conceptual phasing strategy detailed in Section 4.0.
- 9.2.5.7 Early and ongoing consultation with the community will determine acceptable uses and design.
- 9.2.5.8 Development Permit Area designations for protection of the environment and commercial development guidelines will be retained.

**Objective 9.3** *Increased tourism opportunities based on Seymour's natural marine and wilderness assets, which balance economic benefits with local neighbourhood impacts.*

**Policy 9.3.1** Consistent with Sections 3.0 and 7.0 of the Seymour Local Plan, improve access to Seymour's waterfront.

**Policy 9.3.2** Support the District Tourism Task Force in the development of a community -wide tourism strategy, which includes Seymour's community objectives, addresses neighbourhood impacts and promotes the sustainable management of Seymour's local resources.



- Policy 9.3.3** Explore and implement tourism opportunities in co-operation with District staff, other governments, local businesses and associations, Tsleil-Waututh Nation and other partners.
- Policy 9.3.4** Within the context of the District tourism strategy, work with the Arts and Culture Commission of North Vancouver, Chamber of Commerce, Tsleil-Waututh representatives, local cultural groups and neighbours to produce an arts tourism visitor guide, listing studios, galleries, cultural sites, public art sites, events and other attractions in Seymour.
- Policy 9.3.5** Identify and encourage eco-tourism and other tourism strategies related to the natural environment and outdoor recreation.
- Implementation
- 9.3.5.1 Support outdoor festival and recreation events that bring the community together and promote the image of Seymour as a tourism destination.
- 9.3.5.2 Retain existing local and regional tourism activities such as Deep Cove Canoe and Kayak Centre, local events and use of facilities, parks and trails.
- 9.3.5.3 Seek opportunities to host international events, competitions and sports tournaments.
- 9.3.5.4 Within the context of a Cates Park Management Plan and the Cates Park/Whey-ah-Wichen Protocol/Cultural Agreement, support the work of the Tsleil-Waututh Nation to develop eco-tourism opportunities for Cates Park and Indian Arm.
- Policy 9.3.6** Ensure neighbourhood involvement in the design, implementation and evaluation of tourism initiatives and activities.

## **10.0 Arts, Culture & Heritage**

Seymour defines itself by its forest and waterfront setting, local history, neighbourhoods, environmental stewardship and shared goals. A key plan goal is to promote Seymour's identity and sense of place by reflecting these attributes through decision-making and in tangible ways that celebrate the community.

This involves recognition of the importance of heritage in community identity and strategies to promote preservation and awareness of Seymour's heritage assets. Plan policies also recognize the need to work in partnerships—with the community, District departments, developers and Tsleil-Waututh Nation, among others – to appreciate and express cultural heritage through the arts, public art, and other initiatives. Finally, the policies support extending this co-operation to devise strategies for incorporating arts, culture and heritage into the local economy, learning and community life.



**Goal**

***Promote Seymour's unique identity and enhance quality of life through the arts and through recognition and support of the community's natural, built and cultural heritage.***

**Objective 10.1** ***Define and express Seymour's unique character.***

**Policy 10.1.1** Create visible features defining Seymour, such as gateways and a cohesive theme for works within the public domain.

**Policy 10.1.2** Work with Maplewood and Tsleil-Waututh neighbours to identify and create shared land and water gateways that are meaningful to the Seymour community.

**Policy 10.1.3** Work with District departments, local groups and residents to define elements of Seymour's identity to be incorporated into public works, public art and other formats.

**Objective 10.2** ***Ensure that the significant and representative heritage resources of the Seymour area are conserved.***

**Policy 10.2.1** With the Tsleil-Waututh Nation, identify ways to recognize and preserve First Nations heritage in the Seymour area.

**Policy 10.2.2** Within the context of the Cates Park/Whewah-Wichen Protocol/Cultural Agreement, work with the Tsleil-Waututh First Nation to pursue shared objectives regarding Cates Park/Whewah-Wichen.

**Policy 10.2.3** Support the development of a District-wide heritage strategy and subsequent heritage management plan for the Seymour area.

**Policy 10.2.4** Recognize the Heritage Inventories (1900-1929 & 1930-1965) as the basis for managing Seymour's heritage resources. (*Table 10-1*)

**Policy 10.2.5** Work with the Community Heritage Commission, Deep Cove Historical Society and other local groups to identify additional physical and cultural heritage assets, and to document local history and cultural heritage.

**Policy 10.2.6** Include a strategy for the preservation of cultural artefacts and sites of historical significance.

**Policy 10.2.7** Define heritage to include natural, built and cultural resources.

**Policy 10.2.8** Increase awareness of Seymour's heritage as a valuable component of community identity and pride.



## Implementation

- 10.2.8.1 In co-operation with the Community Heritage Commission and local groups, undertake a heritage public education program to include publications, mapping, plaques, tours or other events, for example.
- 10.2.8.2 To raise awareness of its role in the area's development, expand the heritage landmark for the Dollar Mill with an interpretive component.
- 10.2.8.3 Incorporate recognition of local artists within the community trail system. For example, Malcolm Lowry Trail, Dorothy Livesay Poet's Walk.
- Policy 10.2.9** Retain and enhance the village character of Deep Cove.
- Objective 10.3** *Enrich Seymour's quality of life through the arts, culture and public art.*
- Policy 10.3.1** Support the general directions of the District of North Vancouver Cultural Master Plan, ensuring through local involvement that local objectives and impacts are accounted for.
- Policy 10.3.2** Support a District strategy to improve opportunities in the cultural industry, including music recording, film and publishing, for example.
- Policy 10.3.3** Support the development and implementation of a public art plan for the Seymour community.
- Policy 10.3.4** Within the context of the District Master Plan for Public Art, work with the District Public Art Committee, community groups, residents, artists, developers and District departments to identify potential sites and integrate public art into public spaces, public works, new construction and private development projects.
- Policy 10.3.5** In new development or redevelopment, work with District departments, local groups, residents and developers to create urban design and streetscape guidelines, consistent with the District Master Plan for Public Art and appropriate to the local context.
- Policy 10.3.6** With the Tsleil-Waututh Nation, explore opportunities to recognize and incorporate First Nations culture in public art and cultural initiatives.
- Policy 10.3.7** Within the context of the Cates Park/Whey-ah-Wichen Protocol/Cultural Agreement, work to identify and incorporate First Nations culture in public art initiatives within Cates Park/Whey-ah-Wichen.
- Policy 10.3.8** In co-operation with Capilano College, the Arts & Culture Commission of North Vancouver, School District #44, other agencies and groups, support a range of arts job training and education opportunities in Seymour.



**Policy 10.3.9** Identify arts education programming in support of the arts industry, career preparation and for personal growth and learning. Include traditional media (music, theatre, dance, visual arts, literature) and new media forms (design, multimedia, advertising, creative services).

**Policy 10.3.10** Support local arts, cultural groups and community festivals operating within Seymour.

Implementation

10.3.10.1 Continue to support the Deep Cove Cultural Centre as a focal point for community arts cultural groups in Seymour, through grants and other means, subject to regular review of its ongoing contribution to the community and accountable management.

10.3.10.2 Continue to support outdoor festival activities as a means to bring the community together and to attract visitors.



**TABLE 10-1: SEYMOUR AREA HERITAGE INVENTORY****Sources**

- *District of North Vancouver Heritage Inventory (1993)*
- *The Modern Architecture of North Vancouver 1930-1965 (1997)*

INVENTORY	DESCRIPTION	ADDRESS	DATE BUILT	HERITAGE CLASSIFICATION
114	Dollar Mill Office	518 Beachview Drive	1916-20	Secondary
115	Suburban Farms	4342-44 Gallant Avenue	Circa 1930s	Primary
116	Gillis Homestead	1207 Harris Avenue	1926	Primary
117	Panorama Market	2211 Panorama Drive	1920	Primary
118	Dollar Mill House	571 Roslyn Blvd	1920-24	Primary
119	KENT's	Fire Lane 84, Sunshine	1908-12	Secondary
120	Dun Romin	Fire Lane 88, Sunshine	1908-12	Secondary
		2036 Burns Avenue		Supplemental
	Riebolt House	4568 Cove Cliff Road		Supplemental
		5711 Indian River Drive		Supplemental
		4245 Mt. Seymour Parkway		Supplemental
Modern	Chow Residence	1319 Riverside Drive		Primary
Modern	Kovach Residence	2357 Riverside Drive		Primary
Modern	Pollock Residence	1886 Berkley Road		Secondary
Modern	Riebolt Residence	4568 Cove Cliff Road	1947	Secondary



## **11.0 Plan Implementation**

This section outlines three implementation strategies that should be undertaken following adoption of the Seymour Local Plan and its incorporation into the District of North Vancouver Official Community Plan. These strategies are general in nature and separate from the detailed implementation items related to the objectives and policies expressed throughout the Plan. Unless otherwise noted, it is assumed that District staff bears primary responsibility for the initiation of recommended implementation statements throughout the Plan.

1. **Plan Monitoring Committee & Strategy:** The fundamental philosophy of this Plan is a slow, managed approach to change. Given the myriad variables associated with growth in Seymour and surrounding neighbourhoods, plan criteria and outcomes must be assessed over time to ensure that the strategy continues to meet community objectives. Following adoption of the Seymour Local Plan, it is recommended that a *monitoring committee*, made up of community representatives and District staff, be established to develop a strategy for monitoring Plan implementation, impacts and progress. (See also *policy 4.5.1*)
2. **Community Benefits Strategy:** A general principle of the Plan's growth perspective is that growth and change must be seen to benefit the community by contributing to its liveability in ways considered important by Seymour residents. Once the Plan is adopted, it is recommended that a representative community group(s) work with District staff to determine what specific benefits or amenities the community considers as priorities, and to create a framework which relates these desires to planned growth and development activities.
3. **Seymour "Theme" for Gateways & Public Works:** Seymour derives its identity from its forest and waterfront setting, local history and distinct neighbourhoods. Key elements, to be incorporated into future developments and improvements to reflect these themes, should be established through a consultative process. It is highly recommended that a cooperative effort be pursued among all neighbourhoods east of the Seymour River, to define and design meaningful gateways to the shared community.

## **12.0 Plan Map Designations**

This section describes the land use designations shown on the Plan Map (*Chapter 12 Map -Seymour Plan Designations*). The map and descriptions must be considered in conjunction with Plan policy statements for a complete understanding of the future use of any site.

**RESIDENTIAL:** These areas are either presently developed or designated for development over time and at various densities. Residential land uses are categorized as:



- *Single Family (Detached) Residential:* describes areas intended primarily for detached single-family dwellings. Based on current zoning designations (RS1, RS2, RS3, RS4, RS5), densities could range from 1 unit per acre up to approximately 11 units per acre (27 units or less per hectare) on a gross basis.
- *Multi-Family Residential:* describes areas of predominantly attached dwellings in a variety of forms and with a range of densities. Multi-family forms can include duplexes, triplexes, townhomes and apartments, or a combination of these. Current zoning densities range from 6 units per acre to 58 units per acre. (14 to 143 units per hectare.)  
Densities for future multi-family developments specified in the Plan are at a Floor Space Ratio up to 1.2. (See Section 6.0 for details). The Plan does not include multi family residential in high-rise form.
- *Future Mixed Residential:* applies to the CMHC-owned Northlands area. Residential mix and densities will be determined by the development of a comprehensive plan in accordance with Sections 4.0 and 6.0 of the Plan.

**PARKS, RECREATION AND WILDERNESS:** These areas are principally for the preservation and enjoyment of the natural environment, outdoor recreational use, or water supply. Certain portions of these areas have been further designated as *Conservation Areas*, primarily for the protection of fish or wildlife habitat, or environmentally sensitive areas. *Table 7-1* of the Plan describes regional, community and neighbourhood parks within these designated areas.

**INSTITUTIONAL OR PUBLIC USE:** These areas include existing or proposed sites for schools, churches, libraries, theatres, recreation or community centres and other public use buildings, such as childcare. The following categories are depicted on *Chapter 12 Map*:

ES	<i>Elementary School</i>
SS	<i>Secondary School</i>
FS	<i>Future Potential School Site</i>
W	<i>Place of Worship</i>
C	<i>Community Centre, including Parkgate Community Centre and Library, Deep Cove Cultural Centre, Deep Cove Yacht Club and Deep Cove Boathouse (with the Deep Cove Canoe &amp; Kayak Centre &amp; Deep Cove Rowing Centre)</i>
D	<i>Day care Centres, including Parkway Village Childcare and My Little School</i>

**COMMERCIAL:** These are existing and proposed sites suitable for a range of local or community level retail, office, service or a mix of these uses. A separate designation for the McKenzie Barge/Noble Towing site – *Future Mixed Use* -- indicates its suitability for a mix of such uses with a minimal residential component.



**URBAN SERVICES:** Areas currently designated principally for public works, utilities, transit facilities or communications.

**DEVELOPMENT PERMIT AREAS:** Pursuant to the Local Government Act, Development Permit Areas can be established to regulate the development of zoned land in order to protect the natural environment, protect development from hazardous conditions and to regulate the form and character of commercial, industrial or multi-family uses

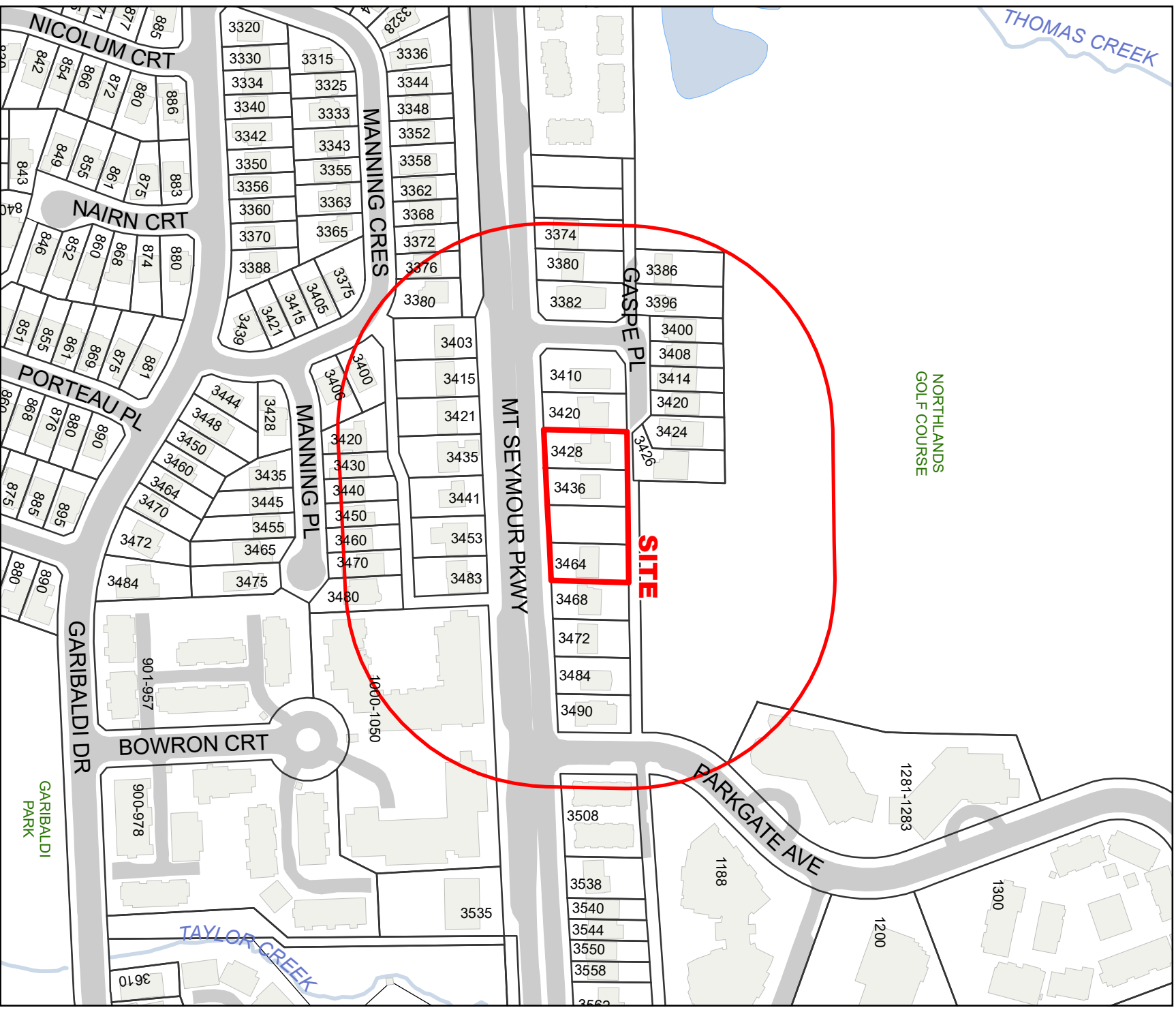
For the most part, Development Permit Areas in the Seymour Local Plan area have been established at the District level through the District Official Community Plan and associated maps. This Plan extends the Development Permit Areas to all creek corridors in Seymour for protection of the natural environment and to the debris flow and flood fan areas presenting conditions hazardous to development. The District Official Community Plan will be amended to designate creeks within the Deep Cove Zone and Indian Arm Zone as debris flow/flood areas where development must be protected from hazardous conditions.

Multi-family residential sites designated by the Plan will also be included in Development Permit Areas for regulation of the form and character of multi-family uses. The District Official Community Plan will be amended to include the newly designated multi family sites along Mount Seymour Parkway (north side 3200 to 3500 blocks and south side 3600 block) as development permit areas.





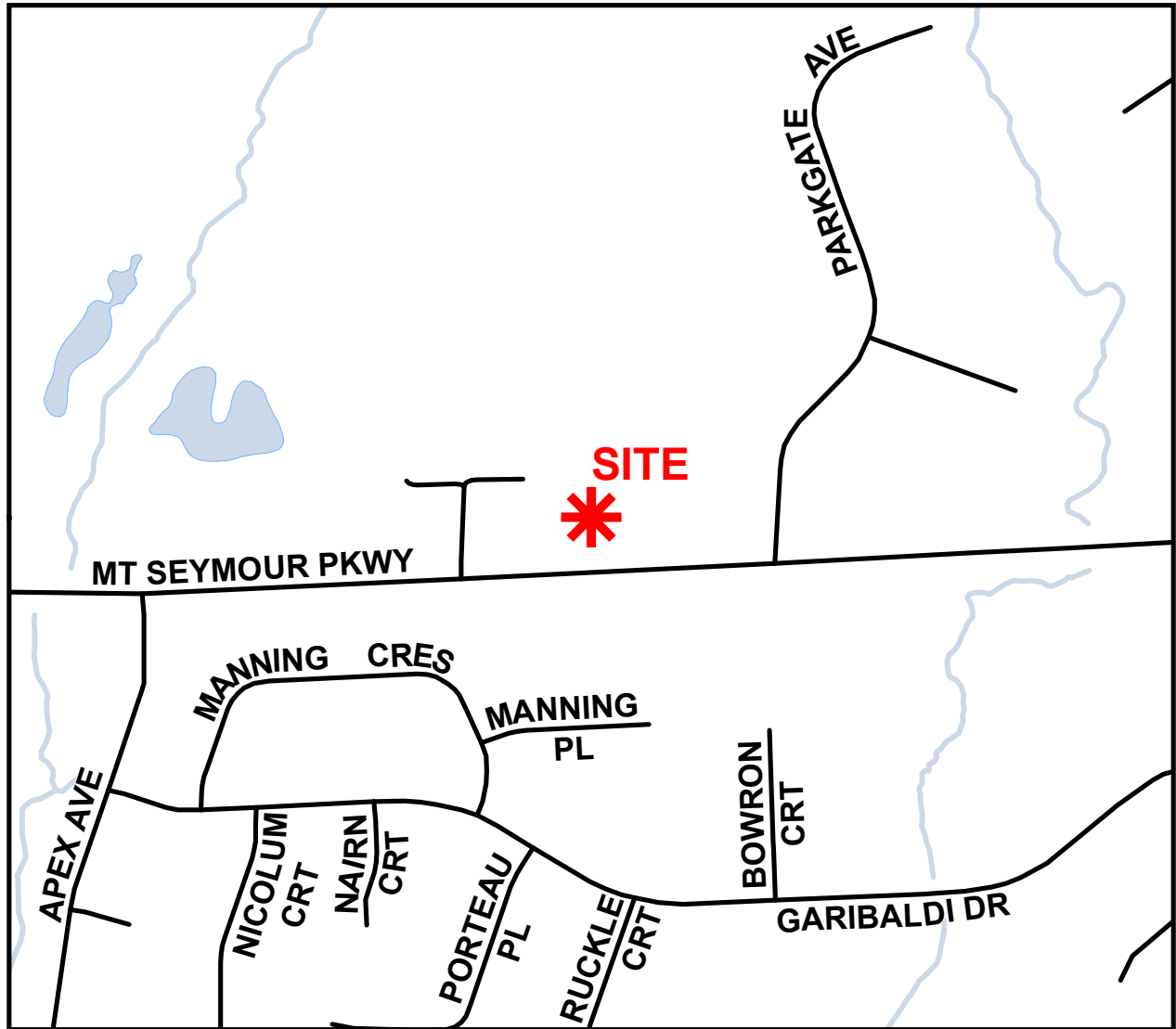




# 3428-3464 Mt Seymour Pkwy 100m BUFFER MAP





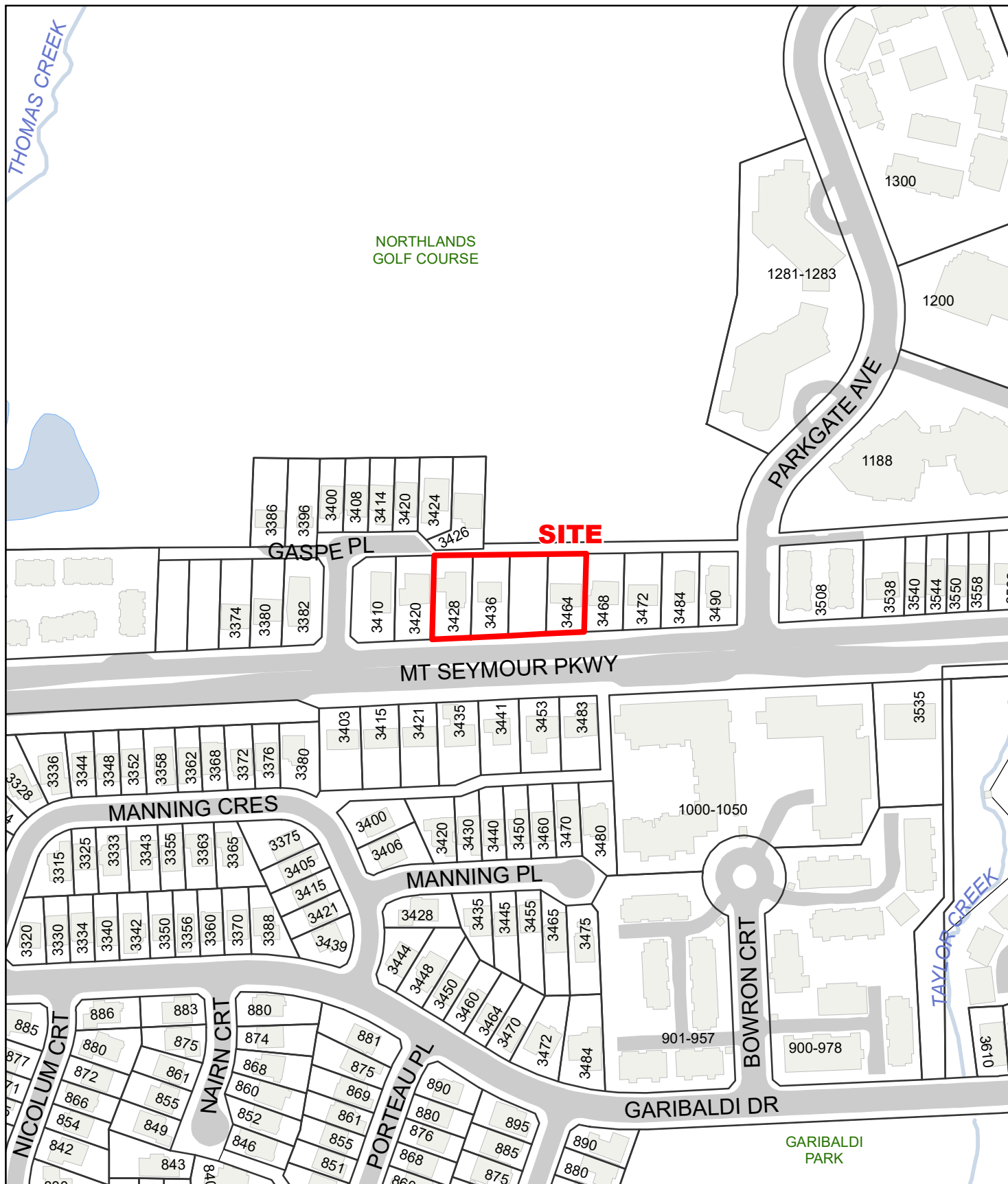


**3428-3464 Mt Seymour Pkwy  
LOCATION MAP**



Published: July 14, 2017



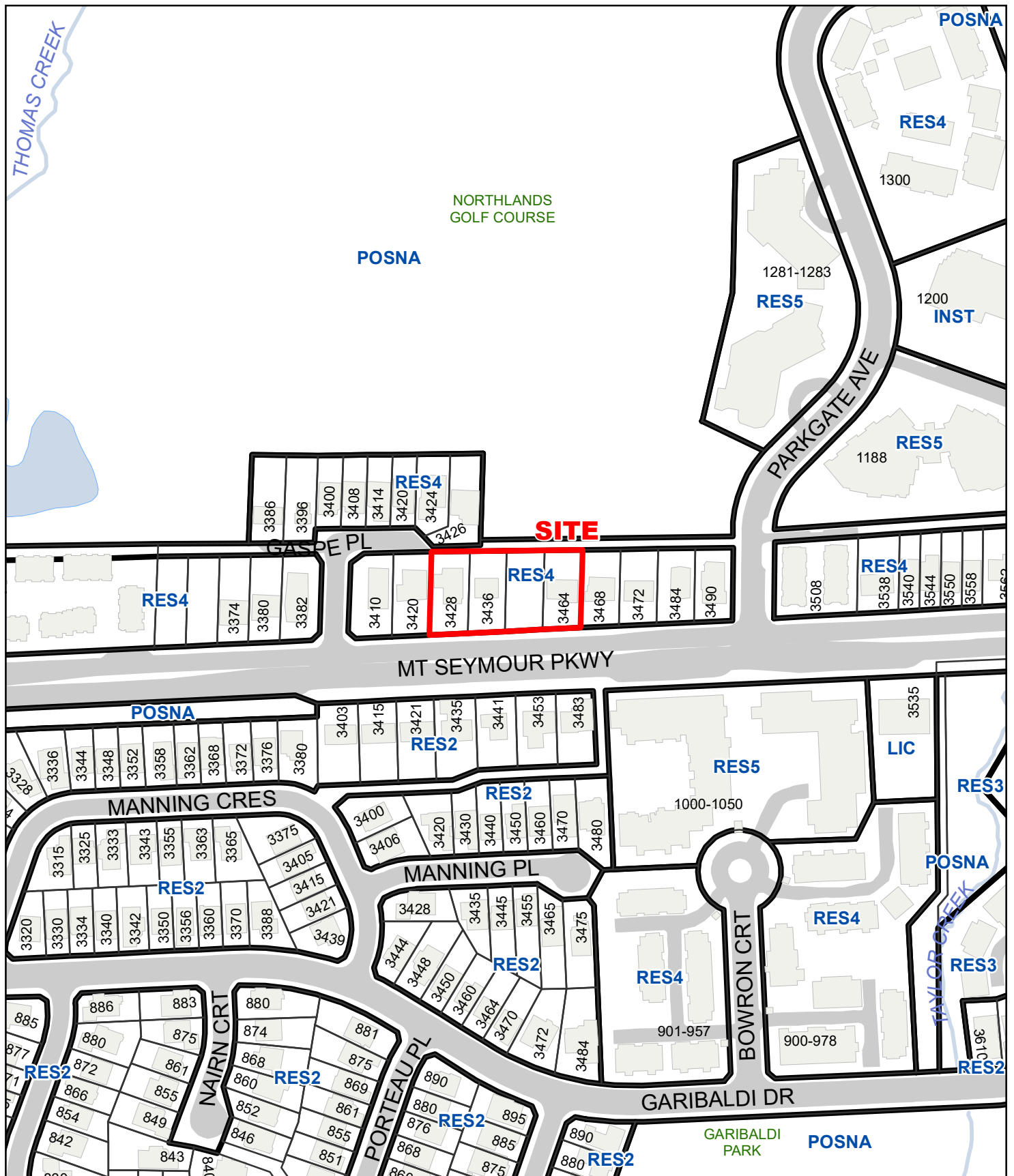


## 3428-3464 Mt Seymour Pkwy SITE MAP



Published: July 14, 2017



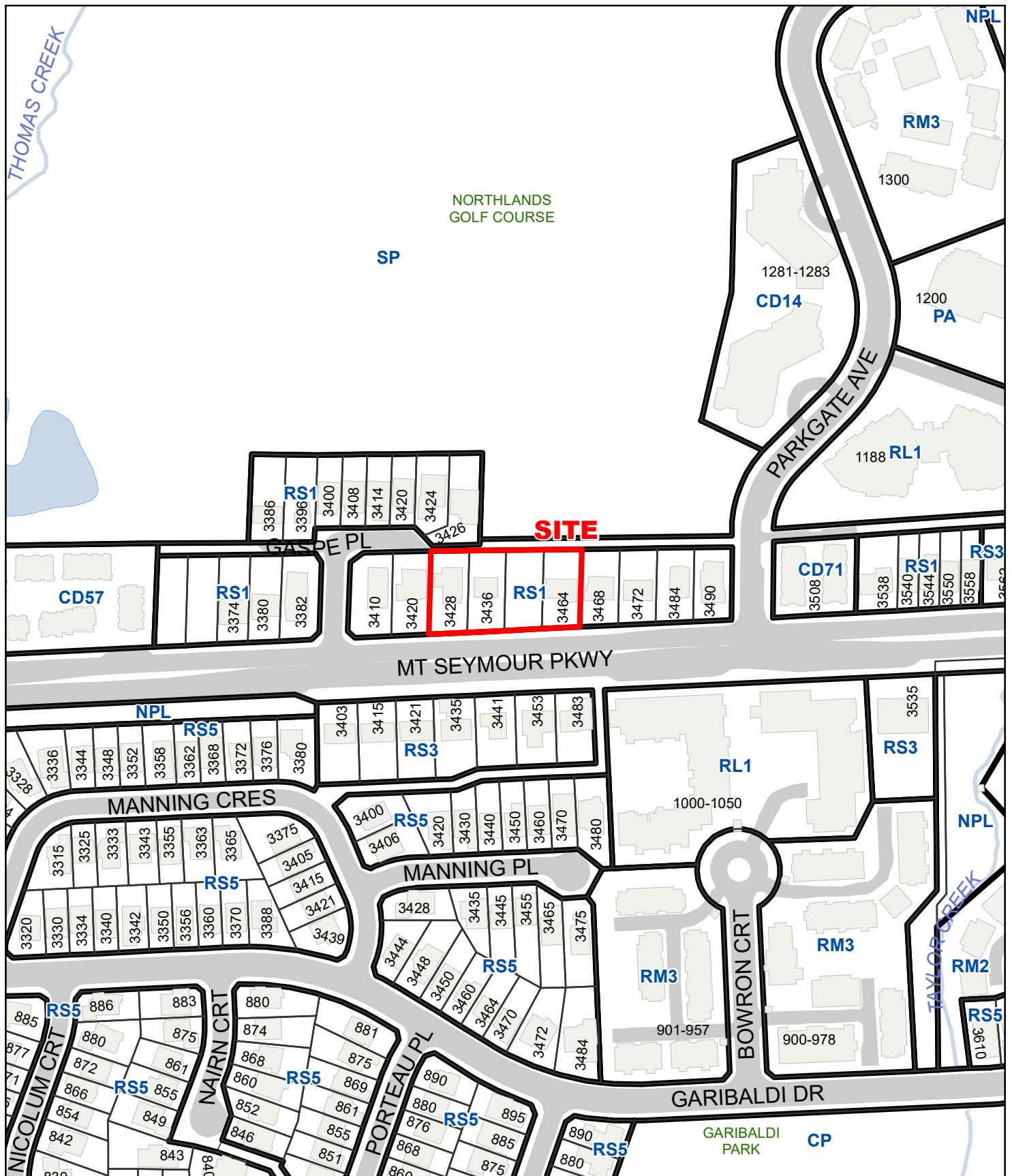


## 3428-3464 Mt Seymour Pkwy OCP CONTEXT MAP



Published: July 14, 2017





## 3428-3464 Mt Seymour Pkwy ZONING CONTEXT MAP



Published: July 14, 2017



GASPE PLACE

LANE

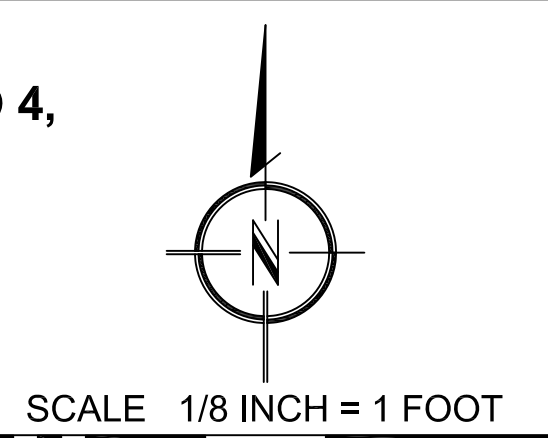
MT SEYMOUR PARKWAY

TOPOGRAPHIC SURVEY PLAN OF  
LOTS 15 TO 17, ALL OF LOT 6, BLOCKS 1 TO 4,  
DISTRICT LOT 622, GROUP 1,  
NEW WESTMINSTER DISTRICT, PLAN 2866.

DISTRICT OF NORTH VANCOUVER  
INTEGRATED SURVEY AREA #16  
CIVIC ADDRESS: 3464 (LOT 15), 3436 (LOT 17) AND  
3428 (LOT 18) MT SEYMOUR PARKWAY  
PID: 012-602-591 (LOT 15) 013-370-073 (LOT 16)  
006-788-467 (LOT 17) 013-370-090 (LOT 18)



FB742 P56-71.75-85 (MA/L), FB695 P54 (H/L/G), FB751 FB37-42 (MA/SB)



LEGEND:	
	- DENOTES POWER POLE
	- DENOTES LAMP STANDARD
	- DENOTES MANHOLE
	- DENOTES WATER VALVE
	- DENOTES SPOT ELEVATION
	- DENOTES INVERT ELEVATION
	- DENOTES INVERT ELEVATION
	- DENOTES 12'0" DECIDUOUS TREE WITH A GROUND ELEVATION OF 333.0'
	- DENOTES 12'0" CONIFEROUS TREE WITH A GROUND ELEVATION OF 333.0'

**DATUM:**  
ELEVATIONS ARE TO DISTRICT OF NORTH VANCOUVER GEODETIC DATUM, AND ARE DERIVED FROM CONTROL MONUMENT 99H1963 PUBLISHED ELEVATION = 251.96 FEET.  
ISA NO. 16 NAD 83, (C.S.R.S.) 2012.

**PROPERTY:**  
PROPERTY DIMENSIONS SHOWN ARE DERIVED FROM: FIELD SURVEY, (LEGAL SURVEY PLAN PENDING)  
OFFSETS TO PROPERTY LINES ARE NOT TO BE USED TO DEFINE BOUNDARIES.  
REFER TO CURRENT CERTIFICATE(S) OF TITLE FOR ADDITIONAL, EXISTING OR PENDING CHARGES.

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CERTIFIED CORRECT

THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 2017.

MICHAEL J. SEITZ, B.C.L.S.

FIELD SURVEY COMPLETED ON APRIL 18, 2017.

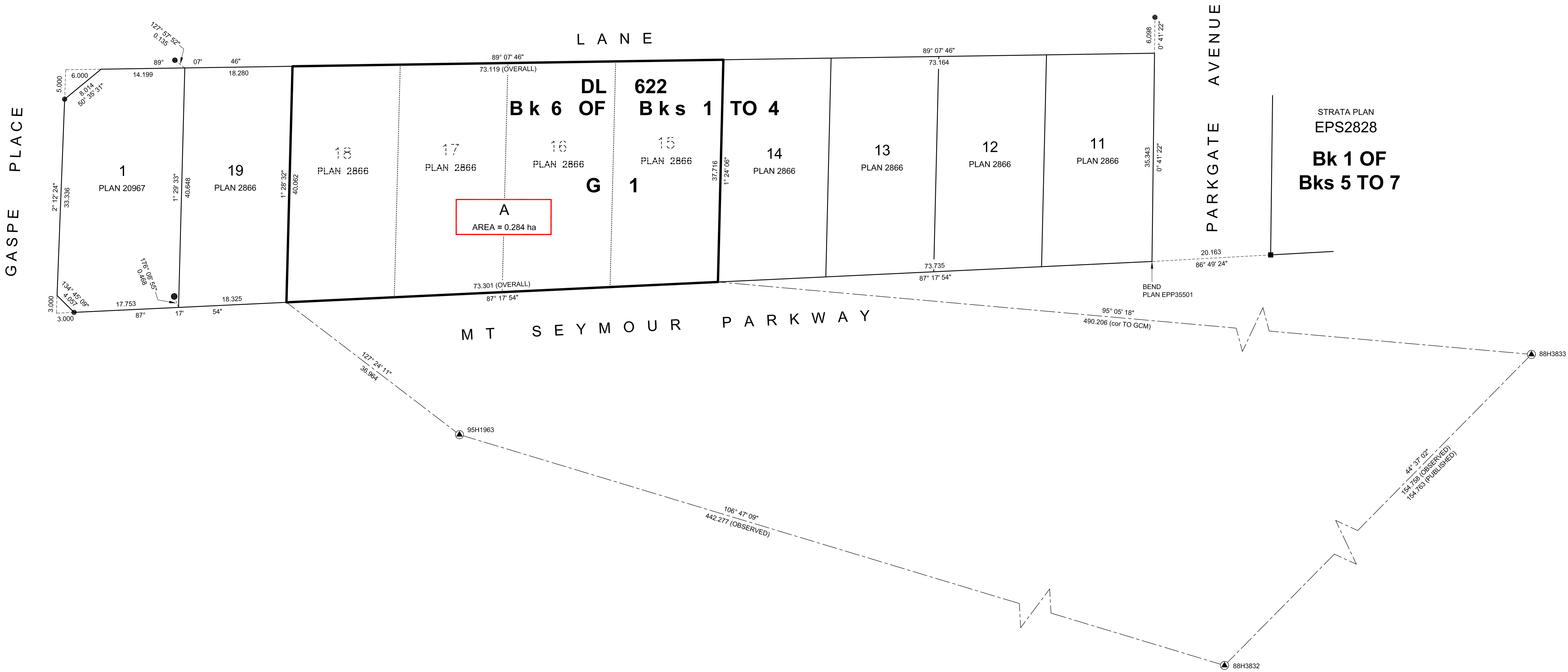
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PROPOSED REFERENCE PLAN OF LOTS 15, 16, 17 AND 18, ALL OF LOT 6 BLOCKS 1 TO 4,  
DISTRICT LOT 622, GROUP 1, NEW WESTMINSTER DISTRICT, PLAN 2866.

PURSUANT TO SECTION 100(1)(b) OF THE LAND TITLE ACT.  
DISTRICT OF NORTH VANCOUVER  
BCGS 92G.035

5 0 5 10 25  
ALL DISTANCES ARE IN METRES AND DECIMALS THEREOF.  
THE INTENDED PLOT SIZE OF THIS PLAN IS 864mm IN  
WIDTH BY 560mm IN HEIGHT (D SIZE) WHEN PLOTTED AT  
A SCALE OF 1:400



LEGEND

INTEGRATED SURVEY AREA No. 16,  
DISTRICT OF NORTH VANCOUVER,  
NAD83 (CSRS) 4.0,0.BC,1.GVRD.

- - DENOTES STANDARD IRON POST FOUND
- - DENOTES STANDARD IRON POST PLACED
- - DENOTES LEAD PLUG FOUND
- - DENOTES LEAD PLUG PLACED
- ▲ - DENOTES CONTROL MONUMENT
- US - DENOTES UNSUITABLE FOR POSTING

THIS PLAN SHOWS HORIZONTAL GROUND-LEVEL DISTANCES  
UNLESS OTHERWISE SPECIFIED, TO COMPUTE GRID DISTANCES.  
MULTIPLY GROUND-LEVEL DISTANCES BY THE AVERAGE COMBINED  
FACTOR OF 0.9995872, WHICH HAS BEEN DERIVED FROM GEODETIC  
CONTROL MONUMENTS 95H1963.

NOTE:  
THIS PLAN SHOWS ONE OR MORE WITNESS POSTS  
WHICH ARE NOT SET ON THE TRUE CORNER(S).

DATUM: NAD83 (CSRS) 4.0,0.BC,1.GVRD, UTM ZONE 10.				
MONUMENT	NORTHING	EASTING	ESTIMATED HORIZONTAL POSITIONAL ACCURACY	COMBINED FACTOR
88H3832	5462416.099	502142.158	0.03	0.9995872
88H3833	5462526.216	502250.813	0.02	0.9995872
NOTE: FOR MAPPING PURPOSES ONLY.				

GRID BEARINGS ARE DERIVED FROM OBSERVATIONS BETWEEN  
GEODETIC CONTROL MONUMENTS 88H3832 AND 88H3833.

THE UTM COORDINATES AND ESTIMATED HORIZONTAL POSITIONAL  
ACCURACY ACHIEVED ARE DERIVED FROM THE MASCOOT PUBLISHED  
COORDINATES AND STANDARD DEVIATIONS FOR GEODETIC CONTROL  
MONUMENTS 88H3832 AND 88H3833.

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FB742 P56-71,75-85 (MA/HL) FB695 P94 (HL/GK) FB751 FB37-42 (MA/SB)

THIS PLAN LIES WITHIN THE  
GREATER VANCOUVER REGIONAL DISTRICT

THE FIELD SURVEY REPRESENTED BY THIS PLAN WAS  
COMPLETED ON THE DAY OF , 2017,  
MICHAEL J. SEITZ, BOLS #661

DM#3276141





Mount Seymour Parkway Residential  
Developments  
Transportation Impact  
Assessment  
Draft

---

Prepared for

Tatla Developments & Allaire Properties

Date

May 5, 2017

Project No.

6127.02

DM#3207378 (Saved  
May 15, 2017)







May 5, 2017

Mr. Kuldeep Tatla  
Tatla Developments, Ltd  
Suite 170, 1200 West 73 Avenue  
Vancouver, BC V6P 6G5

Mr. Marc Allaire  
Allaire Properties  
#245 - 9600 Cameron Street  
Burnaby, BC V3J 7N3

Dear Kuldeep & Marc:

**Re: Mount Seymour Parkway Residential Developments - Traffic Impact Assessment  
Draft Report v1**

Bunt & Associates has completed the following traffic impact assessment draft for two proposed residential developments of Seymour Estates in the District of North Vancouver. This report has been prepared to accompany the projects' rezoning applications. More specifically, the report outlines our understanding of existing and future (with development) traffic conditions at the key intersections for accessing the sites, and in the context of other road capacity improvements in the vicinity of the new Highway 1 Interchange, and District concerns over the width of Lytton Street.

We trust the attached letter is satisfactory for your needs. If you have any questions or require further use of our services please get in touch.

Yours truly,  
**Bunt & Associates**



Dan Ross, CPeng. (NZ) MUP  
Senior Transportation Planner



Thea Wilson, MSc, MRTPI  
Transportation Planner







## CORPORATE AUTHORIZATION

Prepared By:      Thea Wilson, MSc MRTPI  
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Senior Transportation Planner

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Date:            2017-05-05

Project No.    6127-02

Status:         Draft v1

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# 1. INTRODUCTION

## 1.1 Background

Tatla Development, located to the east, at 3468-3490 Mount Seymour Parkway, is proposing 27 strata townhome units. The site is located on the northwest corner of Mount Seymour Parkway and Parkgate Avenue, and currently consists of 4 single family homes, with vehicle access of Mount Seymour Parkway.

Allaire Properties is proposing a 29 unit market condominium townhome development, located at 3428-3464 Mount Seymour Parkway, in the District of North Vancouver, BC. The site is approximately 0.7 hectares and is located north of Mt Seymour Parkway between Parkgate Avenue and Gaspe Place. The existing site includes three single family homes, with vehicle access of Mount Seymour Parkway.

Both proposed developments will provide underground parking and vehicle access of a rear lane, which will connect to Parkgate Avenue and potentially Gaspe Place. Presently the rear lane is accessible to pedestrians & cyclists only. The developments are planned to be completed for 2022.

## 1.2 Study Purpose

The developments sites are currently designated Residential Level 4: Transitional Multifamily in the Official Community Plan (OCP). As part of the Rezoning and Development Permit application package, a Traffic Impact Assessment has been requested by the District.

The parameters of this study have been agreed to by District staff, and are outlined in a Terms of Reference, provided in **Appendix A**.

The District of North Vancouver requires that a Transportation Impact Assessment be undertaken for the proposed development and the following main objectives:

- Identify the volume of traffic likely to be generated by the proposed development and its potential impact to traffic operations on the area street network;
- Review the impact to traffic operations with the potential laneway connection between Parkgate Avenue and Gaspe Place;
- Conduct a parking assessment to ensure adequate off-street parking is provided to meet the anticipated demand and review current and future on-street demand;
- Review the servicing and parking access for vehicle manoeuvres to/from the proposed rear laneway;
- Identify measures for the proposed development to reduce the reliance on single occupancy automobile trips and promote alternative travel modes including walking, cycling, and transit; and,
- Conduct a high level safety/ speed analysis of Mt Seymour Parkway, and review measures which could reduce speeding and unsafe/ illegal vehicle manoeuvres.



### 1.3 Site Location & Study Area

The sites' location is shown in **Exhibit 1.1** with Gaspé Place to the west, Parkgate Avenue to the east and Mt Seymour Parkway to the south. The sites are further bound by Northlands Golf Course to the north Windsor Secondary School to the southeast and District-owned Greenfield land to the south. Mt Seymour Parkway connects the community of Deep Cove to the rest of North Vancouver and Highway 1 via the Ironworkers Memorial Bridge to Vancouver and Burnaby. **Exhibit 1.2** shows the study area, as specified in the TOR.

### 1.4 Reference Documents

The following table summarizes various sources of information and guidelines that were used for the study.

**Table 1.1: Reference Documents**

REFERENCE	DATE
North Shore Area Transit Plan	2012
Highway 1 – Lower Lynn Improvements Mountain Highway Interchange – Public Information Display Boards	2017
DNV Transportation Plan	2012
North Vancouver Bicycle Master Plan	2012
District of North Vancouver Accessible Design Policy for Multi-Family Housing	-
ITE Trip Generation Manual 9th Edition	2012

### 1.5 Organization of Report

This Transportation Impact Assessment is structured as follows:

- **Section 2** provides a review of the existing transportation infrastructure facilities and accessibility, traffic volumes for all travel modes, on-street parking demand and speed surveys;
- **Section 3** provides a summary of existing transit, cycling and pedestrian connections and related multi-modal District and regional transportation policy;
- **Section 4** provides a review of the proposed development site, including vehicle parking provision and a site plan review for each development;
- **Section 5** reviews potential off-site improvements for the local street network;
- **Section 6** reviews the existing traffic conditions and operational analysis;
- **Section 7** provides an assessment of future traffic conditions and proposed development impact;
- **Section 8** reviews applicable transportation demand management measures for the sites; and,
- **Section 9** concludes and summarizes the report.





## Exhibit 1.1 Site Location

3468-3490 Mt Seymour Parkway TIA  
6127.02  
May 2017





## Exhibit 1.2 Study Area

3468-3490 Mt Seymour Parkway TIA  
6217.02  
May 2017



## 2. EXISTING CONDITIONS

This section outlines existing transportation conditions at the site and on the adjacent transportation networks. The site is located in the Northlands neighbourhood and is adjacent to the Parkgate and Parkway neighbourhoods in the District of North Vancouver. This predominantly residential community benefits from its walking distance to the community, retail and office services located at Parkgate Village, and nearby recreational facilities of Northlands Golf Course and Mount Seymour.

### 2.1 Site Accessibility

#### 2.1.1 Walking

**Exhibit 2.1** shows a 400m walking radius from the site, indicating the distances that can be reached within a 5-minute walk. Partially included within this area is Parkgate Village, which has a range of commercial retail services and medical offices. The Parkgate Community Centre and Library are also located not far from the site, at a walking distance of approximately 750m.

Sidewalks are provided along both sides of most streets in the study area, and pedestrian push buttons and/or crosswalks are available at the intersection of Mt Seymour Parkway and Parkgate Avenue and Mt Seymour Parkway and Mt Seymour Road, as shown in Exhibit 2.1.

#### 2.1.2 Cycling

Near the development site, there are two major on-street cycling routes: Mt Seymour Parkway and Mount Seymour Road, as indicated on Exhibit 2.1. There are also off-street cycling connections to the north of the site, also illustrated on Exhibit 2.1. This off-street cycling route has been classified as a 'future urban trail improvement' as per the District's Official Community Plan.

#### 2.1.3 Transit

The site is within close proximity to a number of transit routes and bus stops. Exhibit 2.1 shows the 211, and C-15 bus routes which pass near the site and connect to Phibbs Exchange in the west and Deep Cove and Indian River (Mount Seymour area) in the east. Bus stops are located along Mount Seymour Parkway. **Table 2.1** summarizes these routes and the connections that these routes provide.

**Table 2.1: Existing Transit Services within Walking Distance of the Site**

ROUTE	NEAREST STOP	CONNECTIONS
211 – Phibbs Exchange / Vancouver	Eastbound & Westbound – Mt Seymour Parkway & Parkgate Ave	Deep Cove, Phibbs Exchange, Vancouver
C-15 – Indian River / Phibbs Exchange	Eastbound & Westbound – Mt Seymour Parkway & Parkgate Ave	Seymour Heights to Phibbs Exchange





## Exhibit 2.1 Pedestrians, Cycling and Transit Connections

3468-3490 Mt Seymour Parkway TIA  
6217.02  
May 2017



Transit frequencies are another factor important to transit uses and convenience. **Table 2.2** illustrates the frequency of service for the routes near the site.

**Table 2.2: Existing Transit Service Frequency**

ROUTE	TWO-WAY SERVICE HEADWAYS (MINUTES)				
	AM PEAK	MID-DAY PEAK	PM PEAK	EVENING	WEEKENDS
211- Phibbs Exchange / Vancouver	10	15	6	30	15
214 - Indian River / Phibbs Exchange	15	30	15	60	30

As per Table 2.2, during the peak hours of traffic, combined service headways are approximately 4-6 minutes, resulting in approximately 10-14 buses per hour. During off-peak periods, combined service headways are approximately 10-20 minutes, resulting in approximately 3-6 buses per hour.

Phibbs Exchange is located approximately 6.5 km west of the site, which provides FTN connections to many locations on the North Shore including Lonsdale, West Vancouver, Capilano University, and Grouse Mountain, as well as to municipalities south of Burrard Inlet including Burnaby and Downtown Vancouver.

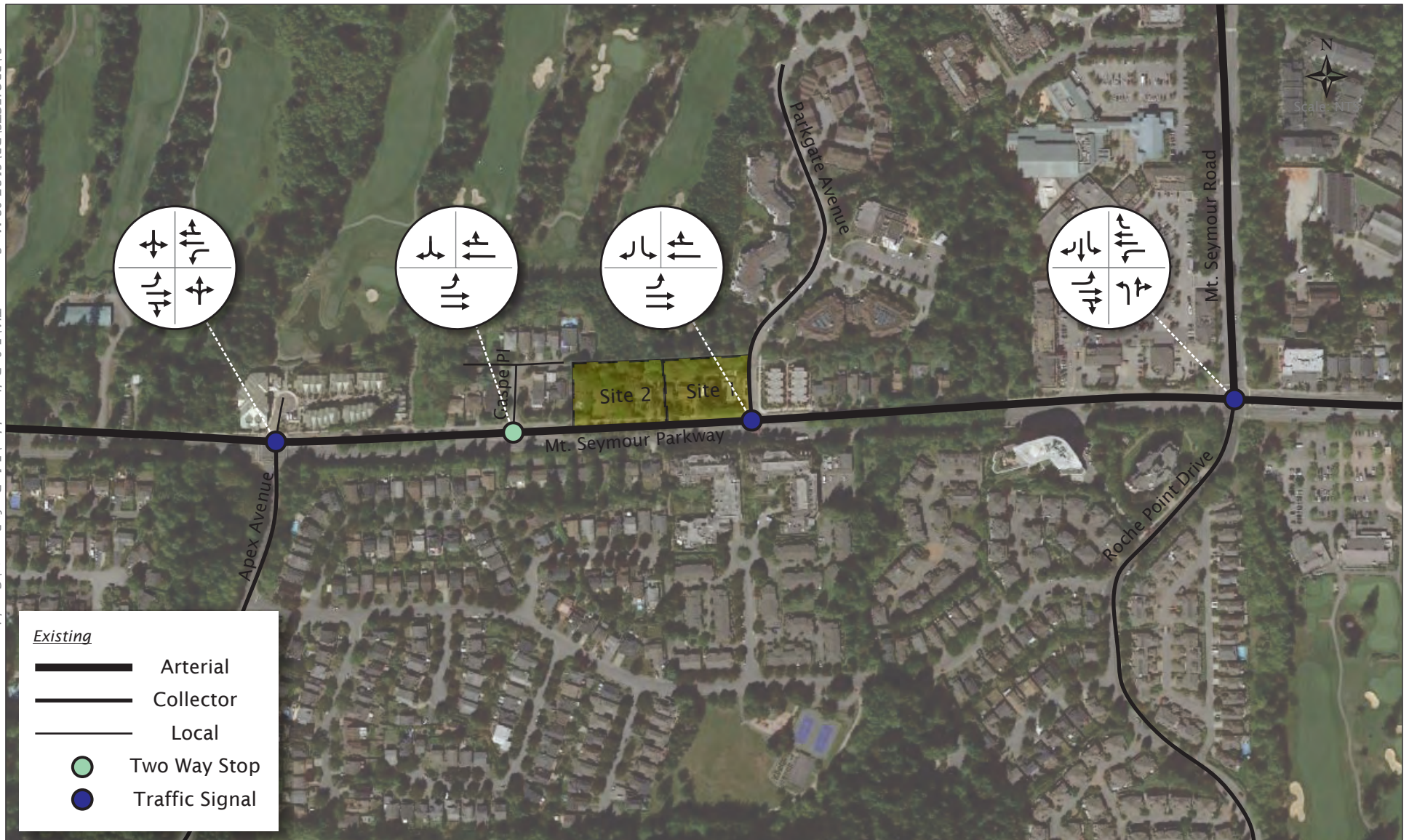
## 2.2 Street Network

**Exhibit 2.2** shows the existing laning and street classifications within the study area. Mt Seymour Parkway is part of TransLink's Major Road Network. **Tables 2.3** and **2.4** summarize the study area intersections and road elements, in the study area.

**Table 2.3: Study Area Elements – Intersections**

STUDY INTERSECTION	EXISTING TRAFFIC CONTROL
Mt Seymour Parkway & Apex Avenue	Signalized
Mt Seymour Parkway & Gaspé Place	Unsignalized
Mt Seymour Parkway & Parkgate Avenue	Signalized
Mt Seymour Parkway & Mt Seymour Road	Signalized





## Exhibit 2.2 Existing Laning and Street Classification

3468-3490 Mt Seymour Parkway TIA  
6217.02  
May 2017



**Table 2.4: Study Area Elements - Roadways**

STREET	DESIGNATION	NUMBER OF TRAVEL LANES	POSTED SPEED	PARKING FACILITIES
Mt Seymour Parkway	Major Arterial	4	60 km/h	No Parking On-street
Mt Seymour Road	Major Arterial	4	50 km/h	No Parking On-street
Apex Avenue	Collector	2	50 km/h	No Parking On-Street
Parkgate Avenue	Collector	2	50 km/h	On-Street
Gaspe Place	Local	2	50 km/h	On-Street

### 2.3 Existing Traffic Volumes

Existing traffic data was collected on Thursday March 30<sup>th</sup> and Saturday April 1<sup>st</sup> 2017. Existing traffic data for the intersection of Gaspe Place and Mt. Seymour Parkway was collected on March 7, 2017.

As advised by the District, the study time periods for the analysis were the weekday AM and PM peak hours and Saturday peak hours. The AM peak hour was found to occur between 8:00 and 9:00 while the PM peak hour occurred between 16:45 and 17:45. The Saturday peak hour was observed between 12:45 and 13:45.

The existing peak hour traffic data for vehicles are illustrated in **Exhibit 2.3 - 2.4** for the Weekday and Saturday peak hours respectively. Pedestrians and cyclists volumes are illustrated in **Exhibit 2.5 - 2.6** for the Weekday and Saturday peak hours respectively.





## Exhibit 2.3 Existing Weekday Peak Hour Volumes

3468-3490 Mt Seymour Parkway TIA  
6217.02  
May 2017





## Exhibit 2.4 Existing Saturday Peak Hour Volumes

3468-3490 Mt Seymour Parkway TIA  
6217.02  
May 2017





## Exhibit 2.5 Existing Weekday Peak Hour Pedestrian & Cycling Traffic Volumes

3468-3490 Mt Seymour Parkway TIA  
6217.02  
May 2017





**Exhibit 2.6**  
Existing Saturday Peak Hour Pedestrian & Cycling Traffic Volumes



**Table 2.5** presents a summary of the two-way peak-hour vehicle movements for the streets in the study area.

**Table 2.5: Existing Peak Hour Roadway Link Volumes**

ROAD LINK	PEAK LINK VOLUMES (VEH/HR)		
	AM	PM	SATURDAY
Mt Seymour Parkway*	1,900	1,700	1,900
Apex Avenue (south leg)	180	200	180
Gaspé Place (north leg)	15	11	15
Parkgate Avenue (north leg)	95	130	105
Mt Seymour Road (north leg)	1,080	1,100	1,110

\* The reported value represents the maximum two way volume between Gaspé Place and Parkgate Avenue.

Mount Seymour Parkway, a Major Arterial, carries the highest volumes, with an approximate two way volume of 1,700 – 1,900 vehicles per peak hour (vph) recorded during the peak hours. Mount Seymour Road (north leg) is also classified as a Major Arterial and carries volumes ranging from 1,080 – 1,110. The volumes recorded are within the typical range for such streets. According to the Transportation Association of Canada (TAC), Major Arterial streets carry volumes ranging from 10,000 to 30,000 vehicles per day (vpd), or about 1,000 - 3000 vehicles per hour.

Parkgate Avenue is classified as a collector street and has peak hour two-way volumes of 95-130 vph. As a collector road in an urban context, these volumes are significantly lower than the typical range of such streets. Urban collector roads generally operate with maximum threshold volumes of up to 12,000 vehicles per day (about 100-1,200 vehicles per hour), though usually operate in the range of 6,000 to 8,000 vpd (600 to 800 vph).

Gaspé Place and Apex Avenue are classified as local roads with vehicle two-way volumes of 10-130 vph.

## 2.4 Existing On-Street Parking on Parkgate Avenue

To understand the current on-street parking utilization fronting the proposed site development, Bunt conducted spot parking demand counts. The parking count was conducted on Thursday, March 30, 2017 and Saturday, April 1, 2017 along Parkgate Avenue between Mt. Seymour Parkway and Parkgate Avenue midblock. Parking demand spot counts were conducted at 9:00AM and 6:00PM for Thursday, March 30, 2017 and at 2:30PM for Saturday, April 1, 2017. The results for the parking spot count are summarized in **Exhibit 2.7** for the AM, PM and Saturday periods.

The parking supply was based on actual in field observations where appropriate, and an assumed vehicle length of 6.5m in areas which did not have high utilization. The total approximate on-street supply for this study area totals 27 spaces.

The parking demand for the AM spot count was observed to be 22% utilization (6 parked vehicles). The parking demand for the PM spot count was observed to be 30% utilization (8 parked vehicles). The Saturday spot count observed a parking utilization of 22% (6 parked vehicles).





## Exhibit 2.7 Parkgate Avenue Existing Parking Supply, Demand & Utilization

3468-3490 Mt Seymour Parkway TIA  
6217.02  
May 2017



### 3. CURRENT PLANS & POLICIES

Within the District of North Vancouver various areas or neighbourhoods have been the subject of considerable land use and transportation planning studies and are undergoing land development and redevelopment. Additionally, Province of British Columbia and the District are on underway on Lower Lynn interchange improvements. The implications of these policies and plans to the site are reviewed in the following section.

#### 3.1 District of North Vancouver

**Official Community Plan & Transportation Plan (2014)** – The OCP sets the policies that guide the development for the District with sections that include land use, transportation and housing. The Transportation Plan describes the existing transportation patterns within the district and provides individual policies, plans and goals for each mode of transportation. The OCP identifies a network of centres and four key growth areas. Plans for three of these four growth areas — Lynn Valley, Lions Gate, and Lynn Creek — are complete, as is a centre plan for Edgemont Village.

The Plan outlines high priority locations for road safety improvements, due to high frequency of driving, cycling and walking crashes. Mt Seymour Parkway was identified for corridor improvements.

The Plan outlines five Policy Directions, which are:

1. Consider neighbourhood liveability in planning of road improvements.
2. Continue to work with stakeholders to improve east-west mobility.
3. Reallocate road space to improve safety and accommodate transit vehicles, cyclists, pedestrians as well as private vehicles, where feasible.
4. Monitor the development of alternative energy vehicles and provide supportive infrastructure, as necessary.
5. Facilitate effective access for trucks and rail to key port, industrial and commercial.

**North Vancouver Bicycle Master Plan (2012)** – this study, produced by the City and District of North Vancouver, was created to reflect evolving priorities and policies in the two municipalities. A number of areas of concern, priority projects and proposed changes to the Bicycle Master plan were developed through the planning and consultation process. While the Mt Seymour Parkway on-street bike route, in the vicinity of the site was not identified as a concern, other sections of Mt Seymour Parkway, and Dollarton Highway were deemed problematic. However, no proposed changes were identified in this plan.

**Maplewood Village Transportation Study (2014)** – This study, conducted by Urban Systems Ltd. was undertaken to assess, at a relatively high level, the anticipated 2014 OCP densities proposed for Maplewood Village. The study concluded that the planned growth could be accommodated on the road network without major upgrades or significant delays. New local roadways as well as modifications to Windridge Drive were proposed, as well as a reduction in travel lanes on Riverside Drive along with



improvements to the pedestrian and cycling networks in order to improve regional connectivity and encourage walking.

### 3.2 Metro Vancouver/TransLink

**North Shore Area Transit Plan (2012)** The North Shore Area Transit Plan was completed in 2012. The Plan was developed with all of the North Shore municipalities and First Nations to develop a long term strategy in concert with community Transportation Plans and Official Community Plans. A key recommendation was passenger and transit improvements for Phibbs Exchange.

**Phibbs Exchange Conceptual Design Study (2013-2014) and Preliminary Design (2015)** – TransLink is planning improvements to Phibbs Exchange located west of Highway 1, within the northwest quadrant of the Main Street/Dollarton Highway interchange. The first phase of the study identified three options for improving the Phibbs bus exchange, which is an important transit node for the North Shore. TransLink has completed preliminary design of Phibbs Exchange, based on the concept that was developed in 2013. This design work was jointly funded and managed by TransLink, MoTI and the District of North Vancouver.

**Figure 3.1** below shows the preferred final concept.

The Mayors' Council received funding in June 2016 from the Government of Canada and the Province of BC towards 14 capital projects from the first 2-3 years of the 10-Year Vision for Metro Vancouver Transit and Transportation. Part of the funding will be invested in the bus and rail network for subsequent phases of the 10-Year Vision by upgrading key transit exchanges, including Phibbs Exchange operational improvements, customer amenities, and safety and accessibility enhancements.



**Figure 3.1: Preferred Phibbs Exchange Concept Plan (2014)**

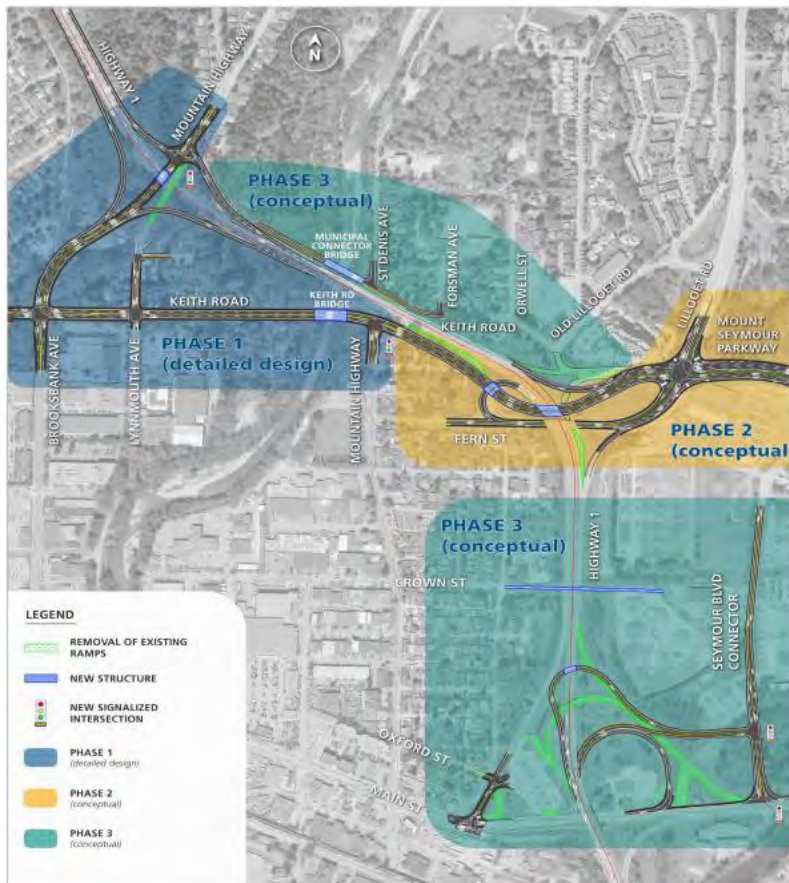
Source: TransLink, Phibbs Exchange Conceptual Design Study, Final Report, March 2014

### 3.3 BC Ministry of Transportation & Infrastructure Highway 1 Lower Lynn Improvements (2015-ongoing)

The BC MoTI, in partnership with the District of North Vancouver and the Squamish Nation, have been planning interchange improvements in the Lower Lynn area, between Mountain Highway and Main/Dollarton for the last several years and \$198m in funding has been allocated for these improvements. **Figure 3.2** illustrates the current design concepts for the three interchanges.



Figure 3.2: Highway 1 Lower Lynn Improvements



Source: [http://www2.gov.bc.ca/assets/gov/driving-and-transportation/transportation-infrastructure/projects/hwy1-lower-lynn/mountain/displayboards\\_january\\_19\\_2016.pdf](http://www2.gov.bc.ca/assets/gov/driving-and-transportation/transportation-infrastructure/projects/hwy1-lower-lynn/mountain/displayboards_january_19_2016.pdf)

The Lower Lynn Improvements includes the following projects

- Phase 1: Mountain Highway Interchange & new E. Keith Road/ Brooksbank Avenue/ Mountain Highway Intersection (*estimated completion 2018*);
- Phase 2: Mount Seymour Parkway/ Lillooet Road (Fern Street) Interchange (*estimated completion 2021*)
- Phase 3: Main /Dollarton Highway Interchange & Seymour Boulevard Connector & Keith Road/ St Denis Avenue improvements (*estimated completion 2021*)

When the conceptual design of the Main/Dollarton interchange in the illustration is implemented, it will remove many of the existing points of congestion between Mt Seymour Parkway and points south (City of Vancouver) and west (North Shore districts/municipalities). It will add redundancy for access to and from the Ironworkers Memorial Bridge while separating local 'through' traffic from vehicles accessing the bridge and significantly improve access between the City, the District, and Mt Seymour Parkway.



## 4. PROPOSED DEVELOPMENT

### 4.1 Allaire Group – 3428, 3436 & 3464 Mt Seymour Parkway

#### 4.1.1 Development Content

Allaire's proposed development is comprised of four buildings, with 29 units and a total GFA of 3,429.6 sq m. The majority of the 29 units (79%) are 3-bedroom units, while the remainder are two-bedroom and den. The site layout as of May 2017 is shown in **Exhibit 4.1**. As shown, there are two buildings fronting Mt. Seymour Parkway with 9 townhome units each and two rear buildings with 5 and 6 units. As the development progresses, there may be some minor modification to this plan and layout; at present this is not anticipated to be significant.

The site will have parking located in an underground parkade, with driveway access off a rear lane accessed from Parkgate Avenue.

#### 4.1.2 Off-street Parking Spaces

The District of North Vancouver's Zoning Bylaw has been used as follows:

- Off-street parking requirement (*Zoning Bylaw 3210, Part 10: Section 1001*) base rate for residential uses; and,
- Accessible stall requirement (*Zoning Bylaw 3210, Part 10: Section 1004*) where one disability space is required for every 100 parking spaces or part thereof provided.

The required and proposed parking supply is summarized in **Table 4.1**.

**Table 4.1: Allaire Off-Street Vehicle and Bicycle Parking Requirements**

USE	UNITS	SQ M GFA	PARKING TYPE	BYLAW PARKING RATE	BYLAW REQUIREMENT (SPACES)	PARKING PROVIDED
Townhome	29	3,429.6	Vehicle – Overall	2 per dwelling unit	58	59
			Vehicle – disability	1 per 100 required spaces	1	1

As illustrated in Table 4.1, the development's overall parking requirements, as per the District's Zoning bylaw for the site are 58 spaces, of which one is a disability space. The development will provide 59, including one disability space, and is therefore in compliance with the bylaw.

As this development is in the form of townhomes, where each unit has direct access to the parkade and/or outside, all long term bicycle storage will be provide inside each unit.



**Exhibit 4.1**

**Site Development Plan: Allaire Group Development**





#### 4.1.3 Site Plan Review

At the time of the release of this report, Allaire's development at 3428-2464 Mt Seymour Parkway has not progressed to the same stage as Tatla Development's 3464-3490 Mt Seymour Parkway. Allaire Properties will work with the District to comply with all service and loading regulations as these plans are further advanced.

Parking for the development will be accessed from the rear laneway and an underground parkade whose access is located at the northeast corner of the property. To ensure sufficient maneuverability for typical residential vehicles, a mid-to-large sized passenger vehicle (2010 Ford Taurus) was used as the design vehicle to test vehicle turning path requirements for access to /from the laneway and parkade driveway (see **Exhibit 4.2**).

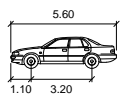
Delivery/moving vehicles and waste/recycling collection will also operate via the rear lane and access the underground parkade. Bins will be collected from the underground parkade and brought to larger pick up vehicles, who, as shown in **Exhibit 4.3**, will be able to maneuver in and out of the rear laneway.





[Based on Drawing A-1.101 from Allaire Group dated March 2017]

[Issued for Discussion; not for Construction]



P	meters
Width	: 2.00
Track	: 2.00
Lock to Lock Time	: 6.0
Steering Angle	: 35.9

## Exhibit 4.2 Allaire Passenger Vehicle Access

6127.02

May 2017

Scale ##### on Letter

Mt. Seymour TIA  
Prepared by MC



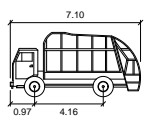


S:\PROJECTS\DR\6127-02 Mt. Seymour TIA\4.0 Analysis & Design\4.1 ACAD\Swpl\_Pth\20170504\_6127-02\_AT\_Loading\_V01.dwg  
2017/05/05 14:08, Plotted by Matthew Chou

# Inbound Movement



[Issued for Discussion; not for Construction]  
[Based on Drawing A-1.101 from Allaire Group dated March 2017]



Garbage Compactors  
Width : 2.32  
Track : 2.32  
Lock to Lock Time : 6.0  
Steering Angle : 40.8

# Outbound Movement



[Issued for Discussion; not for Construction]  
[Based on Drawing A-1.101 from Allaire Group dated March 2017]

## Exhibit 4.3 Allaire Garbage Truck Access

6127.02

May 2017

Scale 1:500 on Letter

Mt. Seymour TIA  
Prepared by MC





## 4.2 Tatla Development -3468-3490 Mt Seymour Parkway

### 4.2.1 Development Content

Tatla's proposed development is comprised of five buildings, with 27 units and a total GFA of 3,214.4 sq m. the majority of the units (95%) are 3-bedroom, while the remainder are 2-bedroom and 2-bedroom plus den. The site layout as of May 2017 is shown in **Exhibit 4.4**. As shown, there are two buildings fronting Mt Seymour Parkway with 5 townhome units each, two rear buildings with 5 units each and a fifth building fronting Parkgate Avenue with 7 units. As the development progresses, there may be some minor modification to this plan and layout; at present this is not anticipated to be significant.

The site will have parking located in an underground parkade, with driveway access off a rear lane accessed from Parkgate Avenue. The developer will also consider providing a recessed parking/service bay on the west side of Parkgate Avenue south of the laneway access.

### 4.2.2 Off-street Parking Spaces

The District of North Vancouver's Zoning Bylaw has been used as follows:

- Off-street parking requirement (*Zoning Bylaw 3210, Part 10: Section 1001*) base rate for residential uses; and,
- Accessible stall requirement (*Zoning Bylaw 3210, Part 10: Section 1004*) where one disability space is required for every 100 parking spaces or part thereof provided.

The required and proposed parking supply is summarized in **Table 4.1**.

**Table 4.2: Tatla Off-Street Vehicle and Bicycle Parking Requirements**

USE	UNITS	SQ M GFA	PARKING TYPE	BYLAW PARKING RATE	BYLAW REQUIREMENT (SPACES)	PARKING PROVIDED
Townhome	27	3,214.4	Vehicle - Overall	2 per dwelling unit	54	54
			Vehicle - disability	1 per 100 required spaces	1	1

As illustrated in Table 4.1, the development's overall parking requirements, as per the District's Zoning bylaw for the site are 54 spaces are required, including 1 disability stall. The development will provide 54 spaces, including 1 disability stall, and is therefore in compliance with the bylaw.

As this development is in the form of townhomes, where each unit has direct access to the parkade and/or outside, all long term bicycle storage will be provide inside each unit. A total of 10 short-term bicycle parking spaces will also be provided.





## Exhibit 4.4 Site Development Plan: Tatla Development

3468-3490 Mt Seymour Parkway TIA  
6217.02  
May 2017



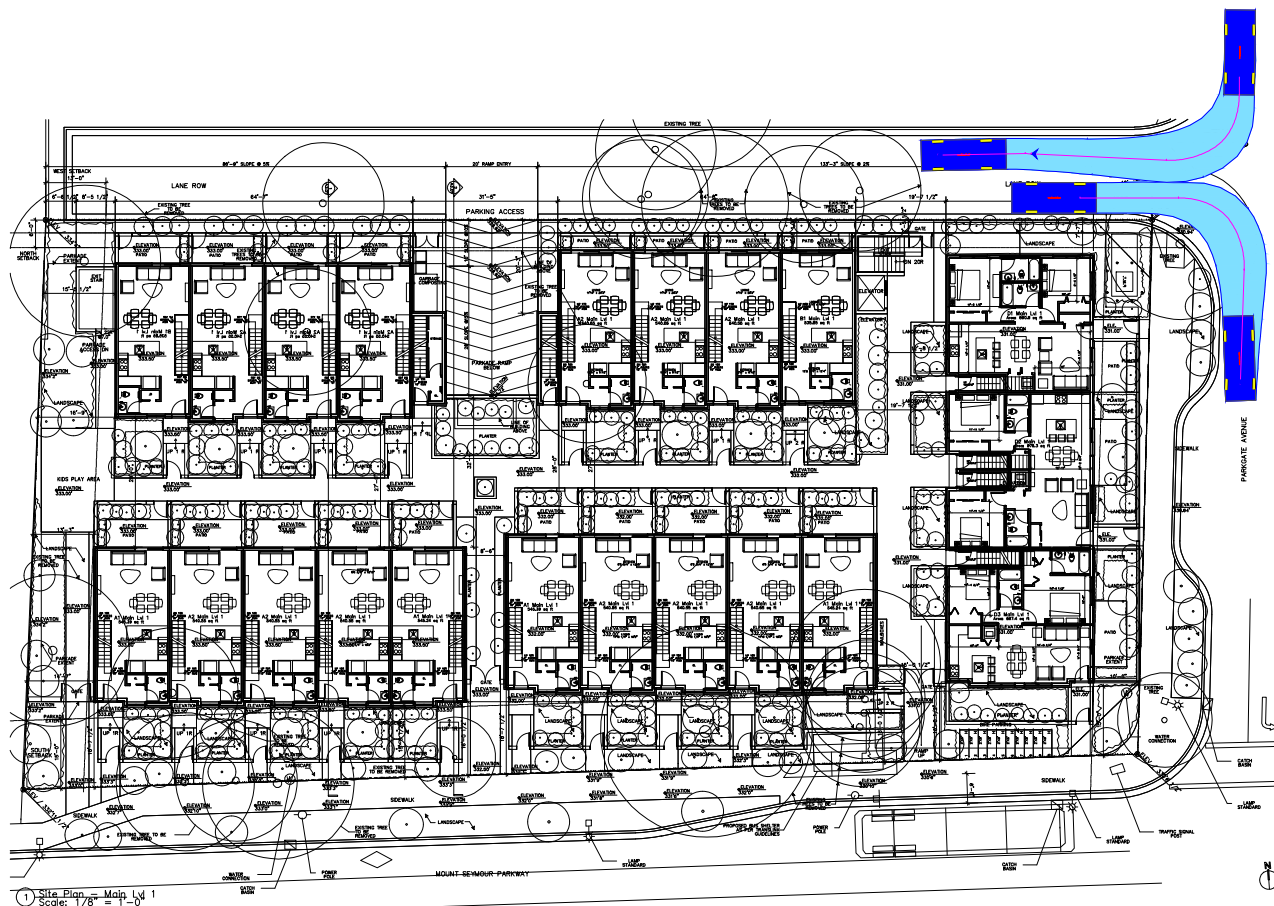
#### 4.2.3 Site Plan Review

Parking for the development is proposed to be accessed from the rear laneway. A mid to large size passenger vehicle (2010 Ford Taurus) was used as the design vehicle to test vehicle turning path requirements for access to /from the laneway and parkade driveway.

Parking for the development will be accessed from the rear laneway and an underground parkade whose access is located at the northeast corner of the property. To ensure sufficient maneuverability for typical residential vehicles, a mid-to-large sized passenger vehicle (2010 Ford Taurus) was used as the design vehicle to test vehicle turning path requirements for access to /from the laneway and parkade driveway (see **Exhibit 4.5**).

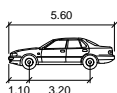
Delivery/moving vehicles and waste/recycling collection will also operate via the rear lane and access the underground parkade. Bins will be collected from the underground parkade and brought to larger pick up vehicles, who, as shown in **Exhibit 4.6**, will be able to maneuver in and out of the rear laneway.





[Based on Drawing A1.01 from STUART HOWARD ARCHITECT INC. dated March 31, 2017]

[Issued for Discussion; not for Construction]



P

Width	: 2.00
Track	: 2.00
Lock to Lock Time	: 6.0
Steering Angle	: 35.9

## Exhibit 4.5 Tatla Passenger Vehicle Access

6127.02 May 2017 Scale 1:500 on Letter

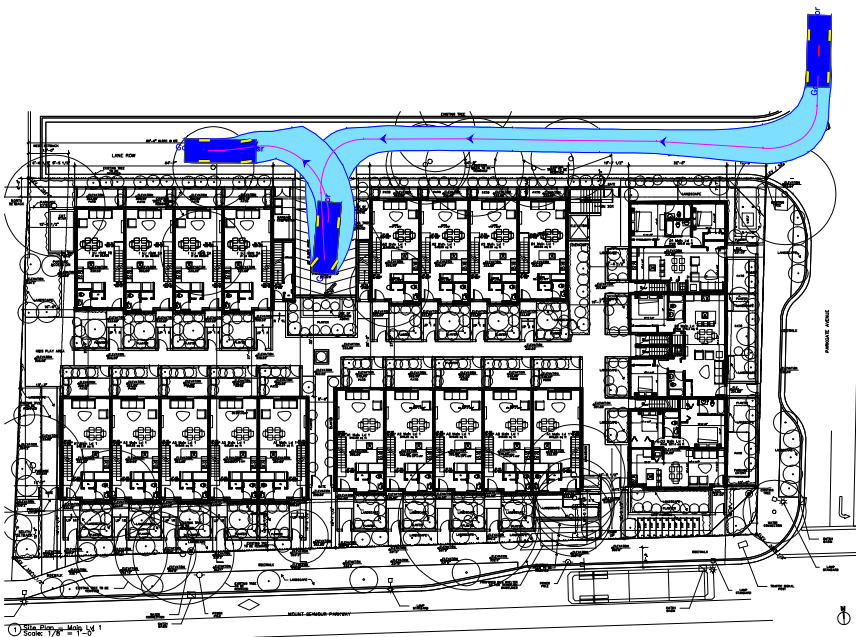
Mt. Seymour TIA  
Prepared by MC



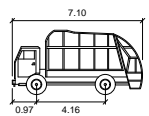


S:\PROJECTS\DR\6127-02 Mt. Seymour TIA\4.0 Analysis & Design\4.1 ACAD\Swpt\_Pth\20170504\_6127-02\_AT>Loading\_V01.dwg  
2017/05/05 16:23, Plotted by Matthew Chou

# Inbound Movement

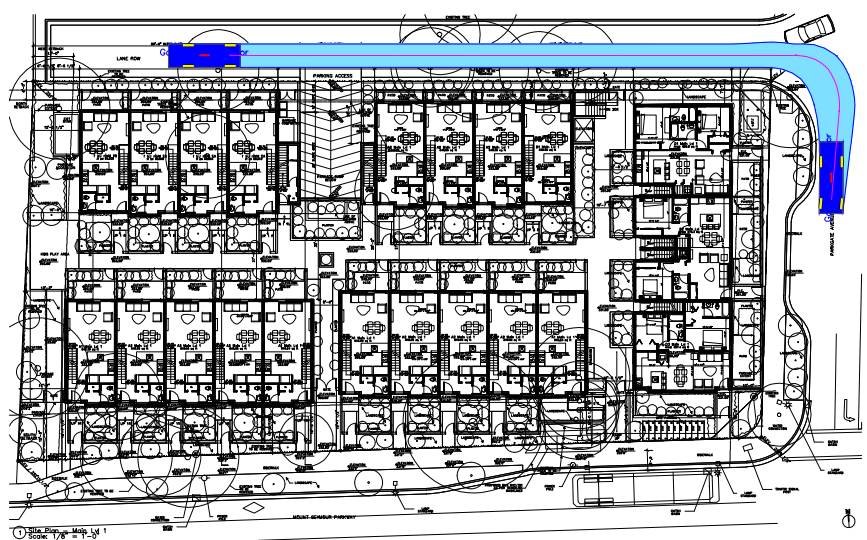


[Issued for Discussion; not for Construction]  
[Based on Drawing A1.01 from STUART HOWARD ARCHITECT INC. dated March 31, 2017]



Garbage Compactors	
Width	: 2.32
Track	: 2.32
Lock to Lock Time	: 6.0
Steering Angle	: 40.8

# Outbound Movement



[Issued for Discussion; not for Construction]  
[Based on Drawing A1.01 from STUART HOWARD ARCHITECT INC. dated March 31, 2017]

## Exhibit 4.6 Tatla Garbage Truck Access

6127.02 May 2017 Scale 1:750 on Letter

Mt. Seymour TIA  
Prepared by MC





## 5. OFF-SITE IMPROVEMENTS

### 5.1 Mount Seymour Parkway

#### 5.1.1 Speed

Recorded speed observations taken approximately 1 km to the west of the development sites demonstrated that vehicles were traveling over the posted speed limit of 60 km/hr. The results of this formal assessment (A sample size of 100 vehicles for both eastbound and westbound directions was collected) corroborates site observations, where the average speed was determined to be 67 and 68 km/hr for the westbound and eastbound directions respectively. The 85<sup>th</sup> percentile speeds were also calculated to be 72 and 73 km/hr for the westbound and eastbound directions respectively, approximately 20% higher than the posted speed limit.

Road design – and to a lesser extent, signage – can have a significant impact on vehicle speeds. A number of proven options are possible for the District to consider along Mt Seymour Parkway to reduce speeds:

- (i) Change the posted speed limit signage from 60 km/hr to 50 km/hr. This will likely see a far greater level of non-compliance and percentage of vehicles technically speeding, but it will likely also reduce both average and median speeds.
- (ii) Narrow the lane widths accordingly for both westbound and eastbound directions (and increase width of cycling facilities).
- (iii) Create more ‘road friction’ with on-street parking, on-street bus facilities, or signalized intersections.
- (iv) Using signal timing to prevent a green wave through multiple intersections (potential only in off-peak direction to prevent capacity concerns).
- (v) Narrow the 4-lane cross section along Mt. Seymour Parkway into a 2-lane cross section.
- (vi) Provide more awareness and speed radar feedback signage along Mt. Seymour Parkway

#### 5.1.2 Sightlines

Existing sightlines at the intersection of Mount Seymour Parkway and Parkgate Avenue were reviewed, with the following qualitative conclusions:

- Eastbound left (Mount Seymour Parkway to Parkgate Avenue) vehicles have clear sightlines of oncoming westbound vehicles as per the straight alignment of the road and lack of median landscaping, as per **Figure 5.1**; and,
- Southbound right (Parkgate Avenue to Mount Seymour Parkway) vehicles have clear sightlines as per **Figure 5.1** and **Figure 5.2**. As shown there are no trees or other obstructions to limit visibility to on-coming traffic, and the building is set back a sufficient distance.



**Figure 5.1 Northeast corner of Mt Seymour Parkway & Parkgate Ave**



**Figure 5.2 Northeast corner (looking east) of Mt Seymour Parkway & Parkgate Ave**



The existing sightlines at the intersection of Mount Seymour Parkway and Gaspe Place were also reviewed, with the following qualitative conclusions:

- Eastbound left (Mount Seymour Parkway to Gaspe) vehicles have reasonable sightlines of oncoming westbound vehicles as per the straight alignment of the road and lack of median landscaping, as per **Figure 5.3**. However there is a grade change between Parkgate Avenue and Gaspe Place (approximately 75m east of Gaspe Place) which could limit the visibility of oncoming westbound vehicles, which would be more concerning at high speeds; and,
- Southbound right and left (to Mount Seymour Parkway) vehicles have limited sightlines as per **Figure 5.4**. As shown the property on the northwest corner has a retaining wall and / or landscaping 1-3m from the sidewalk, which in combination with the trees and hedges could limit visibility to on-coming westbound traffic.



**Figure 5.3 Mount Seymour Parkway and Gaspe Place**



**Figure 5.4 Mount Seymour Parkway and Gaspe Place (looking south)**



### 5.1.3 U-turning

The district has received feedback that there is above average level of U-turning on this section of Mount Seymour Parkway. There are a few measures which would work to discourage or prevent this from occurring, as discussed below:

- Provide high visibility street signs such that way-finding is clear to those not familiar to the area
- Provide no U-turn signage;
- Remove vehicle/ driveway access from mid-block destinations, where a median is present. The existing development sites have multiple single family homes with driveways fronting Mount Seymour Parkway, which can only be accessed from the westbound lane of traffic, as the centre median prevents mid-block eastbound left turns to every driveway.



#### 5.1.4 Multi-modal Signage and Pavement Markings

As shown in Figure 5.2, the on-street cycling lane markings on Mt Seymour Parkway are showing signs of wear, and not highly visible. Visible pavement markings make a huge difference in the safety, purpose, and performance of pedestrian and bike lane marking programs.

The district could consider coloured or thermoplastic pavement within a bicycle lanes where these cross study area streets, to increase the visibility of the facility and identify potential areas of conflict. The coloured pavement can be applied to an entire corridor, or as a spot treatment at a conflict area, or intersection crossing marking.

While the intersections of Mount Seymour Parkway & Mount Seymour Road / Parkgate Avenue are signalized with marked pedestrian crossings, the intersection of Mount Seymour Parkway & Gaspé Place is not signalized nor has any pedestrian crossing markings. The District could undertake a pedestrian crossing control warrant at this location. The nearest marked/ signalized crossing opportunities are approximately 200m to the west and east.

#### 5.1.5 Site Access

The existing sites have most vehicle access off Mount Seymour Parkway, while one driveway is located 2-3m from the northwest corner of Mount Seymour Parkway & Parkgate Avenue. These single family home driveways have little visibility due to heavy wooded/ hedge landscaping.

With future redevelopment of the site, these driveways off Mount Seymour Parkway & Parkgate Avenue will be removed, and all vehicles will access the sites from a rear laneway off Parkgate Avenue. This will eliminate multiple points of conflict on an arterial road, increasing safety conditions, by channelling all traffic to a single signalized intersection.

There is consideration by the District to extend the rear laneway, from Parkgate Avenue to connect with Gaspé Place. With this potential configuration, the District would also consider modifying access at Mount Seymour Parkway & Gaspé Place to right-in/right-out only. Further discussion on the advantages and disadvantage of this from a traffic operations perspective is outlined in **Section 7.3**.

## 5.2 Rear Laneway

As part of the rezoning process, in co-ordination with District staff, appropriate improvements along the property lines of the sites, will be made.

Special consideration should be made to the sight lines of the laneway, in regards to landscaping with respect to sight lines to maintain a clear line of sight at a driver's eye level (1 m-1.5m). This could be ensured by not allowing on-street parking stalls (adjacent to the site's access) and planting landscaping along this street edge (east of the site's access) that would not obstruct sight lines.

In the interim, this rear laneway will have bollards at the west end that will allow only for bikes and pedestrian connections. Ultimately, when the full redevelopment of Gaspé Place occurs, this lane will connect from Parkgate Avenue to Gaspé Place. This will be discussed in more detail in **Section 7.3**.



## 6. TRAFFIC OPERATIONS ASSESSMENT

### 6.1 Traffic Analysis Assumptions

The existing condition models were prepared based on available traffic count data that Bunt collected as outlined in Section 2. The raw traffic counts were adjusted to balance volumes between intersections in the corridor.

The District of North Vancouver provided the current signal timing plans for our study area. The signal timing plans were used for the existing conditions analysis. The following intersection signal timing plans were included:

- Mt. Seymour Parkway / Apex Avenue
- Mt. Seymour Parkway / Parkgate Avenue
- Mt. Seymour Parkway / Mt. Seymour Road

The three signalized intersection along Mt. Seymour Parkway is coordinated with an 80 second cycle length for both the AM, PM and Saturday periods. The 80 second cycle length was maintained throughout the existing and future analysis.

According to the signal timing plan, the intersection at Parkgate and Mt. Seymour Parkway assumed a permitted eastbound left-turn for the AM, PM and Saturday periods. Similarly, the intersection at Mt. Seymour Road and Mt. Seymour Parkway assumed a protected permitted eastbound left-turn for all three time periods as well. These assumptions will also be used for the future scenarios.

### 6.2 Existing Operations

#### 6.2.1 Performance Thresholds

The existing operations of study area intersections and access points were assessed using the methods outlined in the 2000 Highway Capacity Manual (HCM), using the Synchro 9 analysis software. The traffic operations were assessed using the performance measures of Level of Service (LOS) and volume-to-capacity (V/C) ratio.

The LOS rating is based on average vehicle delay and ranges from “A” to “F” based on the quality of operation at the intersection. LOS “A” represents optimal, minimal delay conditions while a LOS “F” represents an over-capacity condition with considerable congestion and/or delay. Delay is calculated in seconds and is based on the average intersection delay per vehicle.

**Table 6.1** below summarizes the LOS thresholds for the five Levels of Service, for both signalized and unsignalized intersections.



**Table 6.1: Intersection Level of Service Thresholds**

LEVEL OF SERVICE	AVERAGE CONTROL DELAY PER VEHICLE (SECONDS)	
	SIGNALIZED	UNSIGNALIZED
A	$\leq 10$	$\leq 10$
B	$>10$ and $\leq 20$	$>10$ and $\leq 15$
C	$>20$ and $\leq 35$	$>15$ and $\leq 25$
D	$>35$ and $\leq 55$	$>25$ and $\leq 35$
E	$>55$ and $\leq 80$	$>35$ and $\leq 50$
F	$>80$	$>50$

Source: Highway Capacity Manual

The volume to capacity (V/C) ratio of an intersection represents ratio between the demand volume and the available capacity. A V/C ratio less than 0.85 indicates that there is sufficient capacity to accommodate demands and generally represents reasonable traffic conditions in suburban settings. A V/C value between 0.85 and 0.95 indicates an intersection is approaching practical capacity; a V/C ratio over 0.95 indicates that traffic demands are close to exceeding the available capacity, resulting in saturated conditions. A V/C ratio over 1.0 indicates a marginally congested intersection during that peak period where drivers may have to wait through more than one signal cycle to clear the intersection. In downtown and Town Centre contexts, during peak demand periods, V/C ratios over 0.90 and even 1.0 are common.

As directed by the District of North Vancouver, the performance thresholds that were used to trigger consideration of roadway or traffic control improvements to support roadway or traffic control improvements employed in this study are listed below:

#### Signalized Intersections:

- Overall intersection Level of Service = LOS D or better;
- Overall intersection V/C ratio = 0.85 or less;
- Individual movement Level of Service = LOS E or better; and,
- Individual movement V/C ratio = 0.90 or less.

#### Unsignalized Intersections and Roundabouts:

- Individual movement Level of Service = LOS E or better, unless the volume is very low in which case LOS F is acceptable.

In interpreting of the analysis results, the HCM methodology reports performance differently for various types of intersection traffic control. In this report, the performance reporting convention is as follows:

- For signalized intersections: HCM 2000 output for overall LOS and V/C as well as individual movement LOS and V/C is reported. 95th Percentile Queues are reported as estimated by Synchro.
- For unsignalized two-way stop controlled intersections: HCM 2000 LOS and V/C output is reported just for individual lanes as the HCM methodology does not report overall performance.



The performance reporting conventions noted above have been consistently applied throughout this document and the detailed outputs are provided in **Appendix B**.

### 6.2.2 Operational Analysis Results

The existing conditions analysis is summarized in **Exhibits 6.1-6.3** for the AM, PM and Saturday periods respectively, and the detailed Synchro analysis printouts are provided in Appendix B at the end of this report. Based on the results of the AM, PM and Saturday peak hour analysis, the existing conditions for the intersections within the study area are operating within acceptable thresholds.

Results for specific locations are provided below, for any movements or overall intersections which do not satisfy the District's performance criteria.

#### *Unsignalized Intersections*

For all unsignalized intersections within the study area, they are currently operating acceptably with minimum queues and delays.

#### *Mt. Seymour Road / Mt. Seymour Parkway*

The intersection of Mt. Seymour Road and Mt. Seymour Parkway currently experience long eastbound left-turn queues that exceed the turning storage length. The AM and PM period shows eastbound left-turn queues of 75m and 84m respectively, which exceed the 70m storage length.

Similarly, during the PM and Saturday periods, the southbound left-turn shows queues of 50m and 56m respectively, which exceed the 50m storage length. The overall intersection operates at LOS C or better and v/c of 0.76 or better for all three time periods. The critical movement during the AM period is the eastbound left with a v/c of 0.93 and LOS D and for the PM period, a v/c of 0.88 and LOS C.

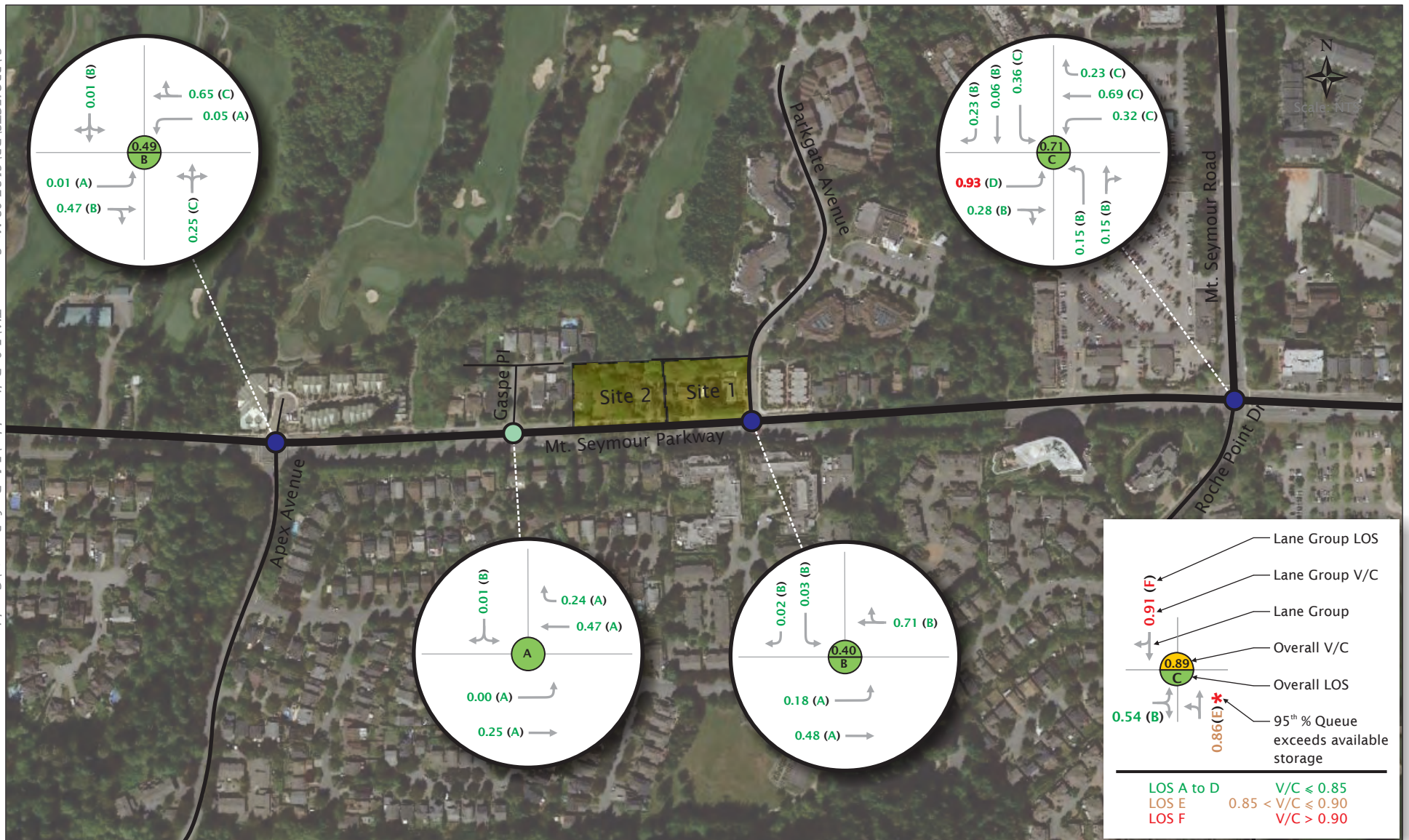
#### *Apex Avenue / Mt. Seymour Parkway*

The intersection of Apex Avenue and Mt. Seymour Parkway is currently operating at acceptable levels with minimal queues and delays. The overall intersection operates at LOS B and v/c of 0.49 or better for all three time periods. The critical movement during the AM period is the westbound through and operates at a v/c of 0.65 and LOS C. The critical movement during the PM and Saturday periods is the eastbound through movement at with LOS B and v/c of 0.64 and 0.54 respectively.

#### *Parkgate Avenue / Mt. Seymour Parkway*

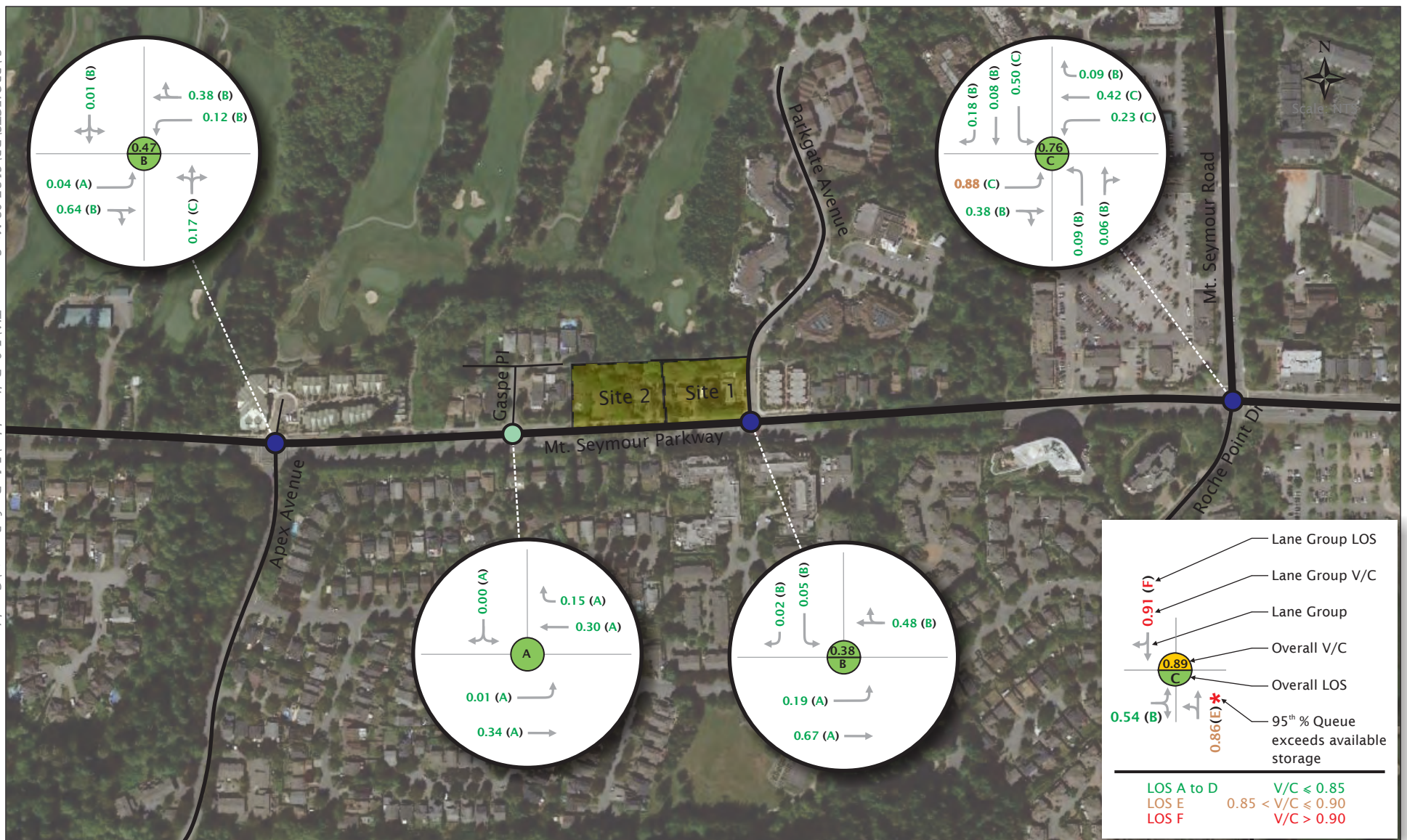
The intersection of Parkgate Avenue and Mt. Seymour Parkway experiences acceptable traffic operations with minimal queues and delays. The overall v/c ratios for all three periods are below 0.40 and LOS B or better. The individual movements for all three scenarios operate at LOS B or better and v/c of 0.71 or better.





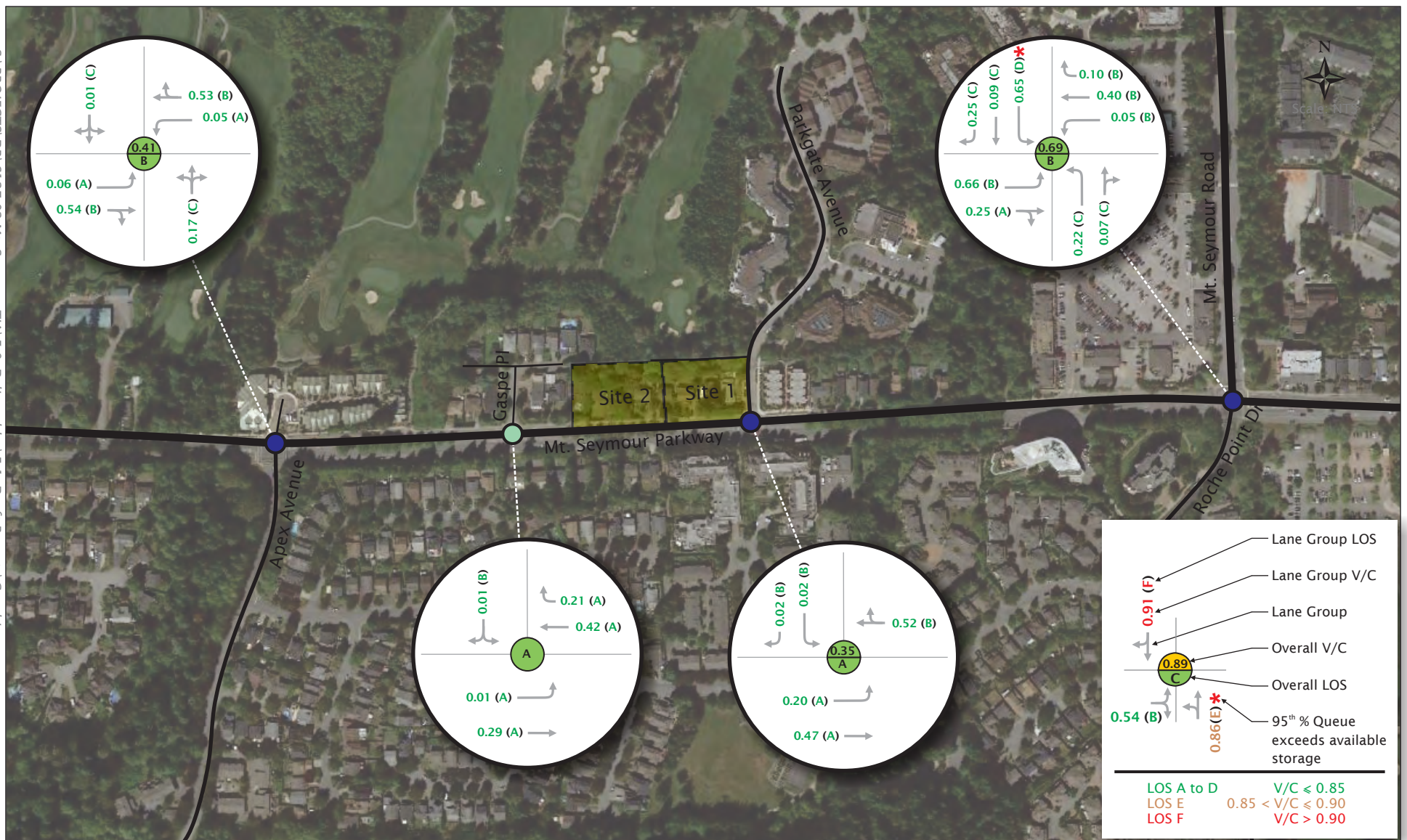
**Exhibit 6.1**  
**Existing Weekday AM Operations**





**Exhibit 6.2**  
**Existing Weekday PM Operations**





**Exhibit 6.3**  
**Existing Saturday Operations**



## 7. FUTURE TRAFFIC CONDITIONS

Local levels of service and v/c ratios show negligible change between the existing (2017) AM, PM, and Saturday peaks and 'worst case scenario' 2030 Total Traffic AM, PM, and Saturday peak conditions. All intersections operate within acceptable parameters during all peak hours and all horizon scenarios.

This and other changes to individual movement capacities are the result of assumed background volume increases of 0.5% p.a., and in some cases, left-turning queues exceeding their allotted bays and partially obstructing through movement capacity.

### 7.1 Traffic Forecasts

#### 7.1.1 Background Traffic Forecasts

Background traffic forecasts include traffic that would be present on the road network if the site did not redevelop. For the purpose of this transportation assessment and based on consultation with District staff, the project's background and traffic (without project) conditions will include an assumed 0.5% per annum background growth rate applied to the peak hour traffic volumes.

The site area is currently occupied by 8 single-family homes. This existing site is currently producing between 6-8 trips during the AM, PM and Saturday periods. The affect of these existing site trips are essentially negligible and for conservative purposes, were not stripped out in the future analysis. **Table 7.1** summarizes the existing site trips being generated for the two proposed parcels under study.

**Table 7.1: Existing Peak Hour Site Trips**

LAND USE	UNITS	AM PEAK HOUR			PM PEAK HOUR			SAT PEAK HOUR		
		IN	OUT	TOTAL	IN	OUT	TOTAL	IN	OUT	TOTAL
3428-3464 Mt Seymour Parkway	4 single-family homes	1	2	3	3	1	4	3	1	4
3468-3490 Mt Seymour Parkway	4 single-family homes	1	2	3	3	1	4	3	1	4
		2	4	6	6	1	8	6	1	8

The project completion is anticipated to be the year 2022 (build out for the development). The future horizon year of 2030 will also be analyzed. The 2022 and 2030 Background Traffic volumes are summarized in **Exhibit 7.1- 7.4**.





## Exhibit 7.1 Background 2022 Weekday Peak Hour Volumes

3468-3490 Mt Seymour Parkway TIA  
6217.02  
May 2017





## Exhibit 7.2 Background 2022 Saturday Peak Hour Volumes

3468-3490 Mt Seymour Parkway TIA  
6217.02  
May 2017





## Exhibit 7.3 Background 2030 Weekday Peak Hour Volumes

3468-3490 Mt Seymour Parkway TIA  
6217.02  
May 2017





## Exhibit 7.4 Background 2030 Saturday Peak Hour Volumes

3468-3490 Mt Seymour Parkway TIA  
6217.02  
May 2017



### 7.1.2 Site Traffic

#### *Trip Generation*

The trip generation for the project was calculated based on Institute of Transportation Engineers (ITE) Trip Generation Handbook (9<sup>th</sup> Edition) for Condominium / Townhome land use (Code 230). For the purpose of this analysis, the Saturday peak hour used the same rates as the PM peak hour. The three rates are summarized in **Table 7.1** below.

**Table 7.1: Peak Hour Vehicle Trip Rates**

LAND USE	UNITS	AM PEAK HOUR			PM PEAK HOUR			SAT PEAK HOUR		
		IN	OUT	TOTAL	IN	OUT	TOTAL	IN	OUT	TOTAL
Tatla Townhomes	27	0.07	0.37	0.44	0.35	0.17	0.52	0.35	0.17	0.52
Allaire Townhomes	29	0.07	0.37	0.44	0.35	0.17	0.52	0.35	0.17	0.52
	56									

**Table 7.2** summarizes the anticipated future site generated vehicle trips for the proposed development based on the above rates.

**Table 7.2: Estimated Peak Hour Site Vehicle Trips**

LAND USE	AM PEAK HOUR			PM PEAK HOUR			SAT PEAK HOUR		
	IN	OUT	TOTAL	IN	OUT	TOTAL	IN	OUT	TOTAL
Tatla Townhomes	2	10	12	9	5	14	9	5	14
Allaire Townhomes	2	11	13	10	5	15	10	5	15
	4	21	25	19	10	29	19	10	29

As shown in Table 6.3, the site is expected to generate approximately 25 vehicle trips during the AM peak hour (4 inbound and 21 outbound) which equates to an average of 2-3 vehicles per minute in the peak morning hour. In the PM and Saturday peak hours 29 trips are expected (19 inbound and 10 outbound) which equates to an average of between 2-3 vehicles a minute during the most active traffic periods of the day.



### *Trip Distribution & Assignment*

The project site traffic was distributed onto the study area road network based on observed area traffic patterns. The trip distribution utilized for this analysis is shown in **Table 7.3** below.

**Table 7.3: Estimated Trip Distribution**

ORIGIN/DESTINATION	AM PEAK HOUR		PM PEAK HOUR		SAT PEAK HOUR	
	IN (%)	OUT (%)	IN (%)	OUT (%)	IN (%)	OUT (%)
North / Mt. Seymour Road	20%	20%	20%	20%	25%	20%
North / Parkgate Avenue	3%	2%	3%	3%	2%	5%
North / Apex Avenue	0%	0%	0%	0%	0%	0%
East / Mt. Seymour Parkway	35%	25%	25%	35%	25%	25%
West / Mt. Seymour Parkway	30%	45%	45%	30%	40%	40%
South / Apex Avenue	5%	3%	3%	5%	3%	5%
South / Roche Point Drive	7%	5%	4%	7%	5%	5%
<b>TOTAL</b>	<b>100%</b>	<b>100%</b>	<b>100%</b>	<b>100%</b>	<b>100%</b>	<b>100%</b>

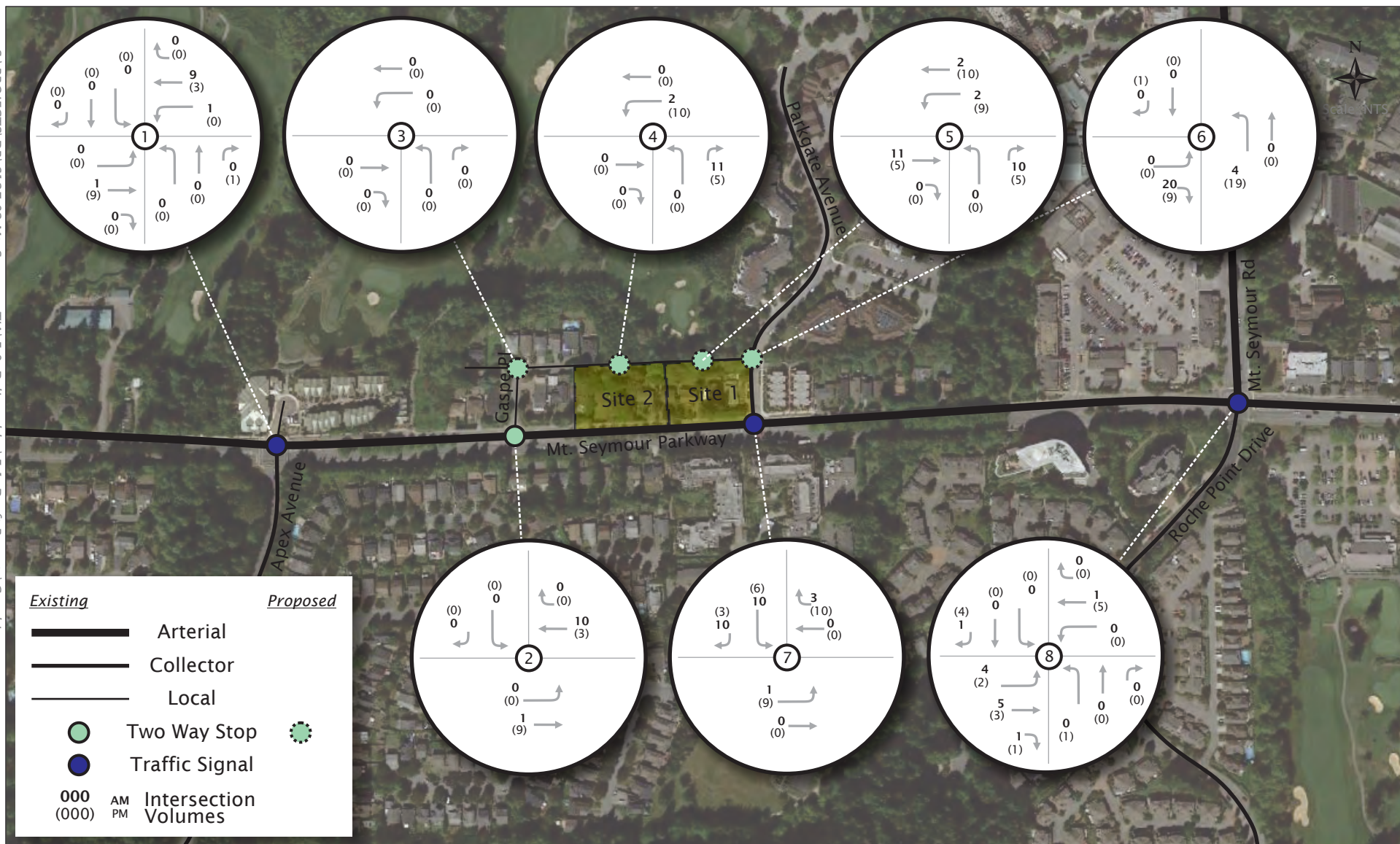
**Exhibit 7.5** and **7.6** illustrates the anticipated site-generated vehicular trips from the proposed site development for AM, PM and Saturday.

#### **7.1.3 Total Traffic**

For this analysis, we have compared the estimated future Background Traffic volumes against the future Total Traffic Volumes at both opening day 2022 and the future 2030 horizon year. The Total Traffic volumes were estimated by superimposing the estimated increase in site traffic volumes onto both the 2022 and 2030 Background Traffic volumes. The year 2022 Total and 2030 Total Traffic volumes are shown in **Exhibit 7.7-7.10**.

As shown, the trips introduced by the site do not significantly increase the signalized intersections traffic volumes (1% increase only) during the AM, PM and Saturday peak hours.

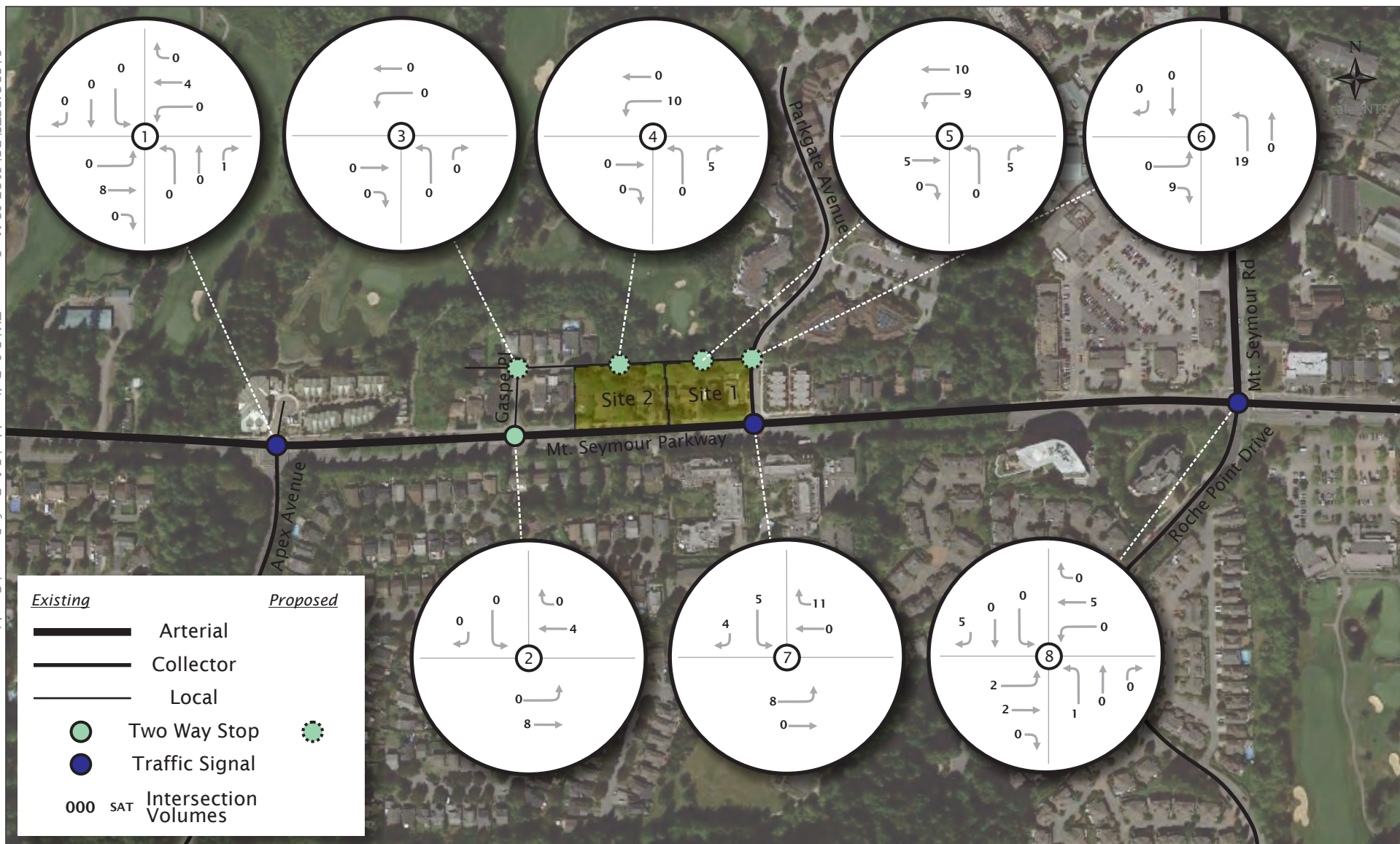




## Exhibit 7.5 Weekday AM & PM Site Traffic Forecasts

3468-3490 Mt Seymour Parkway TIA  
6217.02  
May 2017

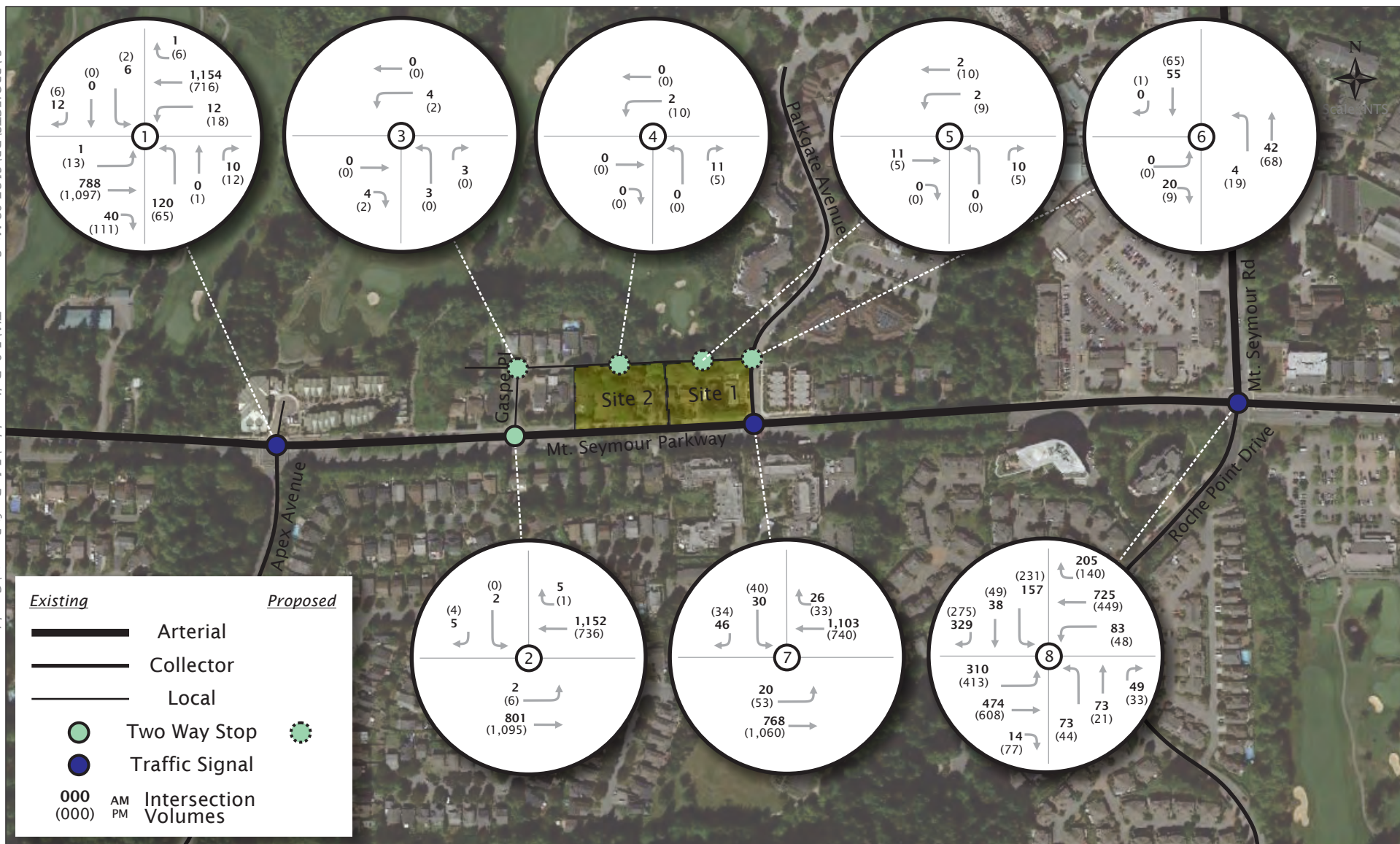




**Exhibit 7.6**  
**Saturday Site Traffic Forecasts**

3468-3490 Mt Seymour Parkway TIA  
6217.02  
May 2017





## Exhibit 7.7 Total 2022 Weekday Peak Hour Volumes

3468-3490 Mt Seymour Parkway TIA  
6217.02  
May 2017





## Exhibit 7.8 Total 2022 Saturday Peak Hour Volumes

3468-3490 Mt Seymour Parkway TIA  
6217.02  
May 2017





## Exhibit 7.9 Total Weekday 2030 Peak Hour Volumes

3468-3490 Mt Seymour Parkway TIA  
6217.02  
May 2017



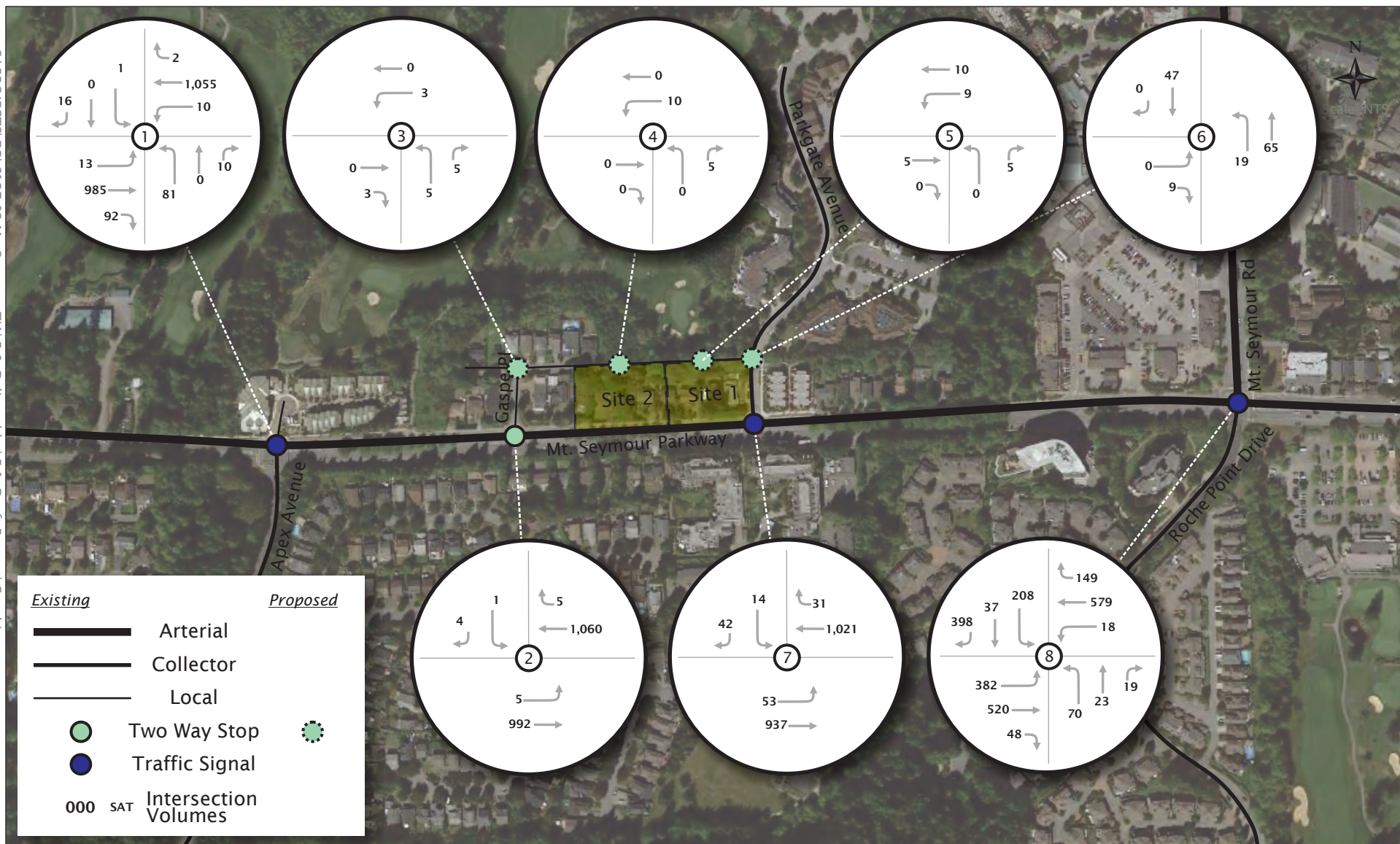


Exhibit 7.10  
Total 2030 Saturday Peak Hour Volumes



## 7.2 Traffic Operations

Capacity analysis of the future traffic condition, both with and without the site redeveloped, was carried out using the same traffic analysis methodology and criteria as noted in Section 6.2. The results of these analyses are presented in the subsequent sections.

### 7.2.1 Background 2022 Traffic Operations

With the new background growth rate of 0.5% per year applied along with an optimization of cycle splits, the results of the Background 2022 traffic operations are illustrated in **Exhibit 7.11-7.13** for AM, PM and Saturday peak hours. The Background volumes in Exhibit 7.11-7.13 contain the existing site traffic generated by the 8 single family homes as the trips being generated are negligible.

#### *Unsignalized Intersections*

For all unsignalized intersections within the study area, they will continue to operate acceptably with minimum queues and delays.

#### *Mt. Seymour Road / Mt. Seymour Parkway*

From optimizing the splits of the signal timing, the long eastbound left-turn queues have decreased to acceptable lengths. The PM period shows the highest eastbound left-turn queue with 46m, which does not exceed the 70m storage length. The southbound left-turn queue also improved with the optimization of the signal splits however, the southbound left-turn queue is expected to continue exceeding the storage length of 50m with a queue of 69m. The overall intersection operates at LOS B or better and v/c of 0.75 or better for all three time periods. As a result of optimizing the splits, the eastbound left improves to a v/c of 0.70 and LOS B during the PM and v/c of 0.67 and LOS B for the AM period.

#### *Apex Avenue / Mt. Seymour Parkway*

The intersection of Apex Avenue and Mt. Seymour Parkway will continue to operate at acceptable levels with minimal queues and delays. The overall intersection will operate at LOS B and v/c of 0.49 or better for all three time periods. After optimization, the westbound through movement during the AM improves to a v/c of 0.60 and LOS B. The critical movement during the PM and Saturday periods is the eastbound through movement at with LOS B and v/c of 0.63 and 0.55 respectively.

#### *Parkgate Avenue / Mt. Seymour Parkway*

The intersection of Parkgate Avenue and Mt. Seymour Parkway will continue to operate within acceptable traffic operations parameters with minimal queues and delays. The overall v/c ratios for all three periods are below 0.40 and LOS B or better. The individual movements for all three scenarios operate at LOS B or better and v/c of 0.71 or better.





**Exhibit 7.11**  
Opening Day Background 2022 AM Traffic Operations





Exhibit 7.12  
Opening Day Background 2022 PM Traffic Operations



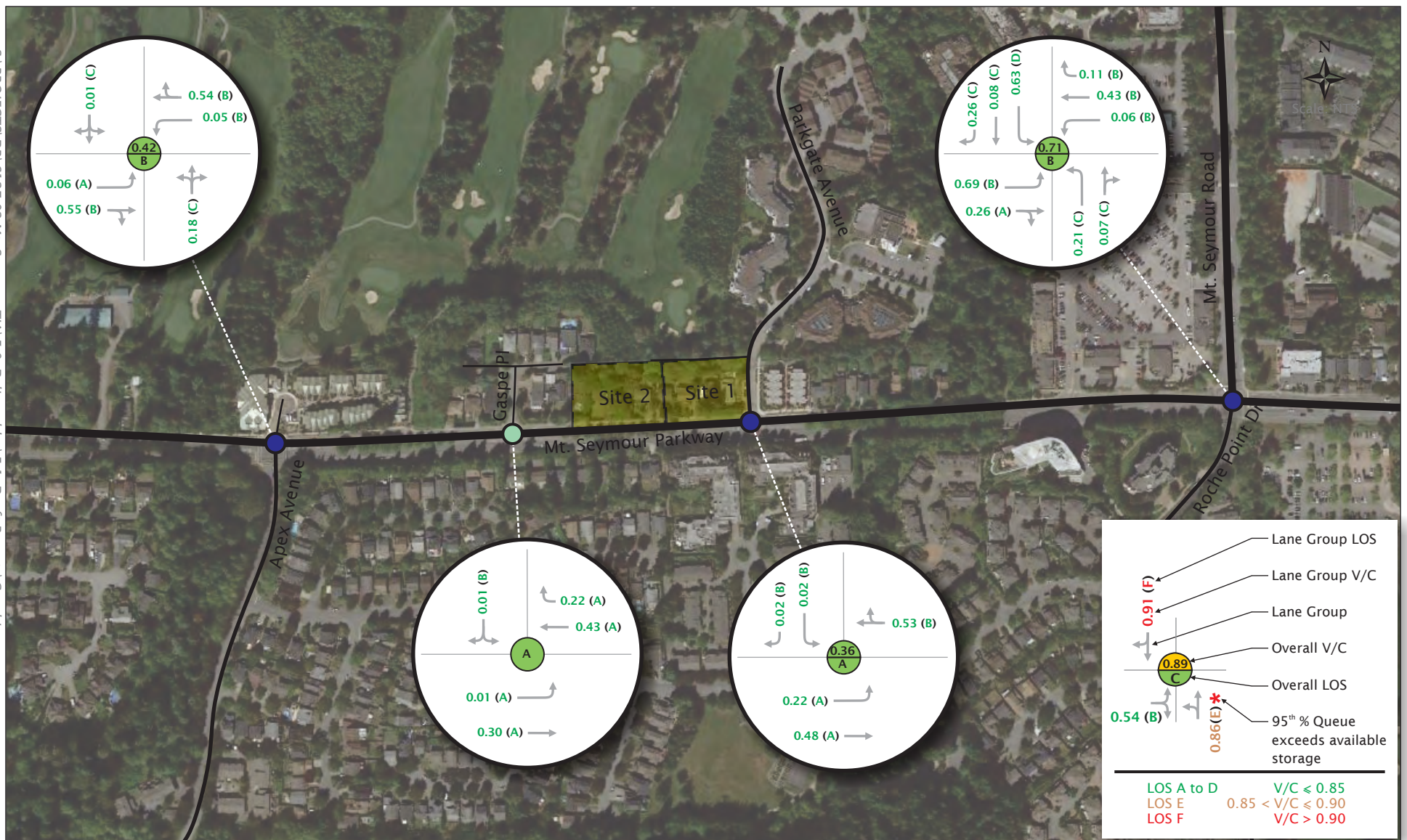


Exhibit 7.13  
Opening Day Background 2022 Saturday Traffic Operations



### 7.2.2 Total 2022 Traffic Operations

The same methodology and criteria as Section 5.1 above was applied for the Total 2022 traffic volumes. The Synchro model involved the optimization of the cycle splits to obtain the capacity results with the added site traffic. The opening day 2022 Total traffic operations are summarized in **Exhibit 7.14-7.16**. Traffic volumes from the development are negligible. The marginally-reduced operating conditions at most signalized intersections were the result of background volume growth applied via the 0.5% p.a. rate.

#### *Unsignalized Intersections*

For all unsignalized intersections within the study area, they are currently operating acceptably with minimum queues and delays.

#### *Mt. Seymour Road / Mt. Seymour Parkway*

From optimizing the splits of the signal timing, the long eastbound left-turn queues have decreased to acceptable lengths. The PM period shows the highest eastbound left-turn queue with 46m, which does not exceed the 70m storage length. The southbound left-turn queue during the PM is expected to continue exceeding the storage length of 50m with a queue of 69m. The southbound left-turn queue during the Saturday period is also forecasted to exceed the storage length with a queue of 52m. The overall intersection will operate at LOS B or better and v/c of 0.76 or better for all three time periods. The eastbound left marginally increases to a v/c of 0.70 and LOS B during the PM and Saturday and v/c of 0.70 and LOS B for the AM period.

#### *Apex Avenue / Mt. Seymour Parkway*

The intersection of Apex Avenue and Mt. Seymour Parkway will continue to operate at acceptable levels with minimal queues and delays. The overall intersection is operating at LOS B and v/c of 0.51 or better for all three time periods. The critical movement during the AM period is the westbound through and operates at a v/c of 0.60 and LOS B. The critical movement during the PM and Saturday periods is the eastbound through movement at with LOS B and v/c of 0.63 and 0.55 respectively.

#### *Parkgate Avenue / Mt. Seymour Parkway*

The intersection of Parkgate Avenue and Mt. Seymour Parkway will continue to operate within acceptable traffic operations parameters with minimal queues and delays. The overall v/c ratios for all three periods are below 0.42 and LOS A or better. The individual movements for all three scenarios operate at LOS C or better and v/c of 0.56 or better.





Exhibit 7.14  
Opening Day Total 2022 AM Traffic Operations



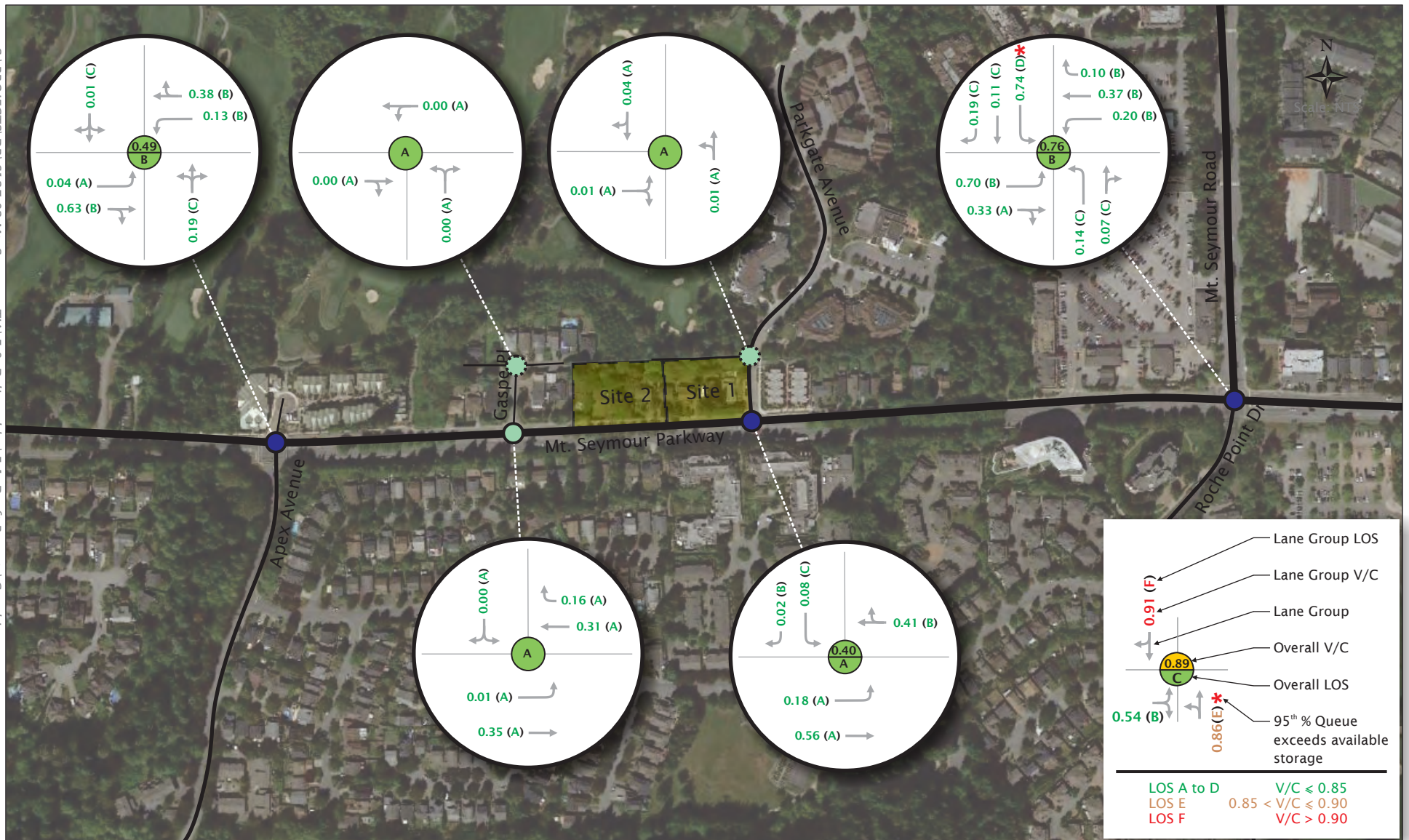


Exhibit 7.15  
Opening Day Total 2022 PM Traffic Operations



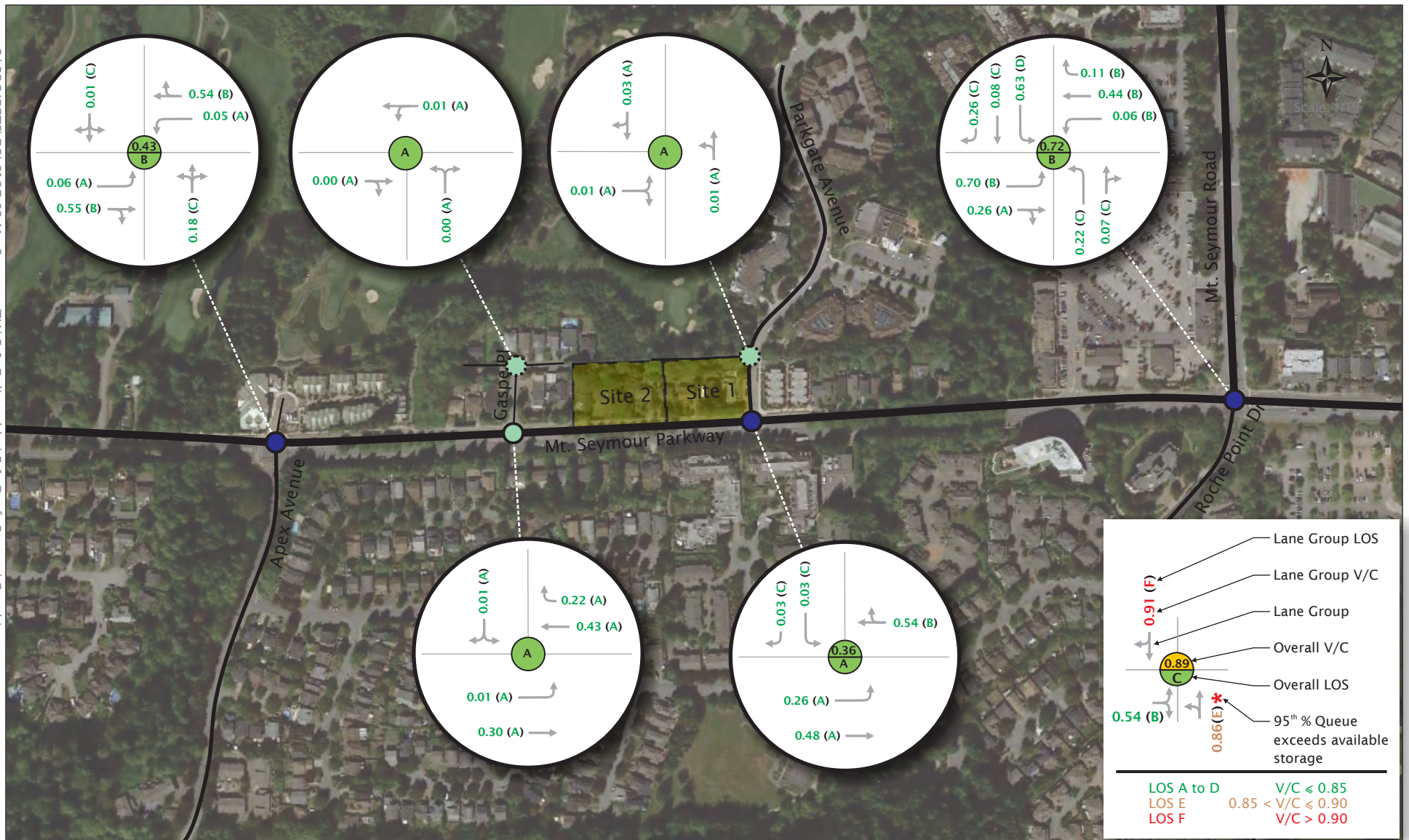


Exhibit 7.16  
Opening Day Total 2022 Saturday Traffic Operations



### 7.2.3 Background 2030 Traffic Operations

Application of 0.5% per year growth for the Background 2030 (without site developed) along with an optimization of cycle splits, the results of the Background 2030 traffic operations are illustrated in **Exhibit 7.17-7.19** for AM, PM and Saturday peak hours. The Background volumes in Exhibit 7.17-7.19 remove the volumes from the existing 114-unit development.

#### *Unsignalized Intersections*

For all unsignalized intersections within the study area, they are currently operating acceptably with minimum queues and delays.

#### *Mt. Seymour Road / Mt. Seymour Parkway*

From optimizing the splits of the signal timing, the long eastbound left-turn queues have decreased to acceptable lengths. The PM period shows the highest eastbound left-turn queue with 48m, which does not exceed the 70m storage length. The southbound left-turn queue during the PM is expected to continue exceeding the storage length of 50m with a queue of 73m. However, this is a result mostly of background growth and not of volumes generated by either development.

The southbound left-turn queue during the Saturday period is also forecasted to exceed the storage length with a queue of 59m. The overall intersection operates at LOS B or better and v/c of 0.79 or better for all three time periods. The eastbound left marginally increases to a v/c of 0.73 and LOS B during the PM and Saturday and v/c of 0.67 and LOS B for the AM period.

#### *Apex Avenue / Mt. Seymour Parkway*

The intersection of Apex Avenue and Mt. Seymour Parkway will continue to operate at acceptable levels with minimal queues and delays. The overall intersection is operating at LOS B and v/c of 0.51 or better for all three time periods. The westbound through movement during the AM is expected to be at v/c of 0.60 and LOS B. The critical movement during the PM and Saturday periods is the eastbound through movement at with LOS B and v/c of 0.63 and 0.55 respectively.

#### *Parkgate Avenue / Mt. Seymour Parkway*

The intersection of Parkgate Avenue and Mt. Seymour Parkway will continue to operate within acceptable traffic operations parameters with minimal queues and delays. The overall v/c ratios for all three periods are below 0.41 and LOS B or better. The individual movements for all three scenarios operate at LOS C or better and v/c of 0.59 or better.



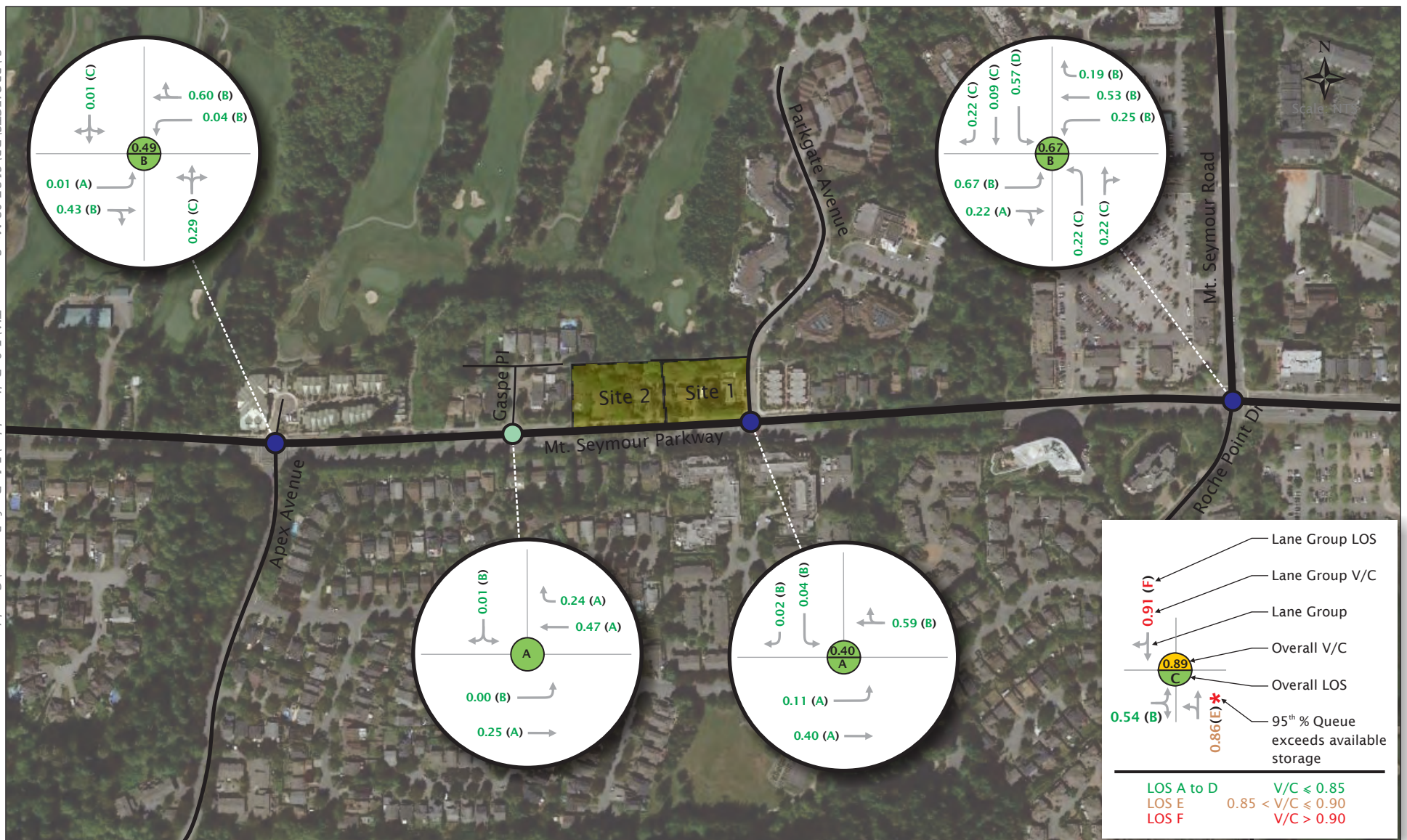


Exhibit 7.17  
Opening Day Background 2030 AM Traffic Operations





Exhibit 7.18  
Opening Day Background 2030 PM Traffic Operations





## Exhibit 7.19 Opening Day Background 2030 Saturday Traffic Operations

3468-3490 Mt Seymour Parkway TIA  
6217.02  
May 2017



#### 7.2.4 Total 2030 Traffic Operations

The same methodology and criteria as Section 5.1 above was applied for the Total 2030 traffic volumes. The Synchro model involved the optimization of the cycle splits to obtain the capacity results with the added site traffic. The opening day 2030 Total traffic operations are summarized in **Exhibit 7.20-7.22**.

Similar to the 2022 results, with traffic volumes from the development negligible, the further-reduced operating conditions at most signalized intersections were the result of background volume growth applied via the 0.5% p.a. rate.

##### *Unsignalized Intersections*

For all unsignalized intersections within the study area, they are currently operating acceptably with minimum queues and delays.

##### *Mt. Seymour Road / Mt. Seymour Parkway*

From optimizing the splits of the signal timing, the long eastbound left-turn queues have decreased to acceptable lengths. The PM period shows the highest eastbound left-turn queue with 49m, which does not exceed the 70m storage length. The southbound left-turn queue during the PM is expected to continue exceeding the storage length of 50m with a queue of 73m. However, this is a result mostly of background growth and not of volumes generated by either development.

The southbound left-turn queue during the Saturday period is also forecasted to exceed the storage length with a queue of 59m. The overall intersection operates at LOS B or better and v/c of 0.79 or better for all three time periods. The eastbound left marginally increases to a v/c of 0.74 and LOS B during the PM and v/c of 0.73 and LOS B for the AM period.

##### *Apex Avenue / Mt. Seymour Parkway*

The intersection of Apex Avenue and Mt. Seymour Parkway will continue to operate at acceptable levels with minimal queues and delays. The overall intersection is operating at LOS B and v/c of 0.53 or better for all three time periods. The critical movement during the AM period is the westbound through and operates at a v/c of 0.63 and LOS B. The critical movement during the PM and Saturday periods is the eastbound through movement at with LOS B and v/c of 0.66 and 0.57 respectively.

##### *Parkgate Avenue / Mt. Seymour Parkway*

The intersection of Parkgate Avenue and Mt. Seymour Parkway will continue to operate within acceptable traffic operations parameters with minimal queues and delays. The overall v/c ratios for all three periods are below 0.43 and LOS A or better. The individual movements for all three scenarios operate at LOS C or better and v/c of 0.63 or better.



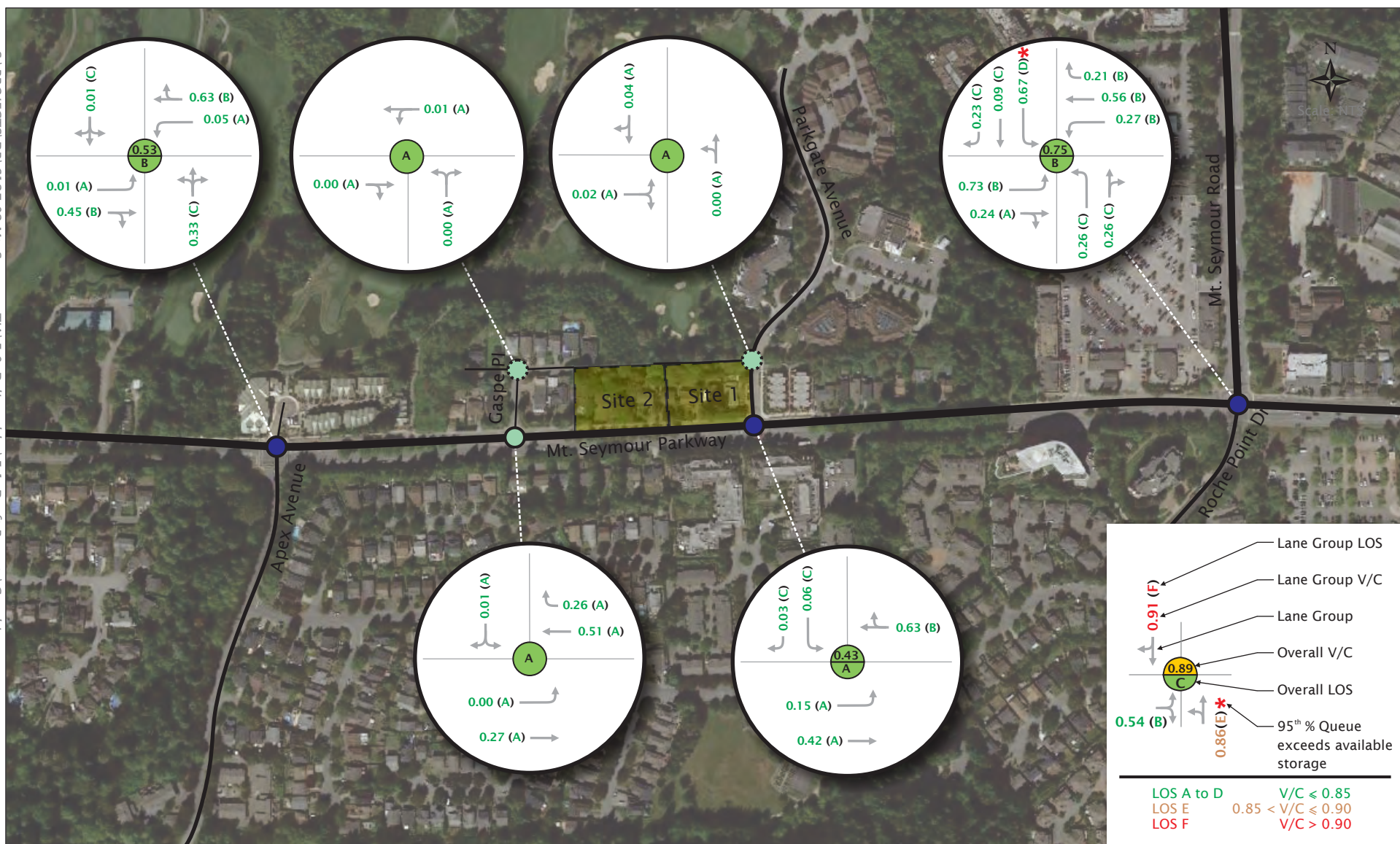


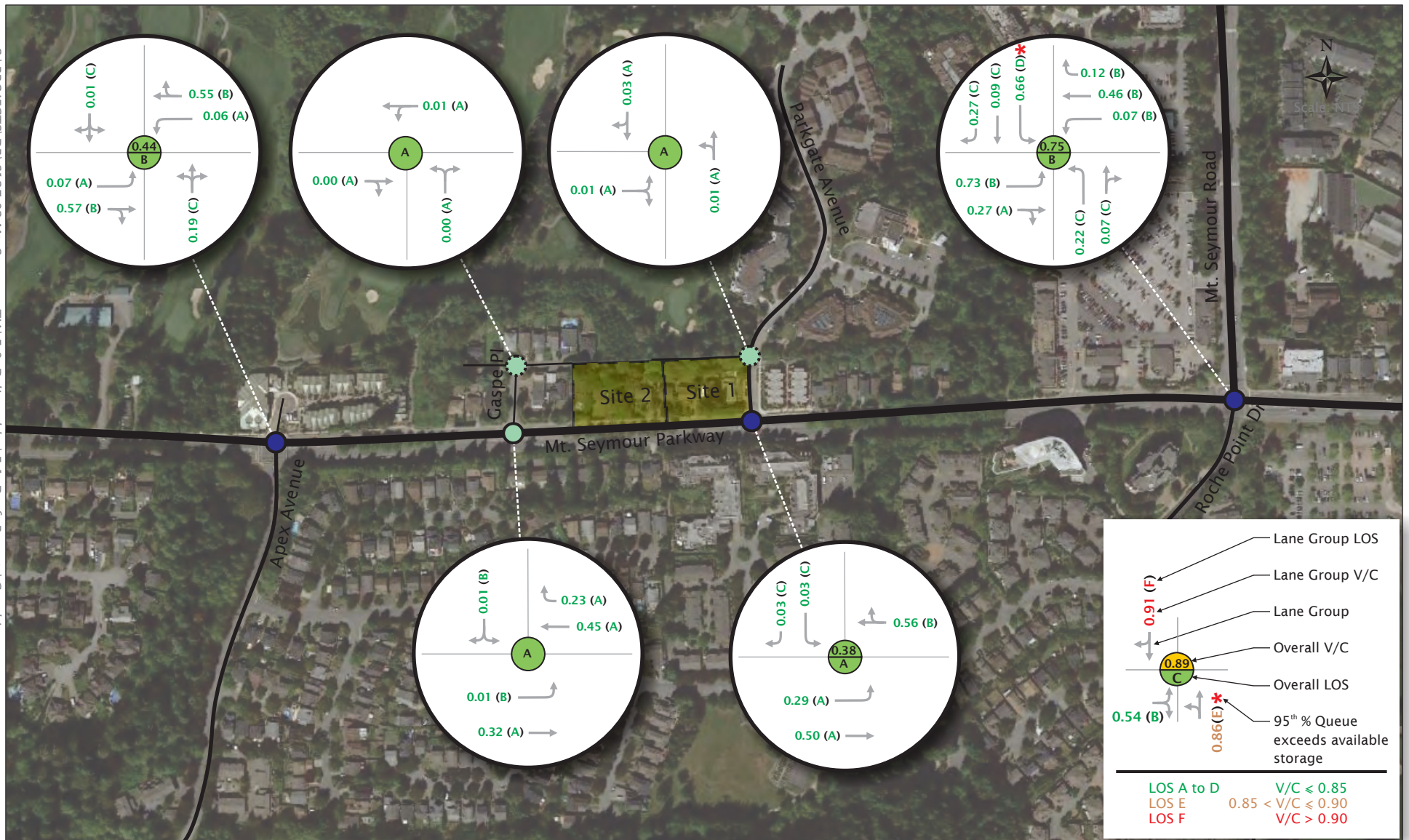
Exhibit 7.20  
Opening Day Total 2030 AM Traffic Operations





Exhibit 7.21  
Opening Day Total 2030 PM Traffic Operations





**Exhibit 7.22**  
Opening Day Total 2030 Saturday Traffic Operations



### 7.3 Gaspe Place to Parkgate Avenue Laneway Connection

The District of North Vancouver has requested that a sensitivity analysis be conducted for the future build-out of this study area. The future plan for the full build-out will include 48 additional townhome units immediately west of Gaspe Place at 3374-3396 Mt. Seymour Parkway. Ultimately, with this full build-out, the District proposes that the rear laneway will connect through, connecting Parkgate Avenue to Gaspe Place.

Upon connecting Parkgate Avenue through to Gaspe Place via the laneway, a feasibility analysis will be conducted on reconfiguring the access to Gaspe Place into a right-in / right-out configuration by closing the gap in the median on Mt. Seymour Parkway. For the purpose of this analysis, only the Total 2030 horizon year will be analyzed.

#### 7.3.1 Trip Generation

Utilizing the same rates as section 7.1.2, **Table 7.4** shows the anticipated future site generated vehicles trips for the proposed 48 unit townhome development.

**Table 7.4: Estimated Peak Hour Site Vehicle Trips**

LAND USE	UNITS	AM PEAK HOUR			PM PEAK HOUR			SAT PEAK HOUR		
		IN	OUT	TOTAL	IN	OUT	TOTAL	IN	OUT	TOTAL
Proposed Townhomes	48	4	18	21	17	8	25	17	8	25

As shown in Table 7.4, the site is expected to generate approximately 21 vehicle trips during the AM peak hour (4 inbound and 18 outbound) which equates to an average of 2-3 vehicles per minute in the peak morning hour. In the PM and Saturday peak hours 25 trips are expected (17 inbound and 8 outbound) which equates to an average of between 2-3 vehicles a minute during the most active traffic periods of the day.

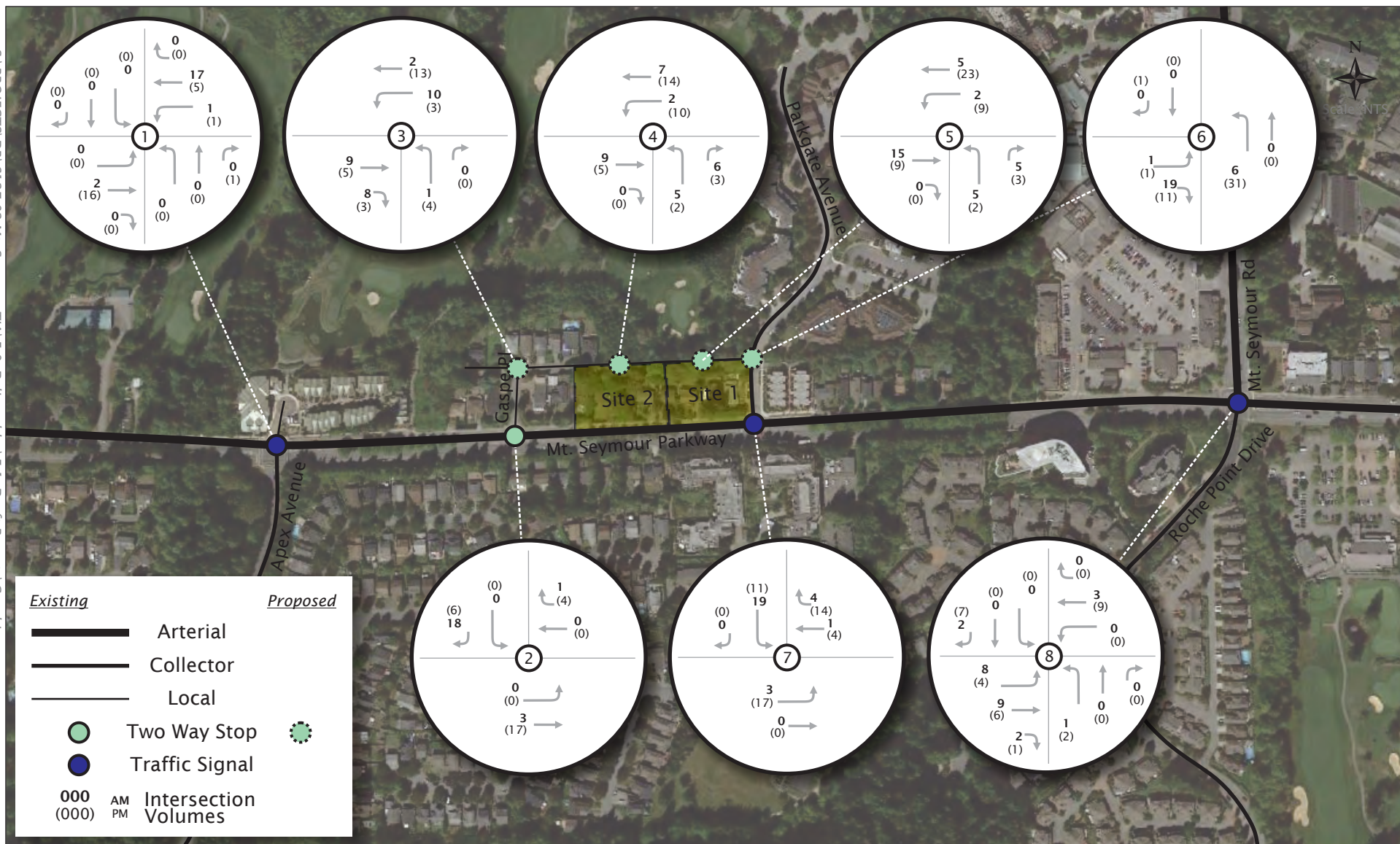
The overall site trips being generated for the three developments are summarized in **Table 7.5**.

**Table 7.5: Estimated Peak Hour Site Vehicle Trips**

LAND USE	AM PEAK HOUR			PM PEAK HOUR			SAT PEAK HOUR		
	IN	OUT	TOTAL	IN	OUT	TOTAL	IN	OUT	TOTAL
Tatla Townhomes	2	10	12	9	5	14	9	5	14
Allaire Townhomes	2	11	13	10	5	15	10	5	15
Proposed Townhomes	4	18	21	17	8	25	17	8	25
	8	38	46	36	18	54	36	18	54

The new site trips with the third development are summarized in **Exhibit 7.23-7.24**.





**Exhibit 7.23**  
**Weekday AM & PM site Trip Forecast - Option 2**



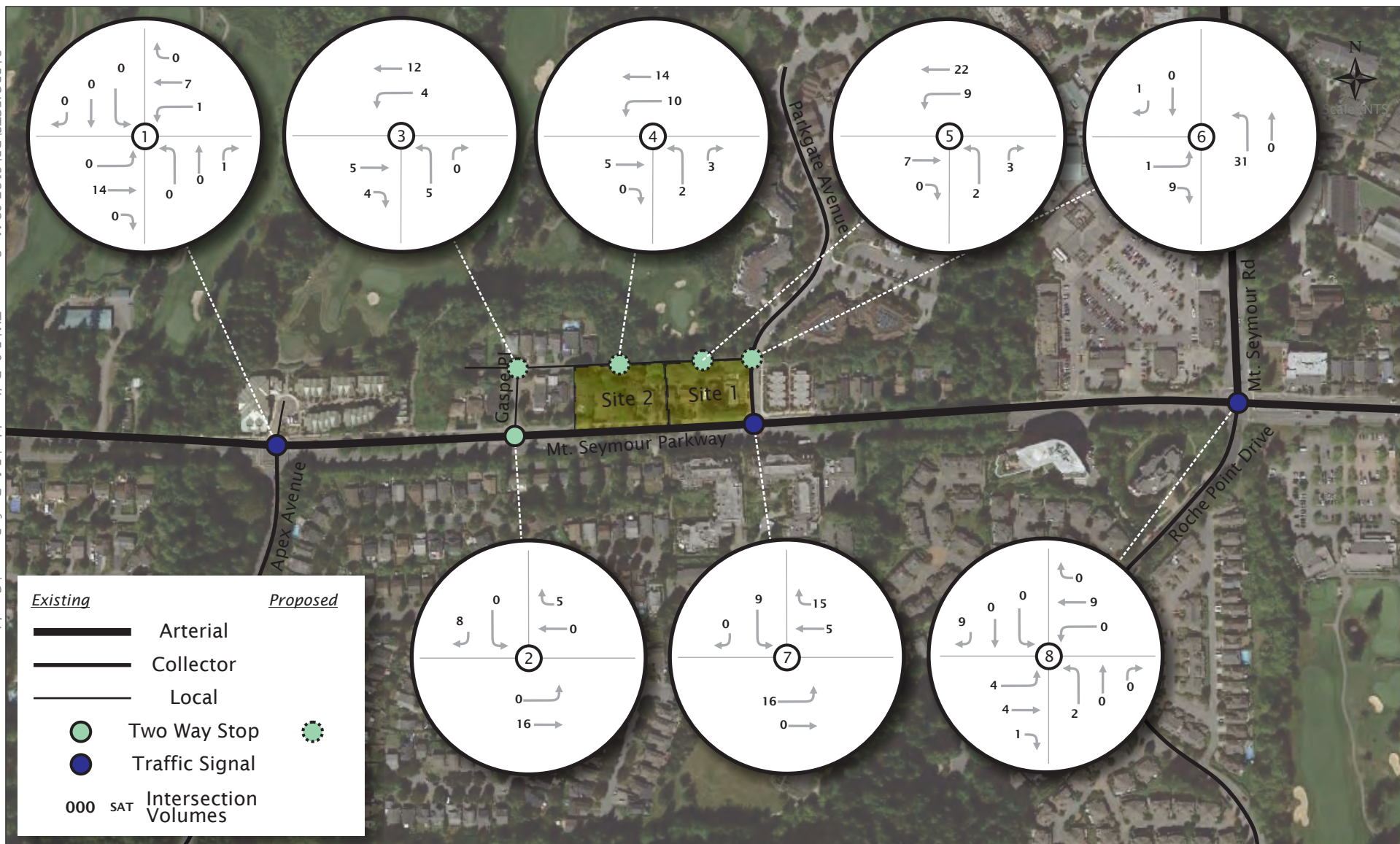


Exhibit 7.24  
Saturday Site Trip Forecast - Option 2



### 7.3.2 Traffic Redistribution

A sensitivity analysis was conducted to determine the feasibility of restricting Gaspe Place to a right-in/right-out movement and with the rear laneway connecting Parkgate Avenue to Gaspe Place. A redistribution of the existing and future traffic was made and this net change and redistribution is summarized in **Exhibit 7.25-7.26** of the AM, PM and Saturday periods.

The existing eastbound left-turn and southbound left-turn demand at the intersection of Mt. Seymour Parkway and Gaspe Place are very low. In the AM and PM period for the 2030 horizon year, 6 vehicles are estimated for the eastbound left-turn and 2 vehicles for the southbound left-turn. The Saturday peak is forecasted to only have 5 vehicles turning left eastbound and 1 vehicle turning southbound left. These volumes are very minimal and the redistribution to Parkgate Avenue only will have negligible effects.

If the access to Gaspe Place is changed to a right-in/right-out in the future, then all vehicles would access the sites from Parkgate Avenue and through the east-west laneway connection. The northbound left-turn movement is forecasted to increase to 38 vph in the PM period. Given the low vehicular volume travelling through Parkgate Avenue coupled with the marginal increase in site traffic, a separated northbound left-turn lane is not necessary.

### 7.3.3 Traffic Operations

#### *Unsignalized Intersections*

For all unsignalized intersections within the study area, they are currently operating acceptably with minimum queues and delays.

#### *Mt. Seymour Road / Mt. Seymour Parkway*

From optimizing the splits of the signal timing, the long eastbound left-turn queues have decreased to acceptable lengths. The PM period shows the highest eastbound left-turn queue with 49m, which does not exceed the 70m storage length. The southbound left-turn queue during the PM is expected to continue exceeding the storage length of 50m with a queue of 73m. However, this is a result mostly of background growth and not of volumes generated by either development.

The southbound left-turn queue during the Saturday period is also forecasted to exceed the storage length with a queue of 59m. The overall intersection operates at LOS B or better and v/c of 0.80 or better for all three time periods. The eastbound left marginally increases to a v/c of 0.74 and LOS B during the PM and v/c of 0.68 and LOS B for the AM period.

#### *Apex Avenue / Mt. Seymour Parkway*

The intersection of Apex Avenue and Mt. Seymour Parkway will continue to operate at acceptable levels with minimal queues and delays. The overall intersection is operating at LOS B and v/c of 0.51 or better for all three time periods. The critical movement during the AM period is the westbound through and operates at a v/c of 0.59 and LOS B. The critical movement during the PM and Saturday periods is the eastbound through movement at with LOS B and v/c of 0.66 and 0.57 respectively.



*Parkgate Avenue / Mt. Seymour Parkway*

The intersection of Parkgate Avenue and Mt. Seymour Parkway will continue to operate within acceptable traffic operations parameters with minimal queues and delays. The overall v/c ratios for all three periods are below 0.42 and LOS A or better. The individual movements for all three scenarios operate at LOS C or better and v/c of 0.59 or better.





**Exhibit 7.25**  
**Total Weekday 2030 Peak Hour Volumes - Option 2**





Exhibit 7.26  
Total Saturday 2030 Peak Hour Volumes - Option 2





Exhibit 7.27  
Opening Day Total 2030 AM Traffic Operations Option 2





**Exhibit 7.28**  
Opening Day Total 2030 PM Traffic Operations Option 2



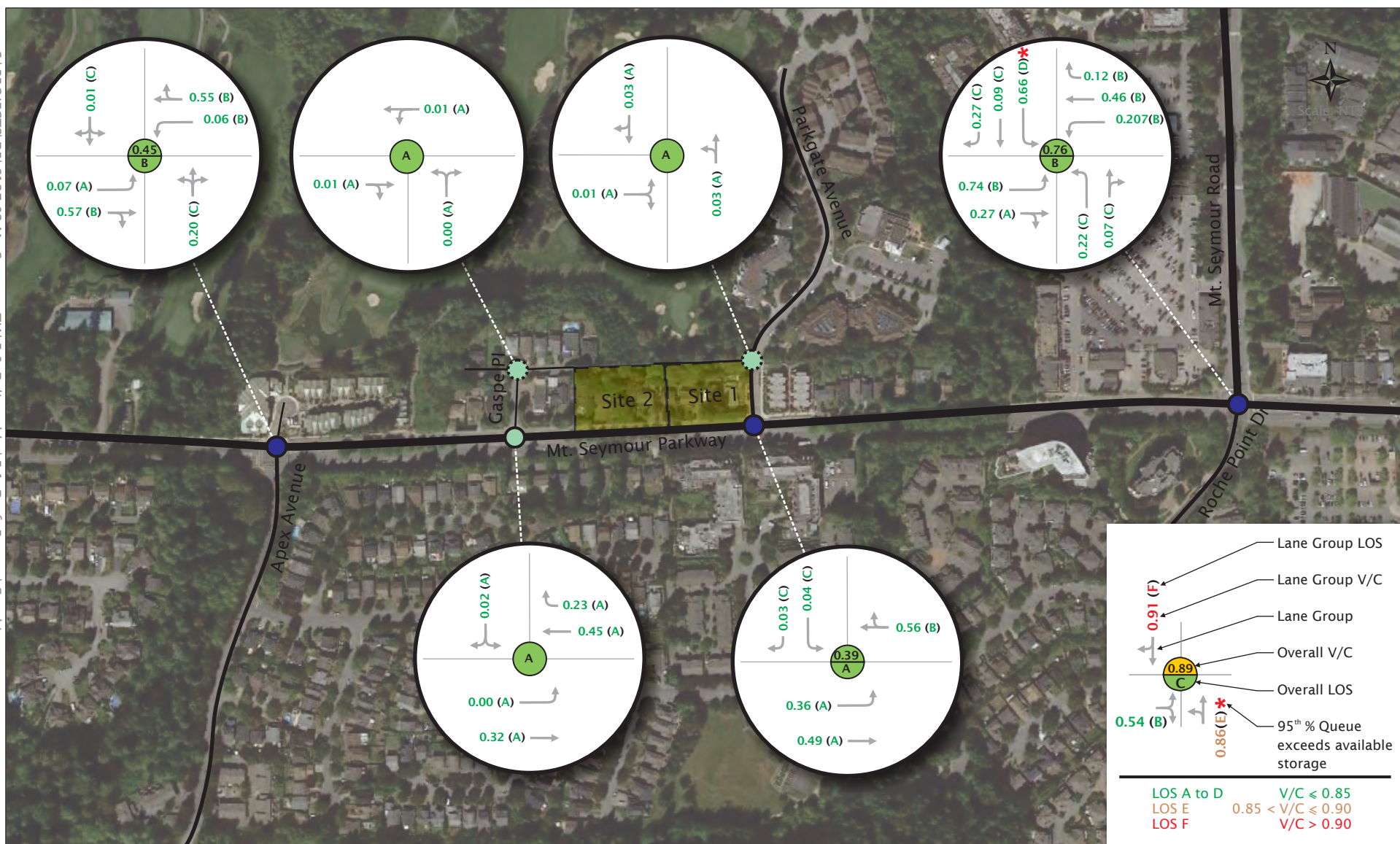


Exhibit 7.29  
Opening Day Total 2030 Saturday Traffic Operations Option 2



### 7.3.4 Summary of Traffic Impacts & Recommended Mitigations

#### *Mt. Seymour Road / Mt. Seymour Parkway*

- Recommend that the southbound left-turn lane be lengthened from 50m to 80m to accommodate the projected future traffic growth and development; and
- Signal timing optimization of signal timing plans may be required to improve v/c and LOS performance.

#### *Gaspe Place / Mt. Seymour Parkway*

- Will continue to operate at acceptable levels with minimum queues and delays for all the future horizons for both Background and Total scenarios;
- With the reconfiguration of right-in/right-out only, safety improvements are predicted since left-turn manoeuvres onto Mt. Seymour Parkway are not desirable; and
- Existing and future traffic travelling eastbound out of Gaspe Place or entering Gaspe from the west will be diverted to Parkgate Avenue, where left-turn manoeuvres will be safer due to the presence of a traffic signal.

#### *Parkgate Avenue / East-West Laneway*

- This new access is predicted to operate at acceptable levels with minimum queues and delays; and
- No separate northbound left-turn lane will be required to meet the site generated traffic demand into the east-west laneway.

#### *Apex Avenue / Mt. Seymour Parkway*

- Recommend signal optimization of the splits.

#### *Parkgate Avenue / Mt. Seymour Parkway*

- Recommend signal optimization of the splits.



## 8. TDM & ACTIVE MODES

### 8.1 Definition

The District of North Vancouver aims to promote sustainable and active modes of transportation through the implementation of new infrastructure as indicated in the District of North Vancouver Transportation Plan (*DNV-2012-01*). The DNV have expressed their interest in promoting sustainable transportation elements surrounding the site, as an integral part of the area transportation network

Transportation Demand Management (TDM) is defined as the “application of strategies and policies to reduce travel demand (specifically that of single-occupancy private vehicles), or to redistribute this demand in space or in time”<sup>1</sup>. A successful TDM program can influence travel behaviour away from Single Occupant Vehicle (SOV) travel during peak periods towards more sustainable modes such as High Occupancy Vehicle (HOV) travel, transit, cycling or walking. The responsibility for implementation of TDM measures can range across many groups, including regional and municipal governments, transit agencies, private developers, residents/resident associations or employers.

TDM is increasingly becoming an important toolkit for municipalities in providing realistic travel choices and alternatives to residents, visitors, and employees to reduce reliance on SOV trips. When efficiently implemented, TDM will materialize such benefits as outlined in the District’s Transportation Plan, including:

- Improved community livability;
- Improved physical fitness and health;
- Greater mobility options;
- Time and cost savings for individuals;
- Reduced congestion;
- Road and parking infrastructure cost savings;
- Greater return on municipal investments in walking, cycling and transit infrastructure;
- Reduced demand on road and parking infrastructure; and
- Reduced traffic collisions.

### 8.2 Potential Measures

**Table 8.1** below summarizes a possible suite of measures based on Bunt’s research that may be appropriate for this site. The strategy is identified in the left column, and the measure in the centre column. The right column on the table shows which parties would be responsible for administering and managing the each initiative. While this is a comprehensive listing of all possible measures, the site developer’s potential role in TDM for the site would be limited to those items identified as “Site Developer” on the far right of this table.

<sup>1</sup> <http://ops.fhwa.dot.gov/tdm/index.htm> FHWA Travel Demand Management home page



**Table 8.1: Potential TDM Strategies Summary Table**

STRATEGY	MEASURE	RESPONSIBILITY FOR IMPLEMENTATION
TDM Site Coordinator & Monitoring Program	Appoint a Site TDM Coordinator, responsible for developing, implementing and maintaining TDM program	Site Developer/Operator
	Establish mode split targets, monitoring methods and surveys and reporting	Site Operator/Strata
Marketing & Promotion	Prepare marketing materials to attract residents who want a car-free lifestyle	Site Developer
	Provide a Welcome Brochure, with an information package on transportation alternatives, that is issued to all new residents and posted in common areas	Site Developer/Site Operator
	Participation in Bike to Work Week and other community and regional promotions/events for sustainable transportation	Regional Transportation Authority/Municipality/Site Operator/Strata
Cycling Infrastructure Improvements	Provide cycling facilities leading to, adjacent to and on the site	Site Developer, Municipality
	Provide safe, marked cycling crossings at intersections, with push button activation at signals	Municipality
Cycling Access	Provide a shared bicycle program	Site Developer/Operator
Cycling Amenities	Provide bicycle maps and way finding signage through site	Site Developer
	Provide a bicycle repair station	Site Developer
End of Trip Cycling Facilities	Provide long term secure and convenient bicycle storage facilities for residents	Site Developer
	Provide a common maintenance area for bicycle maintenance serving residents	Site Developer
	Provide a common and shared long term, secure and convenient bicycle storage facilities for employees	Site Developer
	Provide short term bicycle rack parking at all building entrances (well lit and protected, within view of lobbies for residential visitors and patrons)	Site Developer
Pedestrian Infrastructure Improvements	Provide an off-street pathway system to minimize walking distances; provide sidewalks on both sides of all site and site fronting streets with boulevard improvements to buffer pedestrians from moving traffic	Site Developer
	Provide new protected pedestrian crossing opportunities and pushbuttons	Site Developer
Pedestrian Amenities	Provide amenities in the pedestrian realm	Site Developer
Rideshare Programs	Support resident use of available regional ride share programs	Site Developer/Operator, partnering with regional programs
Car Share	Provide Car Share vehicles and stalls for residents	Site Developer initiates with provision of vehicles and stalls, Car Share Program Provider operates and maintains program
	Provide Car Share stalls in publicly accessible area for site employees, visitors and the neighbourhood	
	Provide initial Car Share membership fee for each unit	Site Developer
Transit	Provide funding for improvements to adjacent bus stops, such as benches and shelters at existing bus stops adjacent to site	Site Developer
	Provide subsidized transit passes to new residents upon move-in	Site Developer/Operator
	Provide a private shuttle service for residents to nearby key destinations	Site Developer/Operator
Parking Management	Install pay parking on-street or in public parking lots to manage demand and encourage walking to the site	Municipality
	Restrict parking supply: provide lower than bylaw supply rates	Site Developer
	Unbundle Parking with Parking Rental Program	Site Developer



### 8.3 Car Share

Perhaps the most well known 'resident focused' TDM measure, car sharing involves having access to a fleet of vehicles and booking them when and if you need them. Research shows that one car share vehicle can replace between 5 and 11 privately owned vehicles from the roads<sup>2</sup>. It has been used in other municipalities to reduce residential parking requirements.

The recent Metro Vancouver Car Share Study provides some information about the reduction of auto ownership based on car share usage. According to the Car Share study the number of vehicles per household decreased between 5 and 36% when the residents joined a car share program.<sup>3</sup> The study also shows that the top home-based amenity that survey respondents indicated would have them shift modes is the availability of car share vehicles.

At this time, only select areas in the District feature car share vehicles (Grouse Mountain, Capilano University). If the service area of Modo or Evo expands in the future, the developer should consider working with either provider to bring a car share vehicle to the development site.

### 8.4 Unbundling Residential Parking Spaces

Relatively new to Metro Vancouver but common in other areas of the country, unbundling parking from condominium units means that units are sold without parking spaces. If a parking space is required then one can be purchased separately. This is potentially a way to build less residential parking based on market conditions. The framework needs to be in place to allow this to happen in an efficient manner without triggers the need for a new Development Permit.

### 8.5 'Mobility Pass'

This relatively new concept goes beyond the 'transit pass' concept that still has not been fully realised across Metro Vancouver. In exchange for buying a unit without a parking space, owners of a new unit would instead be recipients of a monthly 'credit' to be used towards a number of different mobility options. These could include transit, car share, bike share, taxis or other options that are developed in the future. Some municipalities already offer a parking discount for transit passes, so the same concept would be applied to a 'mobility pass'.

### 8.6 Transit Shuttle

Private transit shuttles generally connect developments to transit hubs, making up for a lack of public transit service. They may operate a peak times only or all through the day. Although the details are to be

<sup>2</sup> <http://www.metrovancouver.org/services/regional-planning/PlanningPublications/MetroVancouverCarShareStudyTechnicalReport.pdf>

<sup>3</sup> Metro Vancouver Car Share Study – Page 30 - <http://www.metrovancouver.org/services/regional-planning/PlanningPublications/MetroVancouverCarShareStudyTechnicalReport.pdf>



worked out, such a service would operate from the plaza level of the development and take people directly to Phibbs Bus Exchange and/or Lonsdale Quay.

## 8.7 Bicycle Share

A flexible way for residents to try cycling or use it infrequently is through a bike share program, by providing a few bicycles per building, to be used by the residents for short trips, potentially to Parkgate Village. These bicycles would be located in the Bike Hub rooms (see below) and signed out on a first come-first served basis, with the sign-out process being administered by the tenant or strata association.

This is separate from a public bike share which has only just launched in the City of Vancouver. However, should a public bike share be an option through the build out of the development, a station could be located on site and residents could be given memberships.



## 8.8 Marketing Materials & Transportation Information Package for Residents

Travel patterns are most pliable when residents move from one location to another. Therefore, site developers/rental companies can play a significant role in changing people's travel behaviours, through marketing materials to potential buyers/renters and through provision of information packages to new residents which stress the attractiveness and ease of alternative travel modes. In marketing materials to potential residents, clear and simple messages such as cost savings and health benefits (within the context of life style choice and urban living), along with practical information about local transit services, walking and cycle routes to key locations, carpooling and car-sharing services, would help attract residents who want to live a car-free lifestyle.

For residents who are moving in, a Transportation Information Package should be provided on move-in day. The package should include:



- A map showing amenities and shopping opportunities within a typical walking catchment of 800m;
- A map showing local cycling and transit routes with key destinations and travel times by different modes;
- Information about bicycle safety and local bicycle shops and repair facilities;
- Information pertaining to on-site car share provisions, car share membership sign up and procedures;
- Information pertaining to available bicycle and vehicle parking;
- Information on regional ride-share organizations, such as Jack Bell; and
- A list of websites and apps that can aid in the use of alternative transportation such as transit apps.

## 8.9 Site Planning and Design

In addition to the area improvements for cyclists and pedestrians outside the site, as previously mentioned a number of site-related enhancements could be considered, as summarized below.

### 8.9.1 Pedestrian Focused Site Planning

- Generous sidewalks along the site frontages, and pathways through the site, i.e. 1.8m – 2.0m. These would be buffered from the street by landscaped boulevards to provide added comfort to pedestrians around the site;
- Permeability through the site with multi-use internal pathways for residents and people passing through the site, including for cyclists.

### 8.9.2 Bicycle Rack Design

The site's bicycle racks should be designed in a way that the user can easily understand how to use them. The following are best practices in short term bicycle rack design:

- 2 points of contact between the rack and the frame and wheel of the bicycle, such that it stays upright, perpendicular to the floor, preventing the bike from falling over while parked.
- Both the frame and the wheel should be able to be locked to the rack, with stand locks, such as the U-lock.

In order for a bike rack to function and be well used, other key points should be considered:

- The rack should not have sharp edges, such that a bike's paint could be scratched;
- The rack should not be overly artistic or complicated to understand how to use;
- The rack should be located as close to a destination as possible, in a covered and well lit area. The location should be out of the way of pedestrian and motor vehicle traffic, or other street furniture that would impede its use, but still located in an area of high traffic to improve security.



The following presents recommend bike rack design.

#### Classic Hanger Rack



[http://www.alibaba.com/product-detail/standing-coat-hanger-bike-display-racks\\_888047182.html](http://www.alibaba.com/product-detail/standing-coat-hanger-bike-display-racks_888047182.html)

#### Unique Rack



<http://www.bikecommuters.com/2011/08/11/a-visit-to-the-wright-cycle-company/>

#### Bicycle Rack



<http://fortheloveofbikes.blogspot.ca/2010/03/hello-pi-rack.html>

#### Bicycle Rack



[http://www.huffingtonpost.ca/2013/08/02/vancouver-bike-rack-time-limit\\_n\\_3699082.html](http://www.huffingtonpost.ca/2013/08/02/vancouver-bike-rack-time-limit_n_3699082.html)

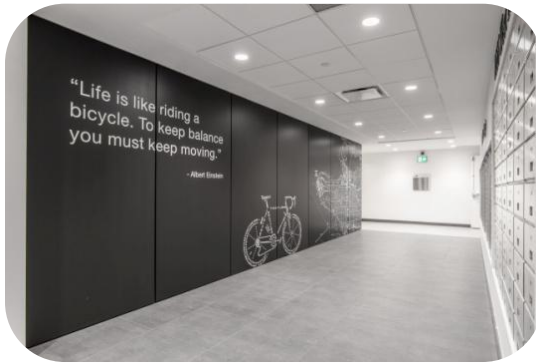


### 8.9.3 Bike Hub Rooms

Provision of a "bike hub" room on the first level of parking level is recommend to promote and facilitate cycling. The bike hub room would have a tool-equipped workshop and work bench, built-in air compressor, a bike washing station, secured lockers for helmets and wet rain gear, dedicated storage for bike tow trailers and kid tricycles. The bike hubs would include the planned long term bicycle parking spaces.

The funding, maintenance and security of this bike hub room will have to be planned in more detail. A combination of residential strata and non-residential lease agreement could provide the funding.

**Entry Area**



**Entry Area**



**Bike Maintenance Area**



**Bike Wash Station in Common Area**



<http://www.avaloncommunities.com/massachusetts/boston-apartments/ava-back-bay/pictures>

<http://plus.usgbc.org/best-of-lead/>

The entry and common room and other areas that make up the bike hub shall be purpose built, be aesthetically pleasing be designed to be welcoming, and an enjoyable place to be.



## 8.10 TDM Effectiveness

TDM effectiveness is highly dependent on the application setting, complementary strategies, nature of the travel market segment being targeted and even the “vigour” with which TDM is implemented and promoted. The effectiveness of TDM measures in terms of reducing vehicle trip-making is difficult to forecast as these measures are typically applied at different levels, in different mixes, on different sites. While several models existing to estimate the effects of TDM (EPA Commuter Model, TDM Effectiveness Evaluation Model, Worksite Trip Reduction Model, Trip Reduction Impacts of Mobility Management Strategies), these models have all been developed to address USA conditions and require extensive and detailed knowledge about the base conditions as well as individual TDM measures which are not known at the Master Planning level.

Nevertheless, research has shown that TDM programs which are very focused and site-specific, with aggressive financial incentives, disincentives and parking management have been proven to reduce trip making by over 15%.<sup>4</sup> Some communities identify/allow vehicle trip reductions for TDM measures based on transit service levels combined with the level of TDM applied. For example, **Table 8.2** below provides anticipated ranges of “net mode shift” from auto trips for various levels of TDM programs and various levels of transit provision from Fairfax County, VA, USA planning guidelines.

The reductions noted in the table below have been corroborated for work trips by other studies such as the recent TCRP report on “Employer and Institutional TDM Strategies” which shows that at work sites with “high performing” and aggressive TDM programs, employee vehicle trip reductions of up to 25% are possible with “High” transit services and pay parking.

**Table 8.2: TDM Effectiveness**

TDM PROGRAM OR STRATEGY	HIGH TRANSIT	MODERATE TRANSIT	LOW TRANSIT
Support, Promotion, Information	3-5%	1-3%	<1%
Alternative Commute Services	5-10%	5-10%	1-3%
Financial Incentives	10-20%	5-15%	1-5%
<b>COMBINED STRATEGIES</b>			
With Free Parking	15-20%	10-15%	3-7%
With Pay Parking	15-30%	15-20%	n/a

Source: Integrating Demand Management into the Transportation Planning Process: A Desk Reference, U.S. Department of Transportation, Federal Highway Administration, August 2012, page 160. Note that “High” transit = Rail; “Medium” transit = bus with peak headways 20 min. or less; “Low” = bus with headways >20 min.

<sup>4</sup> Integrating Demand Management into the Transportation Planning Process: A Desk Reference, U.S. Department of Transportation, Federal Highway Administration, August 2012



Research has shown that TDM measures tend to have the greatest influence on frequent and regular work-based trips and has lesser impacts on shopping and personal business trips which are less frequent and discretionary. Therefore, most TDM programs, and therefore monitoring of TDM program effectiveness is typically focussed on “Commuter Trip Reduction” or CTR programs.

According to the Victoria Transport Policy Institute, a comprehensive CTR program typically reduces peak-period (work-based) automobile trips by 4-20% at a worksite (Winters and Rudge 1995; Rye 2002; Boarnet, Hsu and Handy 2010), and impacts vary depending on program design, geography and employee demographics. Programs that lack financial incentives (e.g. transit subsidies, parking cash out) generally achieve reductions under 10% (Boarnet, Hsu and Handy 2010).<sup>5</sup>

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<sup>5</sup> VTPI website <http://www.vtpi.org/tdm/tdm9.htm>



## 9. SUMMARY & RECOMMENDATIONS

### Proposed Development

- (i) The proposed sites are currently being occupied by eight detached residential units. Approximately four of these will be replaced with a development consisting of 27 townhomes and another four replaced with a separate townhome development of 29 units. Both developments will share an access off of Parkgate Avenue.
- (ii) At full build-out, the combined sites are anticipated to generate approximately 25 vehicle trips in the AM (in and outbound), 29 total combined trips in the PM peak, and 29 trips on the Saturday peak – approximately one vehicle every two minutes.

### Parking and Loading

- (i) The proposed residential developments will provide sufficient off-street parkade parking to satisfy the bylaw.
- (ii) All service delivery and garbage/recycling deliveries will be via the combined shared lane in the rear of the Tatla Development property off Parkgate Avenue.
- (iii) Existing on-street parking along Parkgate Avenue was demonstrated to be very lightly utilized during spot count survey periods – less than 30% of spaces.

### Pedestrian and Cyclist Connectivity

- (i) The site is located within walking distance to nearby commercial, recreational, and community service facilities. Sidewalks are provided along both sides of most streets in the study area, and pedestrian push buttons and/or crosswalks are available at most major intersections.
- (ii) Near the development site, the only major cycling path runs along the Mt Seymour Parkway, a paved, on-street bike route.
- (iii) During peak commuting hours, the combined service bus transit headways are approximately 4-6 minutes, resulting in approximately 10-14 buses per hour.

### Traffic Operations

- (i) The analysis was conducted assuming a conservative growth rate of 0.5% per annum for the opening day of 2022, and the planning horizon of 2030.
- (ii) From a traffic operations standpoint, all intersections within the study area are all expected to operate acceptably for all analyzed AM, PM and Saturday peak hours and planning horizons.
- (iii) The only intersections with individual movements experiencing turning bay movements exceeding its storage length is the intersection of Mt Seymour Parkway / Mt Seymour Road. These volumes are the result of the conservative background growth assumption; not traffic generated by the developments.



- (iv) The only recommendation related to traffic operations is to optimize the phase splits and signals along the Mount Seymour Parkway corridor.
- (v) The District's proposal to eliminate left turns in and out of Gaspe Place would result in negligible traffic impacts on the Parkgate Avenue / Mt Seymour Parkway intersection.

#### **9.1.1 Site Improvements and TDM**

- (i) Eliminating the current private access driveways along Mt Seymour Parkway and channelizing these to Parkgate Avenue will improve site safety and reduce crash risk exposure. Site distance to and from Parkgate Avenue is good, but hedges and a short wall slightly obstruct visibility of oncoming vehicles at the Gaspe Place approach.
- (ii) Speeding was observed to the west of the current site. Upon confirmation of speeding at this location, the following treatments could be implemented to reduce median speeds:
  - a. Change the posted speed limit signage from 60 km/hr to 50 km/hr. This will likely see a far greater level of non-compliance and percentage of vehicles technically speeding, but it will likely also reduce both average and median speeds.
  - b. Narrow the lane widths accordingly for both westbound and eastbound directions (and increase width of cycling facilities).
  - c. Create more 'road friction' with on-street parking, on-street bus facilitates, or signalized intersections.
  - d. Using signal timing to prevent a green wave through multiple intersections (potential only in off-peak direction to prevent capacity concerns).
  - e. Narrow the 4-lane cross section along Mt. Seymour Parkway into a 2-lane cross section.
  - f. Provide more awareness and speed radar feedback signage along Mt. Seymour Parkway.
- (iii) The District's proposal to eventually eliminate left turn movements in and out of Gaspe Place would have the greatest likely impact on current U-turning movements. Unlike the current situation, where residents and visitors to the detached residences on the north side of Mt Seymour Parkway will perform illegal U-turn movements to travel eastbound from their driveways, all vehicle movements will be channeled to the traffic signal at Parkgate Avenue.
- (iv) The developers can jointly assist to further reduce their developments' impact on the local road network. A number of these TDM measures are outlined in Section 8 of this draft report.



# APPENDIX A

## Terms of Reference





April 12, 2017  
6127-02

Kevan Khoshons, P.Eng., PMP  
Senior Project Engineer  
District of North Vancouver

VIA EMAIL: KhoshonsK@dnv.org

Dear Kevan,:

Re: **3428-3464 and 3468-3490 Mt Seymour Parkway**  
**Terms of Reference – Combined Transportation Impact Assessment**

We have prepared for your consideration the following terms of reference for Bunt & Associates Engineering Ltd. (Bunt) to undertake a Transportation Impact Assessment for the combined developments at 3428-3464 and 3468-3490 Mt Seymour Parkway development project. These terms are the result of agreed tasks from ongoing correspondence with the District between 3/14/2017 and 3/17/2017 and subsequent discussions afterwards.

Based on our current understanding of the project, we provide the following key tasks:

### 1.1 Existing Conditions

- a) Provide descriptions of existing transportation systems of all modes (vehicle, pedestrians, cyclists, and public transit) that service the two development sites. This will extend to forecasting travel mode to other future developments on Gaspe Place as provided by the District.
- b) Provide a high level summary of key transportation-related objectives contained in relevant District plans, including: the DNV OCP, DNV Transportation Plan, Local Seymour Plan, DNV engineering guidelines and road hierarchy.
- c) Conduct a site visit to observe existing conditions (including sidewalk and transit stop quality & street parking restrictions) and collect traffic volumes.
- d) Conduct an on-street parking demand spot count on Parkway Avenue, during a weekday (once between 7:00-9:00 AM and once between 3:00-6:00 PM) and Saturday (12:00PM-3:00PM).
- e) Conduct transportation movement counts for the proposed study area intersections:

Mt Seymour Parkway and Parkgate Avenue

Mt Seymour Parkway and Gaspe Place

**Bunt & Associates Engineering (BC) Ltd.**

Suite 1550 – 1050 West Pender Street, Vancouver, BC V6E 3S7 Tel 604 685 6427 Fax 604 685 6579

Vancouver Victoria Calgary Edmonton [www.bunteng.com](http://www.bunteng.com)



Mt Seymour Parkway and Mt Seymour Rd

Mt Seymour Parkway and Apex Avenue

- f) Obtain traffic signal timing information available through the District for the signalized intersections at Apex, Parkgate, and Mt Seymour Rd.
- g) The study periods will including the following:
  - o Weekday AM Peak hour (7:00-9:00 AM);
  - o Weekday PM Peak hour (3:00-6:00PM); and,
  - o Saturday Peak Hour ( 12:00 –3:00PM)

## 1.2 Development plan

- a) Present the development plans' context and design principles. The development plan for 3468-3490 Mt Seymour will consist of 27 strata townhouse units, consisting of a mix of 2 and 3-bedroom units with all vehicular access via a rear laneway off Parkgate Avenue, with parking located underground. The development plan for 3428-3464 will consist of 29 townhouse units whose composition are as yet unknown but will be included in the draft report.
- b) Review the proposed off-street vehicle and bicycle parking and loading supply for both developments, in relation to by-law requirements and anticipated demand.
- c) Provide commentary regarding whether the proposed supplies are sufficient to meet the site generated parking demand for vehicles and bicycles.
- d) Develop a Travel Demand Management (TDM) strategy to commensurate with any shortcoming in parking identified in this task (the draft report will include potential options, while the final report will include measures agreed to by developer) as well as best practice even if no parking shortcoming is determined.
- e) Review servicing and parking access for the developments and confirm the feasibility of vehicle (passenger vehicle, garbage and emergency) manoeuvres to/from the proposed rear laneway using AutoTURN software.
- f) Provide analysis on the feasibility, issues, and constraints of a single, joint rear laneway from Parkgate Ave to Gaspé Place under the assumption that left turn movements in and out of Gaspé Place may one day be prohibited.
- g) Recommend measures to improve laneway site lines, and pedestrian and bicycle accessibility and safety.

## 1.3 Operational Assessment

- a) Apply the Synchro/SimTraffic traffic analysis model to assess existing (2017) peak period traffic operations (intersection volume/capacity, delay based Level of Service (LOS), and vehicle queues at the surveyed intersections within the traffic study area.



- b) Re-apply the Synchro/SimTraffic traffic analysis model to assess to periods of future Background Traffic operations on the area road network. For the purposes of this analysis, opening day is assumed to be 2022 and 2030 as the future study horizon.
- c) Provide trip comparison of traffic generated by current versus proposed land uses.
- d) Distribute net site generated vehicle traffic based on existing traffic patterns at Mt Seymour Parkway & Parkgate Avenue. Assume 82/15/3 assignment split of trips to/from the west, east, and north respectively.
- e) Re-apply the Synchro/SimTraffic traffic analysis model to assess future Total Traffic operations on the area road network (for 2022 and 2030 study horizons).
- f) Based on the results of the existing and future Background and Total Traffic conditions traffic operations analysis, identify the net traffic impact resulting from (i) general growth in area traffic, and (ii) the incremental impact of the added traffic associated with the redevelopments; and,
- g) Identify potential mitigation measures to address the net traffic impacts identified for both the future Background and future Total Traffic scenarios.

#### 1.4 Safety Analysis & Off- Site Improvements

- a) Undertake qualitative analysis to determine if any evident mitigation measures or improvements could be undertaken at Mt Seymour Parkway & Parkgate Avenue/Gaspe Place in regards to sightlines and speed reduction. Include a speed survey of east and westbound vehicles on Mt Seymour Parkway (at Lytton Street) in this review.
- b) Outline measures which could prevent or discourage U-tuning on Mt Seymour Parkway at Parkgate Avenue and Gaspe Place.
- c) Discuss potential multimodal signs and pavement markings for the study area.
- d) Conduct a qualitative safety review of the existing site access as compared to the proposed, to include access location and sight lines.
- e) Provide a high level rationale of the advantages and/or disadvantages to altering the access at Gaspe Place to right-in/ right-out, thought a closure to the gap of the median on Mt Seymour Parkway. All future assumptions regarding future development other than 3428-3464 Mt Seymour and unit counts will be provided by the District.
- f) Determine if a separated left turn lane will be required for northbound left vehicles from Parkgate Avenue to the site's laneway. This analysis will include future trips from both the site and adjacent *Allaire* development.
- g) Outline multi-modal considerations to better manage conflicts and to improve conditions for all modes within the study area







**Allaire Headwater (Seymour) Residences – 3428 – 3464 Mt. Seymour Parkway**

**Construction Impact Mitigation Strategy**

**Mar 23, 2018**

**Mobility Impact/Community Impact**

- Prior to commencement of construction we will have a meeting with the neighbors to introduce ourselves as well as go over the major construction activities with them. We will also inform them ahead of any major activities that may bring more truck traffic than normal such as excavation.
- We will continue to keep an open dialogue with the immediate community via telephone, email, mailed notices and in person
- Our site does have minimal neighbors as to the south is Mount Seymour Parkway, to the east is the Tatla site and to the north is a golf course and a few houses to the west that are being assembled for development
- There will be overhead trailers above the sidewalk along Mount Seymour Parkway
- Sidewalk will remain open to bikes and pedestrians.
- Unless utilizing the optional pumping area on Mount Seymour Parkway there will be no need to close a lane of traffic along Mount Seymour Parkway.
- We will coordinate truck traffic to happen during off peak times
- We will display signage to DNV standards
- Our project will begin with demolition, excavation, followed by formwork, framing and interior and exterior finishes, with landscaping being the final activity.

**Road Safety & Efficiency**

- Full site safety signage will be provided as well as sidewalk signage indicating construction site ahead. It is our intention to keep the sidewalk along Mount Seymour Parkway open except for a few major concrete pours.
- We will obtain a Highway Use Permit (HUP) for each phase of construction where a right of way alteration and/or closure is required. A Traffic Management Plan will be submitted with each HUP application
- Our site will be fenced and all construction activities will be inside the fenced area. If we need to use the driveway crossing there will be professional qualified and trained flagging personnel as well as signage.
- We will attempt to arrange heavy truck traffic and deliveries around peak traffic periods
- We have coordinated our preliminary construction sequencing with the Tatla site next door and this has been confirmed by Tatla.



### **Communication/Coordination**

- We will have bi-weekly site meetings with minutes provided to DNV and a two week look ahead to be provided. A Tatla representative will be the main point of contact and responsible for relaying this information to DNV as well as surrounding residents.
- We will coordinate with the future site at 3386 Gaspé Place as required
- We will install signage to DNV standards that provide contact information for public inquiries.
- To provide maximum efficiency Allaire and Tatla will commit to using the same companies for waste removal, temporary toilets to limit the amount of trucks coming to and from the site.
- We will use the same traffic management company to make sure there is effective communication between Parkgate Avenue, Mount Seymour Parkway and Gaspé Place

### **Site Stewardship/Monitoring**

- Our site will be fully fenced and enclosed and all construction activities will be within the fenced area. We will control dust by providing temporary roads of gravel that limit the amount of dust. We will have our site safety manager walk the site and surrounding area daily for any garbage that may have come from our site.
- We will provide and install 1 (one) pan-tilt zoom camera. DNV will advise exact location when on site. DNV will own and operate the camera. Tatla will also be responsible for one and we will use the same company for install to ensure consistency.
- The camera we choose will allow DNV to remotely access the footage.
- We will do daily inspections of construction signage and setup for compliance with traffic management

### **Phase**

- Our project is a one phase - approximately 20 months duration with an estimated start in October 2018 subject to building permit issuance.

### **Preferred Access**

- Our primary site access and egress will be off the lane at the back of our property which is serviced via the traffic light at Parkgate. We do expect during construction the trucks to enter off the lane from Parkgate and exit via the one-way lane on to Gaspé with the possibility during excavation to enter through the site and exit on to Mount Seymour. This will be monitored and controlled by a professional flagging crew and will be part of our management plan. The construction of the lane will be continuously coordinated between Tatla and ourselves.
- Please see Appendix A – Gaspé Place and Appendix A – Mount Seymour Parkway for site access



- We will have traffic control located at site exits, will coordinate with Tatla to use the same company

### **Number of Employees trade vehicles**

- We estimate the number of employees on site at one time to fluctuate between around 4 to 7 at the start to 25-40 at peak activity times. We are currently looking at a couple of parking options. Park at the large food store at the beginning of Mount Seymour Parkway and bus or carpool to the site from there, and maybe leasing a few stalls at the nearby Parkgate shopping center.. We will finalize the parking agreement once we are closer to construction have confirmation of building permit. We will also have parking available for 2 or 3 cars at the back of our site
- Once our underground parkade is complete we will make it available for trades parking.
- Trades will be carpooling, if there are 30-40 workers, we would expect 12-15 vehicles
- During demolition we only expect 2-3 vehicles
- During Excavation 3-4 vehicles
- Foundation/Parkade we expect 6-10 vehicles
- Above Grade 8-12 vehicles (parkade)
- Finishes 10-15 vehicles (parkade)
- Landscape 5-7 vehicles (parkade)
- Off-site civil 3-5 (parkade)

### **Excavation and Demolition**

- Demolition will be minimal trucks - maybe one or two a day.
- Excavation will start with 1 -2 trucks an hour and ramp up to 4-6 trucks an hour. Routing at this stage will be per attached Appendix B – Truck Route
- Wheel wash will be provided during excavation for any vehicles driving from the site on to Mt. Seymour Parkway. See Appendix A – Mount Seymour Parkway

### **Construction**

- We have not fully determined if we will be installing a crane on this site at this point. If we do decide to go with a crane it can be installed from the rear lane.
- The rear lane will be a big asset to the project and we will be able to direct a lot of the traffic through there.
- We anticipate the lane will be a main staging area for loading and unloading, as well as the primary pumping location.
- There might be a few concrete pours where through the highway use permit process we will pour off Mount Seymour Parkway during off peak hours. We will have professional traffic



management staff, a traffic plan prepared and reviewed with District of North Vancouver and full flagging personnel and signage prior to any pours being arranged.

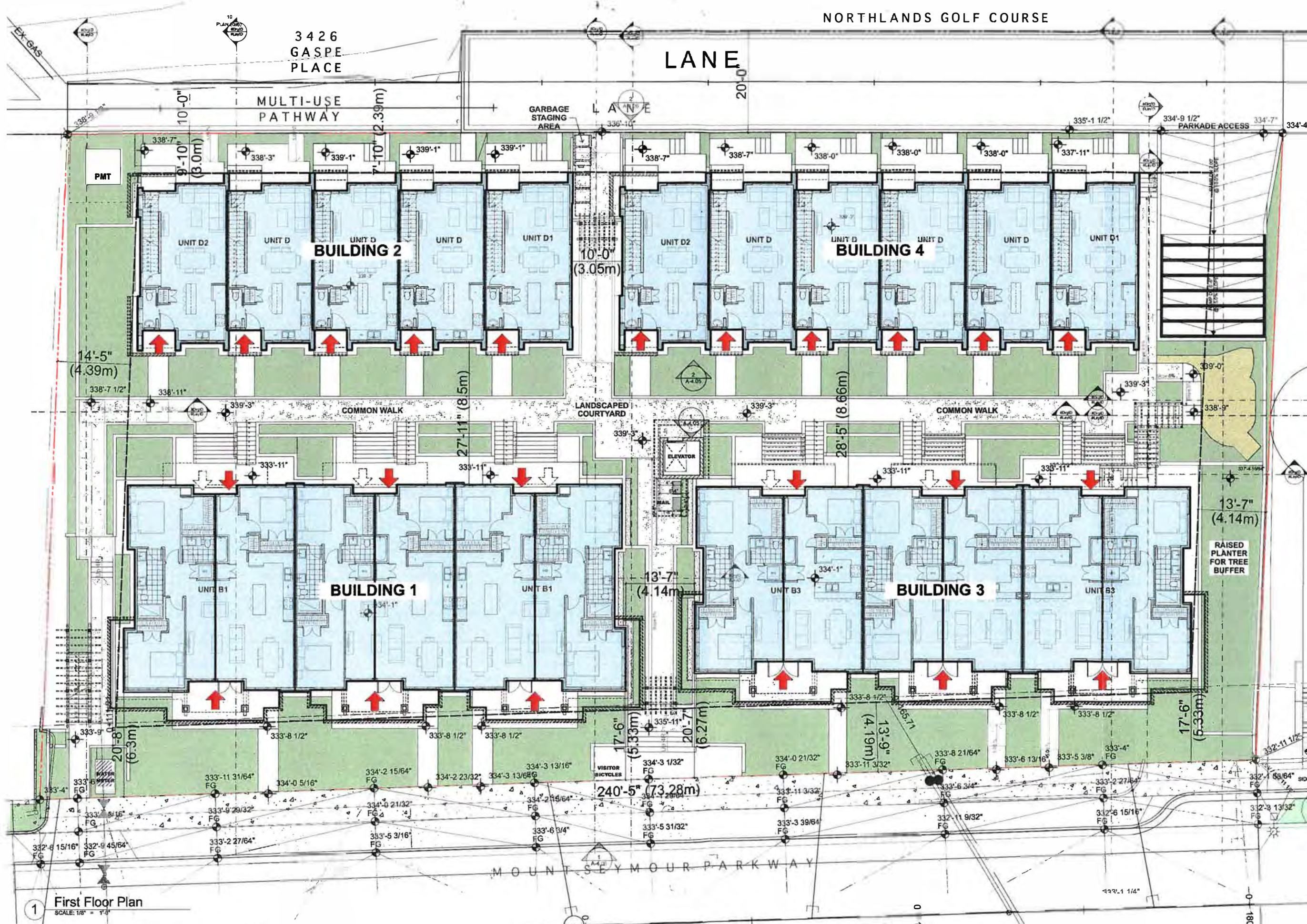
- These pours would be utilized during off-peak times where we would need to leave the lane free for Tatla to access
- We will not impact westbound traffic on MSP parkway before 9:30am
- The size of pumper we plan on using is approximately 18 ft wide with riggers fully extended. Most pours will not require all riggers to be fully extended and can be set up diagonally. – See attached picture for reference.
- Working closely with Tatla we will coordinate trucks and deliveries to avoid any trucks waiting in queue to avoid any potential traffic conflicts and have minimal amount of disturbance to Mount Seymour Parkway, Parkgate Avenue and Gaspe Place.
- Schematic site plan has been updated with dimensions showing the sizes of concrete pour area along south side of site, concrete washout, lane dimensions and remaining area from lane to excavation.

#### **Other DNV Comments**

- Show traffic plans for service connections – TBC in future

**A full report will accompany our building permit application in accordance with the District Guidelines in close conjunction with our selected contractors.**





1 First Floor Plan  
SCALE: 1/8" = 1'-0"

NORTHLANDS GOLF COURSE

LANE

3426  
GASPE  
PLACE

MULTI-USE  
PATHWAY

GARBAGE  
STAGING  
AREA

LANE

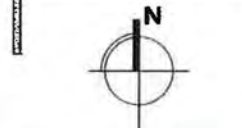
PARKADE ACCESS



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**ALLAIRE**  
HeadwaterLiving

(ARCHITECT SEAL)

ALLAIRE GROUP/  
HEADWATER LIVING

(PROJECT)

Mount Seymour  
Townhomes

3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

(TITLE)

**REVISED**  
**SITE PLAN**  
*USED for Rezoning Report.*

16399 (PROJECT)

1/8" = 1'-0" (SCALE)

Friday, September 1, 2017 (DATE)

Rezoning & DP Application (SUBJECT)

(DRAWING)

A-1.101



# MOUNT SEYMOUR TOWNHOMES

3428 - 3464 Mount Seymour Parkway, North Vancouver, BC

## ISSUE #4 - DEVELOPMENT PERMIT / FIRST READING

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DRAWING INDEX - DOCUMENTATION

ARCHITECTURAL

Ø A-0.000	COVER SHEET	N/A	
Ø A-0.010	SITE MAP	N/A	
Ø A-0.020	DESIGN RATIONALE	N/A	
Ø A-0.030	DESIGN GUIDELINES COMPLIANCE	N/A	
Ø A-0.100	REFERENCE IMAGES	N/A	
Ø A-0.200	DESIGN DATA	N/A	
Ø A-1.00	TOPOGRAPHIC SURVEY	3/32" = 1'-0"	
Ø A-1.01	SITE PLAN	1/8" = 1'-0"	
Ø A-1.02	SITE PLAN (SIMPLIFIED)	1/8" = 1'-0"	
Ø A-1.03	PARKADE PLAN	1/8" = 1'-0"	
Ø A-1.04	CONTEXT PLAN	1" = 20'-0"	
Ø A-1.05	SHADOW ANALYSIS	NTS	
Ø A-1.06	PIRE ACCESS PLAN	1/8" = 1'-0"	
Ø A-1.07	PRIVATE OUTDOOR SPACE COMPLIANCE	1/8" = 1'-0"	
Ø A-1.08	NATURAL GRADE CALCULATION PLAN	1/8" = 1'-0"	
Ø A-1.09	SITE COVERAGE PLAN	1/8" = 1'-0"	
Ø A-2.01	BUILDING 1 FLOOR PLANS	1/8" = 1'-0"	
Ø A-2.02	BUILDING 2 FLOOR PLANS	1/8" = 1'-0"	
Ø A-2.03	BUILDING 3 FLOOR PLANS	1/8" = 1'-0"	
Ø A-2.04	BUILDING 4 FLOOR PLANS	1/8" = 1'-0"	
Ø A-4.01	BUILDING 1 ELEVATIONS	1/8" = 1'-0"	
Ø A-4.02	BUILDING 2 ELEVATIONS	1/8" = 1'-0"	
Ø A-4.03	BUILDING 3 ELEVATIONS	1/8" = 1'-0"	
Ø A-4.04	BUILDING 4 ELEVATIONS	1/8" = 1'-0"	
Ø A-4.05	COURTYARD ELEVATIONS	3/32" = 1'-0"	
Ø A-4.06	STREET ELEVATIONS	3/32" = 1'-0"	
Ø A-5.01	BUILDING SECTIONS	1/8" = 1'-0"	
Ø A-5.02	BUILDING SECTIONS	3/32" = 1'-0"	
Ø A-8.01	PERSPECTIVE VIEW FROM MOUNT SYMOUR PKWY	NTS	
Ø A-8.02	COURTYARD PERSPECTIVE	NTS	
Ø A-8.03	LANEWAY PERSPECTIVE	NTS	
Ø A-8.04	MATERIALS & COLOURS	NTS	

LANDSCAPE

Ø L-0	TREE PLAN	1:100	
Ø L-1	LANDSCAPE MATERIAL PLAN	1:100	
Ø L-1a	MATERIALS PLAN	1:100	
Ø L-1b	DETAILED AMENITY AREAS MATERIALS PLAN	1:50	
Ø L-2	PLANTING PLAN	1:100	
Ø L-3	LANDSCAPE DETAILS	Varies	
Ø L-3a	DETAILS PLAN	1:25	
Ø L-3b	CUT SHEETS	NTS	
Ø L-4	OFFSITE STREET PLAN	1:100	
Ø L-5	OFFSITE PARK PLAN	1:100	

1- OCT 2416 PRELIMINARY DP APPLICATION  
2- JULY 1217 REZONINGDP APPLICATION  
3- SEP 2017 ADVISORY DESIGN PANEL  
4- FEB 1618 DEVELOPMENT PERMITFIRST READING



Architectural Package - Used for  
Rezoning Report (submitted  
Feb. 22, 2018) DM#3492530



## Integra

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ALLAIRE GROUP/  
HEADWATER PROJECTS

[PROJECT]

MOUNT SEYMOUR  
TOWNHOMES

3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[TITLE]

COVER

[PROJECT]

16399

[SCALE]

Thursday, February 15, 2018

4.0 Development Permit

[DRAWING]

A-0.00





**1** LOOKING SOUTH EAST FROM MT SEYMOUR PARKWAY

**2** LOOKING WEST FROM MT SEYMOUR PARKWAY

**3** LOOKING EAST FROM GASPE PLACE

**4** LOOKING WEST FROM MOUNT SEYMOUR PARKWAY



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**ALLAIRE GROUP/  
HEADWATER PROJECTS**

[PROJECT]

**MOUNT SEYMOUR  
TOWNHOMES**

3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[TITLE]

**SITE MAP**

16399 [PROJECT]

NOT TO SCALE [SCALE]

Wednesday, February 14, 2018 [DATE]

4.0 Development Permit [ISSUE]

[DRAWING]

**A-0.010**



## DESIGN RATIONALE

### Current Use & Development of Existing Site and Surroundings:

The subject site consists of Lots 15-18 on the North side of Mount Seymour Parkway, West of Parkgate Avenue. The existing 4 lots are approximately 18.3m wide each and vary in depth. The overall site is slightly irregular in shape with the front property line at the angle of Mount Seymour Parkway creating the trapezoidal shape. The site rises in elevation from South to North approximately one metre from the existing Mount Seymour Parkway sidewalk to the proposed lane to the North. Two further single-family lots will remain to the West of this development towards Gaspé Place. To the North of the proposed development is a currently un-opened lane allowance, a heavily treed green space and the Northlands Golf Course. Immediately East of the development site are 4 single family lots currently under development to create 27 townhouse units.

**Proposed Development:**

This three-storey townhouse development, of 4 buildings and 29 suites, is oriented around an internal, landscaped courtyard with walkways that create linkages to Mount Seymour Parkway, Parkgate Avenue and adjacent recreational areas. Each building has been positioned to reinforce North-South views, maximize solar exposure, and strengthen the connections between the neighbourhood and the pedestrian greenways.

A mixture of housing forms is proposed, including single level 3 bedroom units, 2-level stacked townhomes and 3-level row homes which will appeal to a broad spectrum of the public looking to live in this area of the North Shore.

The proposed massing responds to the Guidelines for Multi-Family Housing of the District of North Vancouver Official Community Plan for low-rise housing (see additional letter for specific guideline compliance). The site is currently designated “Residential Level 4: Transitional Multi-Family” which allows a mix of apartment and townhouses up to approximately 1.2 FSR. The proposal is consistent with this designation. Rezoning is required as the current use is RS1 (Single Family Residential One Acre Zone).

### Site Planning, Landscape & Public Realm:

The proposed development fits into the transitional neighbourhood context. A new sidewalk/boulevard is proposed connecting this development site to transit (bus stop West of Parkgate Avenue) and the neighbourhood amenities.

The shadow analysis provided in the Architectural drawing set indicates shadowing predominantly on the proposed lane on the North side and some shadowing in the central East-West courtyard. Living rooms of units have been purposefully located so that they are never opposite each other and the courtyard maintains a minimum of 9m in width. The courtyard area is essentially semi-private space. Private open space in excess of the minimum 9m<sup>2</sup>/unit is provided in the form of ground level patios/yards for ground oriented units and roof decks for the upper units in the stacked townhouse form and for the 3-level row homes.

The positioning of the buildings addresses potential traffic noise concerns, but also to develop both an important courtyard space encouraging social interaction among residents. Each building will provide a strong street presence with residential character coupled with appropriate landscape planting, while the dominant building form will establish a north/ south axis connecting Mount Seymour Parkway to the rear access lane. Substantial elements such as larger roof overhangs and the use of stone are proposed at the corners and main entries to create a sense of permanence. The building's size, colours, exterior materials, form and height are intended to provide significant curb appeal while suggesting a feeling that the townhomes are established residences. Individual, residential scale entries along landscaped pathways contribute to the strong building façades—a balance of horizontal and vertical elements—designed so that each 3 storey building maintains a scale suitable to this established neighbourhood. The same attention to detail has been applied to the Laneway facades to create an interesting lane-scape as a secondary 'front'. Patios along this edge will bring some activity to the space and take advantage of the pleasing outlook to the North.

The landscape design of this project is intended to create attractive, sustainable green spaces for the site, as well as a unique, friendly streetscape for the neighbourhood. This is achieved with generous 'boulevarding' of the various facades and an interior series of connecting paths via an elevated courtyard. All areas are intended to be well planted to give the site a verdant, lush appearance. The internal courtyard is designed for outdoor activities such as gardening, playing, social gathering, and cooking, to promote a community atmosphere.

In addition, the elevated courtyard intends to draw residents towards the various landscape features, along with a parkade access and mailboxes kiosk where community gathering is expected. To the East is Parkgate Village, which includes a recreation centre, library, and Skate Park as well as other important community services, encouraging family and community participation.

### Building Form:

A mix of strong, bold materials including stone reinforcing corners and fibre cement panels are proposed to highlight the subtle, undulating three storey townhome building form. Careful attention will be paid to roof lines and façade proportions reinforcing a residential character that fits comfortably within the Seymour neighbourhood. Oversized glazing features are proposed for this development allowing for an abundance of natural light, while large roof overhangs will control the added benefit of passive solar heating during the colder months.

Building height respects the 12m maximum height and includes stair 'pop-ups' for the upper stacked units to provide access to the roof decks. These pop-ups have been located to the North side of Building 1 & 3 fronting Mount Seymour Parkway to minimize their impact on the streetscape.

All unit entries are provided with generous weather protection. Elevator access is provided from the parking below grade to the semi-private courtyard space. Garbage, electrical service (transformer), parking access and recycling are located off the new lane to the North. Garbage will be contained in a secure room in the basement parking area and will require to be hauled up to the lane for pick-up.

### Sustainability:

Measurable environmentally responsible and sustainability targets will be developed and assessed for the project to ensure equivalency with an established Green building certification. Features such as storm-water retention are to be finalized in the design development phase to confirm practical implementation of these objectives. Sustainability strategies such as high-performance envelope systems, recycled content of building materials, low VOC interior finishes, waste diversion, parking spaces pre-wired for electric vehicle charging as well as high energy efficient appliances & fixtures will be considered. This development is intended to contribute to the Seymour neighbourhood, according to the District of North Vancouver's guidelines, by creating a more sustainable and livable community, providing a range of ground-oriented, family-friendly townhomes in a practical and cost-efficient manner.

**Accessibility:**

The site has several grading challenges, which cannot easily be resolved with ramps. A 5' grade difference exists between Mount Seymour Parkway along the West property line to the proposed lane to the North. A 4' grade difference occurs along the north property line with the Eastern edge the low point. A little over 2' fall exists along the East property line with the low point at Mount Seymour Parkway. Diagonally, there is a 6' grade difference from a low in the Southeast corner to a high at the Northwest corner.

Buildings 1 & 3 are comprised of stacked townhouse units, the upper units of which are accessed via stairs from the common courtyard. The BC Building Code requires that no more than 1.5m be permitted between grade and the entry door of the uppermost unit, hence the requirement for stairs up to the upper units and steps down to the lower unit rear patios. With some grading manipulation, the six ground level units fronting Mount Seymour Parkway have unobstructed pathways to the entries off the street. It is proposed that 4 of these 6 units be adaptable housing to the District enhanced standard. The buildings along the lane are effectively 4 storeys in height with their storage/access direct from the parking area. The main floor of these units corresponds with the common courtyard walkway – steps are required only off the lane. In total, 17 of 29 units can be accessed without steps.

For convenience, an elevator connects the parkade to the semi-private courtyard space. 4 accessible stalls are provided to serve the adaptable dwelling units.



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[CLIENT]

**ALLAIRE GROUP/  
HEADWATER PROJECTS**

[PROJECT]

## MOUNT SEYMOUR TOWNHOMES

3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[ TITLE ]

## DESIGN RATIONALE

16399 [PROJECT]

Wednesday, February 21, 2018 <sup>[DATE]</sup>

## 4.0 Development Permit (ISSUE)

[DRAWING]

**A-0.020**



DP Guidelines Compliance – Form & Character

1 Public Realm, Streetscape Elements and Neighbourhood Fit

C1.1 Height & massing

Building height is maximum +/- 12 metres with buildings 1&3 having the greatest height at 12.07m to the highest portion of roof. Large overhangs, secondary, feature roofs at the first level, building recesses and additional flat roofs reduce the apparent building height and massing by interjecting some horizontal elements into what is typically a building form expressed vertically.

C1.2 Roof Treatment

Roofs are flat to reflect the contemporary architectural design and to allow the opportunity to use the roof area in the form of roof decks.

C1.3 Street Orientation

Units are oriented to the street and lane, with unit entries visible from each roadway. At grade patios encourage use of the spaces between building and street.

C1.4 Corner Lots

This project is mid-block & not a corner lot.

C1.5 Minimum Frontage

Frontage for this development exceeds 20m – 73.28m.

C1.6 Setbacks

Setbacks vary given the trapezoidal site shape but generally relate to the proposed development immediately to the East and the built townhouse development further East, on the East side of Parkgate Avenue.

2 Site planning and landscaping

C2.1 Tree Retention

No tree retention is possible on the site, however, the landscape architect will look at using some of the wood on site, as well as using the existing heritage stump as part of the amenity area.

C2.2 Sustainable Landscape Design

The landscape features native and adaptive plants that are able to survive both our dry summers and our wet, dark winters. The principles of ‘Right Plant, Right Place’ drive our plant selection process and plants selected are proven through past projects to be successful in multifamily housing situations with minimal intervention. The microclimates of the site have been analysed, the plants selected to match the growing conditions. The planting is dense, to allow for moisture conservation through reducing exposed soil, while allowing the plant to express its natural form. Mulch in all planting beds helps retain soil moisture, and the irrigation system is specified as high-efficiency.

C2.3 Street Interface

The front yard fencing will not exceed 4’ and the landscape is designed to create a pleasant and welcoming street face. Front entrances with low gates add to the sense of connection to the street.

C2.4 Privacy

Dense planting and privacy screens are typical between units and neighbouring sites to maximize privacy. A 6’ wood privacy fence is proposed along the side yard property lines.

C2.5 Shared outdoor Space

Multiple opportunities for intimate and usable shared outdoor spaces have been provided, with a variety of seating types. A small amenity area with a playhouse, a heritage stump and edible plants has been provided. Each unit has usable private outdoor space.

C2.6 Private Outdoor Space

Buildings 2 & 4 have small yards front & back as well as roof decks, which, in total, exceed 9 square metres. Ground level units in Buildings 1 & 3 have a patio off the courtyard and front yards. In total, these exceed 9 square metres. Upper units have roof decks exceeding 9 square metres.

C2.7 Outward Facing Aspect

Units are oriented so that windows from the principal living space of each unit are separated by a minimum of 9 metres from those in another unit. The courtyard varies in width from 9.12m between buildings 1 & 2 to 9.27m between buildings 3 & 4. Principal living rooms are not located opposite to each other anywhere in the project. Living rooms either face Mount Seymour Parkway (Buildings 1 & 3) or the lane (Buildings 2 & 4)

C2.8 Rear Yard Setbacks

Rear Yard setback along the lane is 7'-10" (2.39m) to the building face, reduced to 5'-10" (1.77m) at the feature demising walls of Buildings 2 & 4. This is consistent with the proposed development to the East and provides for some activity and good overlook of the lane.

C2.9 Side Yard Setbacks

West side yard setback is 14'-5" (4.39m). East side yard setback is 11'-11" (3.63m).

C2.10 Pedestrian Access

The main pedestrian access to the development is centrally located, connecting directly to Mount Seymour Parkway.

C2.11 Parking

Parking is located underground, accessed via a driveway from the lane in the North-East corner of the property.

C2.12 Parking Access

Parking is accessed from the Lane. Existing driveways off Mount Seymour Parkway on the 4 lots will be removed and boulevard/ sidewalk enhanced.

C2.13 Shared Driveways

The adjacent development to the East has a parking access centrally located. It was not feasible to propose a shared driveway.

C2.14 Oil & Grit Separators

Oil & Grit separators will be provided in the parking area.

3 Architectural Character

C3.1 Massing

The front façade is broken up with secondary roofs and recesses at entries, projecting corners with feature cladding and sun shading devices. Different materials are applied to each different wall plane to reduce the bulk.

C3.2 Variations in Design

There are two buildings fronting Mount Seymour Parkway. There are subtle differences between the two buildings given that the unit layouts they are comprised of are slightly different. Overall, the architectural approach is the same for both. The architecture is quite different to the proposed development immediately to the East, which will ensure there is not a repetitive appearance to the streetscape.

C3.3 Cladding

The buildings will be wrapped in a variety of fibre cement products (wood-effect siding, horizontal siding and panel) and stone is proposed as an accent material at the corners of the two street fronting buildings. Fibre Cement is proposed for its durability and low maintenance as well as its similar application to wood siding.

C3.4 Varied Rooflines

Flat roofs are proposed with deep overhangs. To animate the street façade and to minimise the visual bulk, secondary roofs over entries to the ground level units are proposed. At the roof level, the roofline is intermittent with parapet walls/ guards providing an interesting roof-scape and avoiding a solid line.

C3.5 Roofing Materials

Roofs are flat and accommodate roof decks. Roof decks will have composite decking as a finish material. Flat roofs will likely be of a bitumen-based product with a granular coat/ballast. Some of the flat roofs will be visible from units within the development but, in general, the materials will not be visible from the street.

C3.6 Noise Levels

The proposed development will meet the required minimum acoustic standards. A professional engineer will prepare an acoustic report and recommendations will be applied to the design at Building Permit.

C3.7 Heating & Ventilation Systems

Mechanical systems will be designed to minimise noise. Any equipment located outside of the building envelope (unknown at time of this application) will be screened.

C3.8 Accessible Entrance

The site has several grading challenges, which cannot easily be resolved with ramps. A 5’ grade difference exists between Mount Seymour Parkway along the West property line to the proposed lane to the North. A 4’ grade difference occurs along the north property line with the Eastern edge the low point. A little over 2’ fall exists along the East property line with the low point at Mount Seymour Parkway. Diagonally, there is a 6’ grade difference from a low in the Southeast corner to a high at the Northwest corner.

Buildings 1 & 3 are comprised of stacked townhouse units. The upper units are accessed via stairs from the common courtyard. The BC Building Code requires that no more than 1.5m be permitted between grade and the entry door of the uppermost unit, hence the requirement for stairs up to the upper units and steps down to the lower unit. The ground level units are double-aspect and have entrances facing Mount Seymour Parkway. Pathways to these entrances are not obstructed by steps. 4 of the 6 ground level units are proposed as adaptable units. Accessible parking is provided (1:1) in the underground parking. The elevator has a stop to allow movement unhindered by stairs from the parking area to the doors fronting Mount Seymour Parkway.

Buildings 2 & 4 have entrances level with the common walkway in the centre of the project. In all, 17 of the 29 proposed units have entrances that can be reached without stairs. The common walkway has a ramped connection to the lane.

C3.9 Weather Protection

Roof protection has been provided over each unit entry.

C3.10 Front Door Width

Front doors are 3’ in width.

C3.11 Accessible Doorbell

Doorbells shall be located no higher than 1.0m above the entry way to each unit.

C3.12 Legible Address

Unit numbering will be sized appropriately for legibility (minimum 10cm tall).



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[ARCHITECT SEAL]

[CLIENT]

ALLAIRE GROUP/  
HEADWATER PROJECTS

[PROJECT]

MOUNT SEYMOUR  
TOWNHOMES

3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[TITLE]

DESIGN  
GUIDELINES  
COMPLIANCE

[PROJECT]

16399

[SCALE]

Wednesday, February 14, 2018

4.0 Development Permit

[DRAWING]

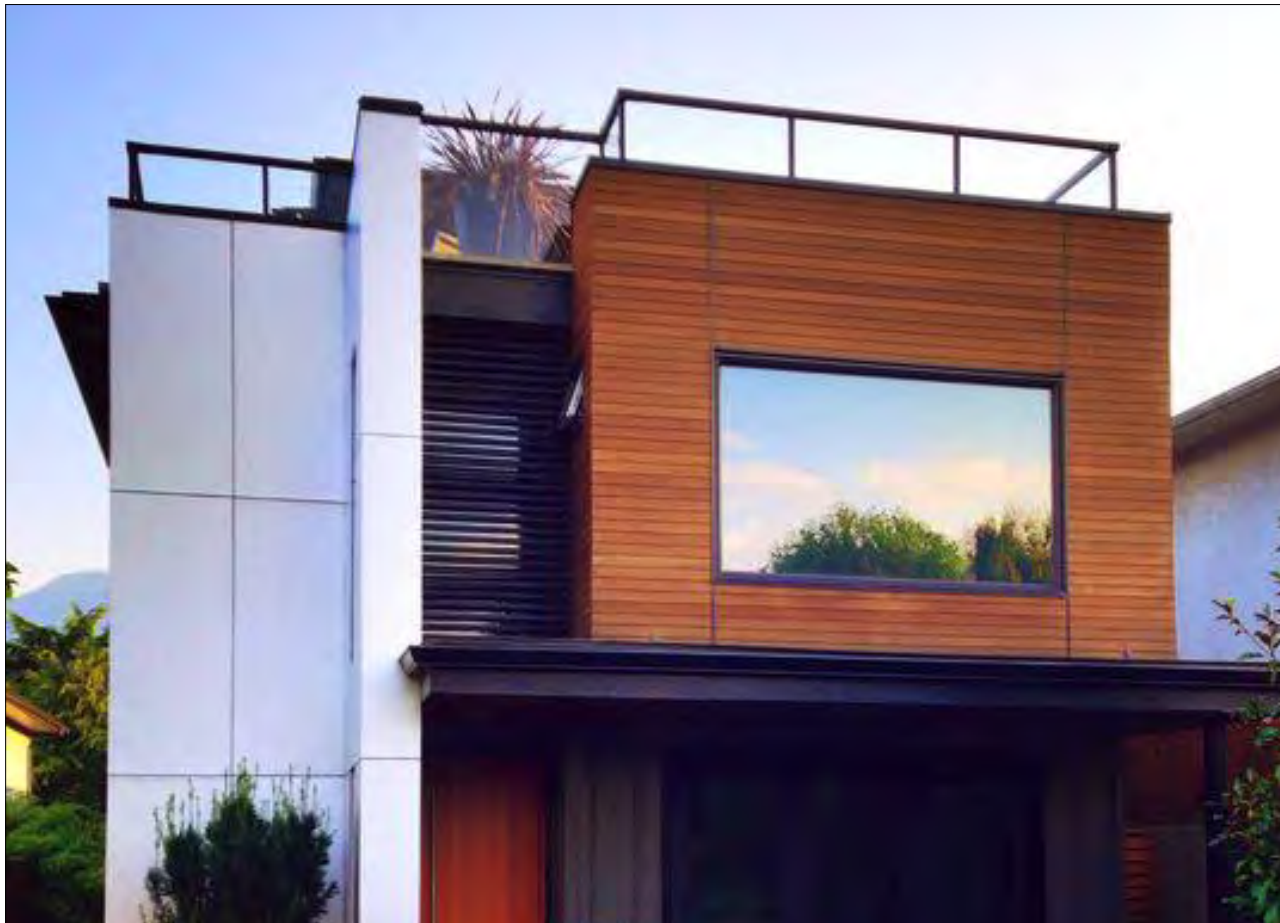
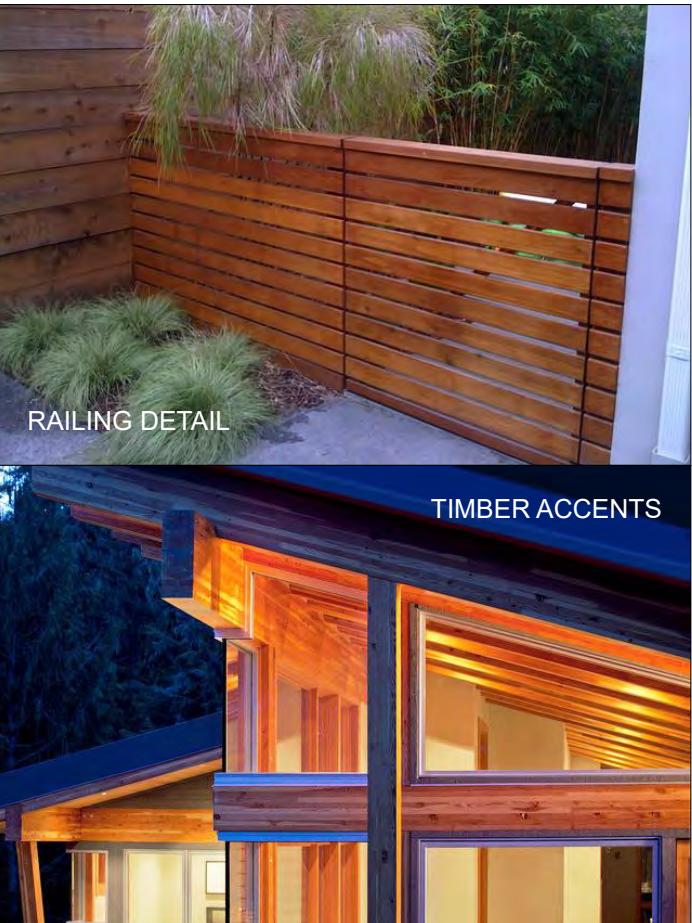
A-0.030



TOWNHOME CHARACTER

DESIGN THEME

Contemporary West Coast aesthetic featuring timber accents, strong lines, large windows, and spacious balconies.



1 OCP DESIGNATION  
NOT TO SCALE



2 ZONING MAP  
NOT TO SCALE



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[PROJECT]

MOUNT SEYMOUR  
TOWNHOMES  
3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[TITLE]

REFERENCE  
IMAGES

16399 [PROJECT]

NOT TO SCALE [SCALE]

Wednesday, February 14, 2018 [DATE]

4.0 Development Permit [ISSUE]

[DRAWING]

A-0.100



MOUNT SEYMOUR PARKWAY TOWNHOMES

Project No. 16399  
Date: February 15, 2018  
Issue: RZ/DP/First Reading

PROJECT SUMMARY:

PROJECT NAME: Mount Seymour Parkway Townhomes

PROJECT OWNER: Allaire Group/Headwater Projects

PROJECT ARCHITECT: Duane Siegrist, Integra Architecture Inc., 2330 - 200 Granville Street, Vancouver, BC, V6C 1S4  
T.604.688.4220

PROJECT ADDRESS: 3428 + 3436 + DNV lot + 3464 Mount Seymour Parkway, District of North Vancouver

LEGAL DESCRIPTION: N/A

ZONING:

Existing Zoning: Residential Level 1  
Proposed Zoning: Residential Level 4  
OCP Designation: Residential Level 4

SITE AREA:

Total gross site area\* 0.7023 Acres Total 30,592.6 SF 2,842.14 m<sup>2</sup>

\* To be confirmed by survey

FLOOR AREA RATIO: (Based off of \*TOTAL AREA FOR FAR\*)

Max. FAR 1.20 36,711.1 SF 3410.6 m<sup>2</sup>  
Proposed FAR - New Buildings 1.20 36,707.1 SF 3,410.2 m<sup>2</sup>

TOTAL GFA:

TOWNHOUSES

Unit Type	No Units	Basement	Level 1	Level 2	Level 3	Roof	Area (SF) per Unit	Total GFA (SF)	Total GFA (m <sup>2</sup> )
A (3 Bed)	4			620.59	618.34	21.03	1,259.96	5,039.84	468.22
A1 (3 Bed)	2			664.83	662.49	21.03	1,348.35	2,696.70	250.53
A2 (2 Bed)	4			556.96	554.75	22.47	1,134.18	4,536.72	421.48
A3 (2 Bed)	2			595.79	593.17	22.47	1,211.43	2,422.86	225.09
B (3 Bed - Adaptable)	1		1,241.19				1,241.19	1,241.19	115.31
B1 (3 Bed - Adaptable)	2		1,285.38				1,285.38	2,570.76	238.83
B2 (3 Bed)	1		1,112.09				1,112.09	1,112.09	103.32
B3 (3 Bed)	1		1,151.82				1,151.82	1,151.82	107.01
B4 (3 Bed - Adaptable)	1		1,151.82				1,151.82	1,151.82	107.01
D (3 Bed + Den)	7	225.51	505.87	506.44	326.83		1,564.65	10,952.55	1,017.53
D1 (3 Bed + Den)	2	225.51	511.13	511.69	329.83		1,578.16	3,156.32	293.23
D2 (3 Bed + Den)	2	225.51	510.46	511.04	330.00		1,577.01	3,154.02	293.02
TOTAL AREA	29							39,186.69	3,640.56

Building	No Units	Basement	Level 1	Level 2	Level 3	Roof	Total GFA (SF)	Total GFA (m <sup>2</sup> )
Building 1	9		3,812.03	3,812.03	3,798.29	126.18	11,548.53	1,072.89
Building 2	5	1,651.92	2,541.87	2,544.65	1,639.99	0.00	6,726.51	624.91
Building 3	9		3,415.35	3,415.35	3,399.60	134.79	10,365.09	962.95
Building 4	6	1,397.57	3,048.31	3,051.74	1,966.95	0.00	8,067.00	749.45
TOTAL AREA FOR FSR	29	0.00	12,817.56	12,823.77	10,804.83	260.97	36,707.13	3,410.20

Unit Bedroom Ratio

Unit Type	No. of Units	Percentage of Total
3+ Bed - A/A1/B/B1/B2/B3/B4/D/D1/D2	22	76%
2 Bed - A2/A3	7	24%
Adaptable units - B/B1/B4	4	14%

PARKING REQUIRED:

Resident Parking	3BR+ (A/A1/B/B1/B2/B3/D/D1/D2)	22 Units @	1.75 Stalls per Unit	39
Visitor Parking	2BR (A2/A3)	7 Units @	1.75 Stalls per Unit	12
			0.25 Stalls per Unit	7
Total Parking Required:				58

PARKING PROVIDED:

Resident Parking	29 Units @	1.79 Stalls per Unit	52
Visitor Parking	29 Units @	0.25 Stalls per Unit	7
Car Share Parking		0.00 Stalls	0
Total Parking Provided:			59

HC Parking Required	(1 per 100 stalls)	1
HC Parking Provided		4 [ 1:1 Ratio for Adaptable Units ]
Small Cars Provided	25% < 35% Max.	15

BICYCLE / STORAGE LOCKERS:

Private Bicycle Parking Provided	(Bicycle storage within units at P1 level) (Common Bicycle Storage Rooms) 2'-0" x 6'-0" bicycle lockers provided	11 18 29 Stalls	** Note: Units with direct access from Level P1 to suite have storage space for more than one bicycle ** Note: Three bicycle storage rooms are proposed in the parkade
Visitor Bicycle Stalls provided		9 Stalls	** Ratio 0.2 / unit - 3 at grade / 3 in the visitor parking area on P1

BUILDING SITE COVERAGE:

Max. Bldg Coverage:	40.00%	12237.04 SF	1136.86 m <sup>2</sup>
Proposed Bldg Coverage:	46.05%	14088.45 SF	1308.86 m <sup>2</sup>
(Excl. overhangs, balconies, porches, stair) (Inc. Covered balconies/porches and landings above 3'-0" AFF)			
Gross Bldg Coverage:	49.95%	15280.01 SF	1419.56 m <sup>2</sup>
(Excl. overhangs, balconies, porches, stair) (Inc. Covered balconies/porches and landings above 3'-0" AFF/exit stairs/elevator/parking ramp)			

BUILDING SETBACKS:

	Proposed Setback	Required Setback	
South	Mount Seymour Parkway	13'-9" (4.19m)	13'-9" (4.19m) [ To entry porch columns ]
North	Rear (Adjacent Property)	7'-10" (2.39m)	7'-10" (2.39m) [ To face of exterior wall ]
North	Rear (Adjacent Property)	5'-9.5" (1.77m)	5'-9.5" (1.77m) [ To face of architectural pilaster element ]
West	Side (Adjacent Property)	14'-5" (4.39m)	14'-5" (4.39m)
East	Side (Adjacent Property)	11'-11" (3.63m)	11'-11" (3.63m)

PROJECT DATA

SCALE: 1:0.57

BUILDING SEPARATION:

		Proposed Setback	Required Setback
Building 1 to Building 2	Courtyard spacing North/South	27'-11" (9.12m)	29'-6" (9.0m)
Building 3 to Building 4	Courtyard spacing North/South	30'-5" (9.27m)	29'-6" (9.0m)
Building 1 to building 3	Project Entry	15'-6" (4.72m)	N / A
Building 2 to Building 3	Side (Adjacent Property)	13'-8" (4.16m)	N / A

BUILDING HEIGHT:

roof slope <1:12 Max. 35.0 ft  
roof slope >1:12 Max. 40.3 ft (15 bonus)

BUILDING HEIGHT:

		Proposed Height	Permitted Height	
Building 1		39'-6" (12.04m)	35'-0" (10.67m)	[ To top of roof of stair pop-up ]
Building 2		36'-4" (11.07m)	35'-0" (10.67m)	[ To top of roof parapet wall ]
Building 3		39'-6" (12.04m)	35'-0" (10.67m)	[ To top of roof of stair pop-up ]
Building 4		38'-1" (11.61m)	35'-0" (10.67m)	[ To top of roof parapet wall ]

Notes:

Note 1: Final areas to be confirmed  
Note 2: Unit areas are measured to the center of partywalls and to the exterior of sheathing of exterior walls (marked on the floor plans as shaded areas)  
Note 3: The proposed FAR excludes all garage area, crawlspace under stair, storage at 1-st storey (100 SF), elevator framing 25 s/f unit where app., bay windows w/ benches Ht.18", areas at top storey with ceiling less than 7ft.  
Note 4: Front and rear yard setback is measured to the outermost build. structure, excluding porches, stair, bay windows  
Porches , balconies, verandas are not permitted to encroach into front and rear setback  
Side yard setback is measured to the main building face.

PROJECT DATA

SCALE: 1:0.57



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ALLAIRE GROUP/  
HEADWATER PROJECTS

[PROJECT]

MOUNT SEYMOUR  
TOWNHOMES

3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[TITLE]

DATA SHEET

16399 [PROJECT]

1:0.57 [SCALE]

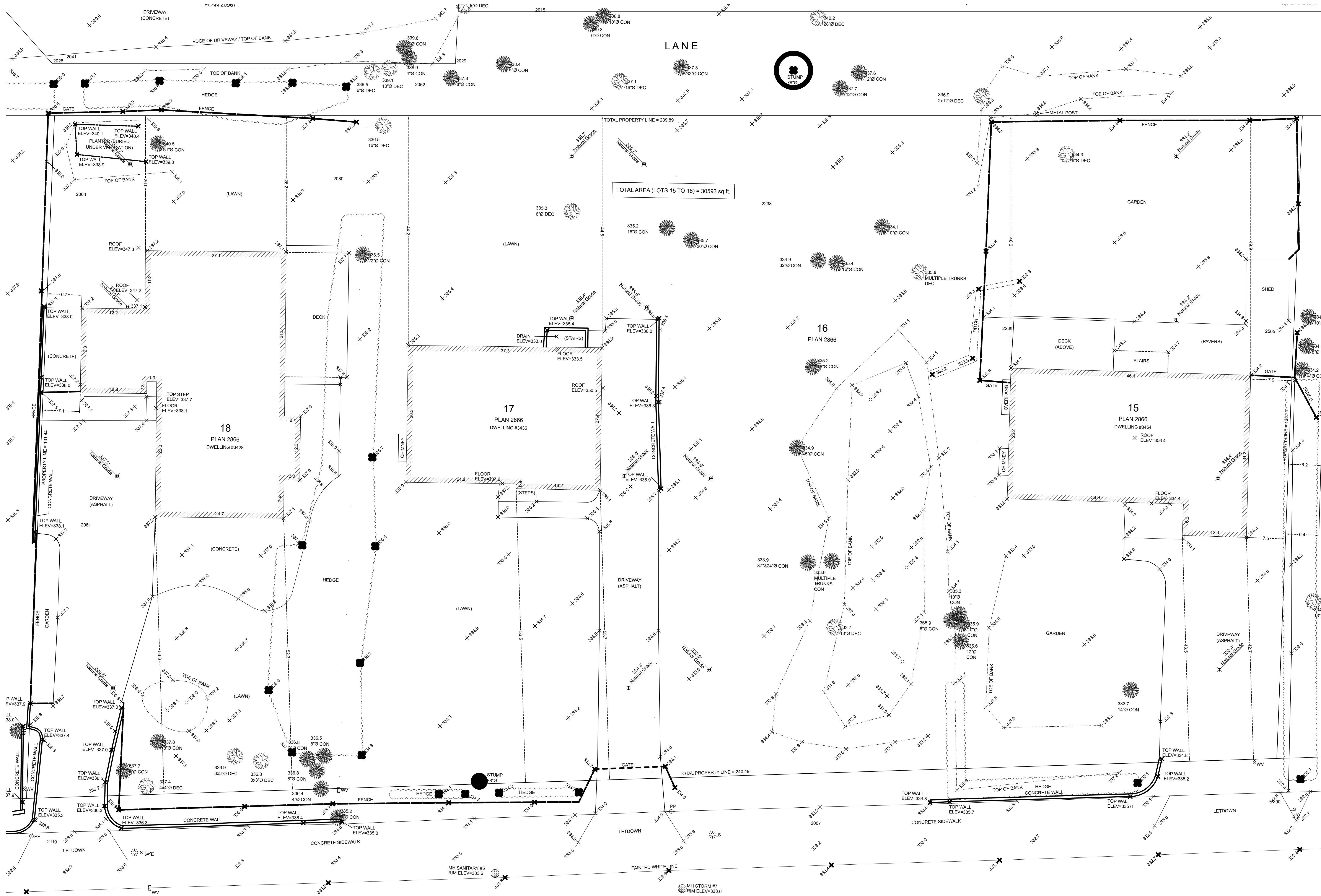
Thursday, February 15, 2018 [DATE]

4.0 Development Permit [ISSUE]

[DRAWING]

A-0.200

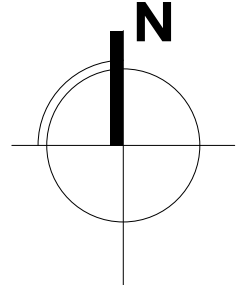




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**ALLAIRE**  
HeadwaterLiving

[ARCHITECT SEAL]

[CLIENT]

**ALLAIRE GROUP/  
HEADWATER PROJECTS**

[PROJECT]

**MOUNT SEYMOUR  
TOWNHOMES**

3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[TITLE]

## SURVEY

[PROJECT]

**16399**

[SCALE]

**1/8" = 1'-0"**

[DATE]

**Wednesday, February 14, 2018**

[ISSUE]

**4.0 Development Permit**

[DRAWING]

**A-1.00**





1 First Floor Plan  
SCALE: 1/8" = 1'-0"

NOTE: REFER TO A-1.08 FOR NATURAL/PROPOSED GRADE CALCULATION AT THE FOUR CORNERS OF EACH BUILDING

LOCATION OF ENHANCED ADAPTABLE UNITS

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**ALLAIRE**  
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[ARCHITECT SEAL]

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**ALLAIRE GROUP/  
HEADWATER PROJECTS**

[PROJECT]

**MOUNT SEYMOUR  
TOWNHOMES**  
3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[TITLE]

**SITE PLAN**

[PROJECT]

**16399**

[SCALE]

1/8" = 1'-0"

**Wednesday, February 21, 2018**

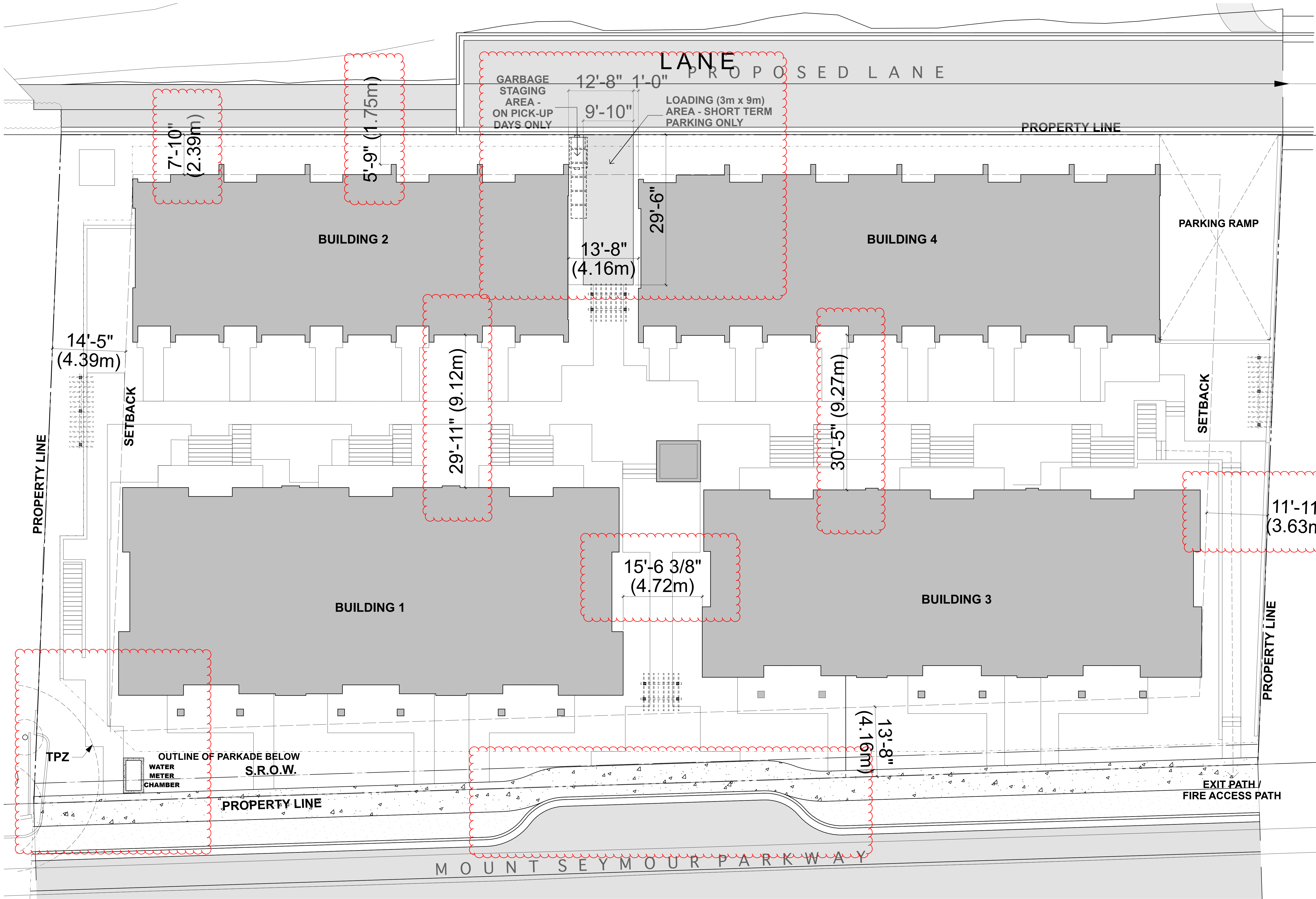
[ISSUE]

**4.0 Development Permit**

[DRAWING]

**A-1.01**



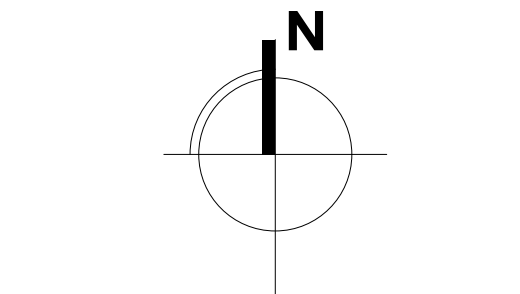


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**ALLAIRE GROUP/  
HEADWATER PROJECTS**

[PROJECT]

**MOUNT SEYMOUR  
TOWNHOMES**

3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[TITLE]

**SITE PLAN  
(SIMPLIFIED)**

[PROJECT]

**16399**

[SCALE]

**1/8" = 1'-0"**

**Wednesday, February 21, 2018**

**4.0 Development Permit**

[ISSUE]

[DRAWING]

**A-1.02**

**1**

**Site Plan (simple)**

SCALE: 1/8" = 1'-0"





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HEADWATER PROJECTS

[PROJECT]

MOUNT SEYMOUR  
TOWNHOMES

3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[TITLE]

PARKADE

[PROJECT]

16399

[SCALE]

1/8" = 1'-0"

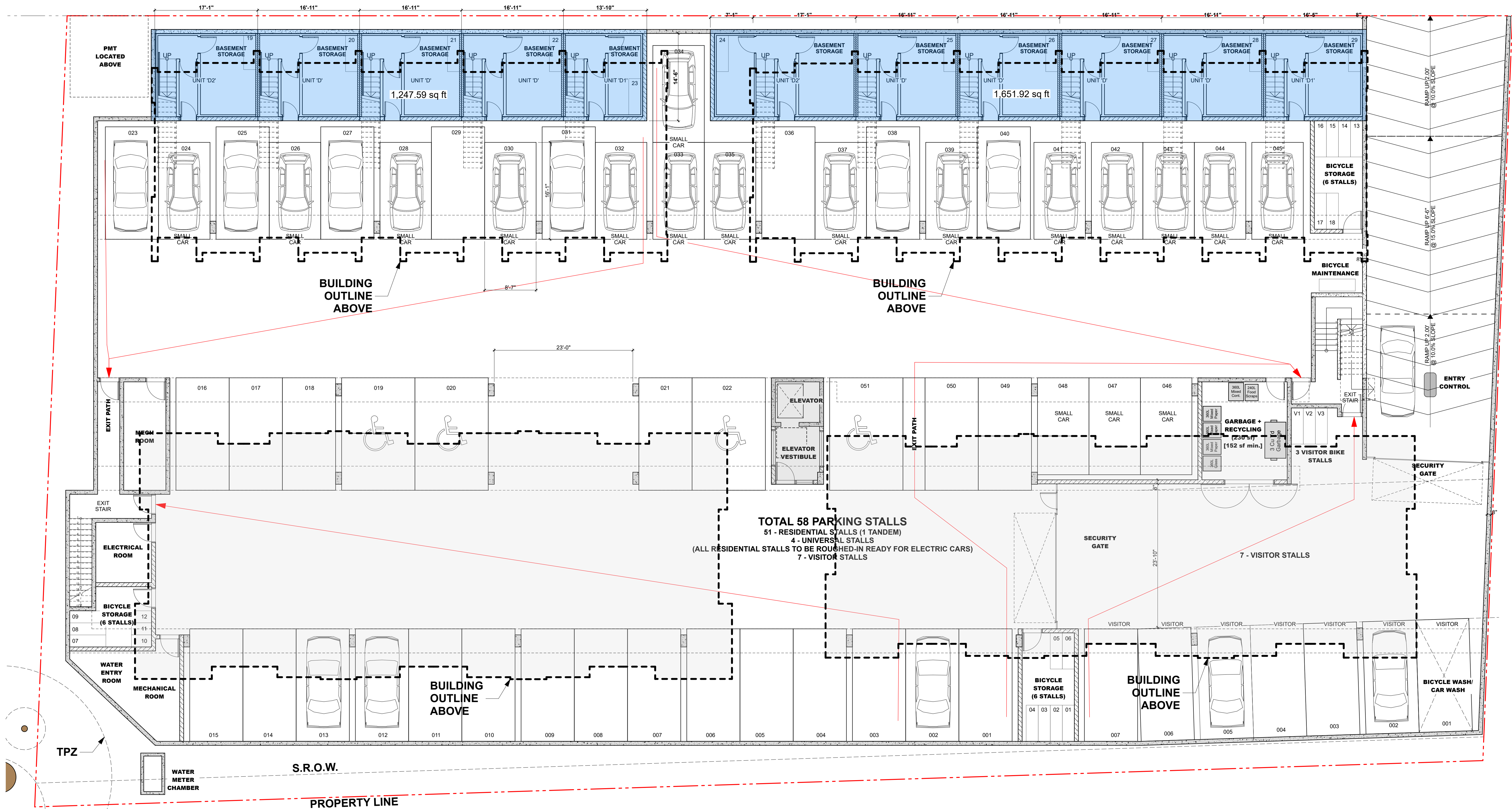
Thursday, February 15, 2018

4.0 Development Permit

[ISSUE]

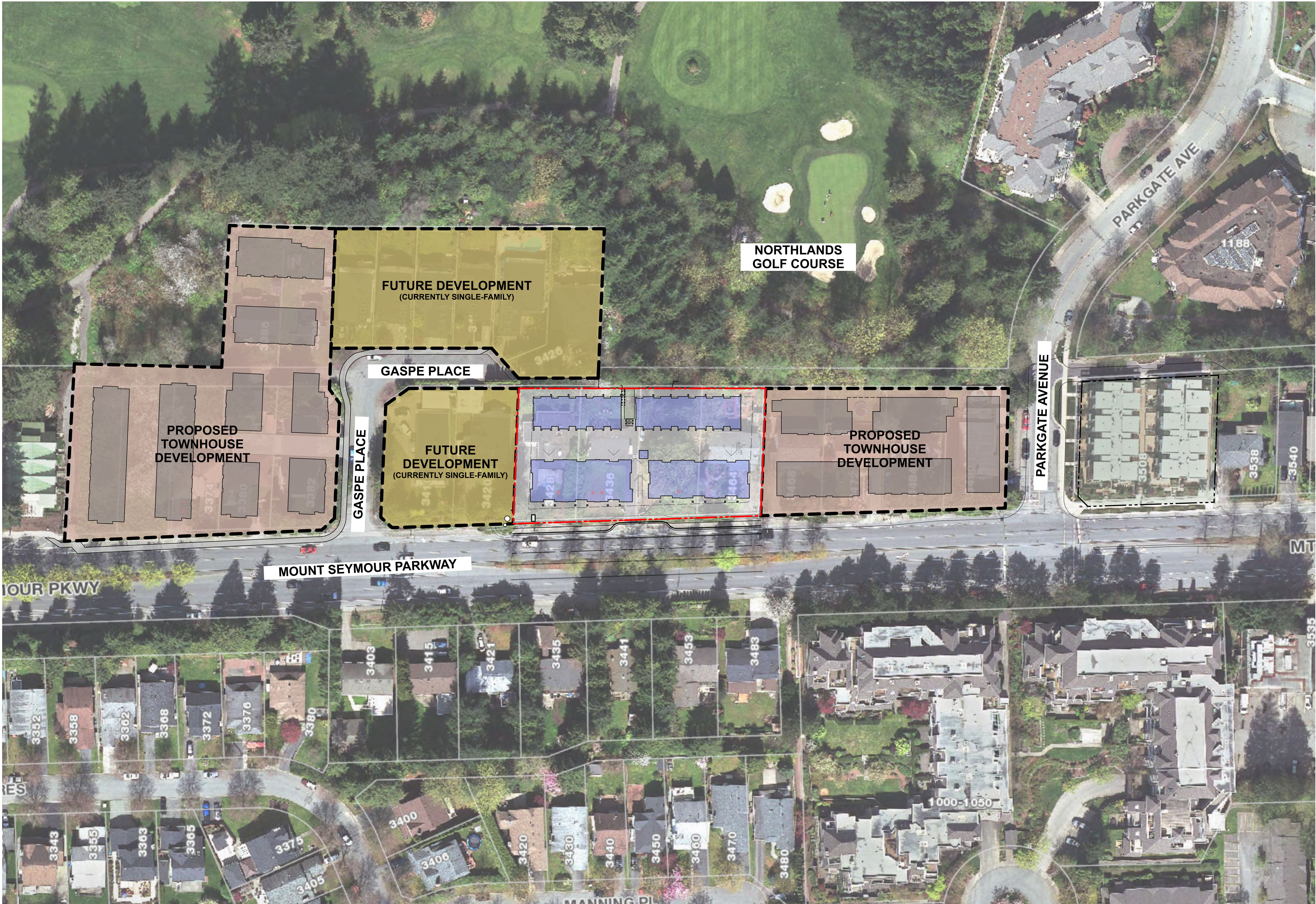
[DRAWING]

A-1.03



1 Parking 1 Level  
SCALE: 1/8" = 1'-0"





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[CLIENT]

ALLAIRE GROUP/  
HEADWATER PROJECTS

[PROJECT]

**MOUNT SEYMOUR  
TOWNHOMES**

3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[TITLE]

## CONTEXT

16399 [PROJECT]

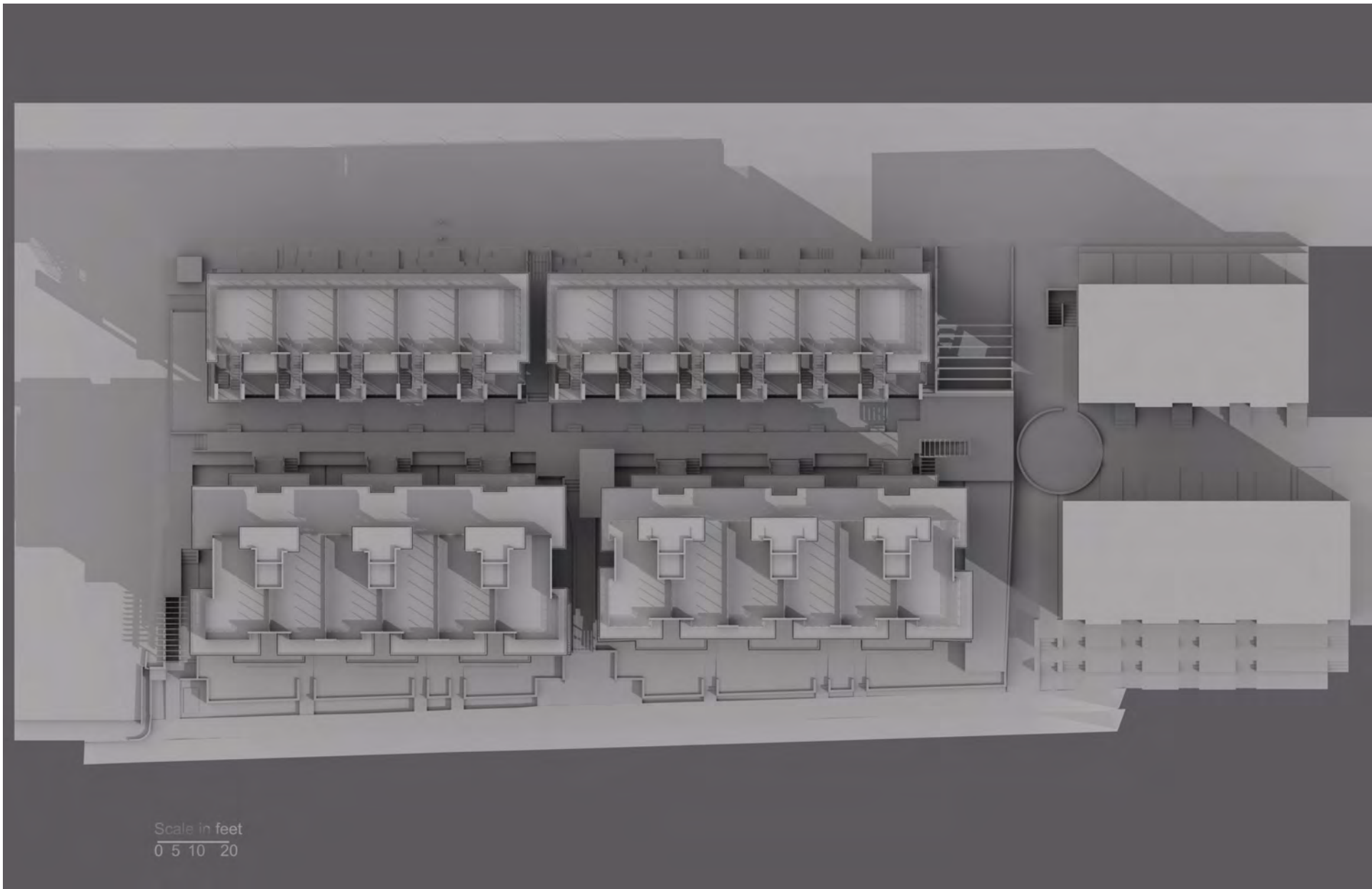
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Wednesday, February 14, 2018 [DATE]

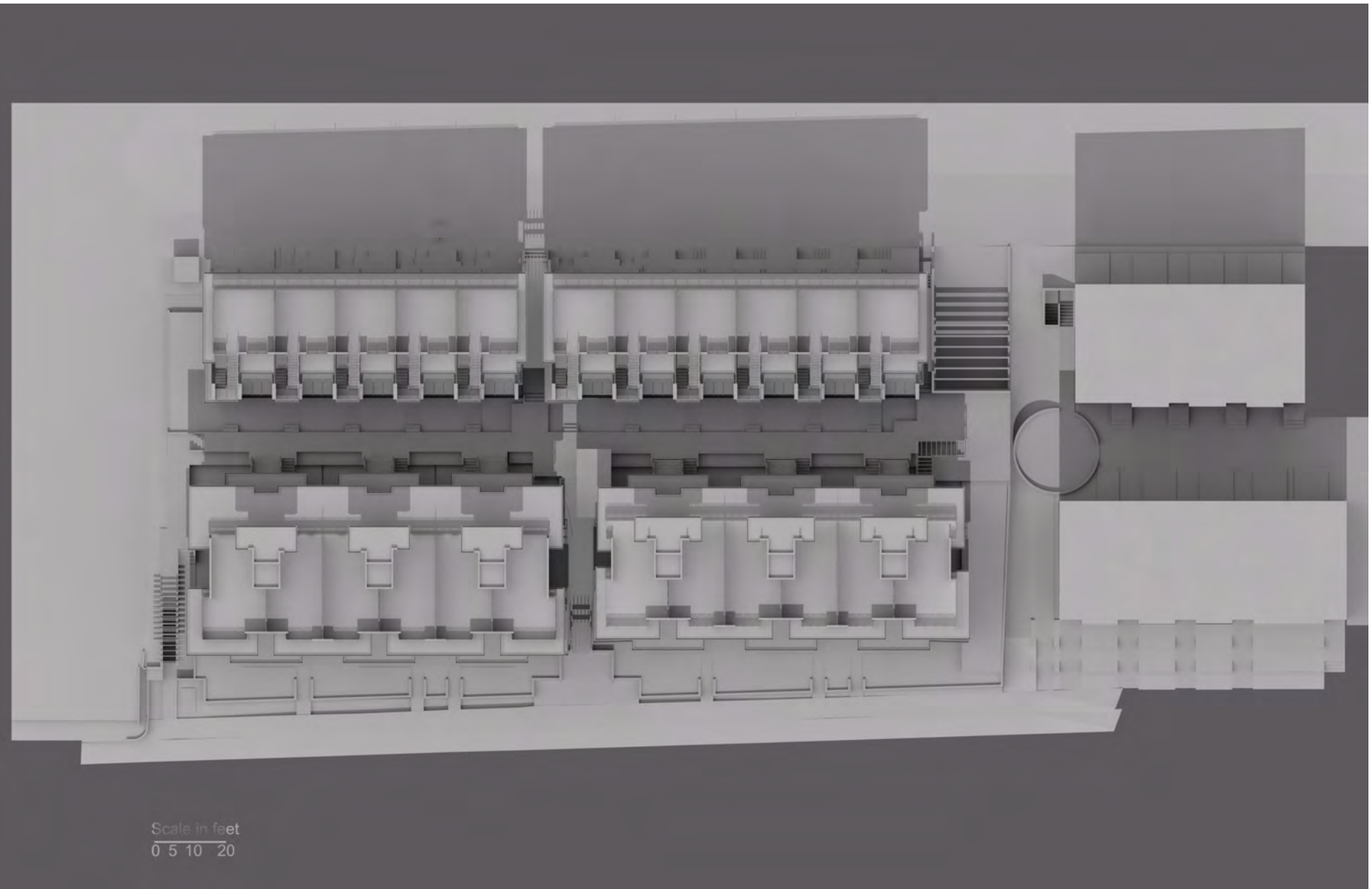
4.0 Development Permit [ISSUE]

[DRAWING]

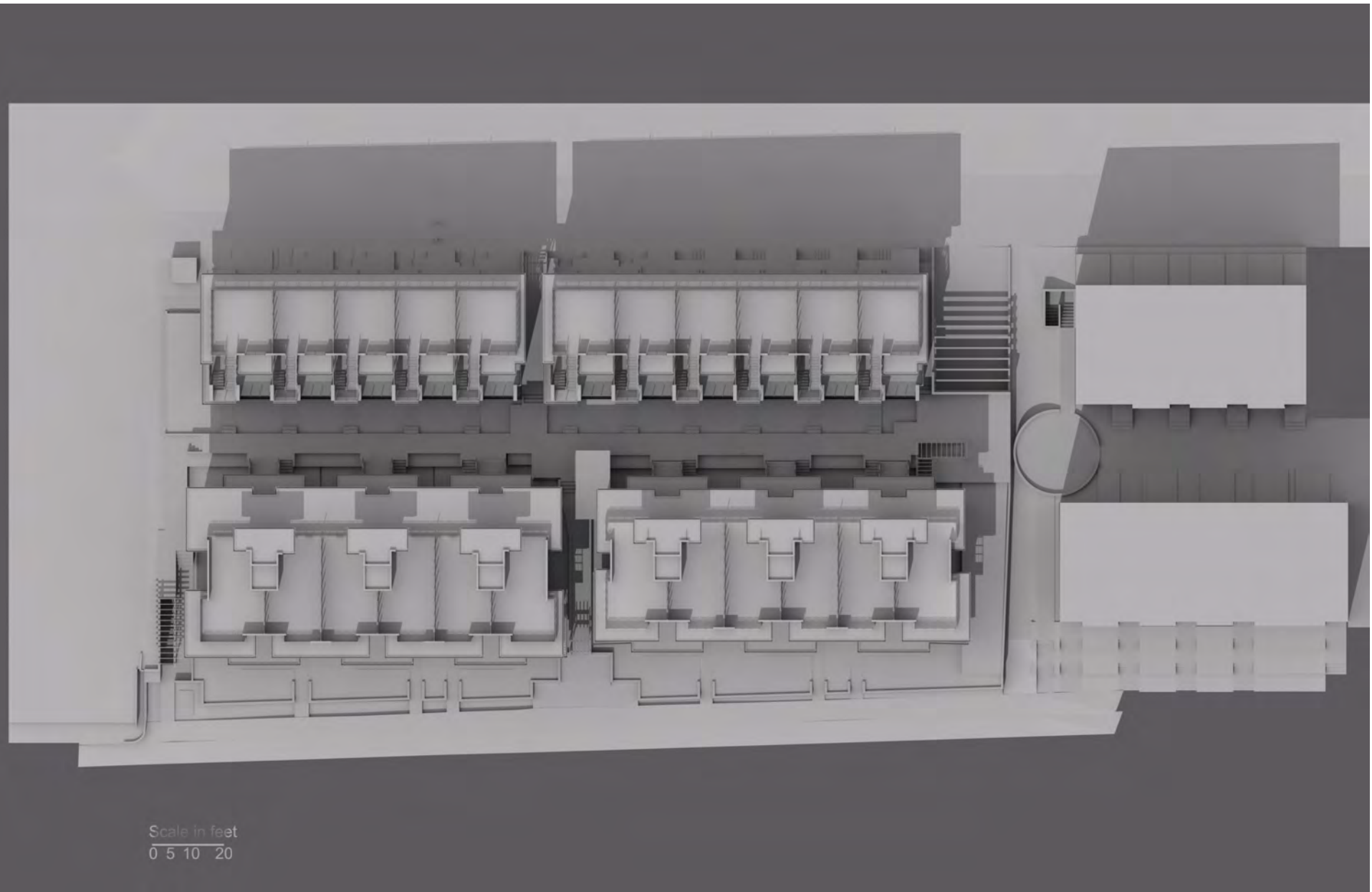




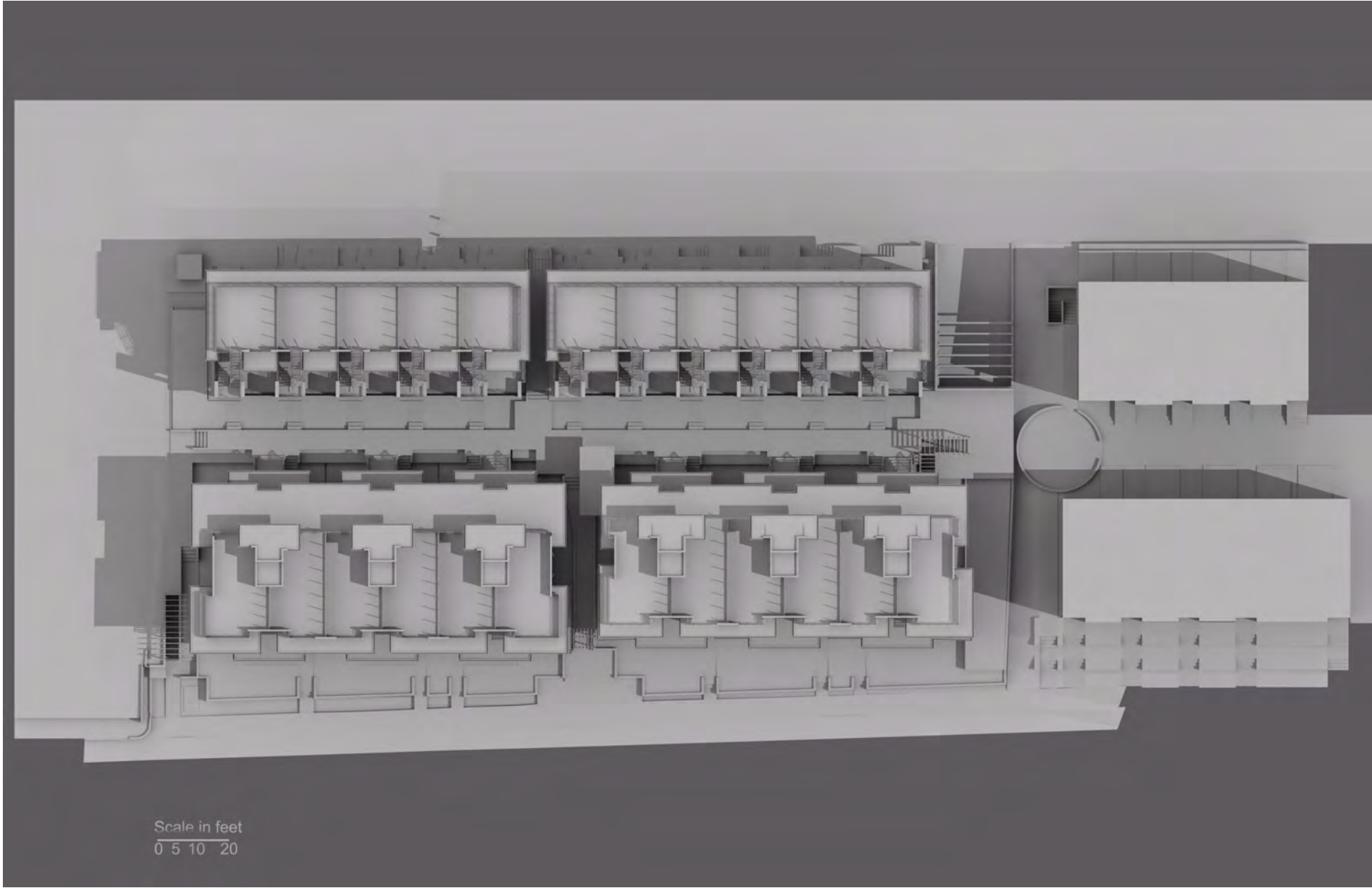
MARCH/SEPTEMBER 21 10:00AM



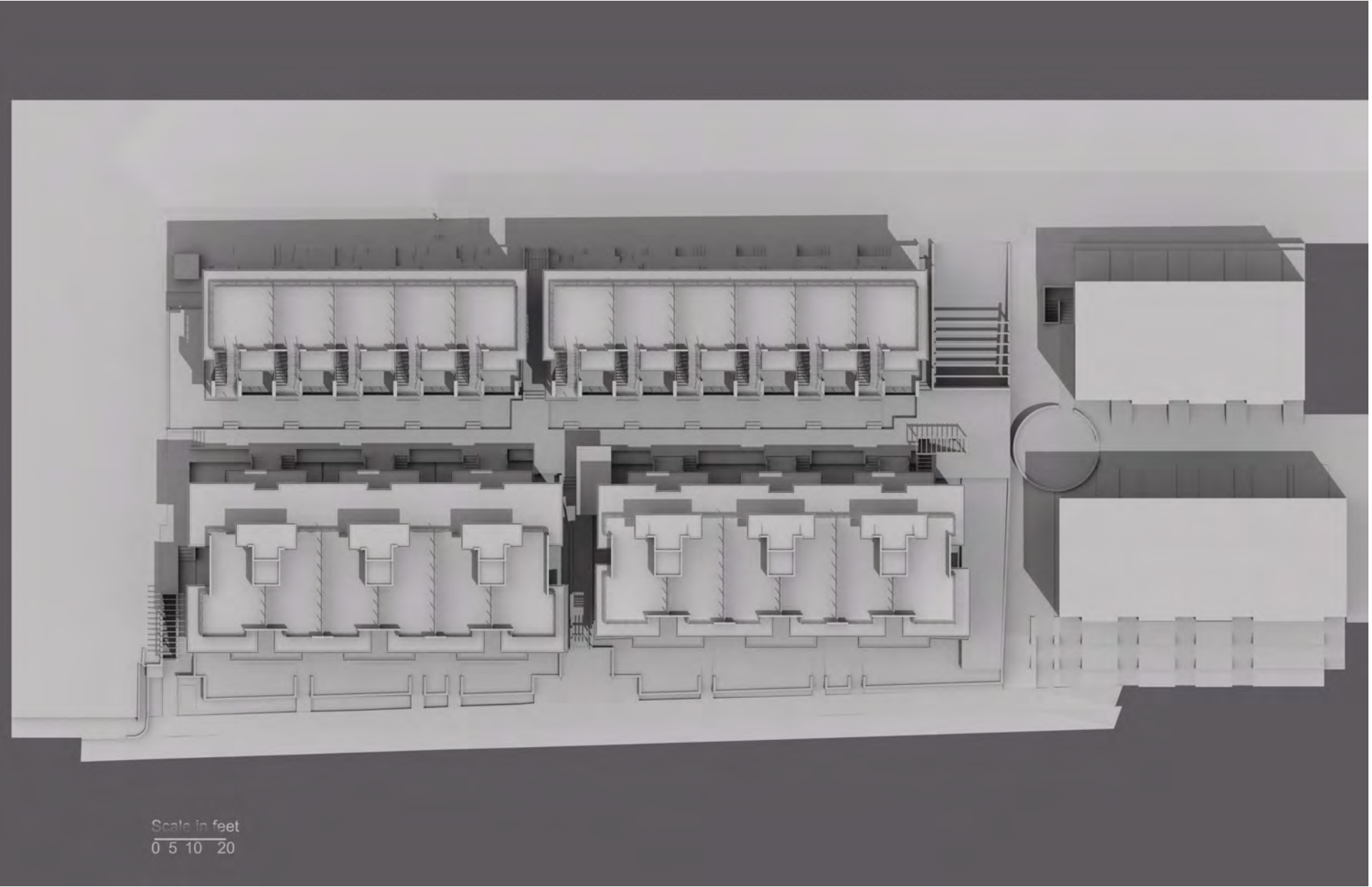
MARCH/SEPTEMBER 21 12:00PM



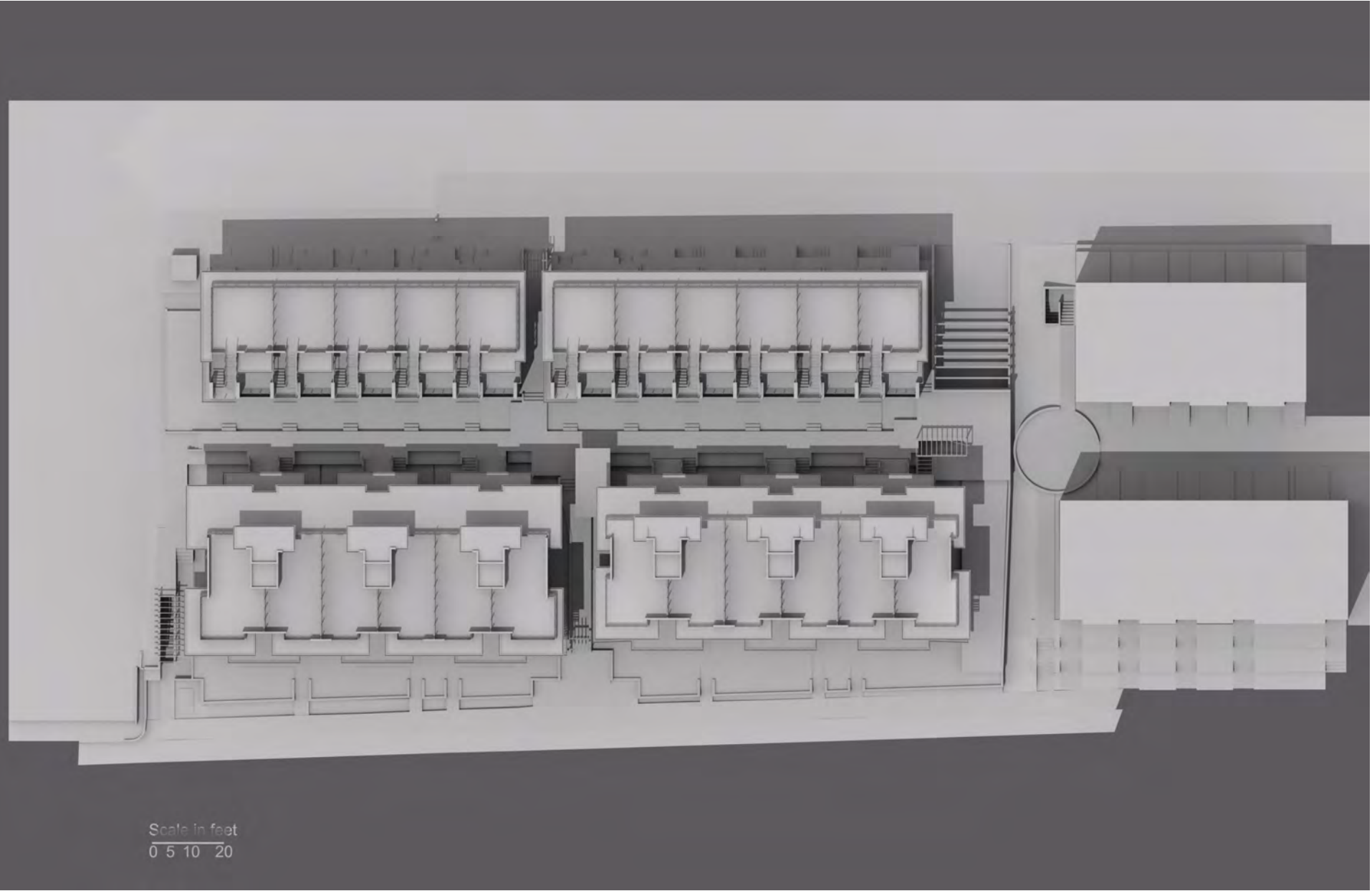
MARCH/SEPTEMBER 21 2:00PM



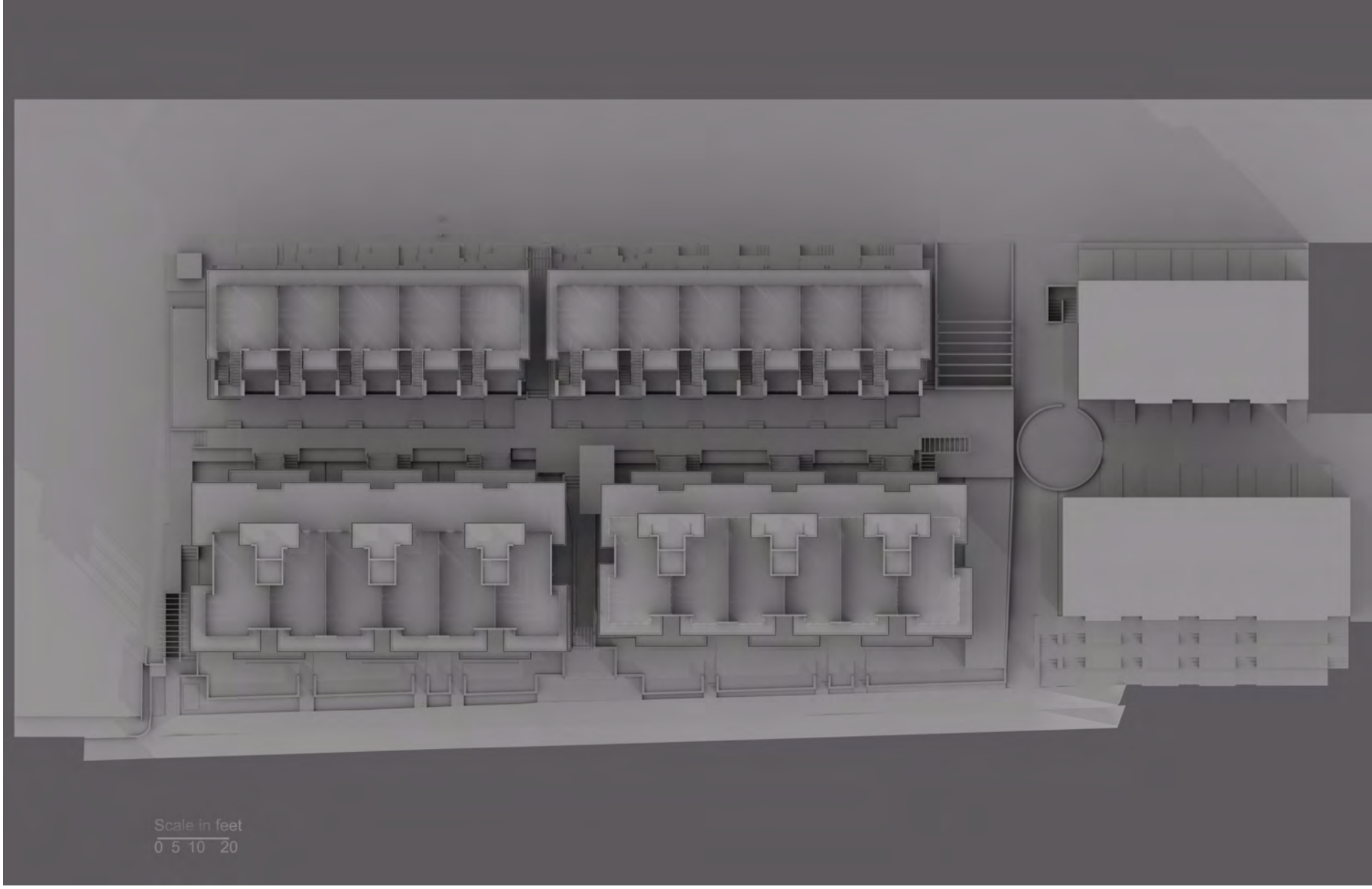
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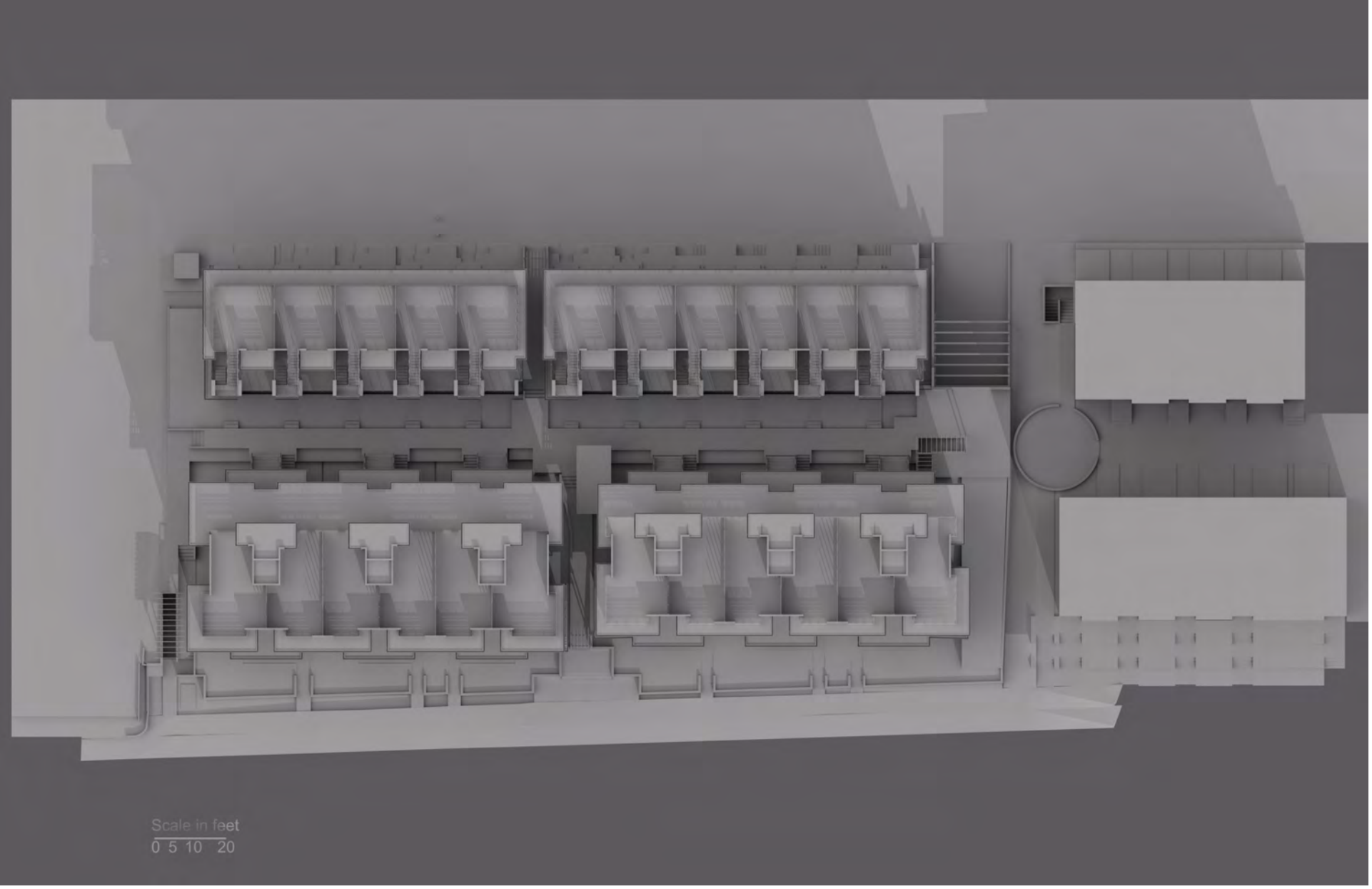
JUNE 21 12:00PM



JUNE 21 2:00PM



DECEMBER 21 10:00AM



DECEMBER 21 12:00PM



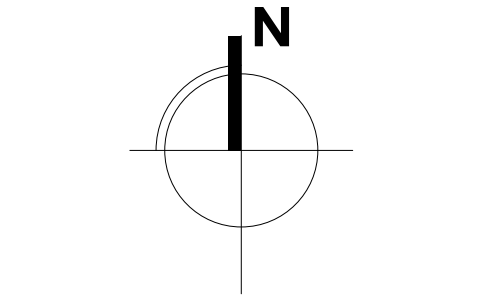
DECEMBER 21 2:00PM



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**ALLAIRE GROUP/  
HEADWATER PROJECTS**

[PROJECT]

**MOUNT SEYMOUR  
TOWNHOMES**

3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[TITLE]

**SHADOW  
ANALYSIS**

[PROJECT]

**16399**

[SCALE]

**NOT TO SCALE**

[DATE]

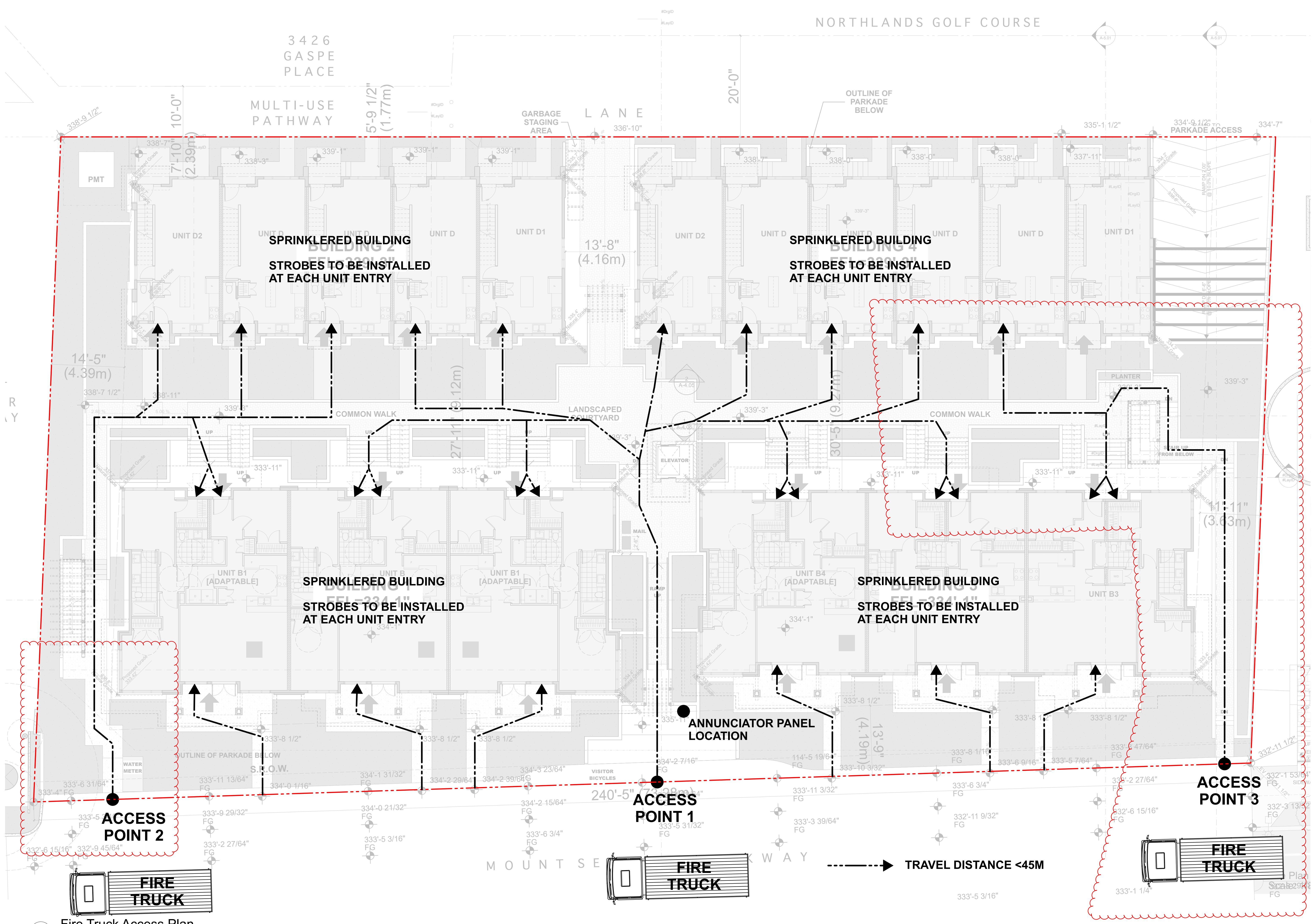
**Wednesday, February 14, 2018**

[ISSUE]

**4.0 Development Permit**

[DRAWING]



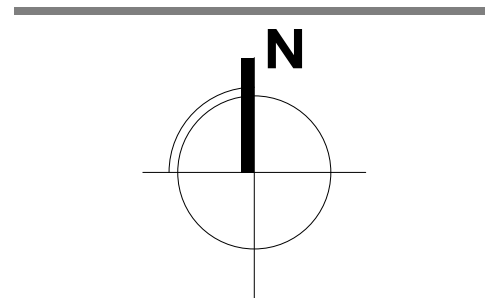


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ALLAIRE GROUP/  
HEADWATER PROJECTS

[PROJECT]

**MOUNT SEYMOUR  
TOWNHOMES**

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North Vancouver, BC

[TITLE]

**FIRE ACCESS  
PLAN**

[PROJECT]

16399 [SCALE]

1/8" = 1'-0"

Wednesday, February 14, 2018

4.0 Development Permit [ISSUE]

[DRAWING]

**A-1.06**

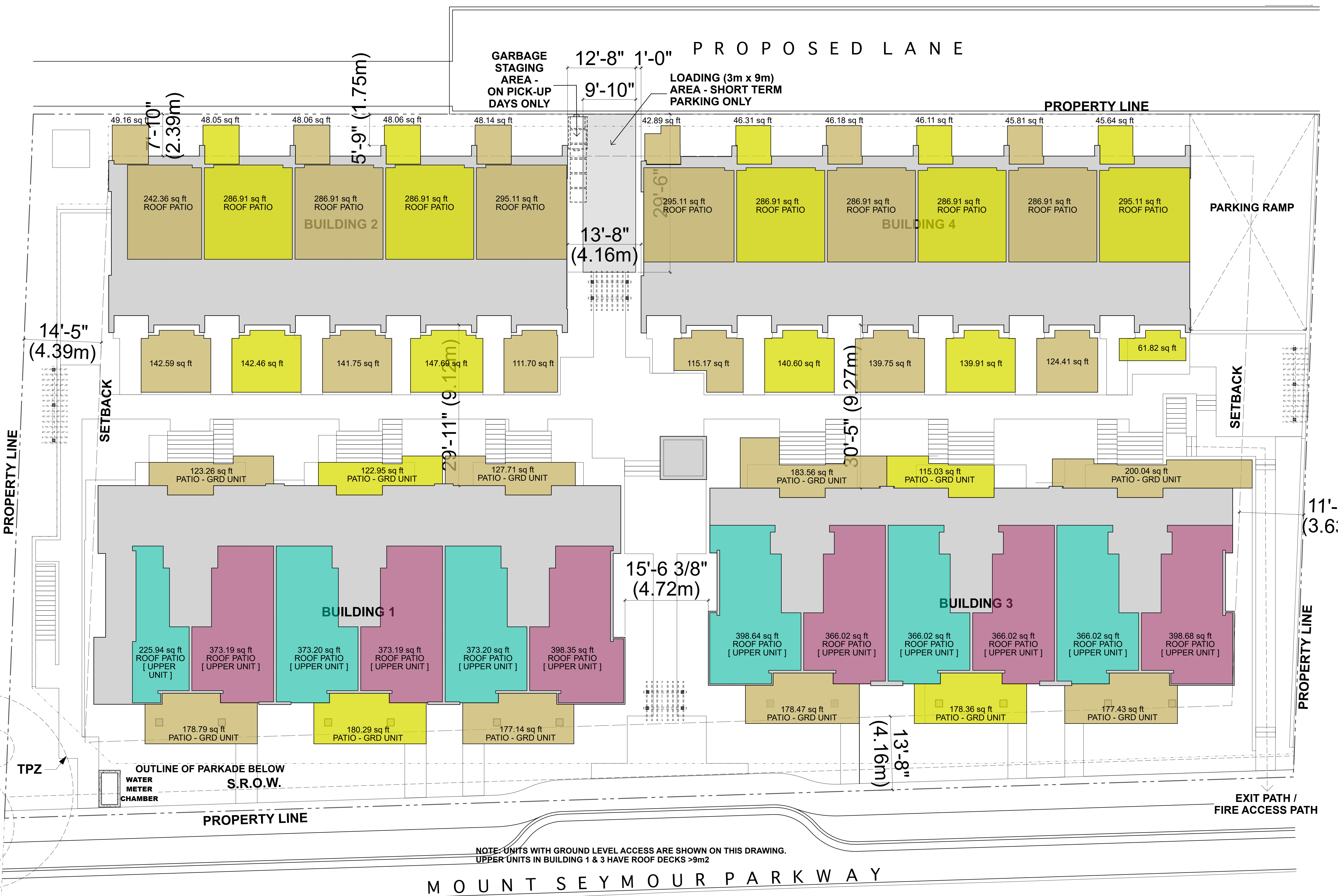




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HEADWATER PROJECTS

[PROJECT]

**MOUNT SEYMOUR  
TOWNHOMES**  
3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

**PRIVATE  
OUTDOOR  
SPACE**

[PROJECT]

16399

[SCALE]

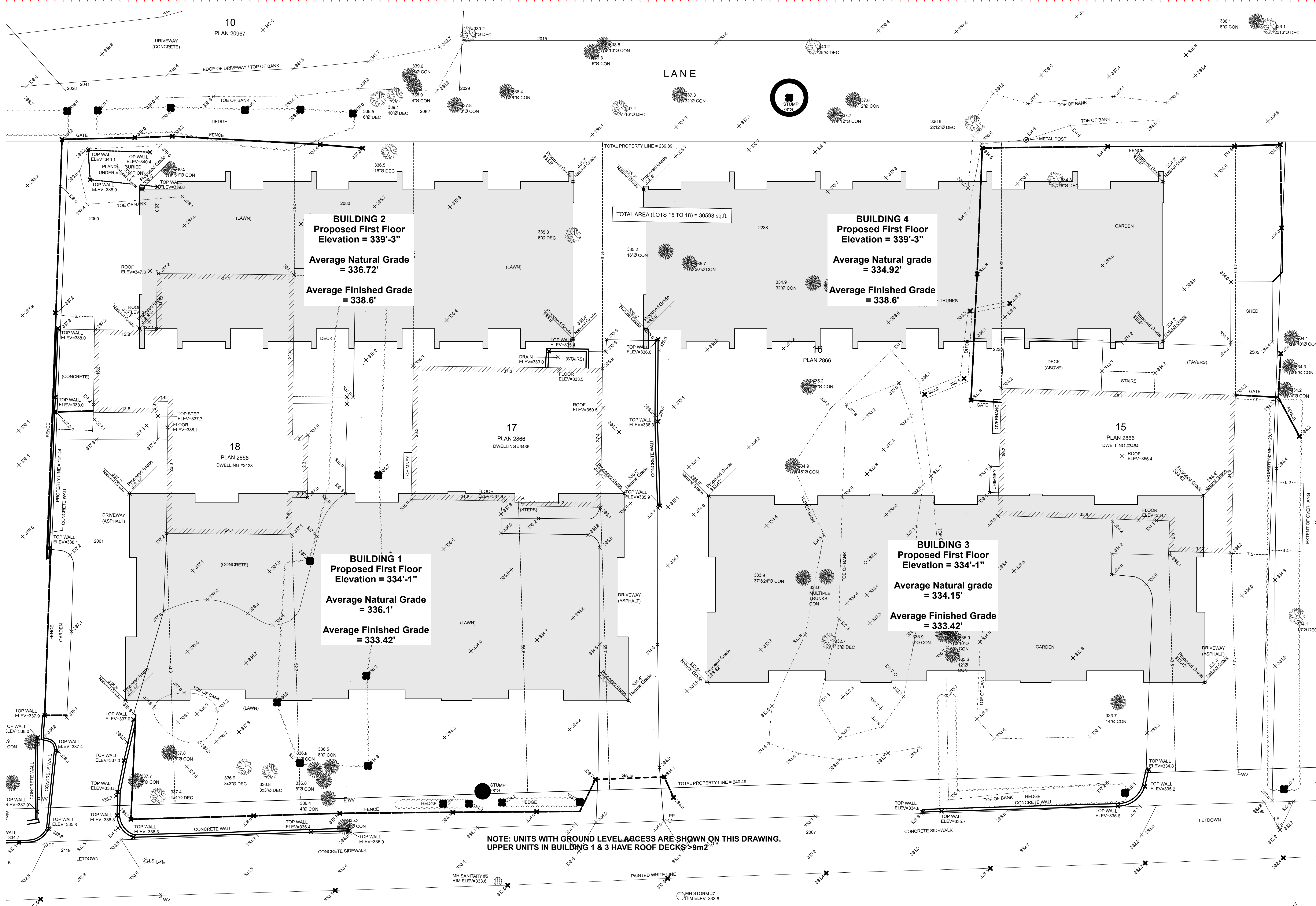
1/8" = 1'-0"

Wednesday, February 21, 2018

4.0 Development Permit

[DRAWING]





NOTE: UNITS WITH GROUND LEVEL ACCESS ARE SHOWN ON THIS DRAWING.  
UPPER UNITS IN BUILDING 1 & 3 HAVE ROOF DECKS >9m<sup>2</sup>

**NATURAL GRADE CALCULATION FOR BUILDING HEIGHT PURPOSES**  
SCALE: 1/8" = 1'-0"



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**MOUNT SEYMOUR  
TOWNHOMES**  
3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

**GRADE  
CALCULATION  
PLAN**

16399  
1/8" = 1'-0"  
Wednesday, February 14, 2018  
4.0 Development Permit



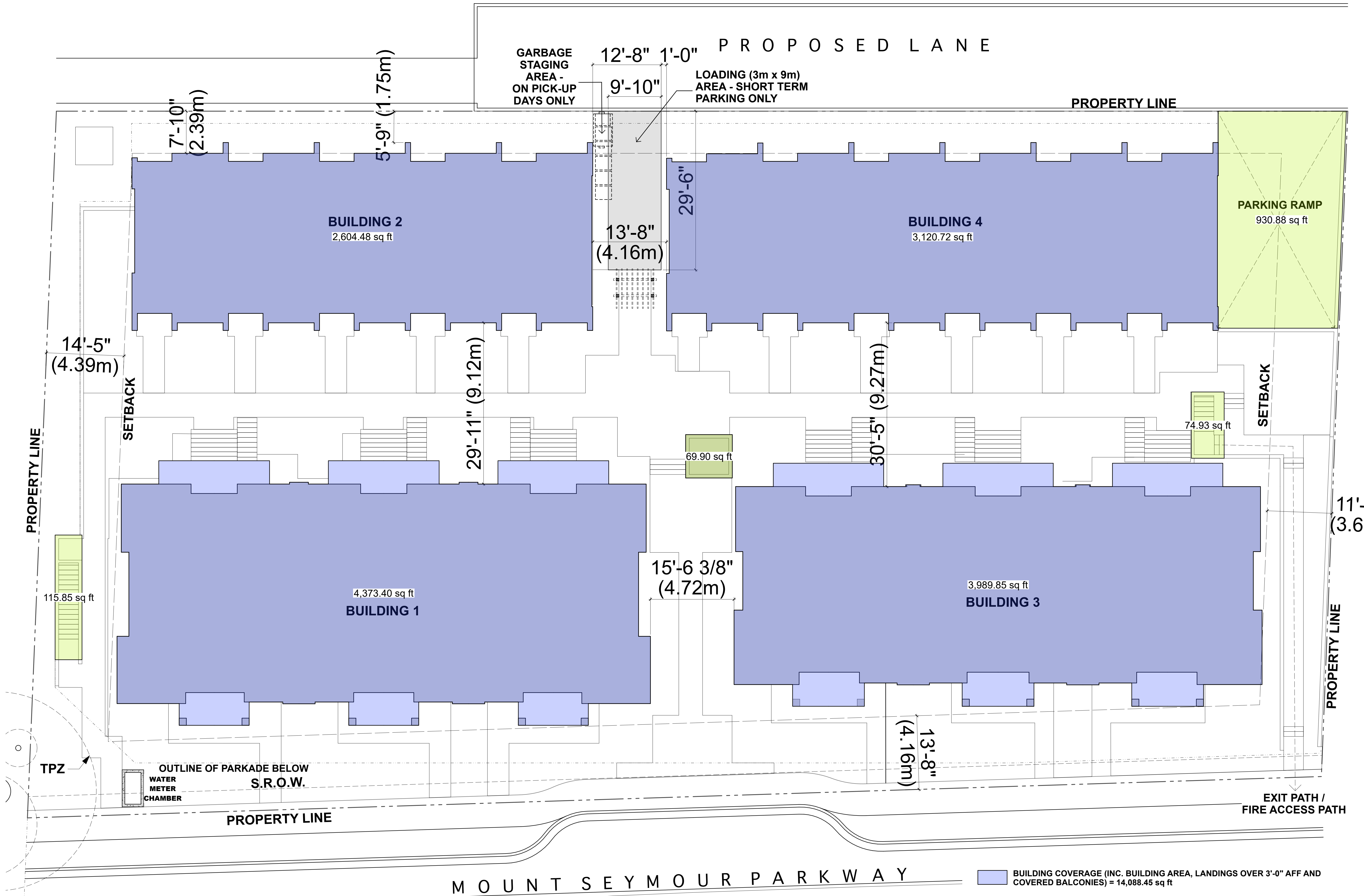


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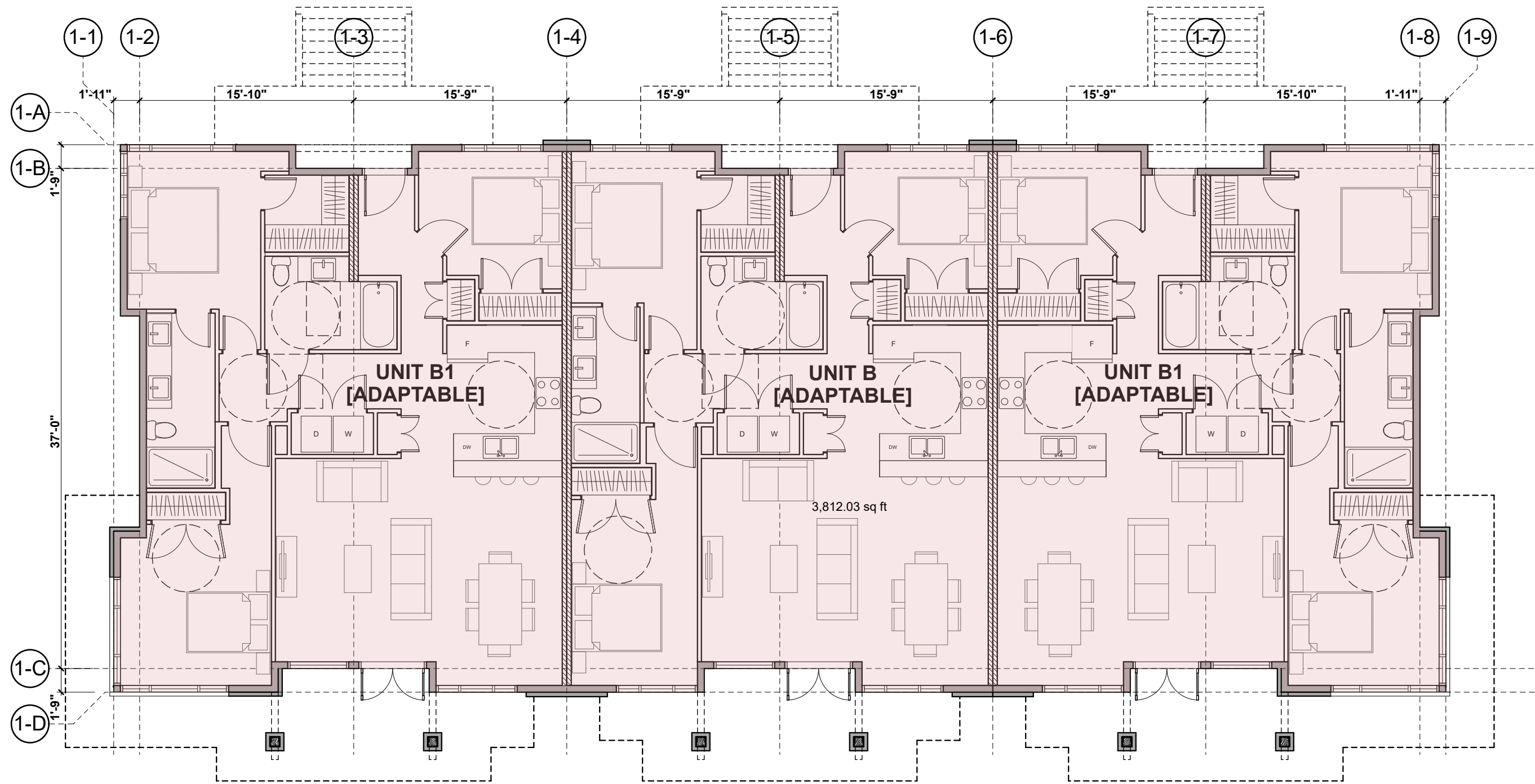


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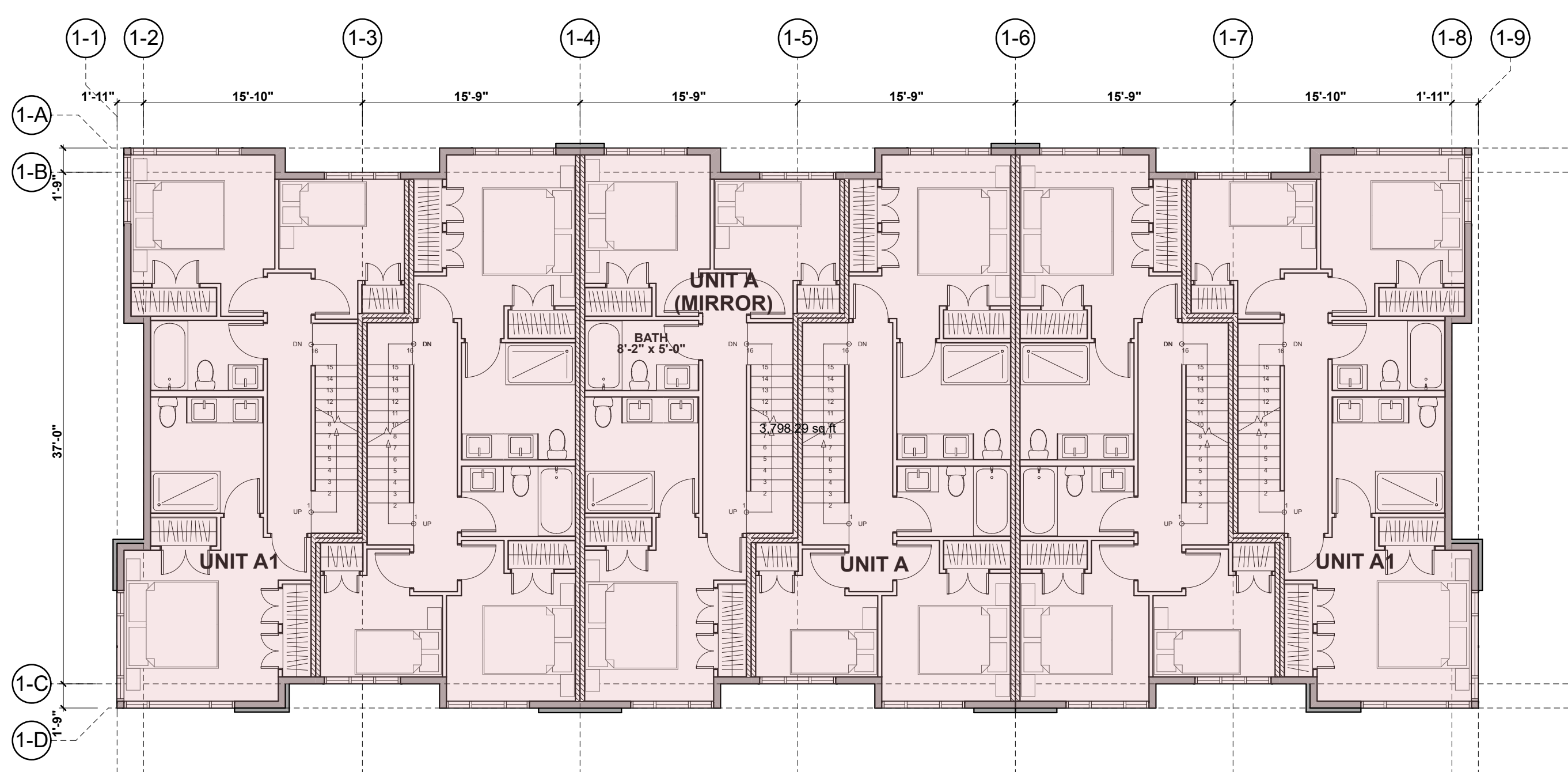
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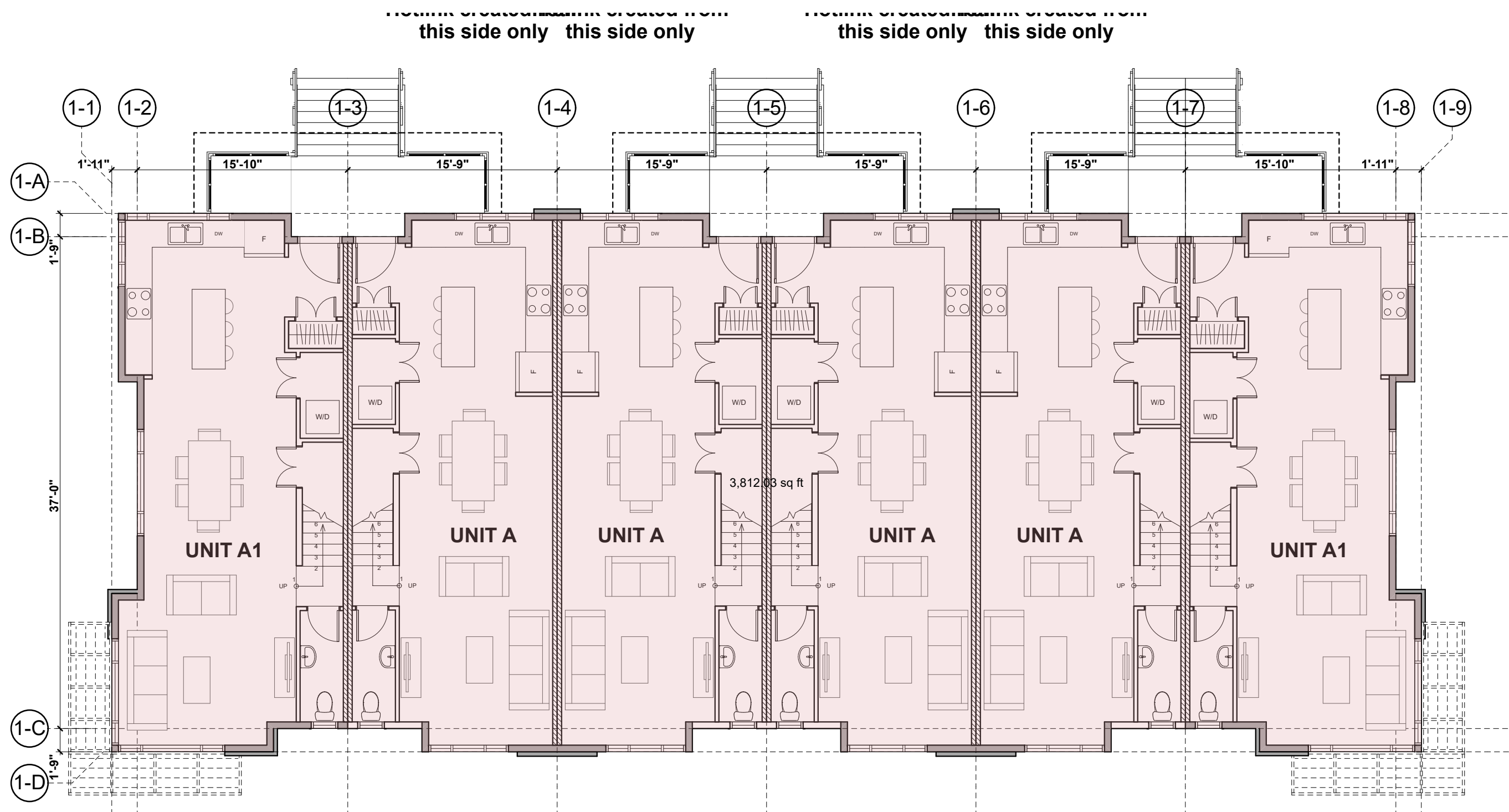
1 First Floor Plan

SCALE: 1/8" = 1'-0"



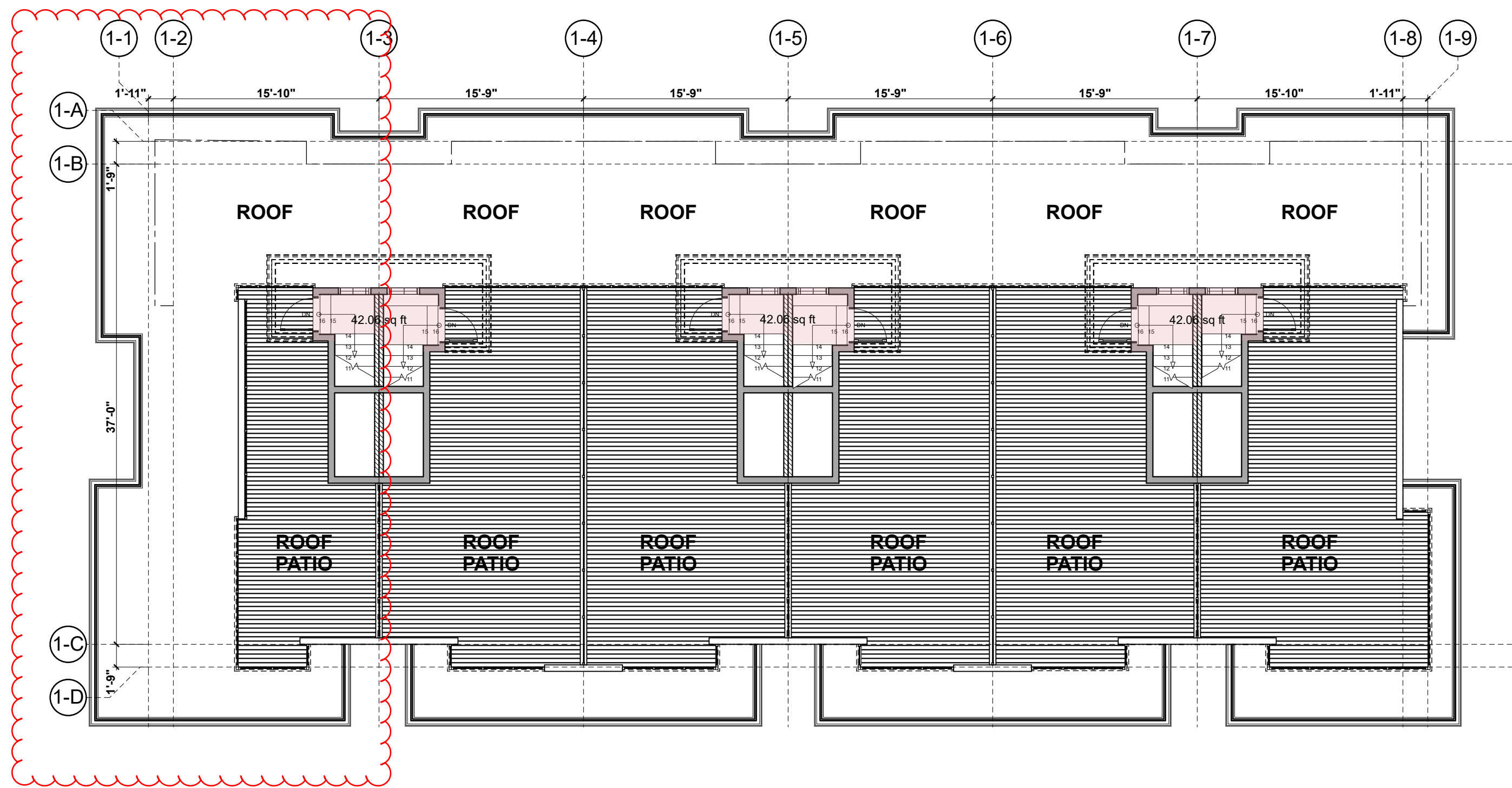
3 Third Floor Plan

SCALE: 1/8" = 1'-0"



2 Second Floor Plan

SCALE: 1/8" = 1'-0"



4 Roof Floor Plan

SCALE: 1/8" = 1'-0"

FIRST FLOOR PLAN AREA (FSR) = 3812.03 SQFT  
SECOND FLOOR PLAN AREA (FSR) = 3812.03 SQFT  
THIRD FLOOR PLAN AREA (FSR) = 3798.29 SQFT  
ROOF PLAN AREA (FSR) = 126.18 SQFT  
TOTAL AREA (FSR) = 11,548.53 SQFT

[ARCHITECT SEAL]

[CLIENT]

ALLAIRE GROUP/  
HEADWATER PROJECTS

[PROJECT]

MOUNT SEYMOUR  
TOWNHOMES

3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[TITLE]

BUILDING 1  
FLOOR PLANS

16399 [PROJECT]

1/8" = 1'-0" [SCALE]

Wednesday, February 14, 2018 [DATE]

4.0 Development Permit [ISSUE]

[DRAWING]

A-2.01

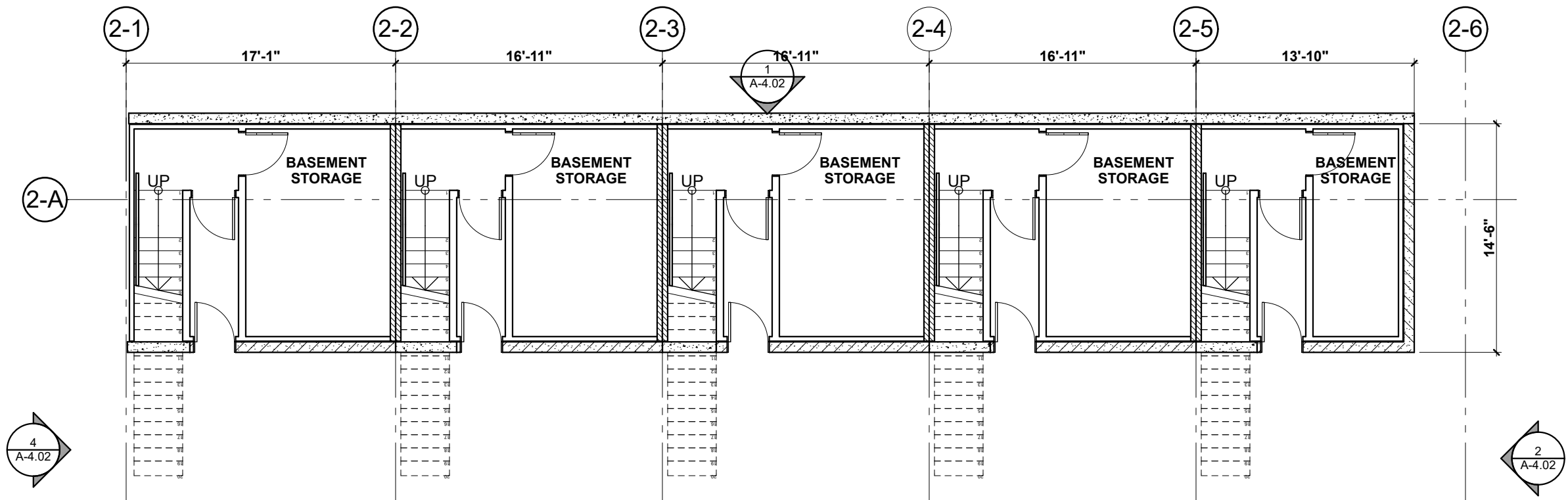




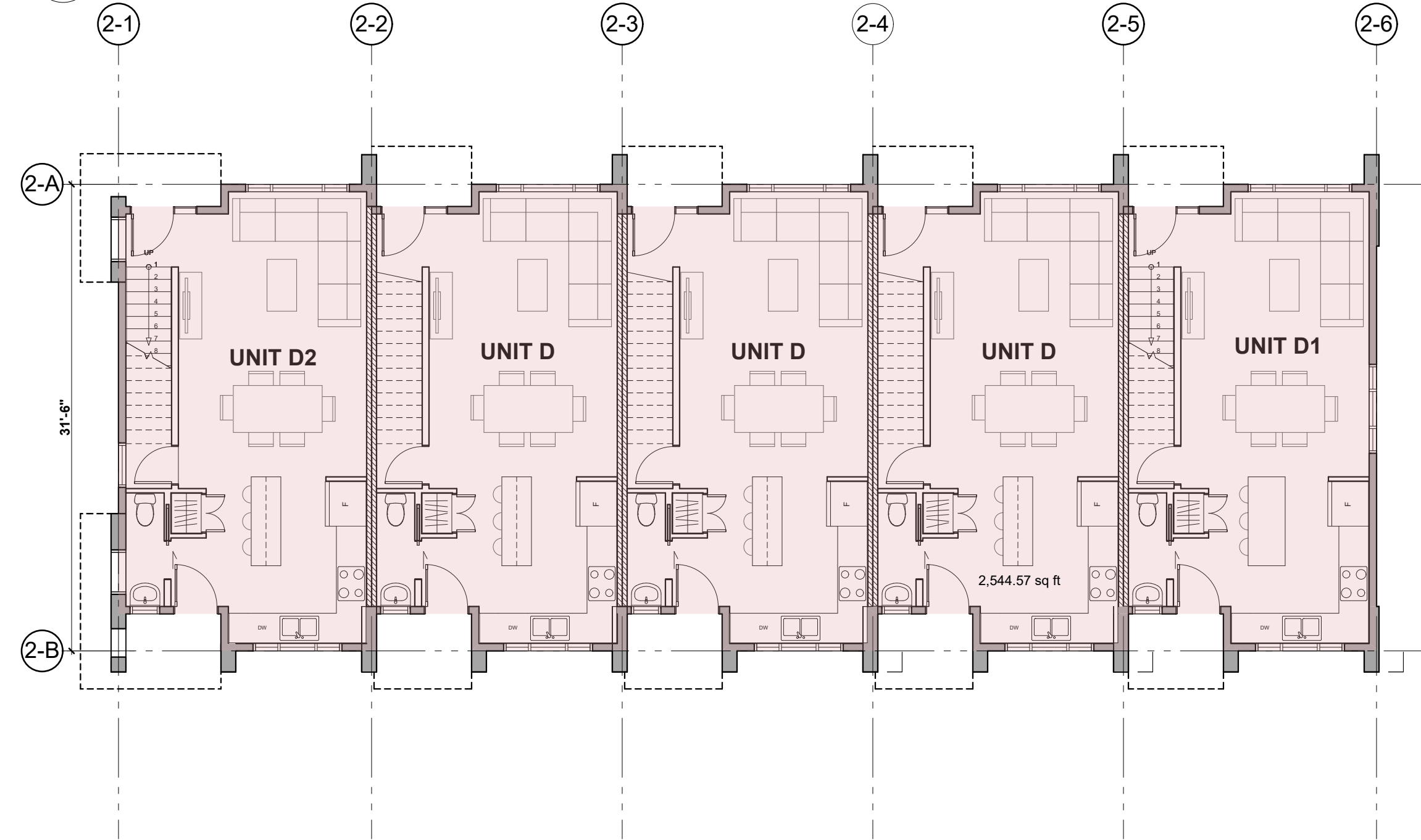
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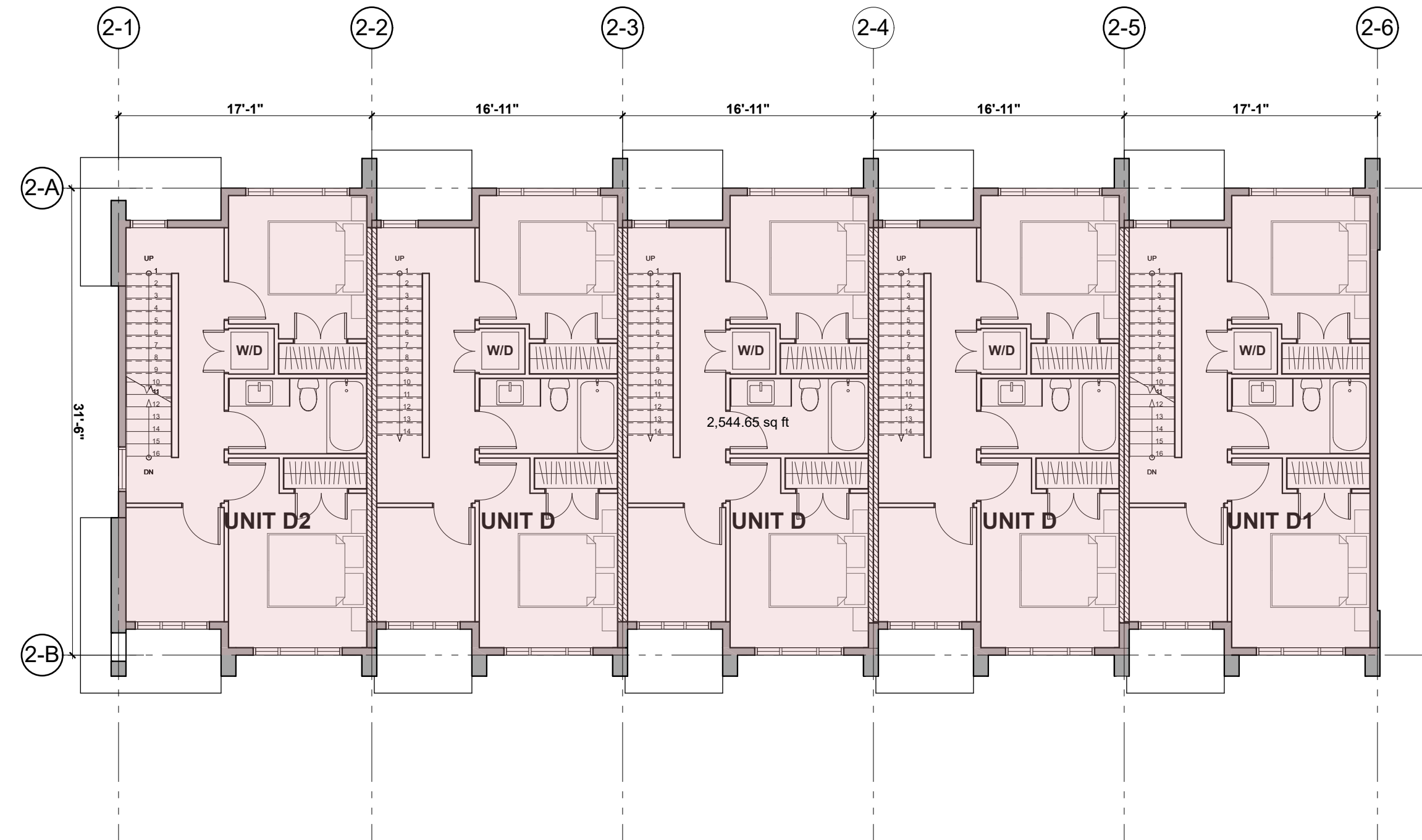
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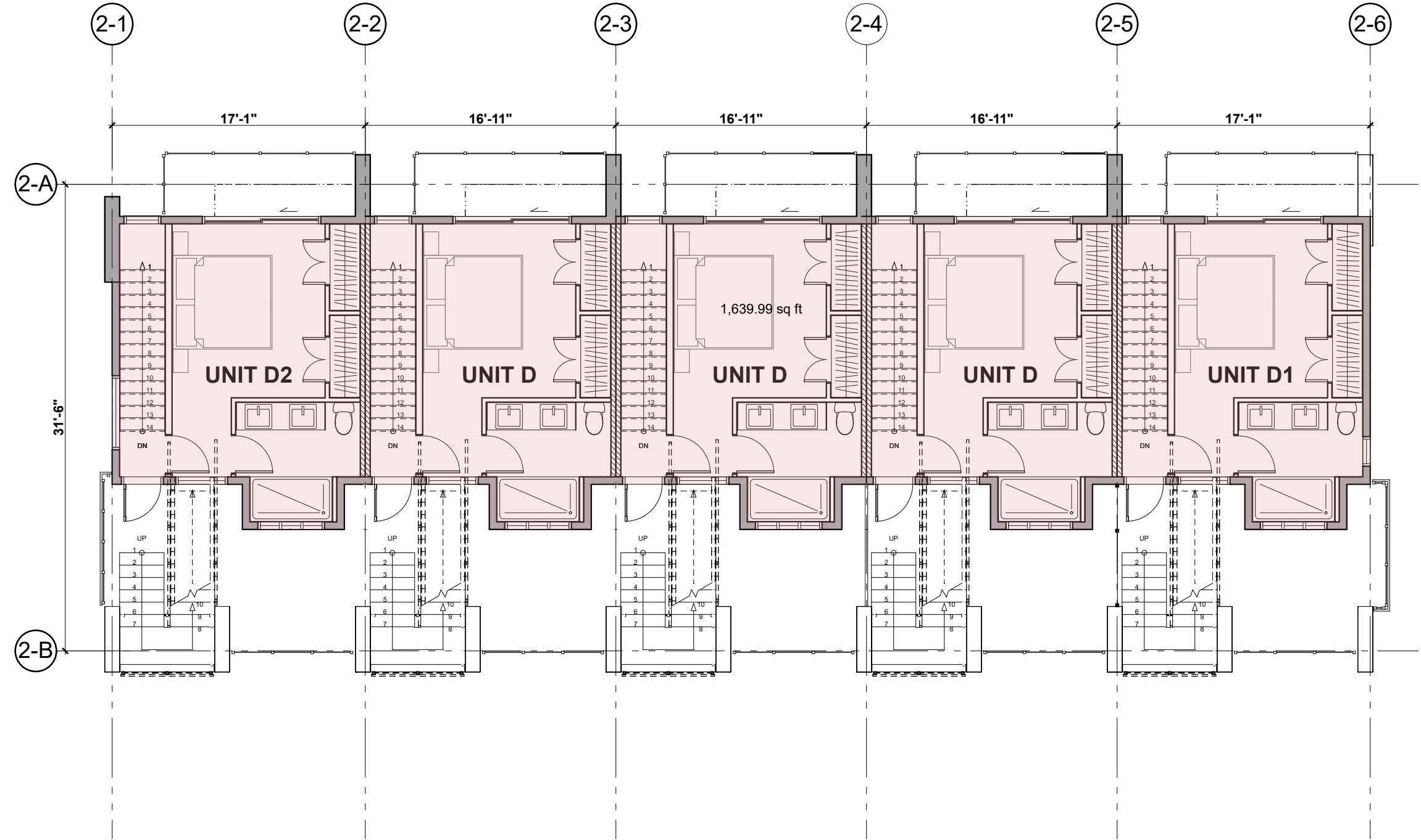
**1** Basement  
SCALE: 1/8" = 1'-0"



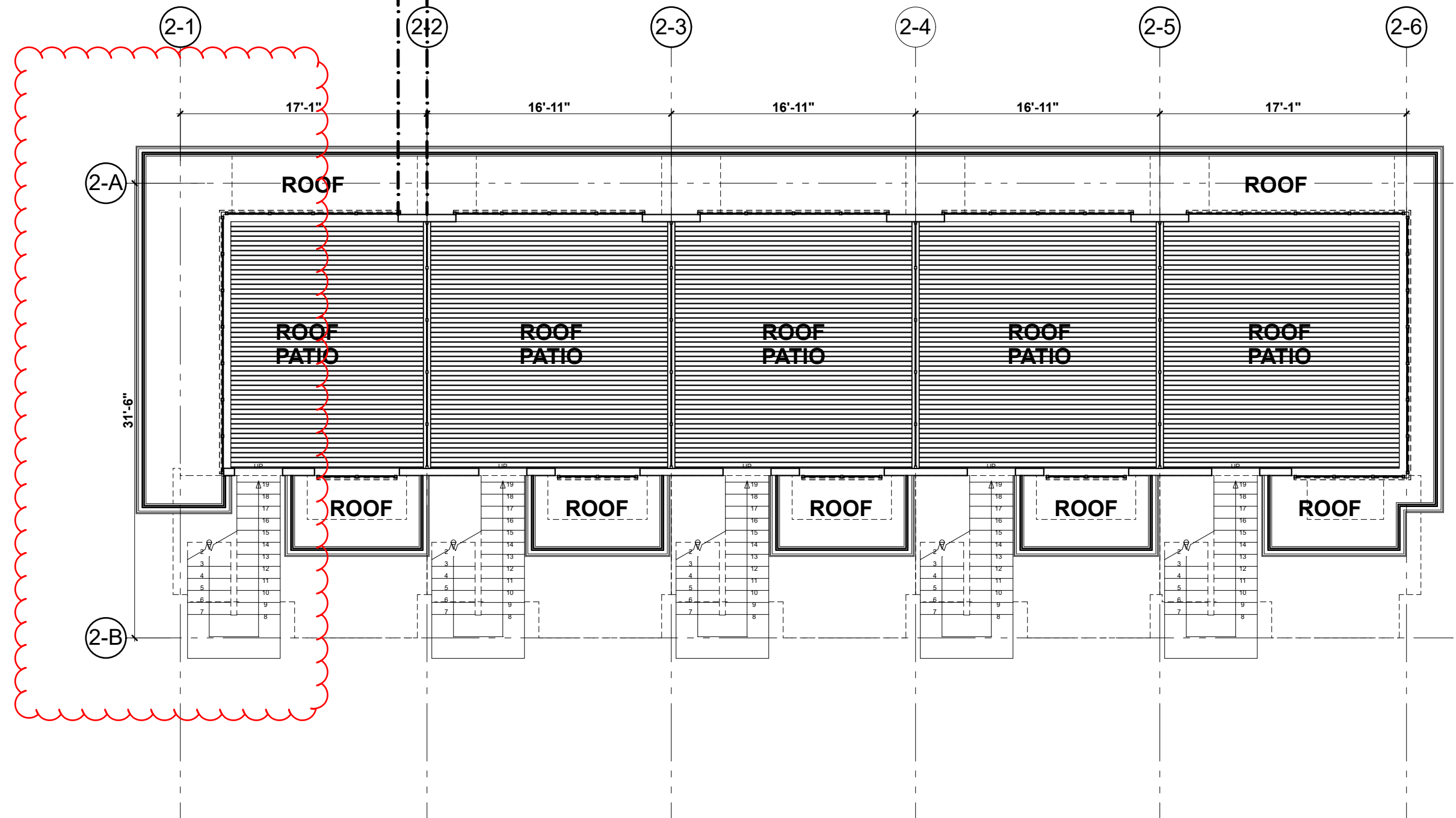
**2** First Floor Plan  
SCALE: 1/8" = 1'-0"



**3** Second Floor Plan  
SCALE: 1/8" = 1'-0"



**4** Third Floor Plan  
SCALE: 1/8" = 1'-0"



**5** Roof Floor Plan  
SCALE: 1/8" = 1'-0"

FIRST FLOOR PLAN AREA (FSR) = 2541.87 SQFT  
SECOND FLOOR PLAN AREA (FSR) = 2519.71 SQFT  
THIRD FLOOR PLAN AREA (FSR) = 1639.99 SQFT  
ROOF PLAN AREA (FSR) = 0.00 SQFT  
TOTAL AREA (FSR) = 6701.57 SQFT

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ALLAIRE GROUP/  
HEADWATER PROJECTS

[PROJECT]

**MOUNT SEYMOUR  
TOWNHOMES**  
3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[TITLE]

**BUILDING 2  
PLANS**

16399 [PROJECT]

1/8" = 1'-0" [SCALE]

Wednesday, February 21, 2018 [DATE]

4.0 Development Permit [ISSUE]

[DRAWING]

**A-2.02**

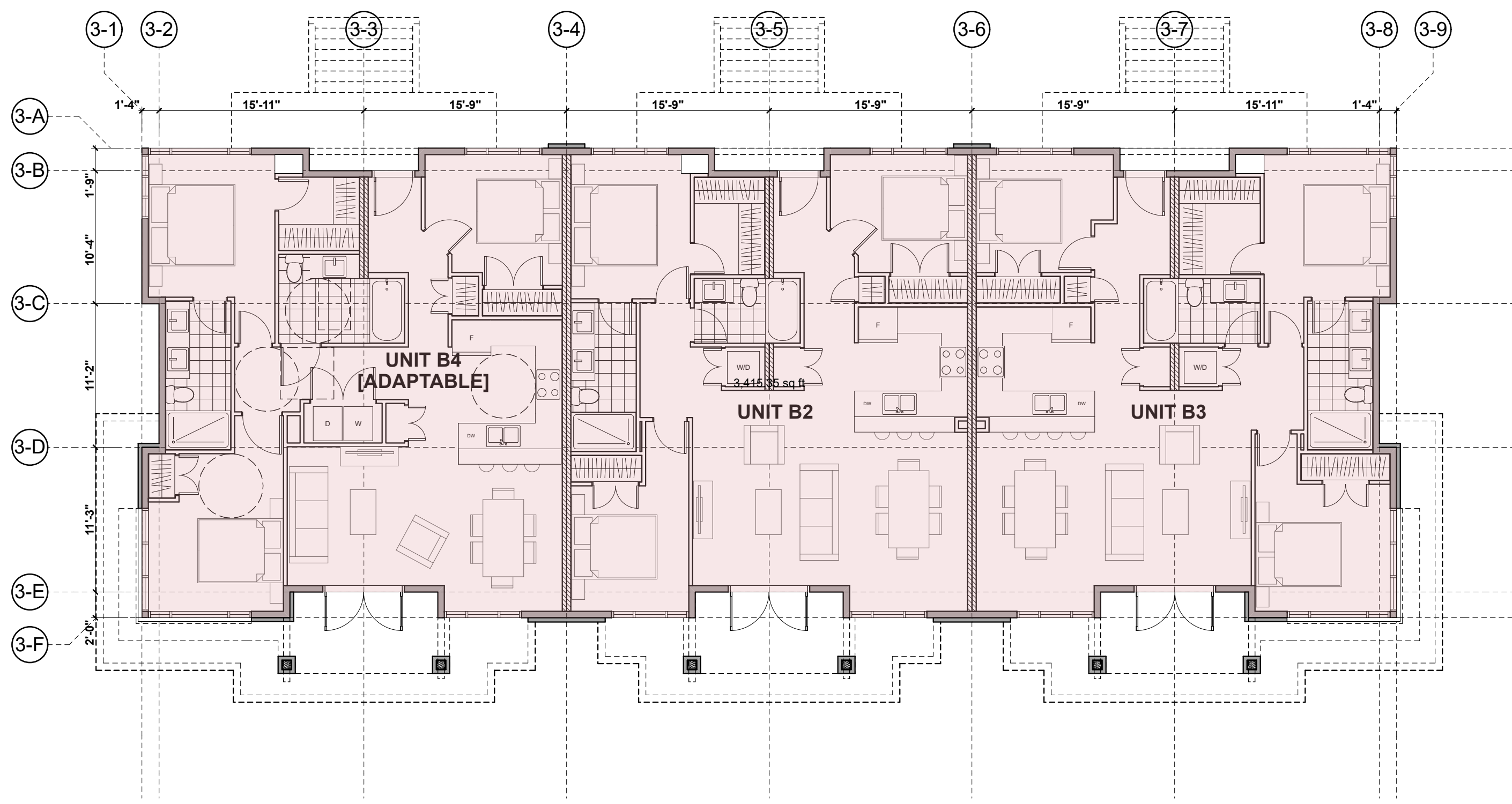




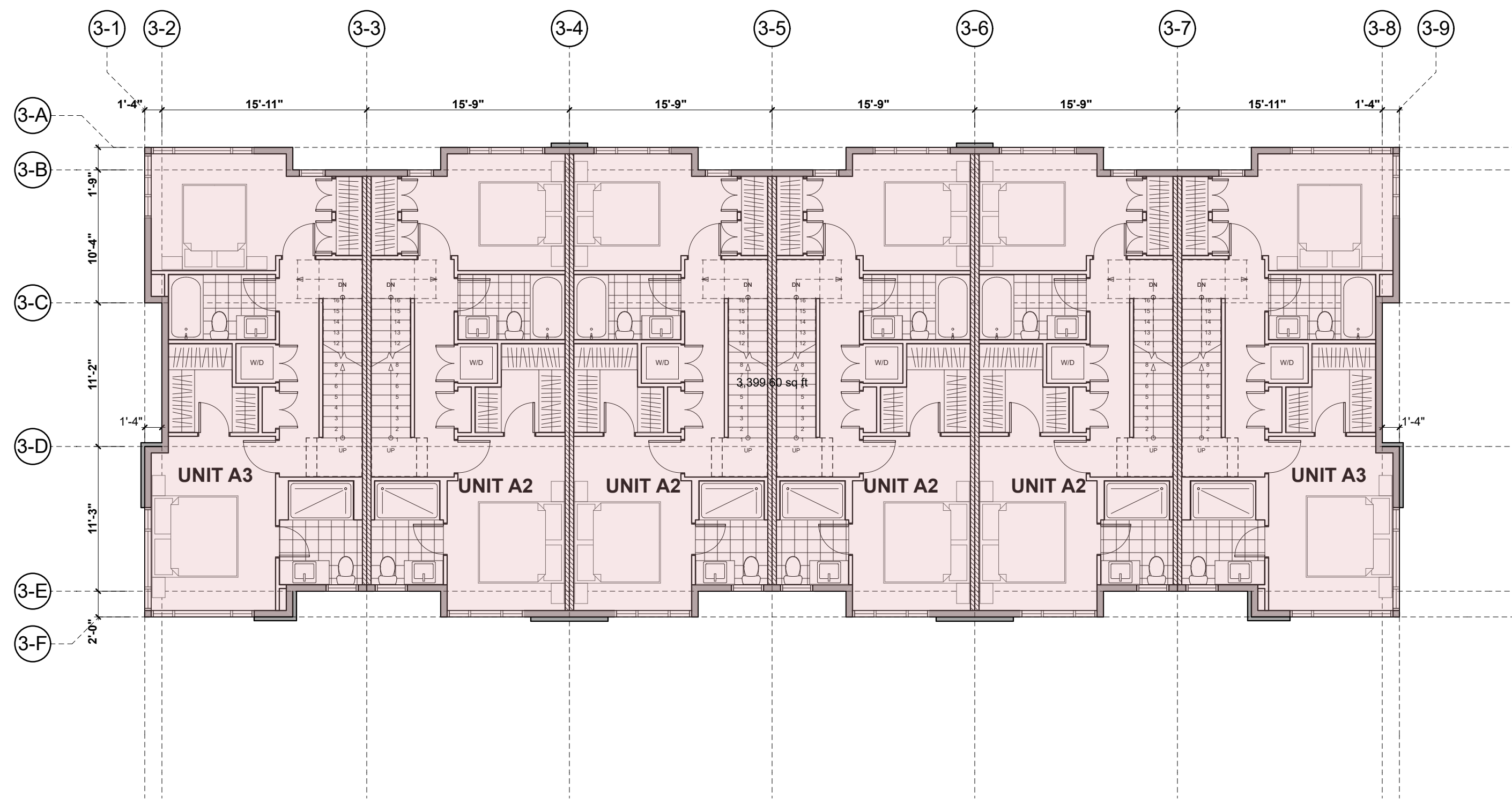
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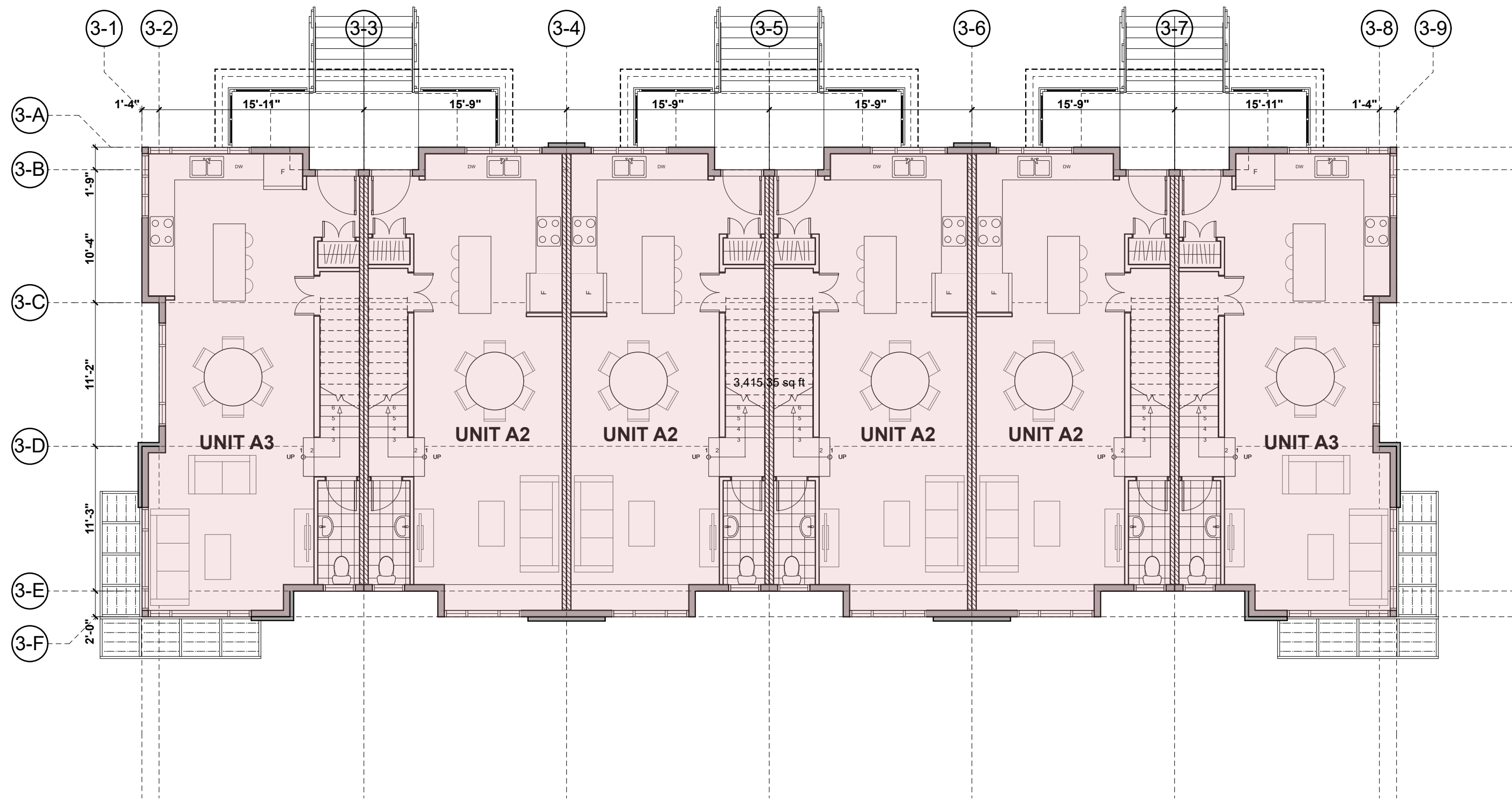
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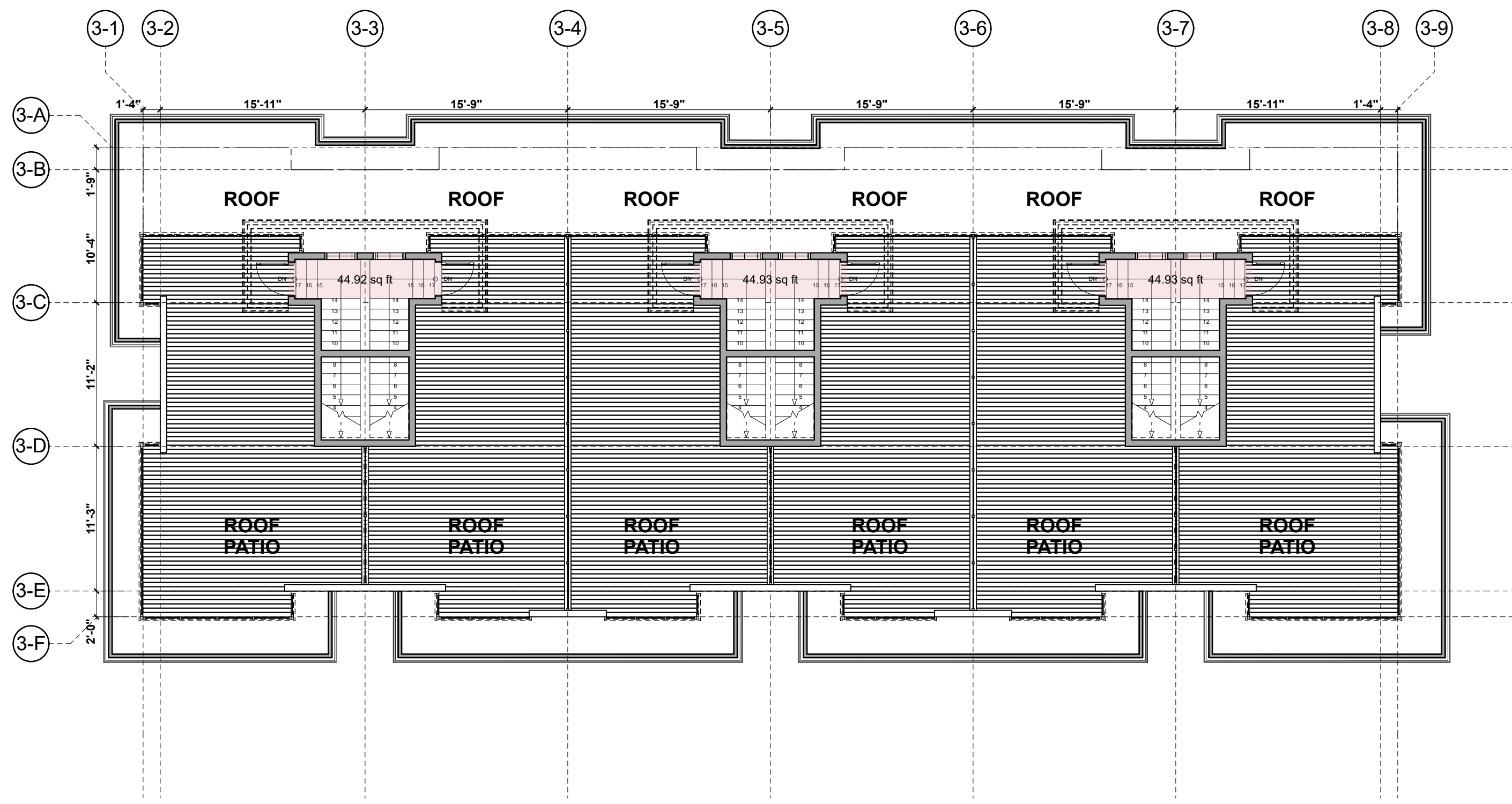
**1 First Floor Plan**  
SCALE: 1/8" = 1'-0"



**3 Third Floor Plan**  
SCALE: 1/8" = 1'-0"



**2 Second Floor Plan**  
SCALE: 1/8" = 1'-0"



**4 Roof Floor Plan**  
SCALE: 1/8" = 1'-0"

FIRST FLOOR PLAN AREA (FSR) = 3415.35 SQFT  
SECOND FLOOR PLAN AREA (FSR) = 3415.35 SQFT  
THIRD FLOOR PLAN AREA (FSR) = 3399.60 SQFT  
ROOF PLAN AREA (FSR) = 134.79 SQFT  
TOTAL AREA (FSR) = 10,365.09 SQFT

[ARCHITECT SEAL]

[CLIENT]

**ALLAIRE GROUP/  
HEADWATER PROJECTS**

[PROJECT]

**MOUNT SEYMOUR  
TOWNHOMES**  
3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[TITLE]

**BUILDING 3  
FLOOR PLANS**

**16399** [PROJECT]

1/8" = 1'-0" [SCALE]

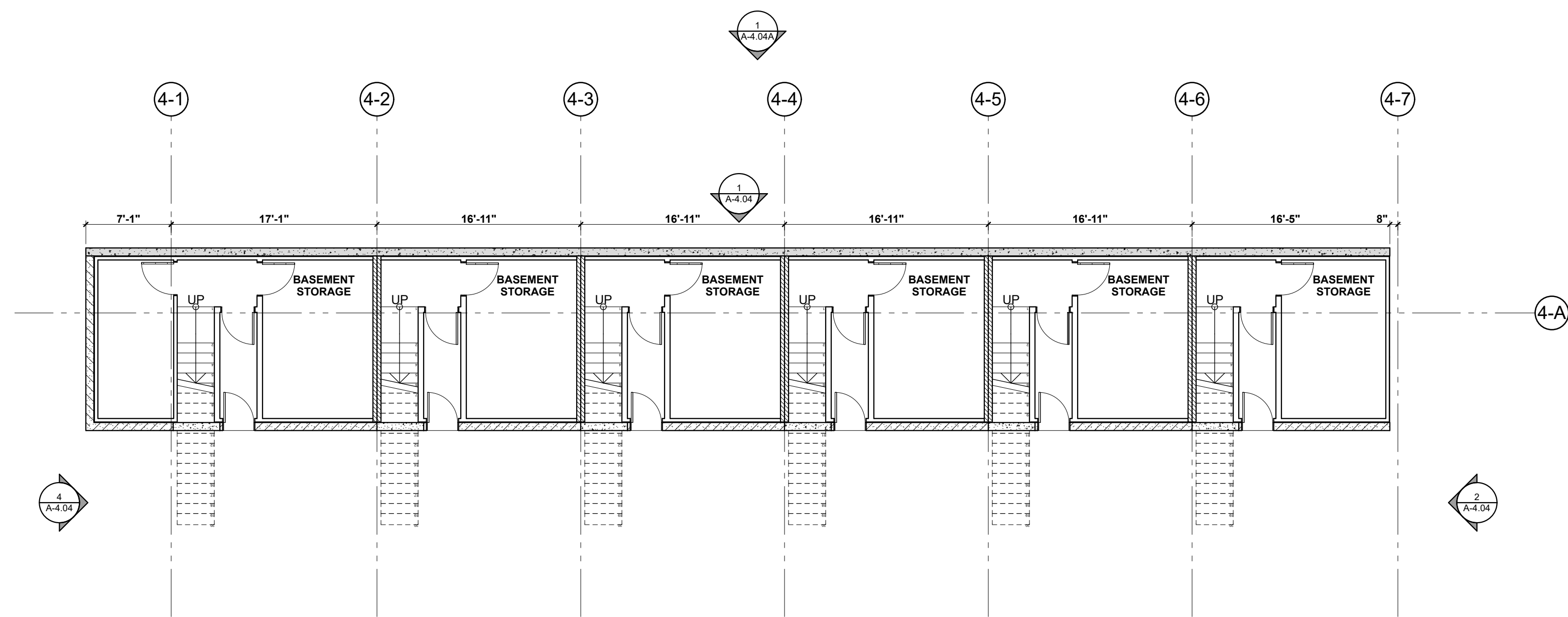
Wednesday, February 14, 2018 [DATE]

4.0 Development Permit [ISSUE]

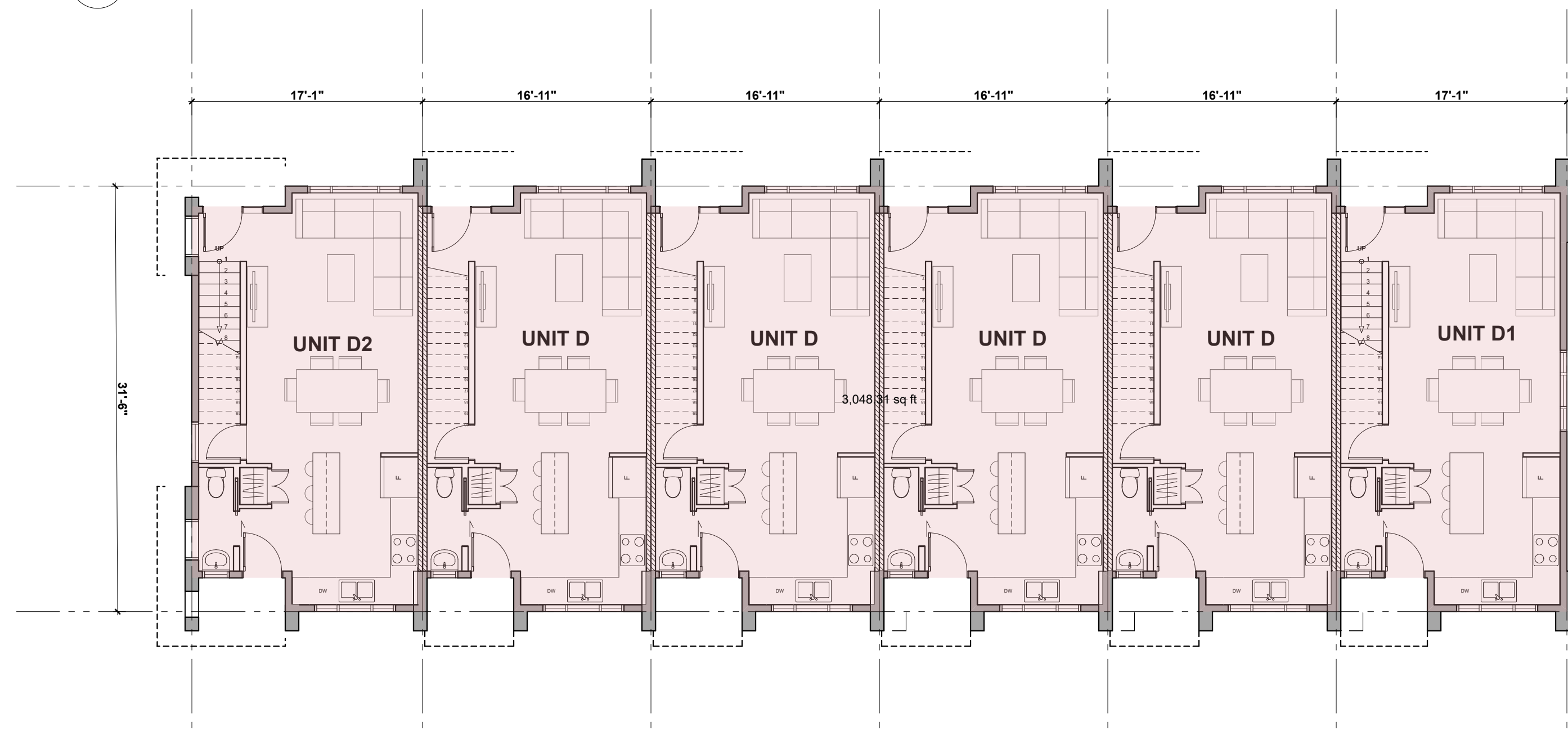
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**A-2.03**

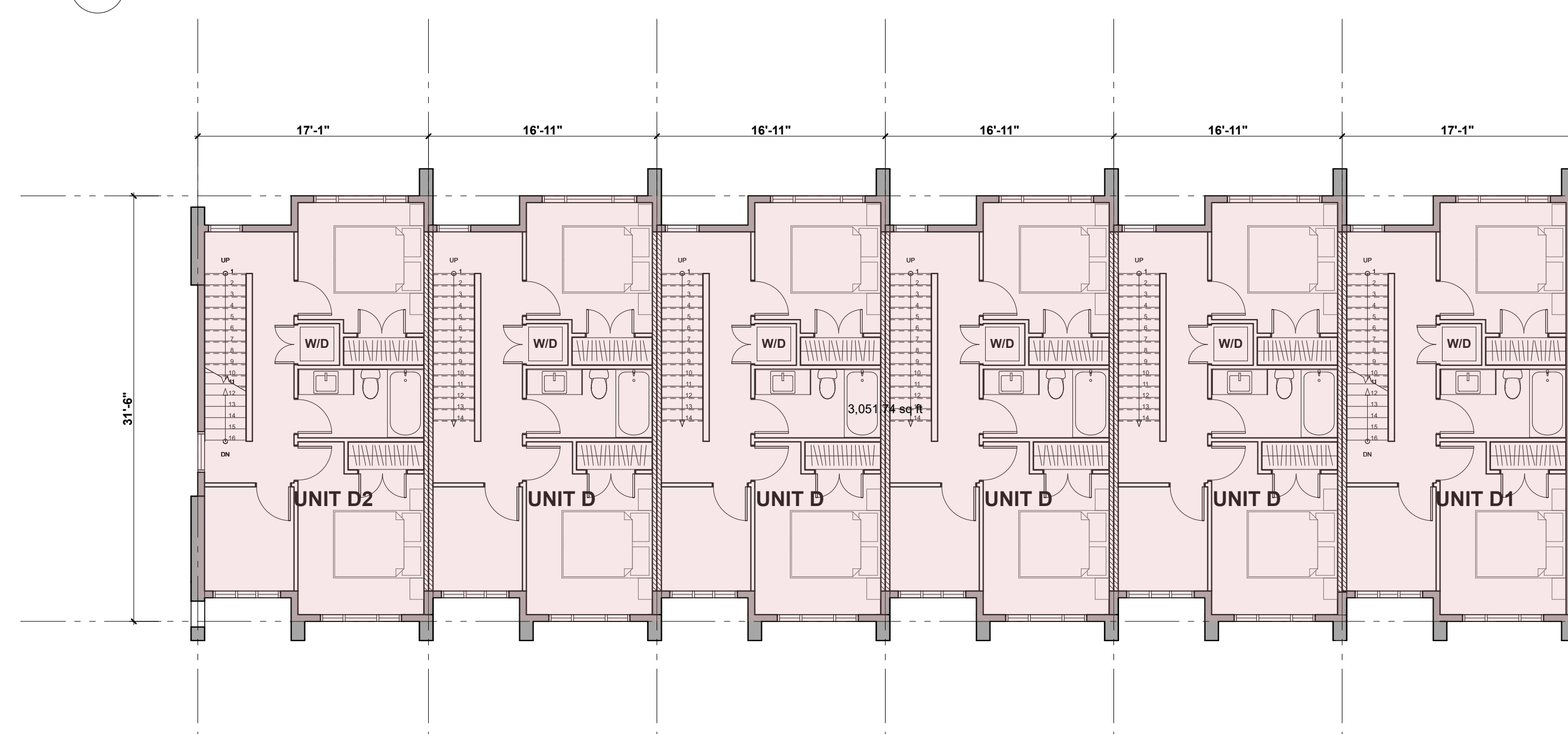




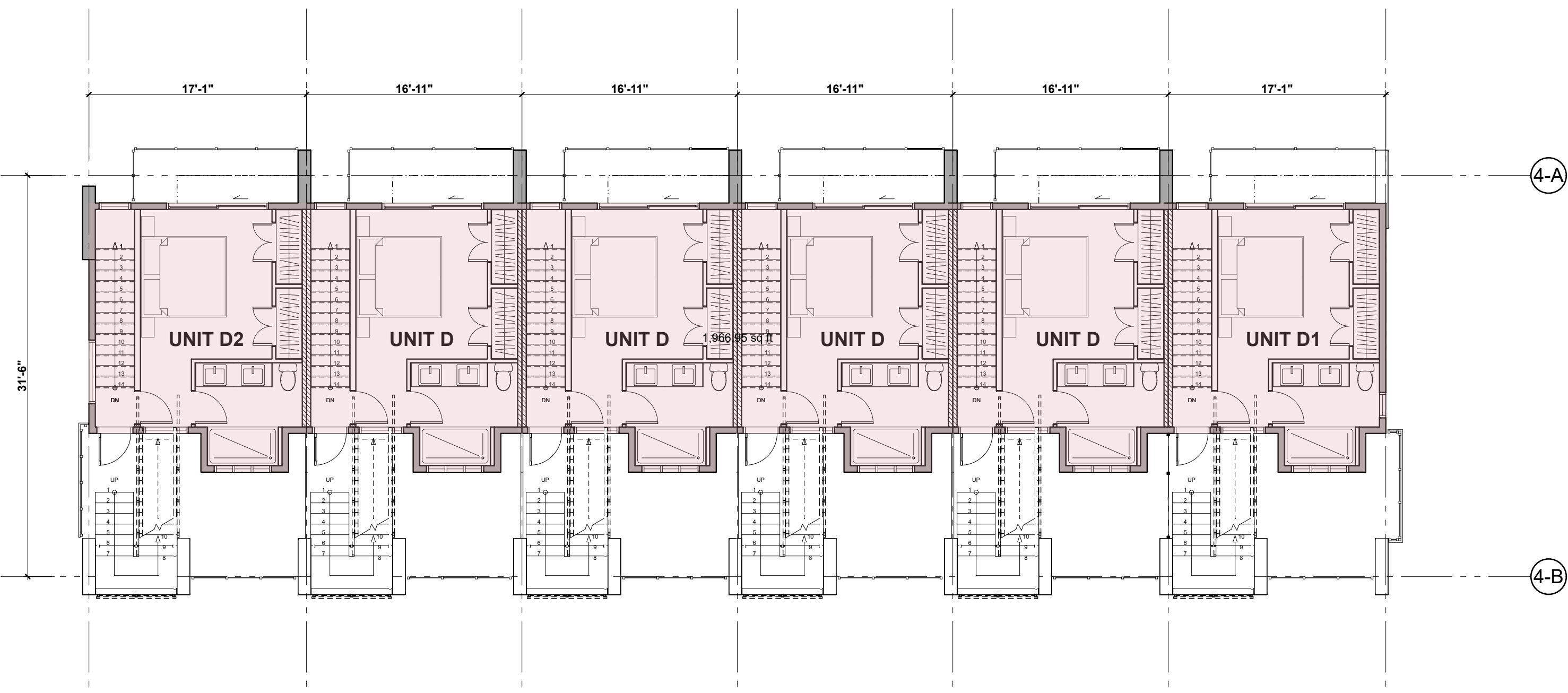
**1** Basement  
SCALE: 1/8" = 1'-0"



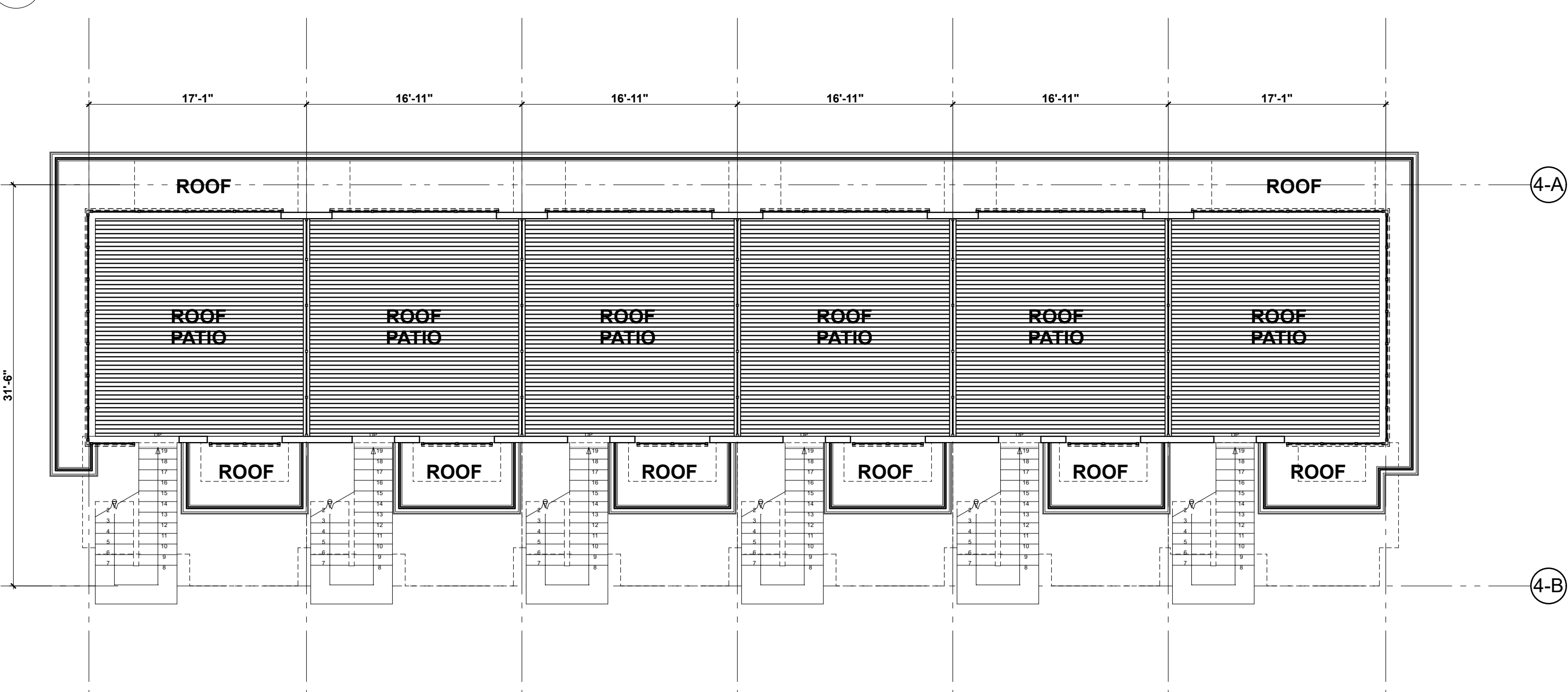
**2** First Floor Plan  
SCALE: 1/8" = 1'-0"



**3** Second Floor Plan  
SCALE: 1/8" = 1'-0"



**4** Third Floor Plan  
SCALE: 1/8" = 1'-0"



**5** Roof Floor Plan  
SCALE: 1/8" = 1'-0"

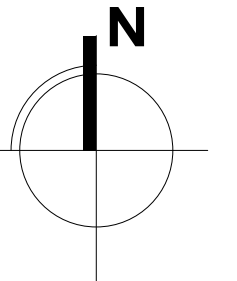
FIRST FLOOR PLAN AREA (FSR) = 3048.31 SQFT  
SECOND FLOOR PLAN AREA (FSR) = 3021.86 SQFT  
THIRD FLOOR PLAN AREA (FSR) = 1966.95 SQFT  
ROOF PLAN AREA (FSR) = 0.00 SQFT  
TOTAL AREA (FSR) = 8037.12 SQFT



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ALLAIRE GROUP/  
HEADWATER PROJECTS

[PROJECT]

**MOUNT SEYMOUR  
TOWNHOMES**

3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[TITLE]

**BUILDING 4  
PLANS**

[PROJECT]

16399

[SCALE]

1/8" = 1'-0"

[DATE]

Wednesday, February 21, 2018

[ISSUE]

4.0 Development Permit

[DRAWING]

**A-2.04**





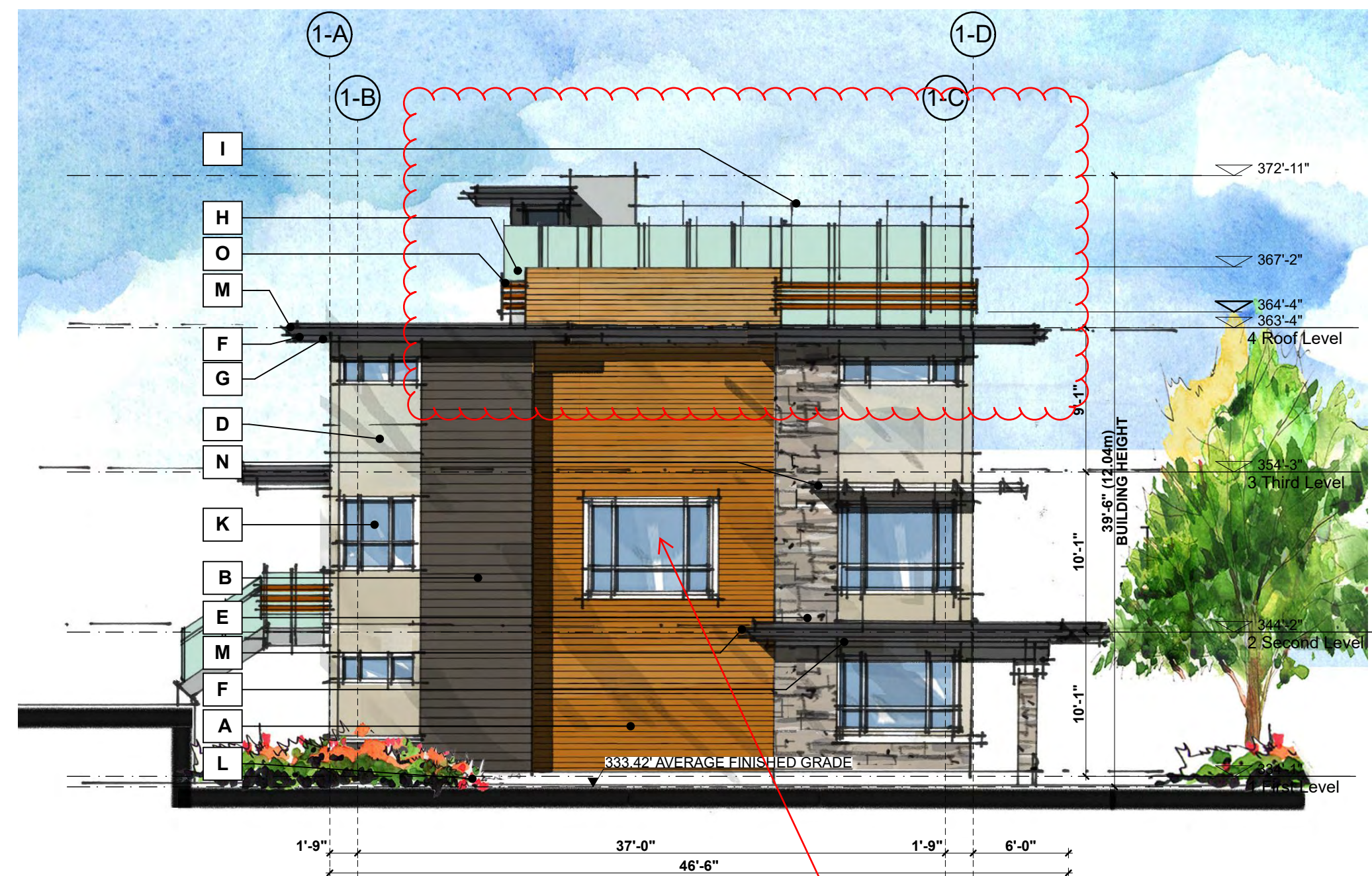
1 BUILDING 1 NORTH ELEVATION  
SCALE: 1/8" = 1'-0"



2 BUILDING 1 EAST ELEVATION  
SCALE: 1/8" = 1'-0"



3 BUILDING 1 SOUTH ELEVATION  
SCALE: 1/8" = 1'-0"



4 BUILDING 1 WEST ELEVATION  
SCALE: 1/8" = 1'-0"

### MATERIAL & COLOUR LEGEND

Colour	Manufacturer	Colour to match... Reference No.	Elements
<b>A</b> Cedar	Woodtone Fiber Cement Plank Siding	Old Cherry	Accent Siding @ Exterior Walls
<b>B</b> Dark Brown	HardiePlank Lap Siding ColorPlus Cedarmill Texture, 6" exposure	Rich Espresso	@ Exterior walls, Selected locations
<b>C</b> Gray	HardiePanel Siding ColorPlus Smooth Texture	Pearl Gray	@ Exterior walls, Select locations
<b>D</b> Beige	HardiePanel Siding ColorPlus Smooth Texture	Cobble Stone	@ Exterior walls, Selected locations
<b>E</b> Dark Gray	Cultured Stone	Cobblefield "Echo Ridge"	Accent Cladding @ Corners

Colour	Manufacturer	Colour to match... Reference No.	Elements
<b>F</b> Dark Gray	Wood Trim Boards	Benjamin Moore 2121-10 Gray	Balcony & Roof Fascias
<b>G</b> Cedar	Woodtone Fiber Cement Plank Siding	Old Cherry	Roof overhangs & balconies soffit
<b>H</b> Light Gray	Engineered Aluminum Railings	To match Anodized aluminum	Pre-finished Aluminum Railing c/w Clear Safety Glass
<b>I</b> Light Gray	Engineered Aluminum Privacy Screens	To match Anodized aluminum	Pre-finished Aluminum frame c/w Obscure Safety Glass Infill Panel
<b>J</b> White	Metal Doors		@ Unit Entrance

Colour	Manufacturer	Colour to match... Reference No.	Elements
<b>K</b> Light Gray	Starline Vinyl Windows & Doors	Standard Silver	Double Glazed Vinyl Windows & Doors
<b>L</b> Gray	Parged Concrete	Gray	Exposed Concrete Foundations / Columns
<b>M</b> Varies	Makin Metals Ltd.	colour to match adjacent material	Gutters + Downspouts, Pre-finished Steel Flashings
<b>N</b> Light Gray	Engineered Aluminum Sunshade	To match Anodized aluminum	Pre-finished Aluminum sunshade
<b>O</b> Cherry Brown	Composite wood siding Trugrain Walnut		Glass guard exterior cladding @ various locations

5 Colour & Material Legend  
SCALE: 1/8" = 1'-0"



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[CLIENT]

**ALLAIRE GROUP/  
HEADWATER PROJECTS**

[PROJECT]

**MOUNT SEYMOUR  
TOWNHOMES**

3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[TITLE]

**BUILDING 1  
ELEVATIONS**

[PROJECT]

**16399**

[SCALE]

**1/8" = 1'-0"**

[DATE]

**Wednesday, February 14, 2018**

[ISSUE]

**4.0 Development Permit**

[DRAWING]

**A-4.01**





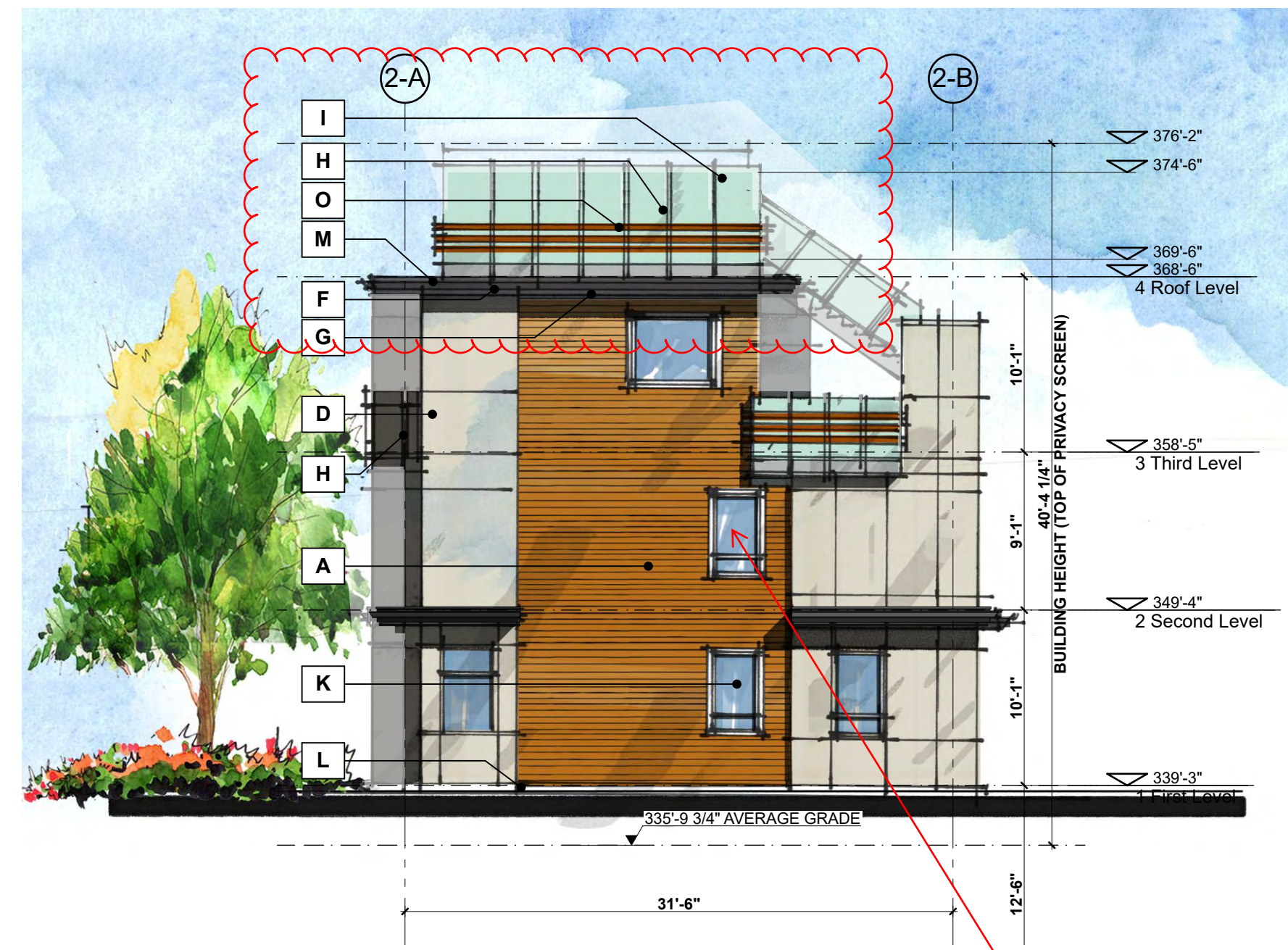
1 Building 2 North Elevation  
SCALE: 1/8" = 1'-0"



2 Building 2 East Elevation  
SCALE: 1/8" = 1'-0"



3 Building 2 South Elevation  
SCALE: 1/8" = 1'-0"



4 Building 2 West Elevation  
SCALE: 1/8" = 1'-0"

### MATERIAL & COLOUR LEGEND

Colour	Manufacturer	Colour to match... Reference No.	Elements
A Cedar	Woodtone Fiber Cement Plank Siding	Old Cherry	Accent Siding @ Exterior Walls
B Dark Brown	HardiePlank Lap Siding ColorPlus Cedarmill Texture, 6" exposure	Rich Espresso	@ Exterior walls, Selected locations
C Gray	HardiePanel Siding ColorPlus Smooth Texture	Pearl Gray	@ Exterior walls, Select locations
D Beige	HardiePanel Siding ColorPlus Smooth Texture	Cobble Stone	@ Exterior walls, Selected locations
E Dark Gray	Cultured Stone	Cobblefield "Echo Ridge"	Accent Cladding @ Corners

5 Colour & Material Legend  
SCALE: 1/8" = 1'-0"

Colour	Manufacturer	Colour to match... Reference No.	Elements
F Dark Gray	Wood Trim Boards	Benjamin Moore 2121-10 Gray	Balcony & Roof Fascias
G Cedar	Woodtone Fiber Cement Plank Siding	Old Cherry	Roof overhangs & balconies soffit
H Light Gray	Engineered Aluminum Railings	To match Anodized aluminum	Pre-finished Aluminum Railing c/w Clear Safety Glass
I Light Gray	Engineered Aluminum Privacy Screens	To match Anodized aluminum	Pre-finished Aluminum frame c/w Obscure Safety Glass Infill Panel @ Unit Entrance
J White	Metal Doors		

Colour	Manufacturer	Colour to match... Reference No.	Elements
K Light Gray	Starline Vinyl Windows & Doors	Standard Silver	Double Glazed Vinyl Windows & Doors
L Gray	Parged Concrete	Gray	Exposed Concrete Foundations / Columns
M Varies	Makin Metals Ltd.	colour to match adjacent material	Gutters + Downspouts, Pre-finished Steel Flashings
N Light Gray	Engineered Aluminum Sunshade	To match Anodized aluminum	Pre-finished Aluminum sunshade
O Cherry Brown	Composite wood siding	Trugrain Walnut	Glass guard exterior cladding @ various locations



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**ALLAIRE GROUP/  
HEADWATER PROJECTS**

[PROJECT]

**MOUNT SEYMOUR  
TOWNHOMES**

3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[TITLE]

**BUILDING 2  
ELEVATIONS**

[PROJECT]

**16399**

[SCALE]

**1/8" = 1'-0"**

**Wednesday, February 14, 2018**

**4.0 Development Permit**

[ISSUE]

[DRAWING]

**A-4.02**





1 BUILDING 3 NORTH ELEVATION  
SCALE: 1/8" = 1'-0"



2 BUILDING 3 EAST ELEVATION  
SCALE: 1/8" = 1'-0"



3 BUILDING 3 SOUTH ELEVATION  
SCALE: 1/8" = 1'-0"



4 BUILDING 3 WEST ELEVATION  
SCALE: 1/8" = 1'-0"

### MATERIAL & COLOUR LEGEND

Colour	Manufacturer	Colour to match... Reference No.	Elements
A Cedar	Woodtone Fiber Cement Plank Siding	Old Cherry	Accent Siding @ Exterior Walls
B Dark Brown	HardiePlank Lap Siding ColorPlus Cedarmill Texture, 6" exposure	Rich Espresso	@ Exterior walls, Selected locations
C Gray	HardiePanel Siding ColorPlus Smooth Texture	Pearl Gray	@ Exterior walls, Select locations
D Beige	HardiePanel Siding ColorPlus Smooth Texture	Cobble Stone	@ Exterior walls, Selected locations
E Dark Gray	Cultured Stone	Cobblefield "Echo Ridge"	Accent Cladding @ Corners

5 Colour & Material Legend  
SCALE: 1/8" = 1'-0"

Colour	Manufacturer	Colour to match... Reference No.	Elements
F Dark Gray	Wood Trim Boards	Benjamin Moore 2121-10 Gray	Balcony & Roof Fascias
G Cedar	Woodtone Fiber Cement Plank Siding	Old Cherry	Roof overhangs & balconies soffit
H Light Gray	Engineered Aluminum Railings	To match Anodized aluminum	Pre-finished Aluminum Railing c/w Clear Safety Glass
I Light Gray	Engineered Aluminum Privacy Screens	To match Anodized aluminum	Pre-finished Aluminum frame c/w Obscure Safety Glass Infill Panel
J White	Metal Doors		@ Unit Entrance

Colour	Manufacturer	Colour to match... Reference No.	Elements
K Light Gray	Starline Vinyl Windows & Doors	Standard Silver	Double Glazed Vinyl Windows & Doors
L Gray	Parged Concrete	Gray	Exposed Concrete Foundations / Columns
M Varies	Makin Metals Ltd.	colour to match adjacent material	Gutters + Downspouts, Pre-finished Steel Flashings
N Light Gray	Engineered Aluminum Sunshade	To match Anodized aluminum	Pre-finished Aluminum sunshade
O Cherry Brown	Composite wood siding	Trugrain Walnut	Glass guard exterior cladding @ various locations



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[PROJECT]

**MOUNT SEYMOUR  
TOWNHOMES**

3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[TITLE]

**BUILDING 3  
ELEVATIONS**

[PROJECT]

**16399**

[SCALE]

**1/8" = 1'-0"**

**Wednesday, February 14, 2018**

**4.0 Development Permit**

[ISSUE]

[DRAWING]

**A-4.03**





1 Building 4 North Elevation  
SCALE: 1/8" = 1'-0"



2 Building 4 East Elevation  
SCALE: 1/8" = 1'-0"



3 Building 4 South Elevation  
SCALE: 1/8" = 1'-0"



4 Building 4 West Elevation  
SCALE: 1/8" = 1'-0"

### MATERIAL & COLOUR LEGEND

Colour	Manufacturer	Colour to match... Reference No.	Elements
<b>A</b> Cedar	Woodtone Fiber Cement Plank Siding	Old Cherry	Accent Siding @ Exterior Walls
<b>B</b> Dark Brown	HardiePlank Lap Siding ColorPlus Cedarmill Texture, 6" exposure	Rich Espresso	@ Exterior walls, Selected locations
<b>C</b> Gray	HardiePanel Siding ColorPlus Smooth Texture	Pearl Gray	@ Exterior walls, Select locations
<b>D</b> Beige	HardiePanel Siding ColorPlus Smooth Texture	Cobble Stone	@ Exterior walls, Selected locations
<b>E</b> Dark Gray	Cultured Stone	Cobblefield "Echo Ridge"	Accent Cladding @ Corners

5 Colour & Material Legend  
SCALE: 1/8" = 1'-0"

Colour	Manufacturer	Colour to match... Reference No.	Elements
<b>F</b> Dark Gray	Wood Trim Boards	Benjamin Moore 2121-10 Gray	Balcony & Roof Fascias
<b>G</b> Cedar	Woodtone Fiber Cement Plank Siding	Old Cherry	Roof overhangs & balconies soffit
<b>H</b> Light Gray	Engineered Aluminum Railings	To match Anodized aluminum	Pre-finished Aluminum Railing c/w Clear Safety Glass
<b>I</b> Light Gray	Engineered Aluminum Privacy Screens	To match Anodized aluminum	Pre-finished Aluminum frame c/w Obscure Safety Glass Infill Panel
<b>J</b> White	Metal Doors		@ Unit Entrance

Colour	Manufacturer	Colour to match... Reference No.	Elements
<b>K</b> Light Gray	Starline Vinyl Windows & Doors	Standard Silver	Double Glazed Vinyl Windows & Doors
<b>L</b> Gray	Parged Concrete	Gray	Exposed Concrete Foundations / Columns
<b>M</b> Varies	Makin Metals Ltd.	colour to match adjacent material	Gutters + Downspouts, Pre-finished Steel Flashings
<b>N</b> Light Gray	Engineered Aluminum Sunshade	To match Anodized aluminum	Pre-finished Aluminum sunshade
<b>O</b> Cherry Brown	Composite wood siding	Trugrain Walnut	Glass guard exterior cladding @ various locations



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ALLAIRE GROUP/  
HEADWATER PROJECTS

[PROJECT]

**MOUNT SEYMOUR  
TOWNHOMES**

3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[TITLE]

**BUILDING 4  
ELEVATIONS**

[PROJECT]

16399

[SCALE]

1/8" = 1'-0"

[DATE]

Wednesday, February 14, 2018

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**A-4.04**





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1 COURTYARD ELEVATION LOOKING SOUTH  
SCALE: 1" = 1'-0"



2 COURTYARD ELEVATION LOOKING NORTH  
SCALE: 1" = 1'-0"

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[CLIENT]

ALLAIRE GROUP/  
HEADWATER PROJECTS

[PROJECT]

**MOUNT SEYMOUR  
TOWNHOMES**  
3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[TITLE]

**COURTYARD  
ELEVATION  
RENDERINGS**

16399 [PROJECT]

1" = 1'-0" [SCALE]

Thursday, February 15, 2018 [DATE]

4.0 Development Permit [ISSUE]

[DRAWING]

**A-4.05**





1 MOUNT SEYMOUR PARKWAY ELEVATION  
SCALE: 1" = 1'-0"



1 LANE ELEVATION  
SCALE: 1" = 1'-0"



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HEADWATER PROJECTS

[PROJECT]

**MOUNT SEYMOUR  
TOWNHOMES**  
3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[TITLE]

**Street Elevation  
Renderings**

16399 [PROJECT]

1' = 1'-0" [SCALE]

Thursday, February 15, 2018 [DATE]

4.0 Development Permit [ISSUE]

[DRAWING]

**A-4.06**

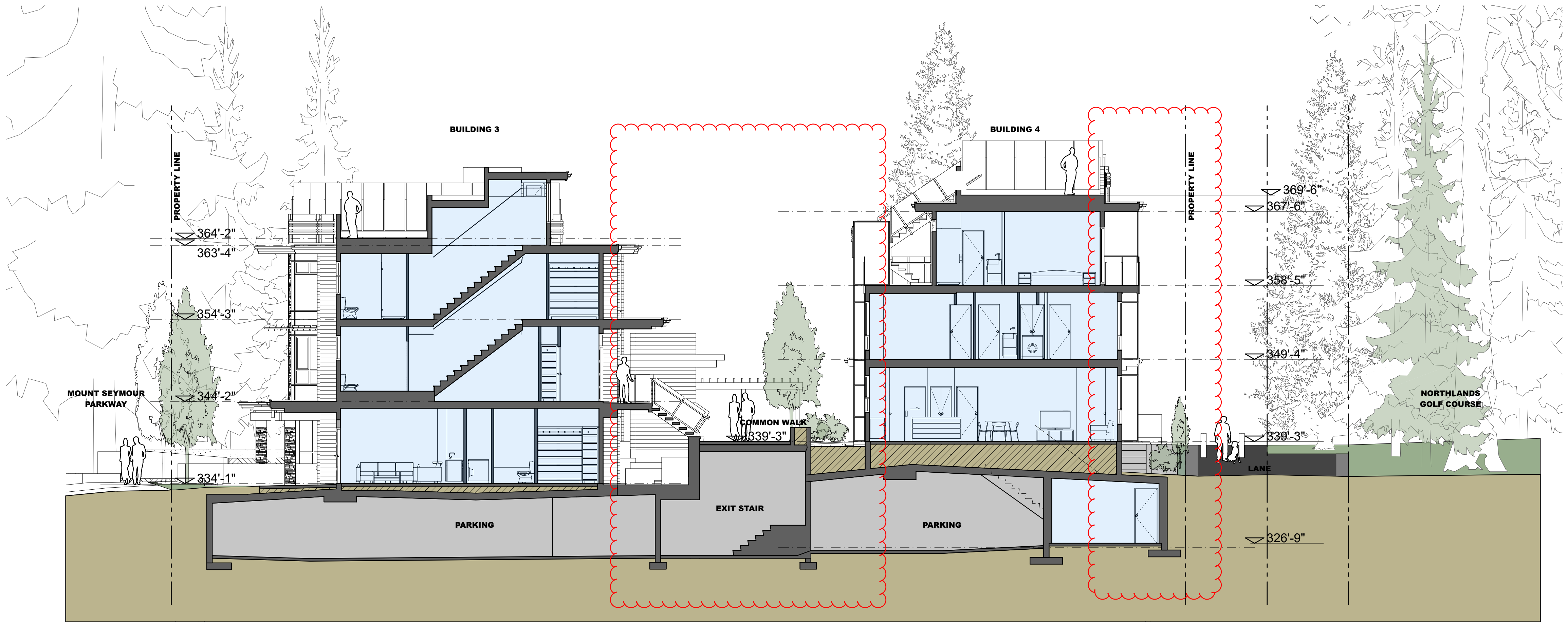




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**1 SECTION**  
SCALE: 1/8" = 1'-0"



**2 SECTION**  
SCALE: 1/8" = 1'-0"

[ARCHITECT SEAL]

[CLIENT]

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HEADWATER PROJECTS**

[PROJECT]

**MOUNT SEYMOUR  
TOWNHOMES**  
3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[TITLE]

**SECTIONS**

**16399** [PROJECT]

**1/8" = 1'-0"** [SCALE]

**Wednesday, February 21, 2018** [DATE]

**4.0 Development Permit** [ISSUE]

[DRAWING]

**A-5.01**



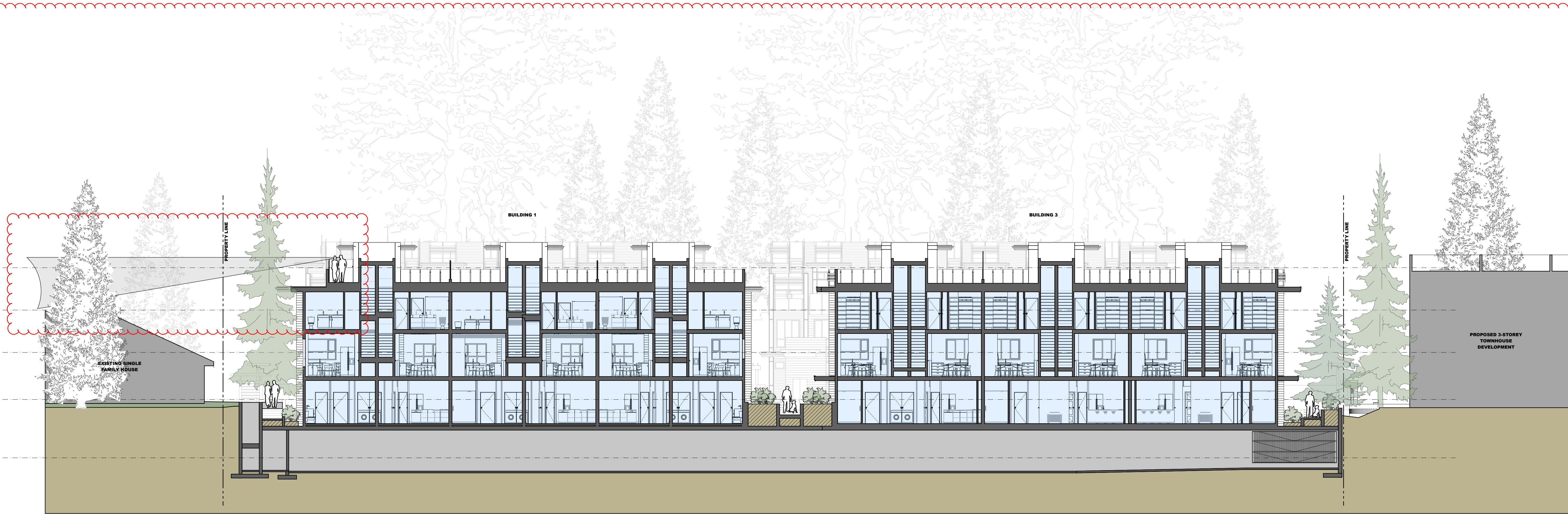
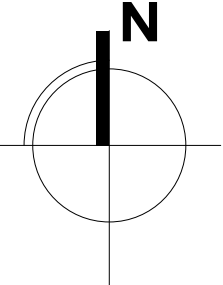


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## 1 SECTION

SCALE: 3/32" = 1'-0"

[ARCHITECT SEAL]

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**ALLAIRE GROUP/  
HEADWATER PROJECTS**

[PROJECT]

**MOUNT SEYMOUR  
TOWNHOMES**  
3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[TITLE]

## SECTIONS

[PROJECT]

**16399**

[SCALE]

**3/32" = 1'-0"**

[DATE]

**Wednesday, February 21, 2018**

[ISSUE]

**4.0 Development Permit**

[DRAWING]

# A-5.02





1

Perspective Looking North-West from Mount Seymour Parkway

NOT TO SCALE

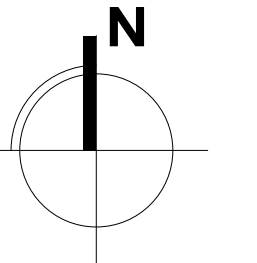


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ALLAIRE GROUP/  
HEADWATER PROJECTS

[PROJECT]

**MOUNT SEYMOUR  
TOWNHOMES**

3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[TITLE]

**Perspective View**

[PROJECT]

16399

[SCALE]

1:50

[DATE]

Wednesday, February 14, 2018

[ISSUE]

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[DRAWING]

**A-8.01**





1

Courtyard Looking South-East

NOT TO SCALE

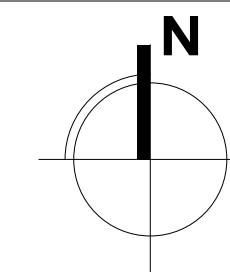


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[PROJECT]

**MOUNT SEYMOUR  
TOWNHOMES**

3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[TITLE]

**Perspective View**

[PROJECT]

**16399**

[SCALE]

**1:47**

[DATE]

**Wednesday, February 14, 2018**

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[DRAWING]

**A-8.02**





1

Lane Perspective Looking South-West

NOT TO SCALE

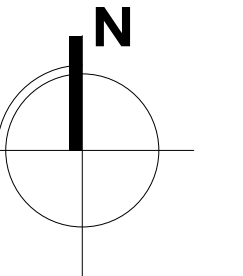


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[PROJECT]

**MOUNT SEYMOUR  
TOWNHOMES**

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North Vancouver, BC

[TITLE]

**Perspective View**

[PROJECT]

16399

[SCALE]

1:45

[DATE]

Wednesday, February 14, 2018

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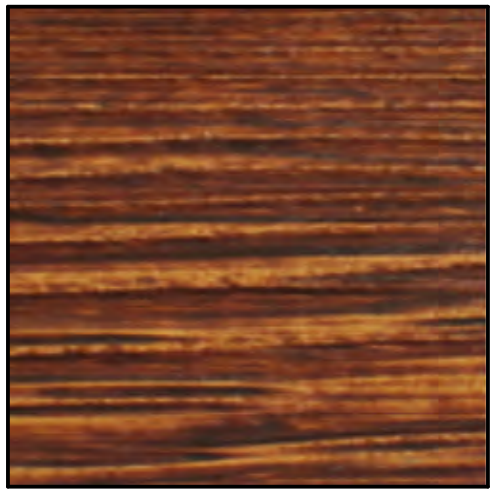
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**A-8.03**



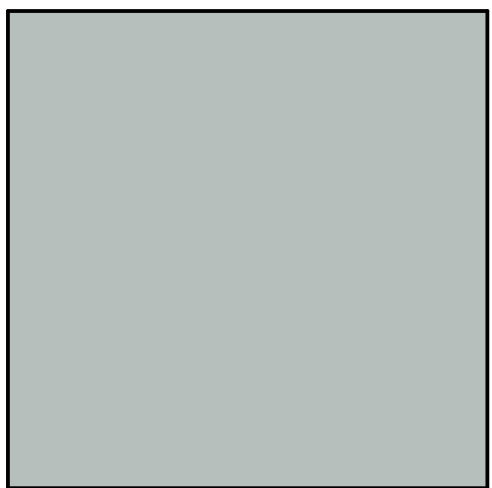
EXTERIOR MATERIALS & COLOURS



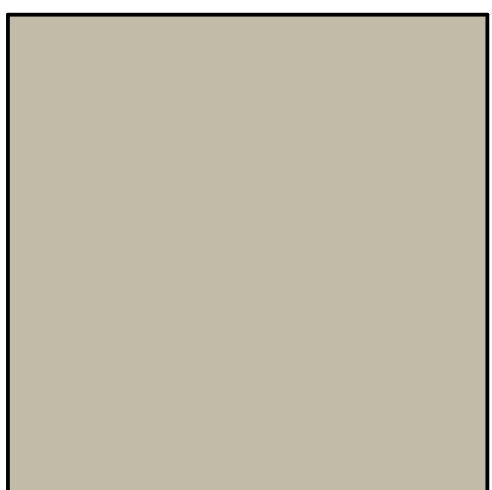
A/G:  
**ACCENT SIDING/SOFFITS**  
WOODTONE  
FIBRE CEMENT LAP SIDING  
RUSTIC SERIES  
'OLD CHERRY'



B:  
**SIDING**  
HARDI-PLANK LAP SIDING  
CEDAR MILL TEXTURE  
'RICH ESPRESSO'



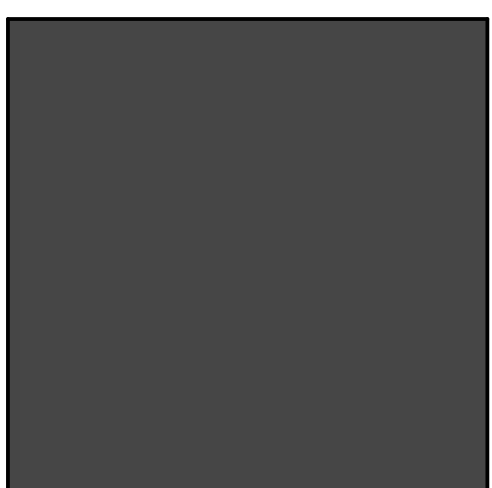
C:  
**SIDING**  
HARDI-PANEL SIDING  
SMOOTH TEXTURE  
'PEARL GRAY'



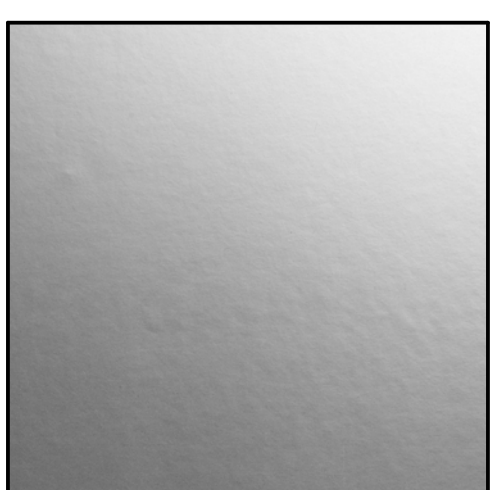
D:  
**SIDING**  
HARDI-PANEL SIDING  
SMOOTH TEXTURE  
'COBBLE STONE'



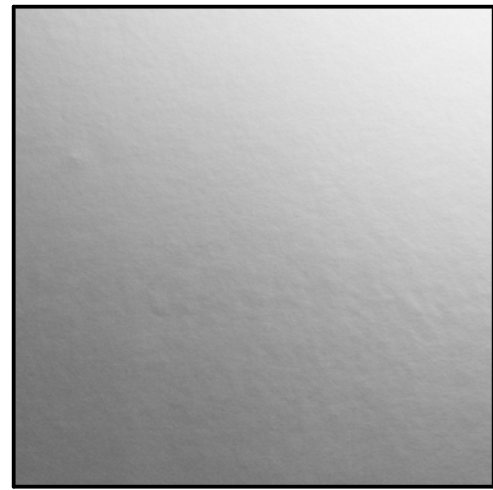
E:  
**ACCENT SIDING**  
CULTURED STONE  
COBBLEFIELD  
'ECHO RIDGE'



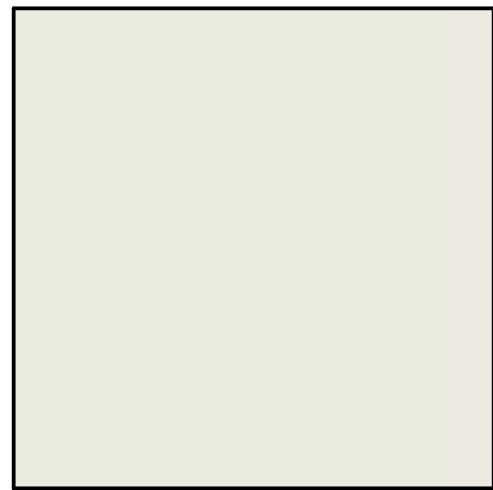
F:  
**WOOD TRIM BOARDS**  
BENJAMIN MOORE  
2121-10 GRAY



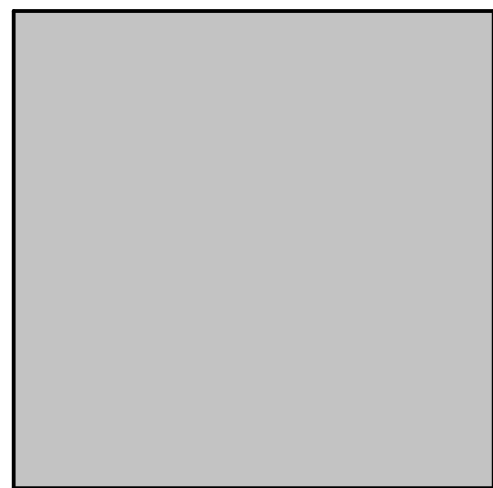
H:  
**ENGINEERED ALUMINUM RAILINGS**  
ANODIZED ALUMINUM FINISH  
C/W CLEAR SAFETY GLASS



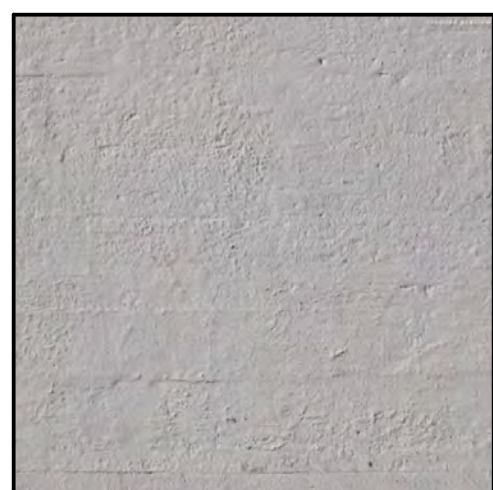
I:  
**ENGINEERED ALUMINUM PRIVACY SCREENS**  
ANODIZED ALUMINUM FINISH  
C/W OBSCURE SAFETY GLASS



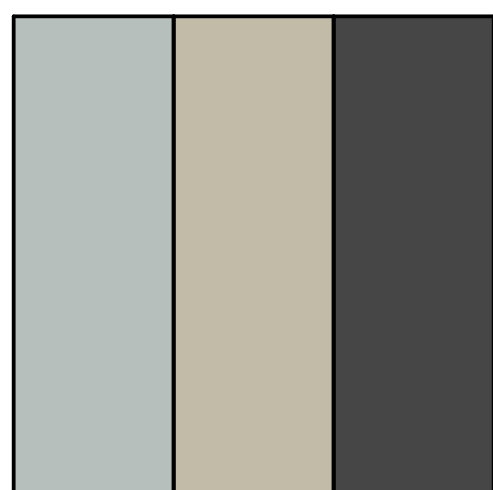
J:  
**METAL UNIT ENTRANCE DOORS**  
BENJAMIN MOORE  
OC17 'WHITE DOVE'



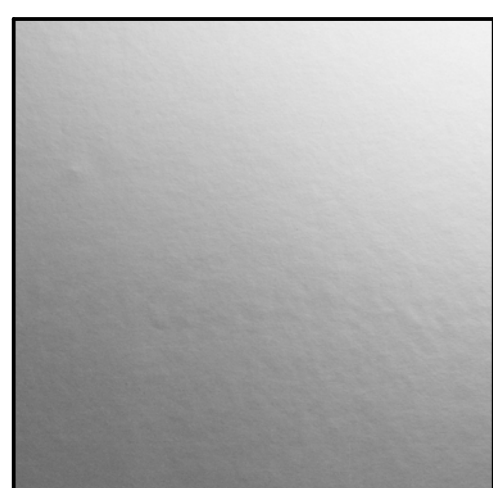
K:  
**DOUBLE GLAZED VINYL DOORS + WINDOWS**  
STARLINE WINDOWS  
'STANDARD SILVER'



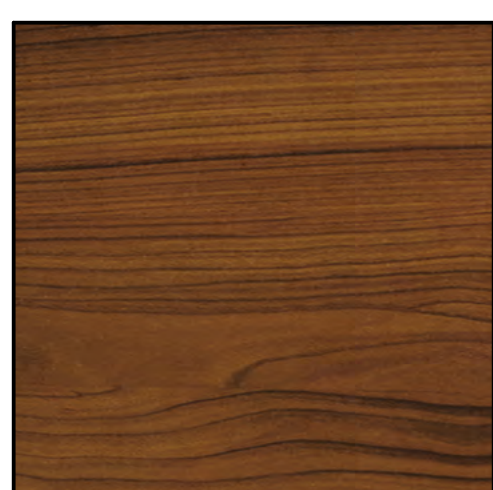
L:  
**EXPOSED CONCRETE FOUNDATIONS, COLUMNS**  
PARGED CONCRETE  
GRAY



M:  
**GUTTERS, DOWNSPOUTS & METAL FLASHINGS**  
MAKIN METALS  
COLOUR TO MATCH ADJACENT MATERIAL



N:  
**ENGINEERED ALUMINUM SUNSHADE**  
ANODIZED ALUMINUM



O:  
**GLASS GUARD EXTERIOR CLADDING**  
LONGBOARD TUBE  
WOOD GRAIN FINISH  
'LIGHT CHERRY'



**Integra**

ARCHITECTURE INC.

2330-200 Granville Street  
Vancouver, BC, V6C 1S4  
www.integra-arch.com  
Telephone: 604 688 4220

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[ARCHITECT SEAL]

[CLIENT]

ALLAIRE GROUP/  
HEADWATER PROJECTS

[PROJECT]

**MOUNT SEYMOUR TOWNHOMES**  
3428 - 3464 Mount Seymour Pkwy  
North Vancouver, BC

[TITLE]

**Materials & Colours**

[PROJECT]

16399

[SCALE]

Wednesday, February 14, 2018

4.0 Development Permit

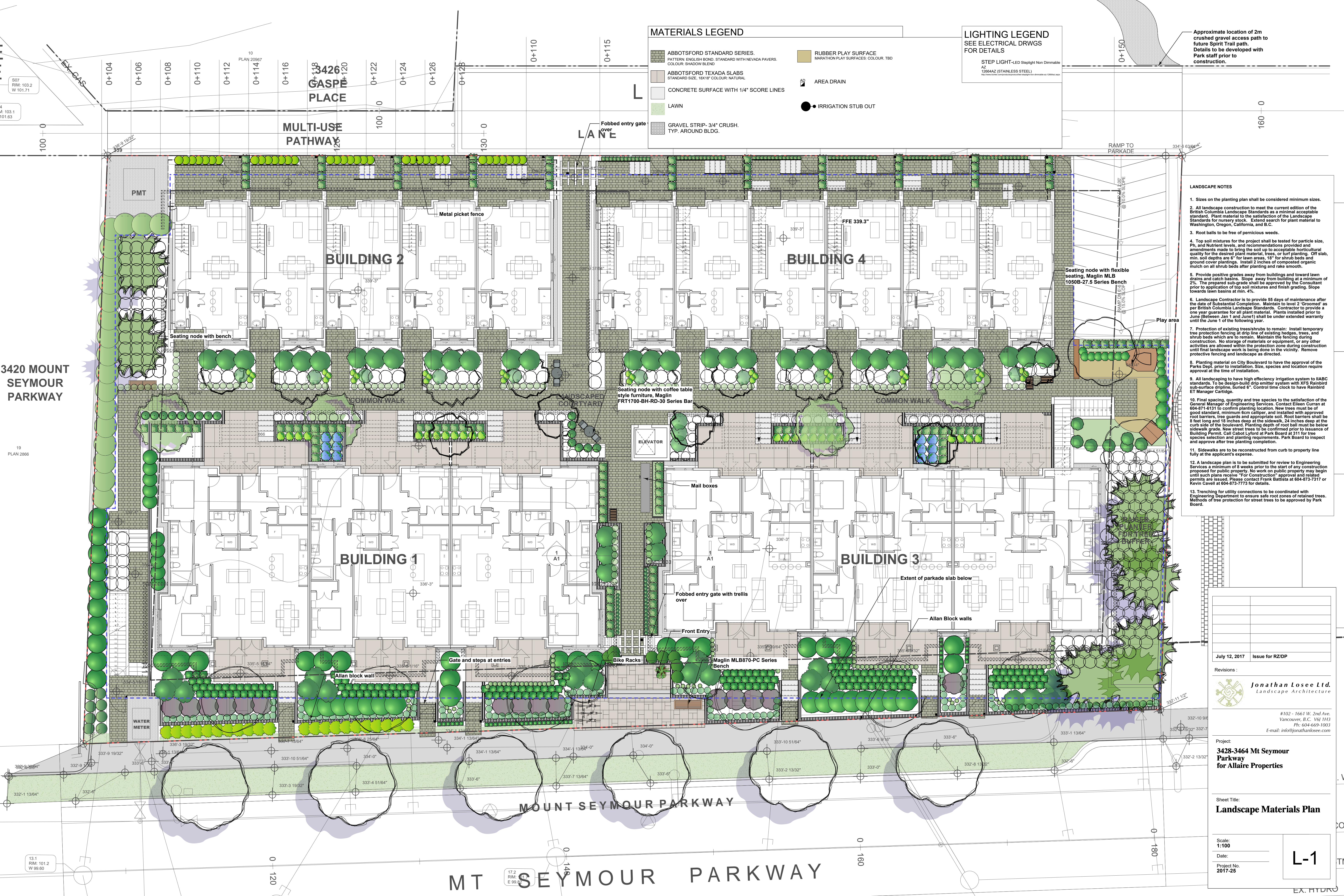
[ISSUE]

[DRAWING]









MATERIALS LEGEND

- ABBOTSFORD STANDARD SERIES.  
PATTERN: ENGLISH BOND, STANDARD WITH NEVADA PAVERS.  
COLOUR: SHADOW BLEND
- ABBOTSFORD TEXADA SLABS  
STANDARD SIZE, 18"x18" COLOUR: NATURAL
- CONCRETE SURFACE WITH 1/4" SCORE LINES
- LAWN
- GRAVEL STRIP- 3/4" CRUSH.  
TYP. AROUND BLDG.
- RUBBER PLAY SURFACE  
MARATHON PLAY SURFACES: COLOUR: T60
- AREA DRAIN
- IRRIGATION STUB OUT

LIGHTING LEGEND  
SEE ELECTRICAL DRWGS  
FOR DETAILS

STEP LIGHT-LED Steplight Non Dimmable  
AZ  
12064AZ (STAINLESS STEEL)

Approximate location of 2m  
crushed gravel access path to  
future Spirit Trail path.  
Details to be developed with  
Park staff prior to  
construction.

LANDSCAPE NOTES

1. Sizes on the planting plan shall be considered minimum sizes.
2. All landscape construction to meet the current edition of the British Columbia Landscape Standards as a minimal acceptable standard. Plant material to the satisfaction of the Landscape Standards for nursery stock. Extend search for plant material to Washington, Oregon, California, and B.C.
3. Root balls to be free of pernicious weeds.
4. Top soil mixtures for the project shall be tested for particle size, Ph, and Nutrient levels, and recommendations provided and amendments made to bring the soil up to acceptable horticultural quality for the desired plant material, trees, or turf planting. Off slab, min. soil depths are 6" for lawn areas, 18" for shrub beds and ground cover plantings. Install 2 inches of composted organic mulch on all shrub beds after planting and rake smooth.
5. Provide positive grades away from buildings and toward lawn drains and catch basins. Slope away from building at a minimum of 2%. The prepared sub-grade shall be approved by the Consultant prior to application of top soil mixtures and finish grading. Slope towards lawn basins at min. 4%.
6. Landscape Contractor is to provide 55 days of maintenance after the date of Substantial Completion. Maintain to level 2 "Groomed" as per British Columbia Landscape Standards. Contractor to provide a one year guarantee for all plant material. Plants installed prior to June (Between Jan 1 and June 1) shall be under extended warranty until the 1st of the following year.
7. Protection of existing trees/shrubs to remain: Install temporary tree protection fencing at drip line of existing hedges, trees, and shrub beds which are to remain. Maintain the fencing during construction. No storage of materials or equipment, or any other activities are allowed within the protection zone during construction until final landscape work is being done in the vicinity. Remove protective fencing and landscape as directed.
8. Planting material on City Boulevard to have the approval of the Parks Dept. prior to installation. Size, species and location require approval at the time of installation.
9. All landscaping to have high efficiency irrigation system to IABC standards. To be design-build drip emitter system with XFS Rainbird sub-surface dripline, buried 6". Control time clock to have Rainbird ET Manager Cartridge.
10. Final spacing, quantity and tree species to the satisfaction of the General Manager of Engineering Services. Contact Eileen Curran at 604-871-6131 to confirm planting location. New trees must be of good standard, minimum 6cm caliper, and installed with approved root barriers, tree guards and appropriate soil. Root barriers shall be 8 feet long and 18 inches deep at the sidewalk, 24 inches deep at the curb side of the boulevard. Planting depth of root ball must be below sidewalk grade. New street trees to be confirmed prior to issuance of Building Permit. Call Cabot Lyford at Park Board at 311 for tree species selection and planting requirements. Park Board to inspect and approve after tree planting completion.
11. Sidewalks are to be reconstructed from curb to property line fully at the applicant's expense.
12. A landscape plan is to be submitted for review to Engineering Services a minimum of 8 weeks prior to the start of any construction proposed for public property. No work on public property may begin until such plans receive "For Construction" approval and related permits are issued. Please contact Frank Battista at 604-873-7317 or Kevin Cavell at 604-873-7773 for details.
13. Trenching for utility connections to be coordinated with Engineering Department to ensure safe root zones of retained trees. Methods of tree protection for street trees to be approved by Park Board.

July 12, 2017 Issue for RZDP

Revisions:



Jonathan Losee Ltd.  
Landscape Architecture

#102 - 1661 W. 2nd Ave.  
Vancouver, B.C. V6L 1H3  
Ph: 604-669-1003  
E-mail: info@jonathanlosee.com

Project:  
3428-3464 Mt Seymour  
Parkway for Allaire Properties

Sheet Title:  
Landscape Materials Plan

Scale:

1:100

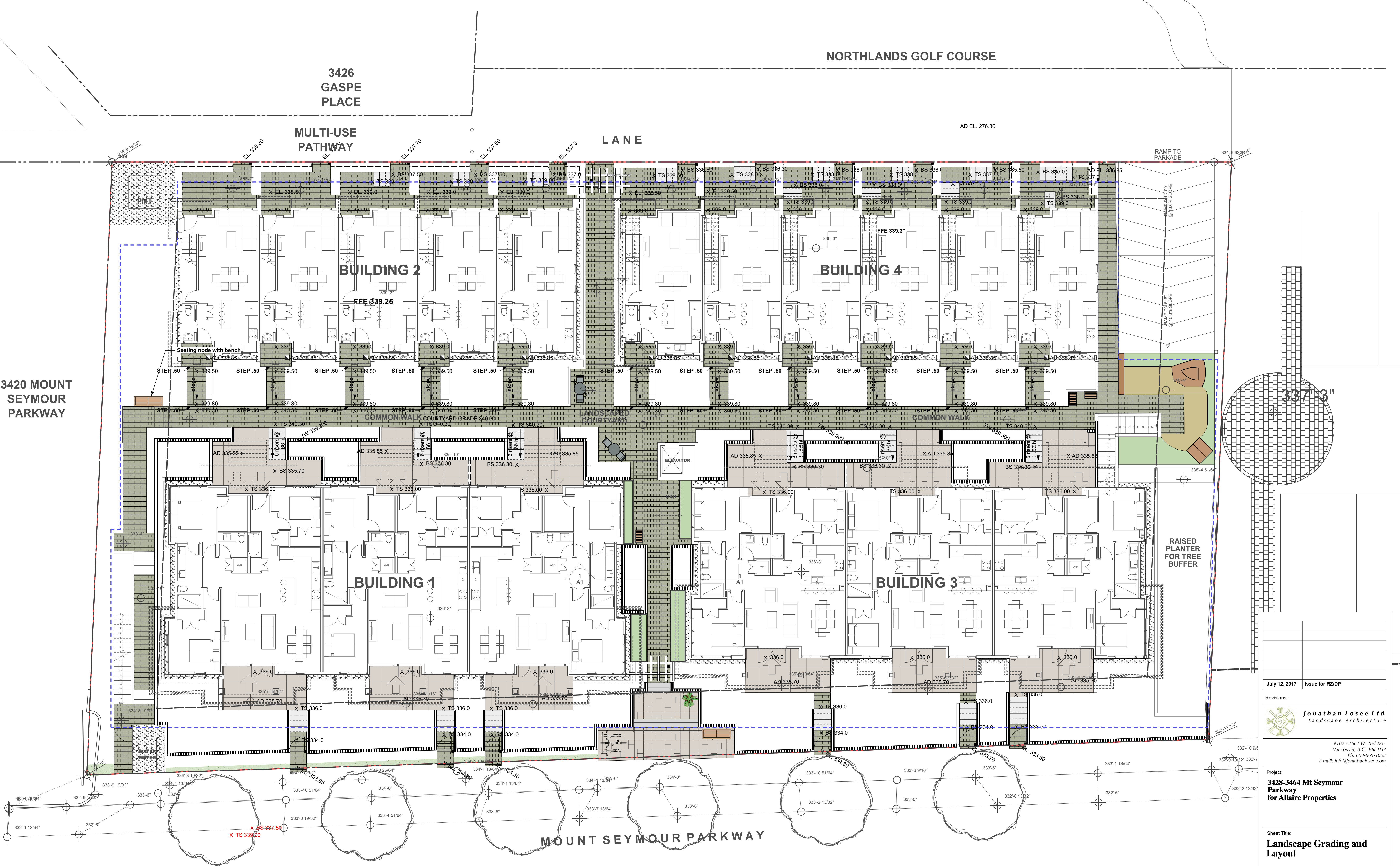
Date:

Project No.

2017-25

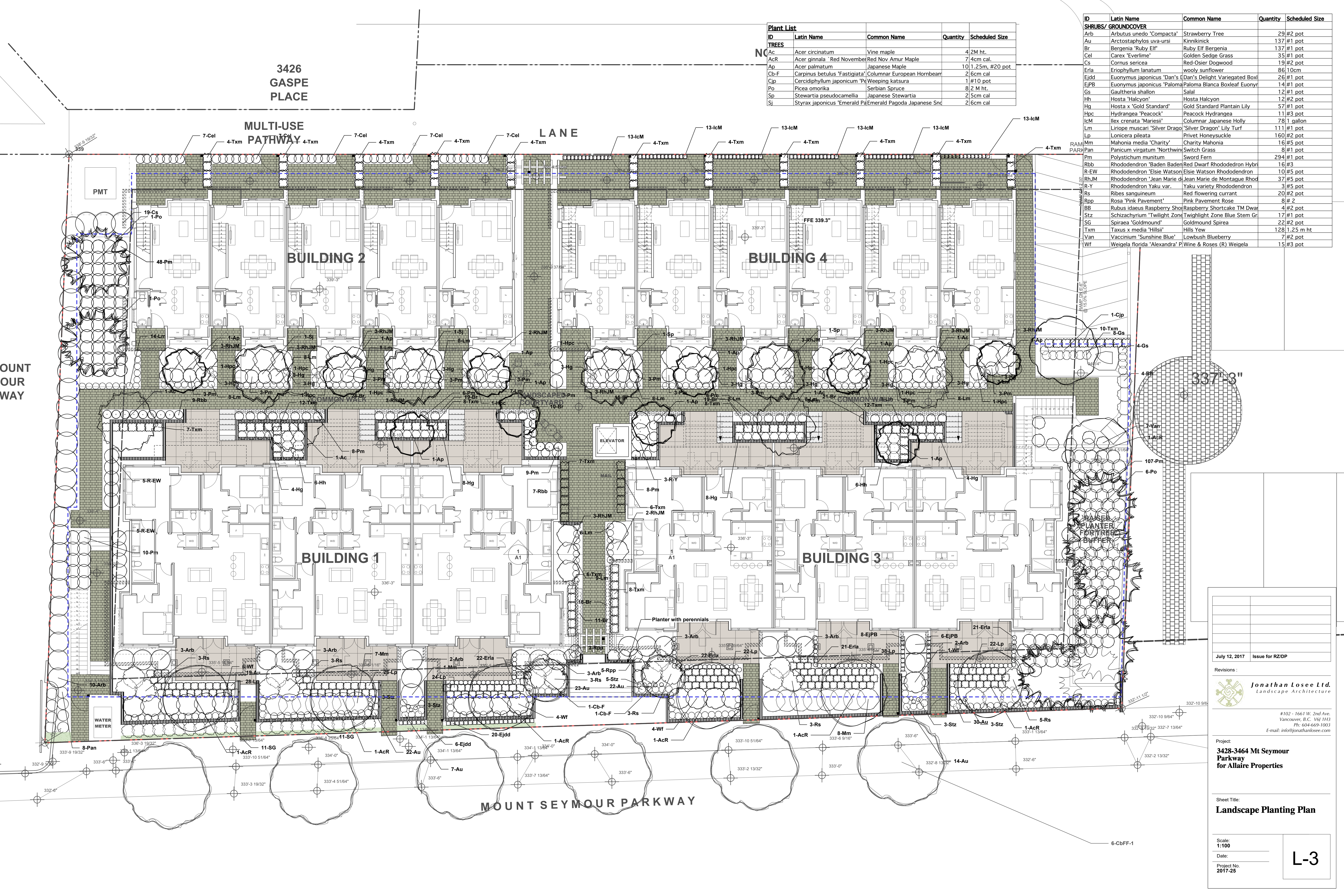
L-1





July 12, 2017		Issue for RZ/DP	
Revisions:			
 <b>Jonathan Losee Ltd.</b> Landscape Architecture			
#102 - 1661 W. 2nd Ave. Vancouver, B.C. V6J 1H3 Ph: 604-669-1003 E-mail: info@jonathanlosee.com			
Project: <b>3428-3464 Mt Seymour Parkway</b> for Allaire Properties			
Sheet Title: <b>Landscape Grading and Layout</b>			
Scale: 1:100		<b>L-2</b>	
Date:			
Project No. 2017-25			





Plant List				
ID	Latin Name	Common Name	Quantity	Scheduled Size
TREES				
Ac	Acer circinatum	Vine maple	4	2M ht.
AcR	Acer ginnala 'Red November'	Red Nov Amur Maple	7	4cm cal.
Ap	Acer palmatum	Japanese Maple	10	1.25m, #20 pot
Cb-F	Carpinus betulus 'Fastigiata'	Columnar European Hornbeam	2	6cm cal
Cjp	Cercidiphyllum japonicum 'PeWeeping katsura		1	#10 pot
Po	Picea omorika	Serbian Spruce	8	2 M ht.
Sp	Stewartia pseudocamellia	Japanese Stewartia	2	5cm cal
Sj	Styrax japonicus 'Emerald Pagoda	Emerald Pagoda Japanese Snc	2	6cm cal

ID	Latin Name	Common Name	Quantity	Scheduled Size
SHRUBS/ GROUND COVER				
Arb	Arbutus unedo 'Compacta'	Strawberry Tree	29	#2 pot
Au	Arctostaphylos uva-ursi	Kinnikinnick	137	#1 pot
Br	Bergenia 'Ruby Elf'	Ruby Elf Bergenia	137	#1 pot
Cel	Carex 'Everlime'	Golden Sedge Grass	35	#1 pot
Cs	Cornus sericea	Red-Osier Dogwood	19	#2 pot
Erla	Eriophyllum lanatum	wooly sunflower	86	10cm
Ejdd	Euonymus japonicus 'Dan's Delight	Dan's Delight Variegated Box	26	#1 pot
EjPB	Euonymus japonicus 'Paloma Blanca	Paloma Blanca Boxleaf Euonym	14	#1 pot
Gs	Gaultheria shallon	Salal	12	#1 pot
Hh	Hosta 'Halcyon'	Hosta Halcyon	12	#2 pot
Hq	Hosta x 'Gold Standard'	Gold Standard Plantain Lily	57	#1 pot
Hpc	Hydrangea 'Peacock'	Peacock Hydrangea	11	#3 pot
lcm	Ilex crenata 'Mariesii'	Columnar Japanese Holly	78	1 gallon
Lm	Liriope muscari 'Silver Dragon'	'Silver Dragon' Lily Turf	111	#1 pot
Lp	Lonicera pileata	Privet Honeysuckle	160	#2 pot
Mm	Mahonia media 'Charity'	Charity Mahonia	16	#5 pot
Pan	Panicum virgatum 'Northwind'	Switch Grass	8	#1 pot
Pm	Polystichum munitum	Sword Fern	294	#1 pot
Rbb	Rhododendron 'Baden Baden'	Red Dwarf Rhododendron Hybrid	16	#3
R-EW	Rhododendron 'Elsie Watson'	Elsie Watson Rhododendron	10	#5 pot
RhJM	Rhododendron 'Jean Marie de Montague'	Jean Marie de Montague Rhod	37	#5 pot
R-Y	Rhododendron Yaku var.	Yaku variety Rhododendron	3	#5 pot
Rs	Ribes sanguineum	Red flowering currant	20	#2 pot
Rpp	Rosa 'Pink Pavement'	Pink Pavement Rose	8	#2
BB	Rubus idaeus Raspberry Short	Raspberry Shortcake TM Dwarf	4	#2 pot
Stz	Schizachyrium 'Twilight Zone'	Twilight Zone Blue Stem Gr	17	#1 pot
SG	Spiraea 'Goldmound'	Goldmound Spirea	22	#2 pot
Txm	Taxus x media 'Hillsii'	Hills Yew	128	1.25 m ht
Van	Vaccinium 'Sunshine Blue'	Lowbush Blueberry	7	#2 pot
Wf	Weigela florida 'Alexandra'	P Wine & Roses (R) Weigela	15	#3 pot

July 12, 2017 Issue for RZ/DP

Revisions :



Jonathan Losee Ltd.  
Landscape Architecture

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Ph: 604-669-1003  
E-mail: info@jonathanlosee.com

Project:  
3428-3464 Mt Seymour  
Parkway  
for Allaire Properties

Sheet Title:  
Landscape Planting Plan

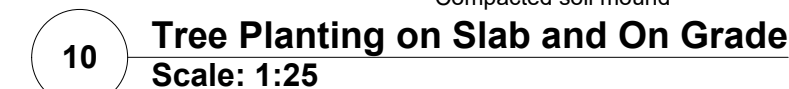
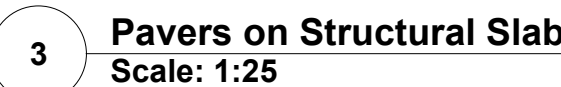
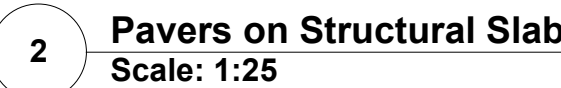
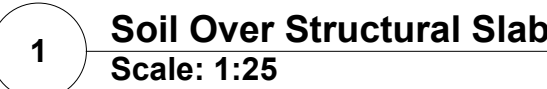
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Date:

Project No.  
2017-25

L-3





## Revisions



Project:

Sheet Title:

### Landscape Details

Scale: **As Noted**

Date: \_\_\_\_\_

Project No.  
**2017-25**

L-4





Maglin MLB1050B-27.5 Series Bench  
Graphite Powder Coat with Ipe wood



Maglin MLB870 -PC Series Bench  
Graphite Powder Coat with Ipe wood



Maglin FRT1700-BH-RD-30 Series Bar Height 30" table and chairs  
Surface mounted.

July 12, 2017	Issue for RZ/DP

Revisions :



**Jonathan Losee Ltd.**  
*Landscape Architecture*

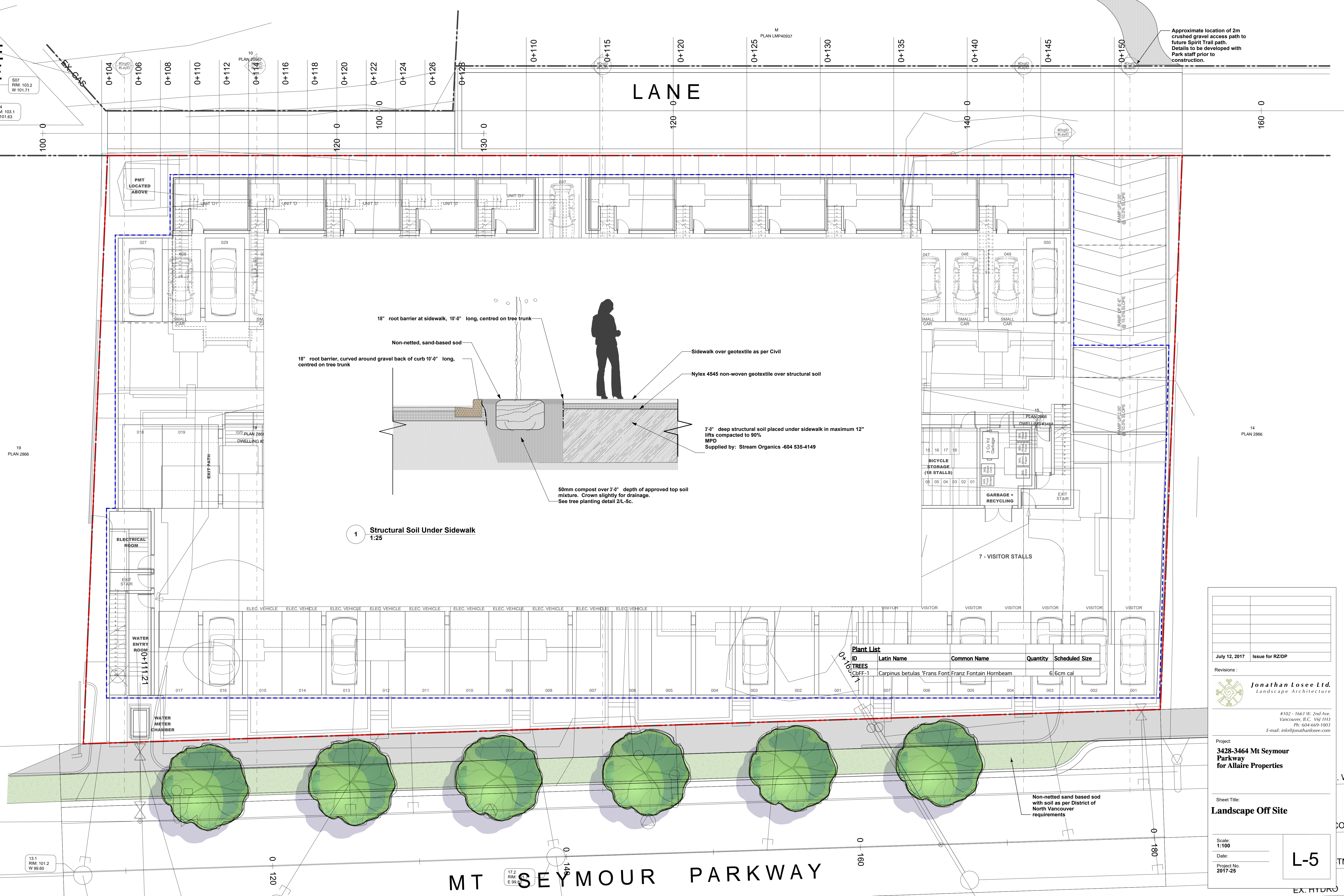
#102 - 1661 W. 2nd Ave.  
Vancouver, B.C. V6J 1H3  
Ph: 604-669-1003  
E-mail: info@jonathanlosee.com

Project:  
**3428-3464 Mt Seymour  
Parkway  
for Allaire Properties**

Sheet Title:  
**Landscape Details**

Scale: <b>As Noted</b>	<b>L-4b</b>
Date:	
Project No. <b>2017-25</b>	





July 12, 2017

Issue for RZ/DP

Revisions :

**Jonathan Losee Ltd.**  
Landscape Architecture

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E-mail: info@jonathanlosee.com

Project:  
**3428-3464 Mt Seymour  
Parkway  
for Allaire Properties**

Sheet Title:  
**Landscape Off Site**

Scale:  
1:100

Date:

Project No.  
2017-25

L-5



## The District of North Vancouver EXPLANATORY MEMO TO ADVISORY DESIGN PANEL

October 4, 2017  
File: 08.3060.20/033.17

**AUTHOR:** Erik Wilhelm  
**SUBJECT:** Detailed – Rezoning with Development Permit (29 Unit Townhouse Proposal)  
3428 -3464 Mt. Seymour Pkwy.

### PROJECT INFORMATION

<b>Application Type</b>	Detailed
<b>Applicant</b>	Marc Allaire
<b>Architecture Firm</b>	Integra Architecture
<b>Landscape Architecture Firm</b>	Jonathan Losee Ltd.
<b>Official Community Plan Designation</b>	RES Level 2: Detached Residential (0.55 FSR)
<b>Proposed OCP Designation</b>	RES Level 4: Transition Multifamily (1.2 FSR)
<b>Existing Zoning</b>	Single-Family Residential One Acre Zone
<b>Proposed Zoning</b>	Comprehensive Development
<b>Development Permit Areas</b>	Form and Character, and Energy and Water Conservation and GHG Emission Reduction
<b>Green Building</b>	Gold Standard Required
<b>Public Art</b>	Not required onsite





**Context** The development site is located mid-block on the north side of Mount Seymour Parkway and includes four single-family lots. There are two single family lots to the west that are slated to remain for the foreseeable future. There is also a proposed 27 unit townhouse development east of the site (which also assumes 4 single-family lots). There is an unconstructed laneway north of the development site and Northwoods golf course north of the laneway.

**Design Guidelines** Guidelines for Ground-Oriented Housing

## **THE PROPOSAL:**

The application proposes a rezoning of the site to accommodate a 29 unit townhouse with underground parking accessed on the eastern edge of the site from the yet to be constructed lane north of the site. There are 4 buildings (2 fronting Mt. Seymour Parkway and two at the rear) separated by a linear courtyard. All buildings include rooftop decks and a west coast modern architectural expression.

The proposal includes 59 parking spaces (which includes 7 stalls allotted for parking); the parkade includes 11 individual underground storage areas and a shared bike room for 18 bikes. There one elevator centrally located within the parkade.

The proposed density, at 1.2 FSR, is compliant with the Official Community Plan.

### **Urban Design Comments:**

The Urban Design Planner's comments will be provided at the ADP meeting.

### **Conclusion:**

Staff look forward to the Panel's comments and direction on this Detailed Application.

Erik Wilhelm  
Development Planner

### **Attachments:**

Attachment 1 – Architectural Drawings (Integra Architecture)



**b.) 3428 – 3464 Mt Seymour Pkwy (Allaire): Detailed Planning Application – Rezoning and Development Permit for a 29 unit townhouse development**

Mr. Erik Wilhelm, Development Planner, introduced the project and explained the context.

The Chair welcomed the applicant team and Mr. Duane Siegrist of Integra Architecture introduced the project.

The Chair thanked the applicant team for their presentation and asked if there were any questions of clarification from the Panel:

Questions were asked and answered on the following topics:

- What is the history behind the implementation of the exterior stairs of how they are built? The client is the builder and wanted to use steel and concrete in the project so certain standards were met to ensure longevity of the materials.
- Why are the featured staircases on some units and not on the others? The staircases are only on the interior side of the courtyard to give life to it and provide easy access across the unit and the site.
- Are the stairs and decks to buildings 1 and 3 code compliant? Yes, the stairs and decks are made out of wood and steel in the interior courtyard with closed risers.

Mr. Alfonso Tejada, District Urban Design Planner, provided the following comments and questions for consideration:

- The organization of space within the central courtyard needs to be improved and enlarged so that efficient and fully functioning open space can be maintained.
- The eastern child's play area is of limited size and lacks functionality (and should be revisited by the design team).
- The size of overhangs don't seem to fit the massing and character of the building.
- The north elevation of the development should be reviewed in order to improve the overall character of the north facing façade and the functionality of the backyard spaces should be reviewed to optimize the north facing amenity areas.

The Chair invited comments from the Panel members, and the following comments and items for consideration were provided:

- Great site plan and layout of units with easy access to the parkade.
- With a main entrance off the north side for the north facing units it would be ideal for empty nesters to be located there for aging in place.
- Appreciate a simple lobby area and landscape looks great in and around the site.
- Rooftop decks look great on the south side but the outside stair on the north side could be worked on especially near the neighbouring unit where it might overhang.
- Thank you for an excellent presentation and well thought out reasoning behind the layout and people oriented development.



- Look at refining the courtyard space to alleviate any pinched areas.
- Consider changing the positioning of outdoor furniture 90 degrees to help create a focal point for socializing in the middle of the courtyard.
- The children's play area works well but the only conflict is the stair to the side could be worked on to improve safety.
- Appreciate the design principles behind the buildings and the smaller scale setting.
- Analyze the site using a site section and try to get as much improve the relation and interaction between people and space.
- Engaging the back lane as a part of the development is well done.
- The stairs are a very creative idea and could use a step guard rail perhaps made out of glass.
- Showing panel joints and sketching really is commendable because it adds to the detail.
- Communal entrance space could be worth looking into to simplify material palette.
- The outside stairs could work as the axis helps with views and practicality. Consider slip prevention techniques to minimize danger which should enhance the usability of the courtyard deck.
- The evolution of row homes on the north side is quite special and the roof decks add to the success of the façade.
- Encourage more ground oriented units for accessibility and livability.

The Chair invited the project team to respond. Mr. Siegrist, project architects, acknowledged the Panel's suggestions, appreciated the comments and was happy to take them into account in the Design development.

The Chair invited the Panel to compose a motion:

**MOVED** by Jordan Levine and **SECONDED** by Steve Wong:

THAT the ADP has reviewed the proposal and recommends APPROVAL of the project SUBJECT to addressing to the satisfaction of staff the items noted by the Panel in its review of the project.

**CARRIED**



**4. OTHER BUSINESS**

None.

**5. ADJOURNMENT**

The meeting was adjourned at 8:21 p.m.

**6. NEXT MEETING**

October 12, 2017

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Chair

---

Date



MOUNTAIN MAPLE GARDEN & TREE SERVICE LTD.  
#200, 8208 SWENSON WAY  
DELTA, BC V4G 1J6  
PHONE: 604 - 488 - 4455  
MOUNTAIN.MAPLE@HOTMAIL.COM

June 29, 2017

**RE: Arborist Report for Jonathan Losee Ltd Landscape Architecture - For property located at 3428 - 3464 Mt. Sevmour Pkwy., North Vancouver**

**Applicant:** Jonathan Losee Ltd. Landscape Architecture  
**C/o:** Alyssa Semczyszyn  
**Phone:** 604 - 669 - 1003  
**Email:** alyssa@jonathanlosee.com

Due to a townhouse development proposal, it was requested that a report be compiled discussing trees located at and adjacent to the address named above. This site was inspected on June 22, 2017. Forty-one trees and five hedges were assessed; they have been numbered 1 - 46 for the purpose of this report, and consist of Trees and hedges on Municipal and neighbouring properties, and trees on private property that are of a size that requires them to be protected by the Tree-by-law. Seventeen photographs and a copy of the site survey, and a copy of the site plan have been included as part of this report. A Tree Replacement Plan has not been included as part of this report; this will be included as part of the landscape plan. Servicing details have not been provided yet, and will need to be considered for impact to any of the retained trees.

This land on these lots is generally flat, but slopes slightly downward to the south. The District of North Vancouver owns land in between and to the north of these lots.


An original large scaled copy of the site plan indicating trees marked for removal, and the locations of Tree Protection Zone fencing has *not* been included with this report; this will be provided by the applicant if required.

**GENERAL CONSTRUCTION NOTES:**

- Any permitted tree removal(s) must be performed by qualified professional to avoid contravention of Work Safe BC regulations.
- No grade changes are to occur within the Critical Root Zones of any retained trees. Any roots exposed from lines of excavation must be covered with burlap and poly plastic, in an effort to prevent desiccation of exposed roots. Trees must be adequately watered during the entire construction process.
- Construction materials are not to be stored within the Tree Protection Zones (TPZ) of trees to be retained, and TPZ fencing is to remain intact for the entire construction process. Signage must be clearly posted on each side of the barrier indicating 'TREE PROTECTION ZONE - DO NOT ENTER OR MODIFY'. Contact your project Arborist for any TPZ barrier issues.

Limitations: This report is based on a visual assessment, from the ground only. No core or tissue samples were taken; no root crown excavations were performed. This report provides no undertakings regarding the future condition or behaviour of the trees reviewed in it. Tree hazards and conditions do change over time, and the evaluation period for this report is valid for the day on which it was performed only. Recommendations are to serve only as a guideline for the care, retention and protection of the tree(s), and are made according to commonly accepted arboricultural practises, and do not guarantee the survival and/or safety of the specimen(s). No responsibility is assumed for any legal matters as a result of this report. The consultant shall not be required to give testimony or attend court by any reason of this report unless subsequent contractual arrangements are made, including payment of additional fees for such services. Loss or alteration of any part of this report invalidates the entire report. Possession of this report or a copy thereof does not imply right of publication or use for any purpose by any other than the person to whom it is addressed, without verbal or written consent of the consultant. No part of this report shall be conveyed by anyone to the public by any means without prior written consent of the consultant.

Yours truly,

  
Kerin Matthews - sent electronically  
ISA Certified Arborist #PN-5648  
ISA Certified Tree Risk Assessor #0123

SCANNED  
Doc # 3274128.



<u>TREE</u>	<u>SPECIES</u>	<u>DBH</u>	<u>HEIGHT</u>	<u>CRZ</u>	<u>Condition</u>	<u>OBSERVATIONS &amp; RECOMMENDATIONS</u>
#		(cm)	(m) est.	radius (ft.)		
1	Cedar ( <i>Thuja occidentalis</i> )	34	2	3'	Fair	<p>This hedge belongs to the neighbouring property. It has sustained snow damage. The existing on-site driveway is immediately adjacent to the west side. The proposed underground parkade is immediately adjacent to the west side of the stems; not including the excavation required.</p> <p><b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain written consent from the neighbouring owner and remove hedge to enable the proposed plan.</p>
2	Maple ( <i>Acer sp.</i> )	34	8	7' (2.1m)	Good	<p>This tree belongs to the neighbouring property. It appears typical. The Critical Root Zone radius is 2.1m. The proposed underground parkade is 1.8m from the west side of the stem; not including the line of excavation required.</p> <p><b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain written consent from the neighbouring owner and remove tree to enable the proposed plan; the excavation required for the underground parkade will pose too much of an encroachment into the Critical Root Zone.</p>
3	Dwarf Alberta Spruce ( <i>Picea sp.</i> )	10	3	2' (0.7m)	Fair	<p>This tree belongs to the neighbouring property; it was viewed from the subject site only. The Critical Root Zone radius is 0.7m. The Proposed underground parkade is adjacent to the property line.</p> <p><b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain written consent from the neighbouring owner, and remove tree to enable the proposed plan; the excavation required for the underground parkade will pose too much of an encroachment into the Critical Root Zone.</p>
4	Spruce ( <i>Picea sp.</i> )	20 per survey	6	4.1' (1.3m)	Fair	<p>This tree belongs to the neighbouring property. It was viewed from the subject site only. The Critical Root Zone radius is 1.3m. The proposed underground parking is 1m from the west side of the stem; not including the line of excavation required.</p> <p><b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain written consent from the neighbouring owner, and remove tree to enable the proposed plan; the excavation required for the underground parkade will pose too much of an encroachment into the Critical Root Zone.</p>
5	Spruce ( <i>Picea sp.</i> )	25 per survey	5	5' (1.6m)	Fair	<p>This tree belongs to the neighbouring property. It was viewed from the subject site only. The Critical Root Zone radius is 1.6m. The proposed underground parking is 1.2m from the west side of the stem; not including the line of excavation required.</p> <p><b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain written consent from the neighbouring owner, and remove tree to enable the proposed plan; the excavation required for the underground parkade will pose too much of an encroachment into the Critical Root Zone.</p>



<u>TREE</u> #	<u>SPECIES</u>	<u>DBH</u> (cm)	<u>HEIGHT</u> (m) est.	<u>CRZ</u> radius (ft.)	<u>Condition</u>	<u>OBSERVATIONS &amp; RECOMMENDATIONS</u>
6	Horse Chestnut ( <i>Aesculus hippocastanum</i> )	50 per survey	13	10.3' (3.1m)	Good	<p>This tree belongs to the neighbouring property; it was viewed from the subject site only. The Critical Root Zone radius is 3.1m. The proposed parkade access ramp is 3.2m from the west side of the stem.</p> <p><b>RECOMMENDATIONS:</b>  <b>RETAIN.</b> Install Tree Protection Zone fencing 3.4m from the base of the stem on the north, west and south sides. Fencing is to be installed on the subject site only. Any excavation into this Zone must be supervised by a Certified Arborist; the exposed roots will be properly pruned, then they must be covered with burlap and poly plastic in an effort to prevent desiccation.</p>
7	Cedar ( <i>Thuja occidentalis</i> )		4	3'	Good/fair	<p>This hedge belongs to the Municipality. neighbouring property. There is an existing retaining wall adjacent to the south side of the stems. The proposed underground parking is immediately adjacent to the north side of the hedge</p> <p><b>RECOMMENDATIONS:</b>  <b>REMOVE.</b> Obtain consent from the Municipality, and remove hedge to enable the proposed plan.</p>
8	Dead	30 per survey	8		Dead	<p>This tree belongs to the Municipality. It is completely dead, but covered with ivy. It is within the proposed building envelope.</p> <p><b>RECOMMENDATIONS:</b>  <b>REMOVE.</b> Obtain consent from the Municipality and remove tree to enable the proposed plan.</p>
9	Dead	25 per survey	10		Dead	<p>This tree belongs to the Municipality. It is completely dead. It is within the proposed building envelope.</p> <p><b>RECOMMENDATIONS:</b>  <b>REMOVE.</b> Obtain consent from the Municipality and remove tree to enable the proposed plan.</p>
10	Dead	25 per survey	10		Dead	<p>This tree belongs to the Municipality. It is completely dead. It is within the proposed building envelope.</p> <p><b>RECOMMENDATIONS:</b>  <b>REMOVE.</b> Obtain consent from the Municipality and remove tree to enable the proposed plan.</p>
11	Dead	15 per survey	9		Dead	<p>This tree belongs to the Municipality. It is completely dead, but covered with ivy. It is within the proposed building envelope.</p> <p><b>RECOMMENDATIONS:</b>  <b>REMOVE.</b> Obtain consent from the Municipality and remove tree to enable the proposed plan.</p>
12	Birch ( <i>Betula sp.</i> )	32 per survey	12	6.6' (2m)	Fair	<p>This tree belongs to the Municipality. It has a corrected lean to the south. The stem is covered</p>



<u>TREE</u> #	<u>SPECIES</u>	<u>DBH</u> cm	<u>HEIGHT</u> (m) est.	<u>CRZ</u> radius (ft.)	<u>Condition</u>	<u>OBSERVATIONS &amp; RECOMMENDATIONS</u>
						with ivy, which has impeded a thorough inspection. It is within the proposed building envelope. <b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain consent from the Municipality and remove tree to enable the proposed plan.
13	Big Leaf Maple ( <i>Acer macrophyllum</i> )	152 comb. per survey (92+60)	21	31.3' (9.5m)	Fair	This tree belongs to the Municipality. It has multiple stems commencing near the base. There appears to be evidence of fungal activity in a small cavity on the northwest side of the stem. This tree is within the proposed building envelope. <b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain consent from the Municipality and remove tree to enable the proposed plan.
14	Western Red Cedar ( <i>Thuja plicata</i> )	112 per survey	24	23.1' (7m)	Good	This tree belongs to the Municipality (CDNV tag #00859). It appears typical. This tree is within the footprint of the underground parkade. <b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain consent from the Municipality and remove tree to enable the proposed plan.
15	Western Red Cedar ( <i>Thuja plicata</i> )	45 per survey	17	9.2' (2.8m)	Good	This tree belongs to the Municipality (CDNV tag #01206). It appears typical. This tree is within the footprint of the underground parkade. <b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain consent from the Municipality and remove tree to enable the proposed plan.
16	Big Leaf Maple ( <i>Acer macrophyllum</i> )	100 est.	18	20.6' (6.3m)	Good	This tree belongs to the Municipality. It has six stems commencing at 1ft. This tree is within the proposed building envelope. <b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain consent from the Municipality and remove tree to enable the proposed plan.
17	Western Red Cedar ( <i>Thuja plicata</i> )	25 per survey	9	5' (1.6m)	Good	This tree belongs to the Municipality. It has a corrected lean to the north. This tree is within the proposed building envelope. <b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain consent from the Municipality and remove tree to enable the proposed plan.
18	Western Red Cedar ( <i>Thuja plicata</i> )	40 per survey	12	8.2' (2.5m)	Good	This tree belongs to the Municipality (CDNV tag #01204). It appears typical. This tree is within the proposed building envelope. <b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain consent from the Municipality and remove tree to enable the proposed plan.



<u>TREE</u> #	<u>SPECIES</u>	<u>DBH</u> (cm)	<u>HEIGHT</u> (m) est.	<u>CRZ</u> radius (ft.)	<u>Condition</u>	<u>OBSERVATIONS &amp; RECOMMENDATIONS</u>
19	Western Red Cedar ( <i>Thuja plicata</i> )	80 per survey	18	16.5' (5m)	Good	This tree belongs to the Municipality (CDNV tag #01210). It appears typical. This tree is within the proposed building envelope. <b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain consent from the Municipality and remove tree to enable the proposed plan.
20	Walnut ( <i>Juglans sp.</i> )	50 per survey	9	10.3' (3.1m)	Good	This tree belongs to the District of North Vancouver. It appears fairly typical. The Critical Root Zone radius is 10.3ft. It is within the footprint of the proposed building envelope. <b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain permission from the Municipality and remove tree; its removal is necessary to enable the proposed plan.
21	Cypress ( <i>Chamaecyparis sp.</i> )	40 per survey	12	8.2' (2.5m)	Dead	This tree belongs to the Municipality (CDNV tag #00691). It is completely dead. It is within the proposed building envelope. <b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain permission from the Municipality and remove tree; its removal is necessary to enable the proposed plan.
22	Big Leaf Maple ( <i>Acer macrophyllum</i> )	60 comb. per survey	10	12.4' (3.8m)	Fair	This tree belongs to the Municipality. It has a corrected lean to the south. There is canker on the stem. It is within the proposed laneway. <b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain permission from the Municipality and remove tree; its removal is necessary to enable the proposed plan.
23	Western Red Cedar ( <i>Thuja plicata</i> )	30 per survey	10	6.2' (1.9m)	Good	This tree belongs to the Municipality. It appears typical. It is within the proposed laneway. <b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain permission from the Municipality and remove tree; its removal is necessary to enable the proposed plan.
24	Western Red Cedar ( <i>Thuja plicata</i> )	30 per survey	10	6.2' (1.9m)	Good	This tree belongs to the Municipality. It appears typical. It is within the proposed laneway. <b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain permission from the Municipality and remove tree; its removal is necessary to enable the proposed plan.
25	Big Leaf Maple ( <i>Acer macrophyllum</i> )	70 per survey	16	14.4' (4.4m)	Fair	This tree belongs to the Municipality. It has two stems commencing at 2ft; included bark was observed. There is decay in the union. It is within the proposed laneway. <b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain permission from the Municipality and remove tree; its removal is necessary to enable the proposed plan.
26	Heritage stump					This stump belongs to the Municipality. It is in a degrading condition. The springboard notches do not appear visible. The centre of the stump has been burned, and there is garbage in it.



<u>TREE</u> #	<u>SPECIES</u>	<u>DBH</u> (cm)	<u>HEIGHT</u> (m) est.	<u>CRZ</u> radius (ft.)	<u>Condition</u>	<u>OBSERVATIONS &amp; RECOMMENDATIONS</u>
						<p><b>RECOMMENDATIONS:</b>  <b>REMOVE.</b> Obtain permission from the Municipality and remove stump; its removal is necessary to enable the proposed plan.</p>
27	Western Red Cedar <i>(Thuja plicata)</i>	80 per survey	21	16.5' (5m)	Fair	<p>This tree belongs to the Municipality (CDNV tag #00822). The canopy is slightly sparse. It is within the proposed laneway.</p> <p><b>RECOMMENDATIONS:</b>  <b>REMOVE.</b> Obtain permission from the Municipality and remove tree; its removal is necessary to enable the proposed plan.</p>
28	Alder <i>(Alnus rubra)</i>	40 per survey	9	8.2' (2.5m)	Fair	<p>This tree belongs to the Municipality. It appears to have failed at some point, but continued to grow. It has canker along the stem, and has a corrected lean to the south. It is within the proposed laneway.</p> <p><b>RECOMMENDATIONS:</b>  <b>REMOVE.</b> Obtain permission from the Municipality and remove tree; its removal is necessary to enable the proposed plan.</p>
29	Hemlock <i>(Tsuga heterophylla)</i>	25 per survey	9	5' (1.6m)	Poor	<p>This tree belongs to the Municipality. It is growing on a nurse stump and has stilted roots as a result. The canopy is very sparse and mostly dead. It is within the proposed laneway.</p> <p><b>RECOMMENDATIONS:</b>  <b>REMOVE.</b> Obtain permission from the Municipality and remove tree; its removal is necessary to enable the proposed plan.</p>
30	Hemlock <i>(Tsuga heterophylla)</i>	15 per survey	7	3' (.9m)	Poor	<p>This tree belongs to the Municipality. It is growing on a nurse stump and has stilted roots as a result. The canopy is very sparse and mostly dead. It is within the proposed laneway.</p> <p><b>RECOMMENDATIONS:</b>  <b>REMOVE.</b> Obtain permission from the Municipality and remove tree; its removal is necessary to enable the proposed plan.</p>
31	Western Red Cedar <i>(Thuja plicata)</i>	10 per survey	2	2' (.7m)	Fair	<p>This tree belongs to the Municipality. It has been suppressed by the adjacent trees. It is within the proposed laneway.</p> <p><b>RECOMMENDATIONS:</b>  <b>REMOVE.</b> Obtain permission from the Municipality and remove tree; its removal is necessary to enable the proposed plan.</p>
32	Douglas Fir <i>(Pseudotsuga menziesii)</i>	20 per survey	7	4.1' (1.3m)	Fair	<p>This tree belongs to the Municipality. It has been suppressed by the adjacent trees. It is within the proposed laneway.</p> <p><b>RECOMMENDATIONS:</b>  <b>REMOVE.</b> Obtain permission from the Municipality and remove tree; its removal is necessary to enable the proposed plan.</p>
33	Alder	20 per survey	8	4.1' (1.3m)	Fair	<p>This tree belongs to the Municipality. It is tall and spindly. It is within the proposed laneway.</p>



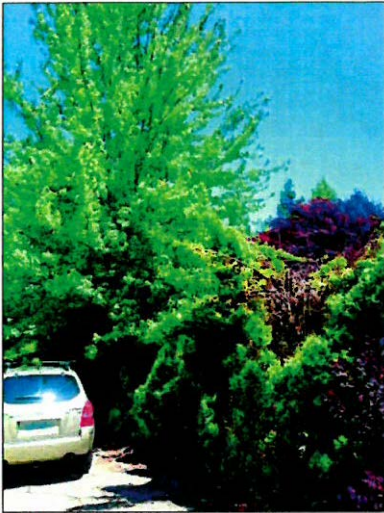
<u>TREE</u>	<u>SPECIES</u>	<u>DBH</u>	<u>HEIGHT</u>	<u>CRZ</u>	<u>Condition</u>	<u>OBSERVATIONS &amp; RECOMMENDATIONS</u>
#		(cm)	(m) est.	radius (ft.)		
	<i>(Alnus rubra)</i>					<b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain permission from the Municipality and remove tree; its removal is necessary to enable the proposed plan.
34	Alder <i>(Alnus rubra)</i>	20 per survey	8	4.1' (1.3m)	Fair	This tree belongs to the Municipality. It is tall and spindly. It is immediately adjacent to the proposed multi-use pathway. <b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain permission from the Municipality and remove tree; its removal is necessary to enable the proposed plan.
35	Hemlock <i>(Tsuga heterophylla)</i>	10 per survey	2	2' (.7m)	Fair	This tree belongs to the Municipality. It has been suppressed by the adjacent trees. It is within the proposed multi-use pathway. <b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain permission from the Municipality and remove tree; its removal is necessary to enable the proposed plan.
36	Alder <i>(Alnus rubra)</i>	25 per survey	8	5' (1.6m)	Fair	This tree belongs to the Municipality. It has a slight phototropic lean to the south. It is within the proposed multi-use pathway. <b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain permission from the Municipality and remove tree; its removal is necessary to enable the proposed plan.
37	Alder <i>(Alnus rubra)</i>	15 per survey	6	3' (.9m)	Fair	This tree belongs to the Municipality. It has a slight phototropic lean to the south. It is within the proposed multi-use pathway. <b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain permission from the Municipality and remove tree; its removal is necessary to enable the proposed plan.
38	Cedar <i>(Thuja occidentalis)</i>			3' (1m)	Good	This hedge belongs to the Municipality. It is within the proposed multi-use pathway. <b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain permission from the Municipality and remove tree; its removal is necessary to enable the proposed plan.
39	Douglas Fir <i>(Pseudotsuga menziesii)</i>	127 per survey	30	26.1' (7.9m)	Good/Fair	This tree has been previously topped at about 50ft, and has two leaders commencing at this point. It is on top of a berm, that slopes sharply downward to the south. The Critical Root Zone radius is 7.9m. It is within the proposed building envelope. <b>RECOMMENDATIONS:</b> <b>REMOVE.</b> The removal of this tree is necessary to enable the proposed building.
40	Western Red Cedar	15 per survey	2	3' (.9m)	Good	The tree belongs to the neighbouring property. It was viewed from the subject site only. There is an existing retaining wall immediately adjacent to the east side of the stem, and another retaining wall about 1.5ft from the east side of the stem. The Critical Root Zone radius is 1m. The proposed



<u>TREE</u>	<u>SPECIES</u>	<u>DBH</u>	<u>HEIGHT</u>	<u>CRZ</u>	<u>Condition</u>	<u>OBSERVATIONS &amp; RECOMMENDATIONS</u>
#		(cm)	(m) est.	radius (ft.)		
	<i>(Thuja plicata)</i>					underground parkade is 1.4m away; not including the line of excavation required. <b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain written consent from the neighbouring property owner and remove tree to enable the proposed plan.
41	Western Red Cedar <i>(Thuja plicata)</i>	77 per survey	19	15.9' (4.8m)	Good	This tree belongs to the neighbouring property. The existing concrete retaining walls are 4ft and 7ft from the east side of the stem, then the existing asphalt driveway is at lower grade just east of the retaining wall. The Critical Root Zone radius is 4.8m. The proposed underground parkade is about 3.4m from the northeast side of the stem; not including the line of excavation required. <b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain written consent from the neighbouring property owner and remove tree to enable the proposed plan.
42	Western Red Cedar <i>(Thuja plicata)</i>	40		8.2' (2.5m)	Good	This tree belongs to the Municipality. It has been previously topped to provide clearance to the Hydro lines, reduction lines, and communication cables. There is existing retaining walls 5ft from the south side, and 7ft and 9ft from the east side of the stem. The Critical Root Zone radius is 2.5m. The proposed Allan block retaining wall is 1.4m away; not including the excavation and grading required. <b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain permission from the Municipality and remove tree to enable the proposed plan.
43	Western Red Cedar <i>(Thuja plicata)</i>	10 per survey	2	2' (.7m)	Good	This tree may be shared ownership with the Municipality. It is immediately adjacent to the proposed Allan Block Retaining wall. <b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain permission from the Municipality and remove tree to enable the proposed plan.
44	Douglas Fir <i>(Pseudotsuga menziesii)</i>	15 per survey	4	3' (.9m)	Good	This tree belongs to the Municipality. It is immediately adjacent to an existing retaining wall; this is to be removed and replaced with an Allan block wall along the property line. <b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain permission from the Municipality and remove tree to enable the proposed plan.
45	Cedar <i>(Thuja occidentalis)</i>		1.5	3' (1m)	Good	This hedge belongs to the Municipality; although it has been fenced in and forms part front yard of the subject site. It is within the footprint of the proposed Allan Block retaining wall. <b>RECOMMENDATIONS:</b> <b>REMOVE.</b> Obtain permission from the Municipality and remove hedge to enable the proposed plan.



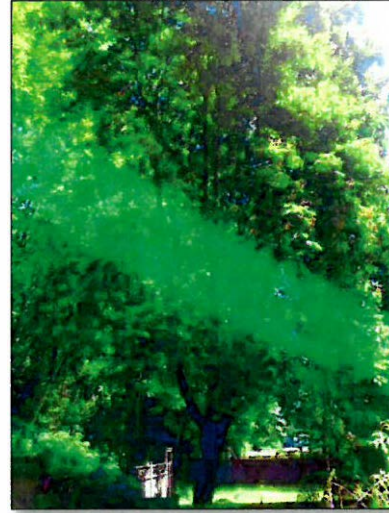
<u>TREE</u>	<u>SPECIES</u>	<u>DBH</u>	<u>HEIGHT</u>	<u>CRZ</u>	<u>Condition</u>	<u>OBSERVATIONS &amp; RECOMMENDATIONS</u>
#		(cm)	(m est.)	radius (ft.)		
46	Cedar  ( <i>Thuja occidentalis</i> )	-	1.5	3' (1m)	Good	<p>This hedge belongs to the Municipality; although it has been fenced in and forms part front yard of the subject site. It is within the footprint of the proposed Allan Block retaining wall.</p> <p><b>RECOMMENDATIONS:</b>  <b>REMOVE.</b> Obtain permission from the Municipality and remove hedge to enable the proposed plan.</p>



Tree #2 and Hedge #1



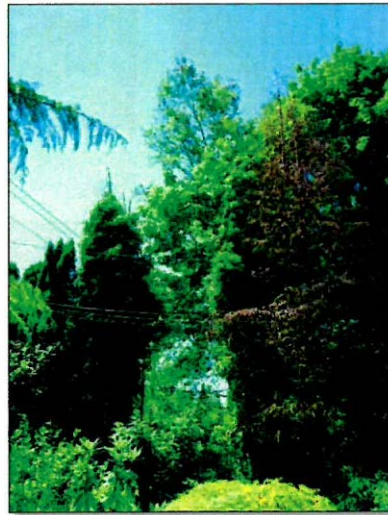
Tree # 5 - #3



Tree #6



Hedge #7



Tree#8 - #11



Tree #16

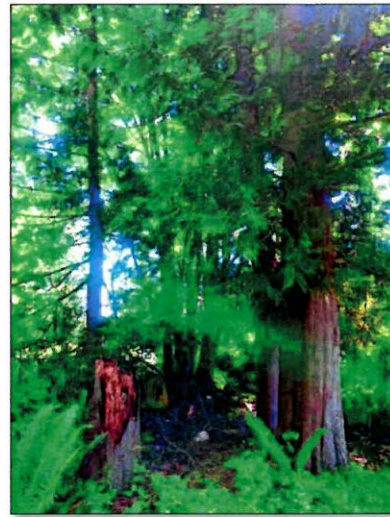




**Tree #13 and #12**



**Tree # 15 & #14**



**Tree #17, #16 and #18 & #19**



**Tree #22**



**Heritage stump – falling apart**



**Tree # 40 - #42**



**Tree #29 & #30**



**Tree#28**



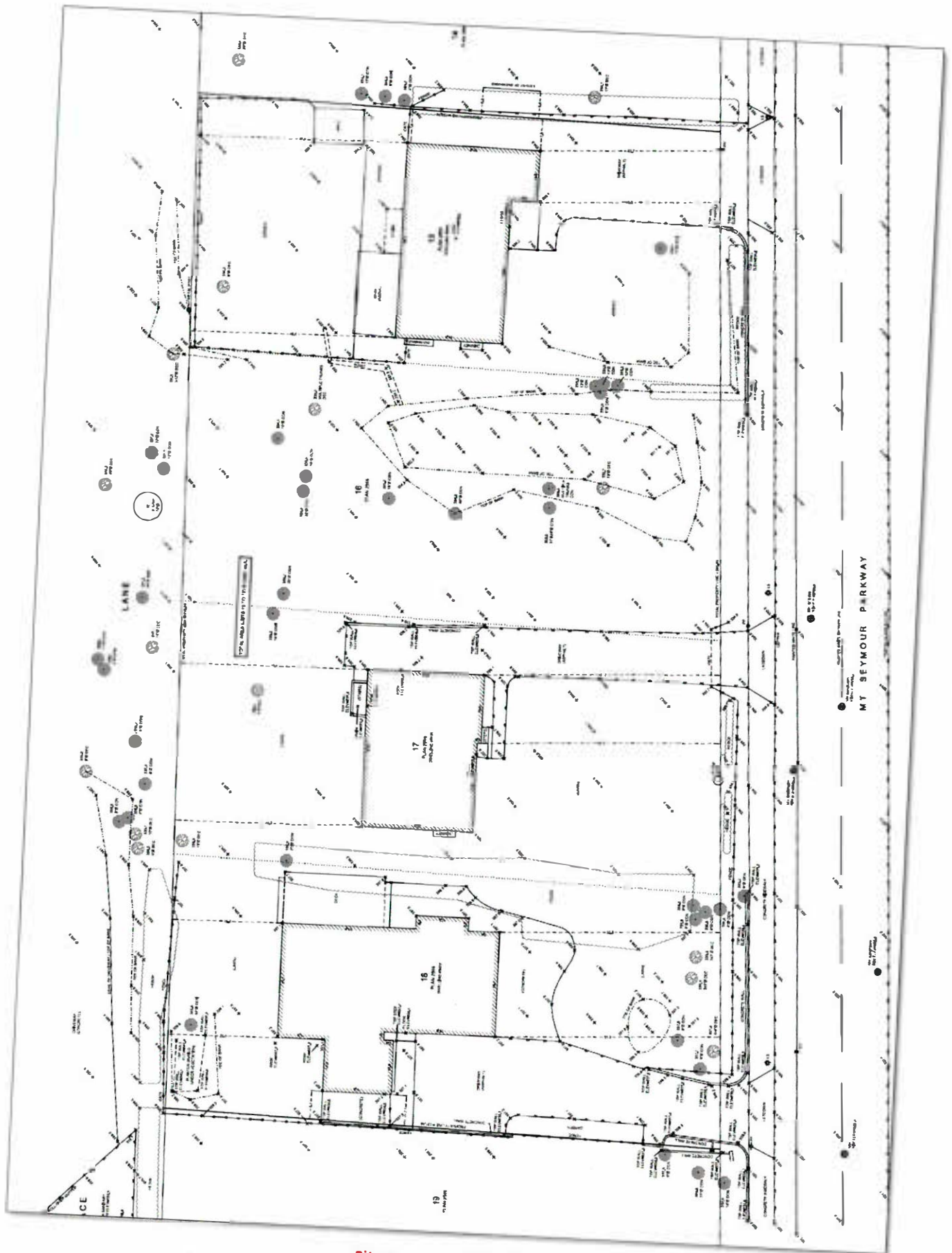


**Tree #39 and Hedge #38**



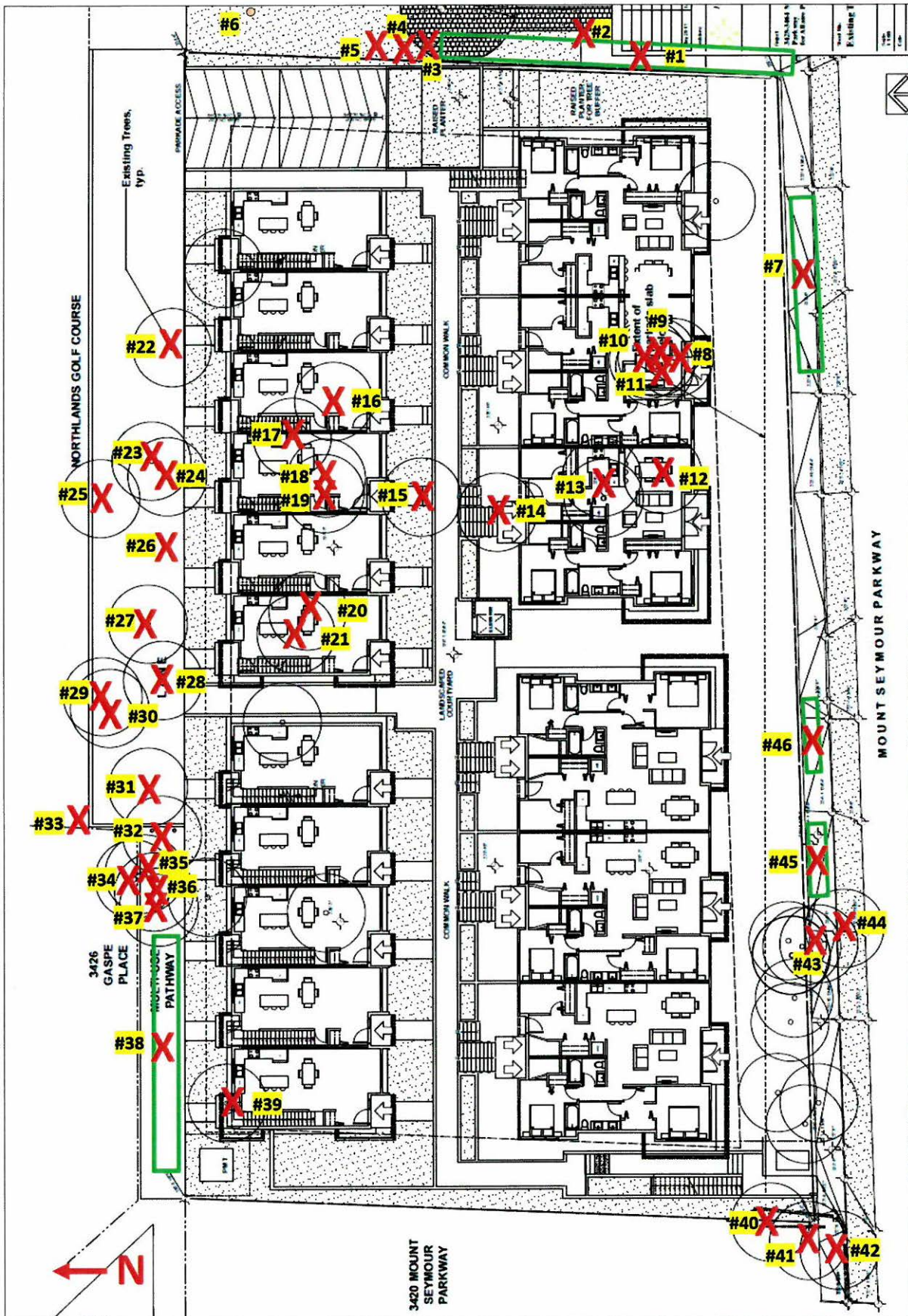
**Tree #44**





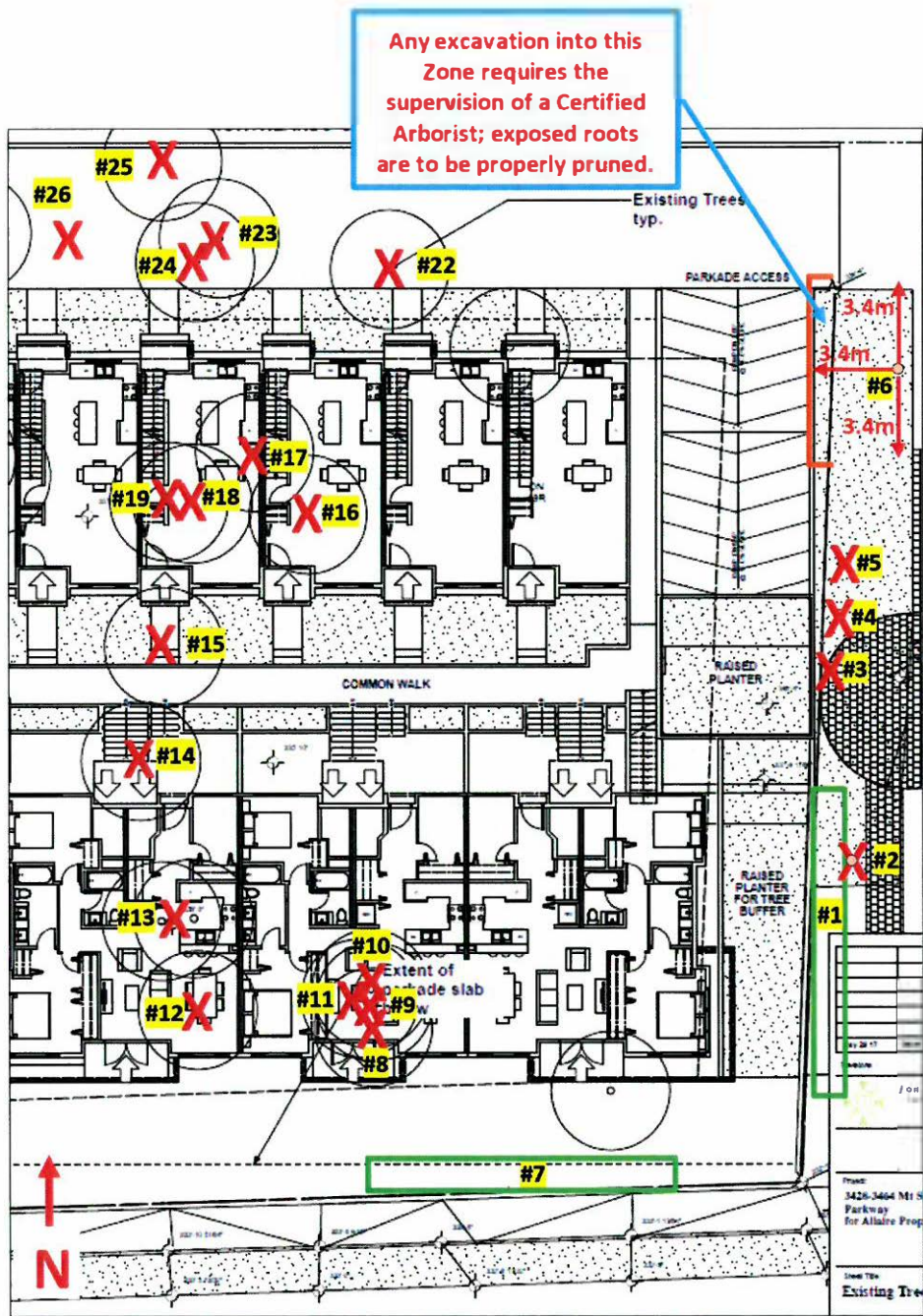
Site Survey – Not to Scale





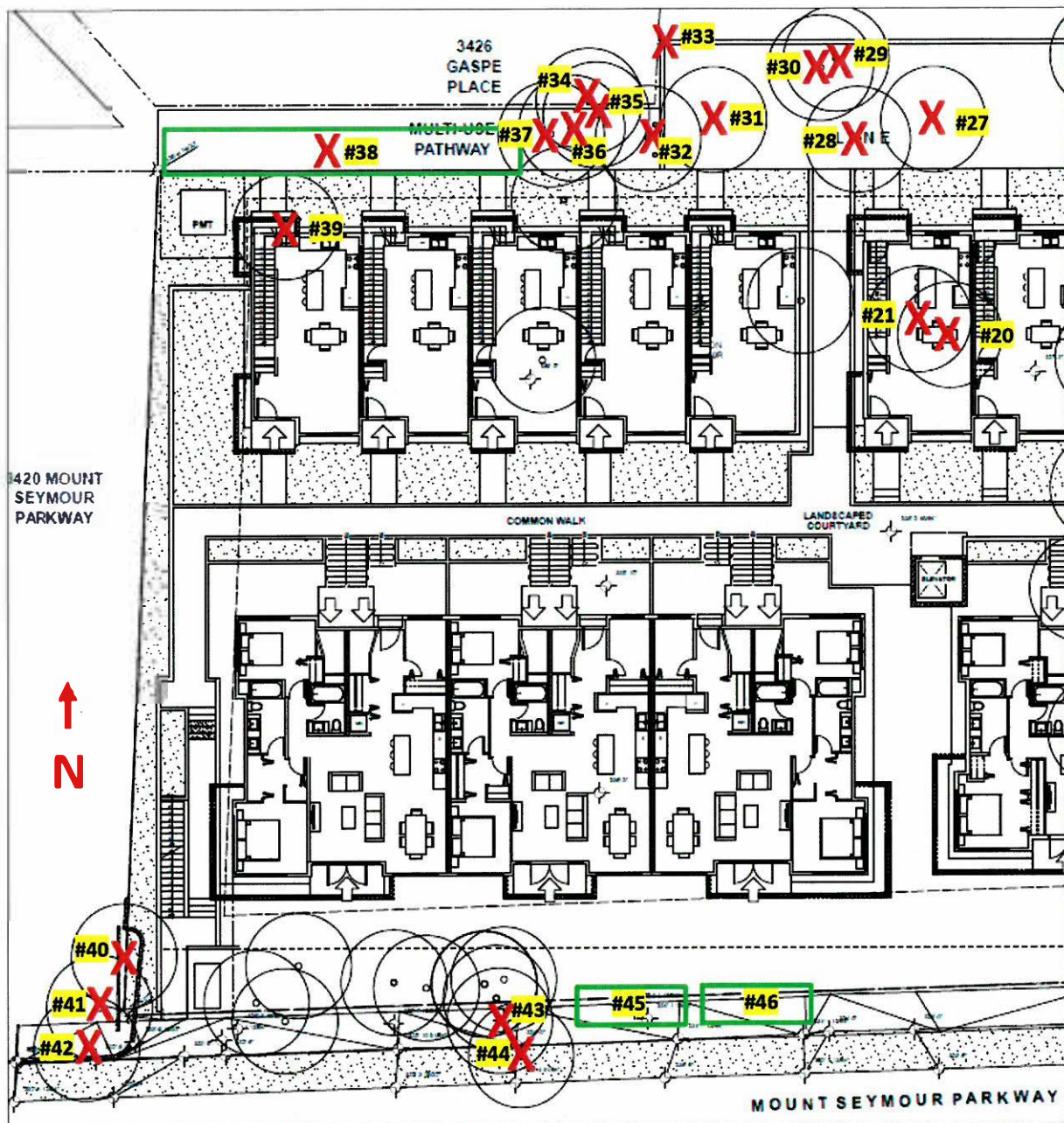
Site Plan – Not to Scale





East Property Tree Management Plan, Scale 1:300





West Property Tree Management Plan, Scale 1:300