

AGENDA

REGULAR MEETING OF COUNCIL

Monday, July 20, 2015

7:00 p.m.

**Council Chamber, Municipal Hall
355 West Queens Road,
North Vancouver, BC**

Council Members:

Mayor Richard Walton

Councillor Roger Bassam

Councillor Mathew Bond

Councillor Jim Hanson

Councillor Robin Hicks

Councillor Doug MacKay-Dunn

Councillor Lisa Muri



www.dnv.org

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REGULAR MEETING OF COUNCIL

**7:00 p.m.
Monday, July 20, 2015
Council Chamber, Municipal Hall,
355 West Queens Road, North Vancouver**

AGENDA

BROADCAST OF MEETING

- Live broadcast on Shaw channel 4
- Re-broadcast on Shaw channel 4 at 9:00 a.m. Saturday
- Online at www.dnv.org

CLOSED PUBLIC HEARING ITEMS NOT AVAILABLE FOR DISCUSSION

- Bylaw 7984 – Rezoning 3568 Mt. Seymour Parkway
- Bylaw 8101 – Rezoning 1241-1289 East 27th Street (Mountain Court)
- Bylaw 8103 – Rezoning 1203 & 1207 Harold Road
- Bylaw 8122 – Rezoning 3260 Edgemont Boulevard

1. ADOPTION OF THE AGENDA

1.1. July 20, 2015 Regular Meeting Agenda

Recommendation:

THAT the agenda for the July 20, 2015 Regular Meeting of Council for the District of North Vancouver be adopted as circulated, including the addition of any items listed in the agenda addendum.

2. PUBLIC INPUT

(limit of three minutes per speaker to a maximum of thirty minutes total)

3. PROCLAMATIONS

4. RECOGNITIONS

5. DELEGATIONS

- 5.1. Ms. Kathleen Campbell, Executive Director, and Zahir Pirani, p. 9-30**
President, Vancouver's North Shore Tourism Association
Re: VNSTA Destination Marketing Organization Presentation re: Value of Tourism

6. ADOPTION OF MINUTES

- 6.1. July 6, 2015 Regular Council Meeting p. 33-40**

Recommendation:

THAT the minutes of the July 6, 2015 Regular Council meeting be adopted.

7. RELEASE OF CLOSED MEETING DECISIONS

8. COMMITTEE OF THE WHOLE REPORT

8.1. July 13, 2015 Committee of the Whole

8.1.1. Lower Lynn Town Centre Industrial Lands Strategy

File No. 13.6480.30/002.001 .000

Recommendation:

THAT the June 7, 2015 joint report of the Section Manager – Policy Planning and Policy Planner entitled Lower Lynn Town Centre Industrial Lands Strategy be received for information;

AND THAT staff be directed to proceed with preparing, for Council's consideration, new zoning regulations for the Lower Lynn Town Centre industrial area.

8.1.2. Names for Lower Capilano Village Centre, Lower Lynn Town Centre and the New Community Recreation Facility in Delbrook

p. 43-46

File No. 01.0380.20/074.000

Recommendation:

THAT "Lions Gate" as the new name for the Lower Capilano Village Centre is endorsed;

AND THAT "Lynn Creek" as the new name for Lower Lynn Town Centre is endorsed.

9. REPORTS FROM COUNCIL OR STAFF

With the consent of Council, any member may request an item be added to the Consent Agenda to be approved without debate.

If a member of the public signs up to speak to an item, it shall be excluded from the Consent Agenda.

*Staff suggestion for consent agenda.

Recommendation:

THAT items _____ be included in the Consent Agenda and be approved without debate.

9.1. Bylaw 8124 – Turning Point Men's Support Recovery House on Burr Place

p. 49-57

File No. 08.3060.20/022.15

Recommendation:

THAT “The District of North Vancouver Rezoning Bylaw 1331 (Bylaw 8124) is given FIRST Reading and is referred to a Public Hearing.

- 9.2. Local Area Service Bylaw 8137 – Lane Paving 100/200 Block Kensington Crescent, Southwest of Kensington Crescent and Northeast of Carisbrooke Crescent between West Braemar Road and Carnarvon Avenue** **p. 59-68**
File No. 16.8310.01/000.000

Recommendation:

THAT Pursuant to Section 211 of the Community Charter, a local area service project for the paving of the existing gravel lane located in the 100/200 block Kensington Crescent, be approved;

AND THAT Bylaw 8137 is given FIRST, SECOND and THIRD Readings.

- 9.3. Development Variance Permit 53.14 – 170 East Osborne Road** **p. 69-87**
File No. 08.3060.20/053.14

Recommendation:

THAT Development Variance Permit 53.14, to enable subdivision of 170 East Osborne Road, is ISSUED.

- 9.4. Delbrook Community Recreation Centre Site Planning** **p. 89-97**
File No. 13.6680.20/005.000

Recommendation:

THAT staff be directed to proceed with a planning and public engagement process for the Delbrook site based on the scope and objectives outlined in the July 7, 2015 report of the Policy Planner entitled Delbrook Community Recreation Centre Site Planning.

- 9.5. Lions Gate Public Realm Strategy** **p. 99-197**
File No. 13.6480.30/002.004.000

Recommendation:

THAT the “Lions Gate Public Realm Strategy” attached to the June 24, 2015 report of the Section Manager, Policy Planning, Community Planner and Planning Assistant entitled Lions Gate Public Realm Strategy be approved;

AND THAT the “Lower Capilano Marine Village Centre Design Guidelines and Streetscape Details” Schedule of the Lower Capilano Marine Village Centre Implementation Plan be rescinded.

- 9.6. Affordable Housing** **p. 199-203**
File No. 13.6480.30/003.002.000

Recommendation:

THAT further to Council's June 15, 2015 direction to staff to prepare a Green Paper on affordable housing outlining regulatory and policy tools to ensure a future supply of affordable housing in the District, staff be further directed to include options such

as property acquisition, utilization of existing District properties, rental property management and partnership models, considering the District's financial and human resource capacity to support such initiatives. The options should include opportunities that may exist to provide temporary relief for those renters being displaced as early as the spring of 2016.

- * **9.7. Request for Noise Bylaw Variance – 2151 Front Street – Dollarton Road Works** **p. 205-207**
File No. 11.5460.01/000.001

Recommendation:

THAT the provision of Noise Regulation Bylaw 7188, which regulates construction noise during the night, be relaxed to complete the off-site works for the new development at 2151 Front Street.

- * **9.8. Request for Noise Bylaw Variance – Paving Mountain Highway Between Keith Road Extension and the Lane South of Fern Street, and Fern Street from Mountain Highway to Cul-de-sac East** **p. 209-211**
File No. 11.5460.01/000.001

Recommendation:

THAT the provision of Noise Regulation Bylaw 7188, which regulates construction noise during the night, be relaxed to complete the paving of Mountain Highway from the Keith Road extension to the lane south of Fern Street, and Fern Street from Mountain Highway to the cul-de-sac east.

- 9.9. 3075 Fromme Road**
File No. 08.3060.10/12.15

Materials to be circulated via agenda addendum.

10. REPORTS

10.1. Mayor

- 10.1.1. Mayor's Special One Time Contingency Fund** **p. 217-219**

10.2. Chief Administrative Officer

10.3. Councillors

10.4. Metro Vancouver Committee Appointees

11. ANY OTHER BUSINESS

12. ADJOURNMENT

Recommendation:

THAT the July 20, 2015 Regular Meeting of Council for the District of North Vancouver be adjourned.

DELEGATIONS

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Delegation to Council Request Form 5.1

District of North Vancouver

Clerk's Department

355 West Queens Rd, North Vancouver, BC V7N 4N5

Questions about this form: Phone: 604-990-2311

Form submission: Submit to address above or Fax: 604.984.9637

COMPLETION: To ensure legibility, please complete (type) online then print. Sign the printed copy and submit to the department and address indicated above.

Delegations have five minutes to make their presentation. Questions from Council may follow.

Name of group wishing to appear before Council: Vancouver's North Shore Tourism Assoc.

Title of Presentation: VNSTA Destination Marketing Organization Presentation re: Value of Tourism

Name of person(s) to make presentation: Kathleen Campbell Exec. Dir. VNSTA & Zahir Pirani, Director

Purpose of Presentation:

- ☒ Information only
- ☐ Requesting a letter of support
- ☐ Other (provide details below)

Please describe:

This year's presentation will focus on VNSTA's Value of Tourism Study which was conducted by Destination BC and VNSTA. The study was conducted to determine a credible estimate of tourism expenditures in both the City and District of North Vancouver for the year 2013. It provides Council with a financial figure to assist them in understanding the value of tourism in the combined North Vancouver's.

Contact person (if different than above): Kathleen Campbell

Daytime telephone number: 604 960 0785

Email address: kathleen@vancouvernorthshore.com

Will you be providing supporting documentation? ☒ Yes ☐ No

If yes:

☐ Handout ☐ DVD
☒ PowerPoint presentation

Note: All supporting documentation must be provided 12 days prior to your appearance date. This form and any background material provided will be published in the public agenda.

Presentation requirements:

- ☒ Laptop ☐ Tripod for posterboard
- ☐ Multimedia projector ☐ Flipchart
- ☐ Overhead projector

Arrangements can be made, upon request, for you to familiarize yourself with the Council Chamber equipment on or before your presentation date.

Delegation to Council Request Form

Rules for Delegations:

1. Delegations must submit a Delegation to Council Request Form to the Municipal Clerk. Submission of a request does not constitute approval nor guarantee a date. The request must first be reviewed by the Clerk.
2. The Clerk will review the request and, if approved, arrange a mutually agreeable date with you. You will receive a signed and approved copy of your request form as confirmation.
3. A maximum of two delegations will be permitted at any Regular Meeting of Council.
4. Delegations must represent an organized group, society, institution, corporation, etc. Individuals may not appear as delegations.
5. Delegations are scheduled on a first-come, first-served basis, subject to direction from the Mayor, Council, or Chief Administrative Officer.
6. The Mayor or Chief Administrative Officer may reject a delegation request if it regards an offensive subject, has already been substantially presented to council in one form or another, deals with a pending matter following the close of a public hearing, or is, or has been, dealt with in a public participation process.
7. Supporting submissions for the delegation should be provided to the Clerk by noon 12 days preceding the scheduled appearance.
8. Delegations will be allowed a maximum of five minutes to make their presentation.
9. Any questions to delegations by members of Council will seek only to clarify a material aspect of a delegate's presentation.
10. Persons invited to speak at the Council meeting may not speak disrespectfully of any other person or use any rude or offensive language or make a statement or allegation which impugns the character of any person.

Helpful Suggestions:

- have a purpose
- get right to your point and make it
- be concise
- be prepared
- state your request, if any
- do not expect an immediate response to a request
- multiple-person presentations are still five minutes maximum
- be courteous, polite, and respectful
- it is a presentation, not a debate
- the Council Clerk may ask for any relevant notes (if not handed out or published in the agenda) to assist with the accuracy of our minutes

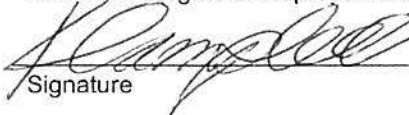
I understand and agree to these rules for delegations

Kathleen Campbell

May 19, 2015

Name of Delegate or Representative of Group

Date


Signature

For Office Use Only

Approved by:

Municipal Clerk

Deputy Municipal Clerk

☒

Appearance date:

Receipt emailed on:

July 20, 2015
May 19, 2015

Rejected by:

Mayor

CAO

☐
☐

Applicant informed on:

Applicant informed by:

The personal information collected on this form is done so pursuant to the Community Charter and/or the Local Government Act and in accordance with the Freedom of Information and Protection of Privacy Act. The personal information collected herein will be used only for the purpose of processing this application or request and for no other purpose unless its release is authorized by its owner, the information is part of a record series commonly available to the public, or is compelled by a Court or an agent duly authorized under another Act. Further information may be obtained by speaking with The District of North Vancouver's Manager of Administrative Services at 604-990-2207 or at 355 W Queens Road, North Vancouver.

Value of Tourism Study Calendar Year 2013

For the City and District of North Vancouver

Conducted by: Vancouver's North Shore Tourism Association
with the assistance and support of Destination BC Research Planning

May 11, 2015 Rev.1

Executive Summary

Vancouver's North Shore Tourism "Value of Tourism" research project was recently completed and we can now provide a conservative and credible estimate of visitor volume and direct tourism expenditures within the aggregated communities of the City and District of North Vancouver. West Vancouver was not included in the scope of the project due to its lack of accommodation within the community, which is used as the basis for the model.

Destination BC's Value of Tourism main data source includes collecting data from the City and District of North Vancouver's commercial accommodation sector. The questionnaire gathered data from local hoteliers, several Bed and Breakfasts and additionally visitor estimates were obtained from the two major attractions in the area – Grouse Mountain Resorts and the Capilano Suspension Bridge. Each of the participants provided a snapshot of their 2013 performance.

Destination BC Research Planning and Evaluation then quantified the data and applied additional indicators to generate an estimate of the visitor volume and direct expenditures on tourism in the combined City and District of North Vancouver.

The Value of Tourism results are significant because they quantify what has long been unknown for the area. The results of this project provide a significant contribution to the level of understanding of the overall local tourism industry and its economic significance to the community.

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Location and Area of Study – City and District of North Vancouver

City of North Vancouver

The City of North Vancouver has a population of 48,196 (2011 Census) and a land base of 11.83 square kilometres. Its “front door” is directly across from downtown Vancouver on the Burrard Inlet waterfront. The District of North Vancouver surrounds the City on three sides. Also adjacent to the City, and within its perimeter boundaries, is the Squamish Nation Mission 1 Reserve. The City is located on land described as part of several Coast Salish First Nations traditional territories.

While its history includes logging and shipbuilding, the City itself is becoming “urbanized” as residential towers rise toward the sky, waterfront industry capacity expands, and new amenities for a “sustainable community” develop. The vertical growth is, in part, due to the fact that the City is limited by its geographical space. New bike lanes and walking trails, a new art centre and enhanced meeting places have opened and/or are now in development. Also a vibrant waterfront with animated public areas is evolving and becoming increasingly popular with locals and visitors alike.

From a visitor’s perspective, the City is likely best known for the Lonsdale Quay Market on the waterfront and the SeaBus passenger ferry terminal – gateway to local attractions and the great outdoors. Having said that, we would suggest that many metro residents and most visitors are not aware that there are two different “North Vancouvers”. Those from afar typically view the entire Metro region as “Vancouver”.

District of North Vancouver

The District of North Vancouver with a population of 84,412 (per 2011 Census) is a suburban municipality. It is located north of Vancouver and Burnaby, separated by the waters of Burrard Inlet. The District covers an area of 160.76 square kilometres.

On its western flank is the Municipality of West Vancouver. To the east, and separated by Indian Arm, is Belcarra Regional Park and Indian Arm Provincial Park.

The District of North Vancouver surrounds the City of North Vancouver on three sides and is recognized by its geographical attributes. In many areas it extends from seaside to mountaintop. It is also known for its top attractions including Capilano Suspension Bridge Park, Grouse Mountain and Lynn Canyon Park. The District enjoys a reputation of having a “natural” environment and is viewed as a playground for outdoor enthusiasts and those wanting to reside close to nature while able to enjoy an urban lifestyle.

District lands are recognized as part of Coast Salish Nation’s traditional territories. As such, visitors have been arriving in the area long before any European history was recorded here.

“Vancouver’s North Shore”

From a visitors perspective “Vancouver’s North Shore” (including West Vancouver, the City and District of North Vancouver) is recognized primarily for its mountainside setting, variety of outdoor activities, accessibility to nature, and as home to major attractions. As such, the North Shore is fortunate to be able to clearly distinguish itself from other Metro Vancouver destinations, and capitalize on the broad recognition of destination Vancouver.

For more information about North Vancouver see <http://www.cnv.org/> ;
<http://www.dnv.org/> ; and <http://vancouvernorthshore.com/>

Introduction

British Columbia's tourism industry provides an important revenue source for the province and its residents. In 2013 tourism revenue in BC was calculated to be \$13.9 billion (Destination British Columbia (DBC) Value of Tourism November 2014). A third (33.6%) of tourism revenue can be attributed by the accommodation and food services sector, 31.3% by transportation, and 27.2% by retail services.

Vancouver, Coast and Mountains, one of six tourism regions in the province represents 46% of provincial overnight visitation and 54% of related spending. The region covers a vast territory that stretches from the Fraser Canyon in the east to Metro Vancouver, plus the Sunshine Coast and Sea to Sky area between Lions Bay and Lillooet. The Vancouver Coast and Mountains Region, in which the City and District of North Vancouver are contained, accounted for 58% of tourism businesses, and almost 64% of BC tourism industry employment.

In 2012, 45% of all overnight visitation within the Vancouver Coast and Mountain region was made up of British Columbians; 15% by other Canadians, 26% by US residents, and 14% by international travellers. At the same time, 22% of total expenditures were made by BC residents, 20% from other Canadians, 22% by US visitors, and the remaining 35% in spending came from international visitors.

The City and District of North Vancouver are often referred to as bedroom communities within the Metro Vancouver region. Located on the "mountain side" of Vancouver, the area has a lot to offer residents and visitors alike. It is quite possible for someone living or visiting the area to not spend time in Vancouver itself, as virtually all services and amenities that anyone requires are available on the North Shore. It is a distinct and total destination unto itself.

A significant proportion of the Metro areas major tourism offerings are located in North Shore. Most notably the Capilano Suspension Bridge, Grouse Mountain Resorts, Sewell's Sea Safari and Marina, BC Ferries to Vancouver Island and the Sunshine Coast, the Lonsdale Quay Market, Cypress Mountain and Mount Seymour. The Value of Tourism is important for the North Shore and the tourism industry in total, as it provides an estimate of visitation and annual direct expenditures for the City and District of North Vancouver for the year 2013.

The information presented in this report is intended to benefit the private sector and all levels of government involved in tourism related planning, development, marketing and analysis.

Background: Destination BC's Value of Tourism Project

Having defensible estimates of tourism's contribution to a community is central to developing an informed planning approach, strengthening the support of community stakeholders and local government, and encouraging an appreciation of tourism's economic development potential.

In 2005, Destination British Columbia (DBC) introduced the Community Tourism Foundations® (CTF) Program, which was designed to assist communities in developing their tourism potential. In order to track the performance of communities participating in the CTF Program a model was developed by DBC. The Value of Tourism model provides a structured, consistent approach to estimate the volume and value of tourism; specifically the objective of the model is: "To provide a conservative and credible estimate of visitor volume and direct tourism expenditures within a community."

The core data collection component of the model is an accommodation survey to gather the most recent annual performance data from commercial accommodation operators. This information is then combined with local, regional, provincial and national tourism indicators to provide an estimate of the total number of visitors (overnight, day and visiting friends & relatives) and their expenditures in the community.

The Value of Tourism model can be applied to communities throughout the province. As of November 2011, eight communities successfully completed VOT Research.

- Lillooet,
- Squamish,
- Fort Nelson (Northern Rockies),
- Fort St. John (North Peace),
- Dawson Creek (South Peace),
- Quadra Island,
- Smithers,
- Terrace.

As of January 2015 the City and District of North Vancouver was added to this list of communities.

Method

The survey tools used for this study are included as Appendix #2. A complete inventory of all commercial accommodations in the community, fixed-roof hotels, motels and bed and breakfasts were required to proceed with the Value of Tourism model. Normally RV parks and camping site are also included in the Value of Tourism study but in this case, none of those accommodation types exist in the City or District. Once the inventory was finalized, all commercial accommodations were approached to participate.

Participating commercial properties were asked to provide monthly information for the most recent year (2013), which included: Number of Rooms Available; Total Occupied Room Nights; Average Achieved Daily Room Rate; Average Length of Stay; and the Average Party Size. Where applicable, the collected information was additionally split between business and leisure guests for fixed-roof accommodation properties.

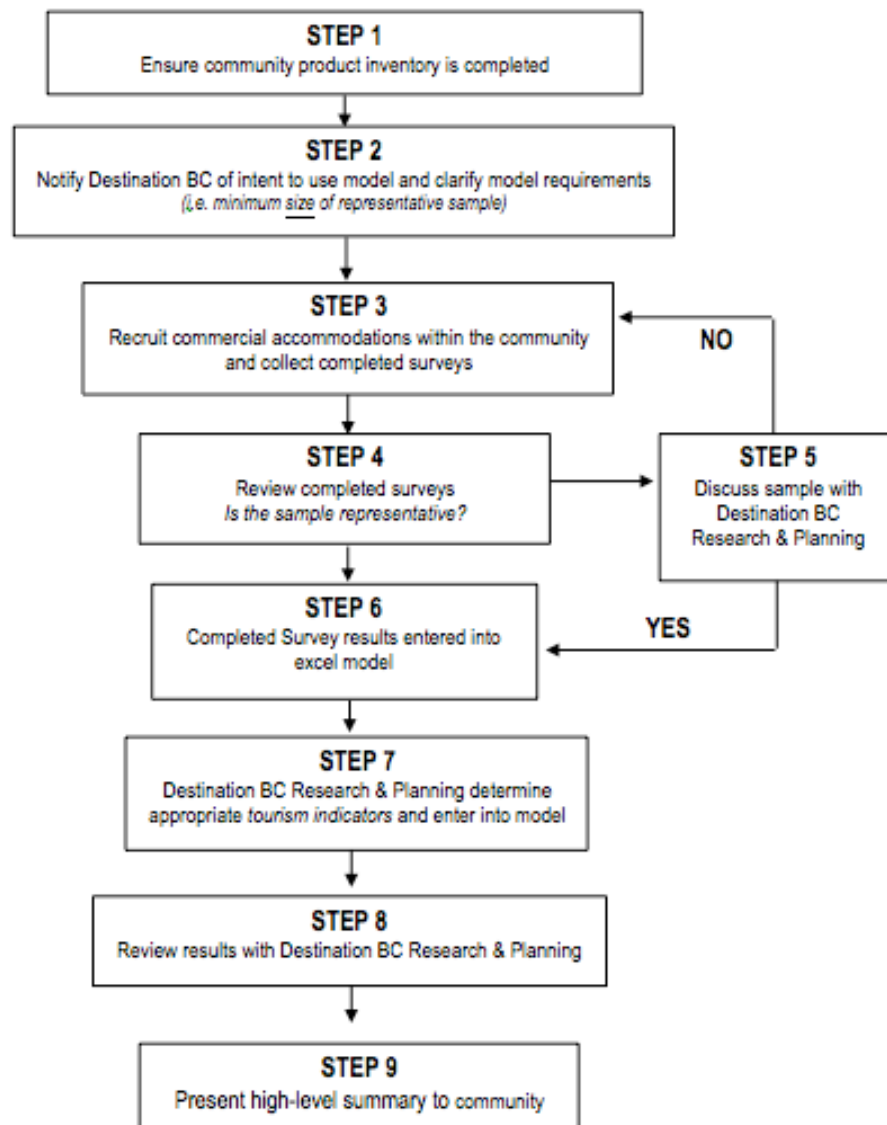
The completed form was faxed or emailed to DBC or to the Project Coordinator at Vancouver's North Shore Tourism (VNSTA). All the data collected is strictly confidential and individual accommodation information is not published. The goals of the accommodation survey process include: to collect surveys from at least 50% of the total room/site base; to collect surveys from at least 50% of each accommodation type (i.e. hotel, motel, B&B,); and within each accommodation type, to collect a representative sample (i.e. small/large establishments, and other community specific factors). Once the representative sample of commercial accommodations was obtained the survey was entered into the Value of Tourism model.

Due to the fact that North Vancouver District is also home to two of the top regional attractions, both Grouse Mountain and the Capilano Suspension Bridge were asked to provide their out of town visitor statistics for 2013. Both attractions complied and helped revise the same day volume estimate.

After data entry, DBC implemented the Value of Tourism model, selecting appropriate regional, provincial and national tourism indicators for estimating the visitor volume and direct tourism expenditures within the communities of North Vancouver City and District combined. The Value of Tourism model provides an estimate of direct visitor spending only and does not estimate the economic impact (GDP, employment, etc.) of tourism within a community.

Destination BC'S Value of Tourism survey methodology follows the path outlined on the flowchart below.

Determining the Volume and Value of Tourism



2013 Results

A total of 100% of the hotel/motel fixed roof accommodations room-base participated in the project plus 50% of local Bed and Breakfasts.

There were a total of 726 commercial rooms available per night in North Vancouver City and District in 2013. These were included in the inventory used to determine the Value of Tourism in this study. It included 337 hotel rooms, 377 motel rooms and 12 Bed and Breakfast rooms. (Only three B&Bs located in the City or District of North Vancouver, each with three rooms, did not participate in this study.)

During the study period, 163,300 visitors stayed in commercial accommodation, with 124,100 identified as “Leisure” visitors and 39,200 as “Business” visitors. Also, 62,200 people spent time visiting friends and relatives, and an additional 1,200,000 people passed through the community as day visitors. i.e. did not stay overnight in the community.

In 2013, total visitation to North Vancouver was estimated as 1,425,500 people.

Based on the total visitation data, and factoring in average length of stay and average total spend per visitor per trip the **“Annual Direct Visitor Expenditures” in the City and District in 2013 was estimated at \$103,880,000***

** In order to protect the confidentiality of the original data collected, as well as the anonymity of the business owners, these numbers cannot be further broken down.*

An Estimate of 2013 Tourism Volume and Direct Expenditures in North Vancouver, British Columbia

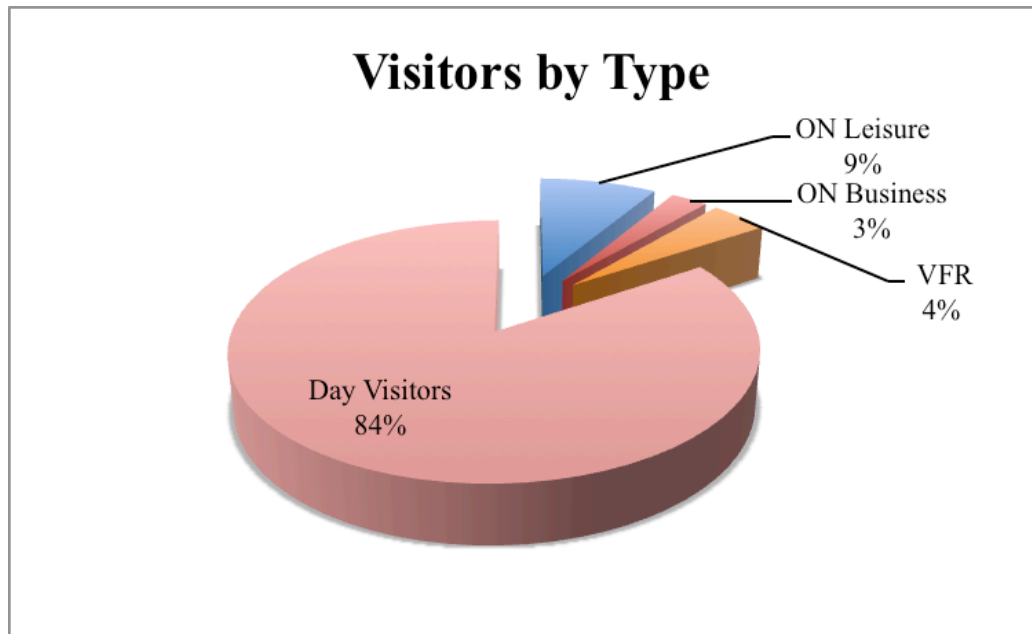
Estimates of Visitors

Type of Visitor	Volume of Visitors
Fixed Roof (Hotels, Motels, B&B)	
Leisure	124,100
Business	39,200
RV & Campground	0
Total Visitors in Commercial Accommodations	163,300
Visiting Friends & Relatives (VFR)	62,200
Day Visitors	1,200,000
Overall Visitation	1,425,500

Estimates of Direct Visitor Expenditures

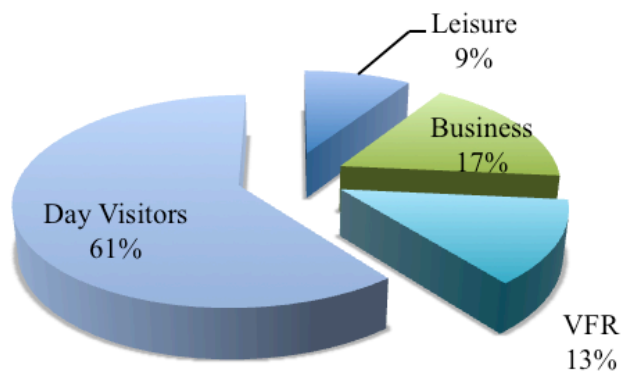
Type of Visitor	# Visitors	Average length of stay (days)	Average total spend (\$) per visitor per trip	Totals (\$)
Leisure (commercial accommodation)	124,100	2.4	\$210	\$26,480,000
Business	39,200	1.5	\$380	\$14,740,000
Visiting Friends & Relatives (VFR)	62,200	3.7	\$180	\$10,980,000
Day Visitors	1,200,000		\$40	\$51,680,000
Annual Direct Visitor Expenditures				\$103,880,000

NOTE: The Estimates of Direct Visitor Expenditures are determined from information collected from commercial accommodation properties within the community and from indicators collected in visitor surveys including those conducted by Destination British Columbia and Statistics Canada (Travel Survey of Residents of Canada (TSRC) and International Travel Survey (ITS)). Therefore, the data listed by type of visitor is not multiplied together to determine the total direct visitor expenditures by visitor type, but provides information regarding the characteristics of each traveler type.



Type	Volume
Day Visitors	1,200,000
Overnight Leisure	124,100
Overnight Business	39,200
Visiting Friends & Relatives	62,200
TOTAL VISITORS	1,425,500

Estimates of Direct Visitor Expenditures



Type	Total \$
Leisure (commercial accommodation)	\$26,480,000
Business	14,740,000
Visiting Friends and Family (VFR)	10,980,000
Day Visitors	51,680,000
Annual Direct Visitor Spending	\$ 103,880,000

Additional Local Indicators

In addition to the 2013 Value of Tourism results presented in this document, there is other local data that can be used as indicators for North Vancouver's tourism industry. Visitor Centre attendance data is a useful indicator that can help one determine industry trends as well as the overall state of the local tourism industry.

The charts below depict the two local Visitor Centre's annual visitor data for 2013. That is, 9552 visitors in 2013 went to a local VC. This total includes visits to both the North Vancouver Chamber of Commerce Visitor Centre (year-round) and the summer season satellite Visitor Centre located at the Lonsdale Quay Market.

Source: Destination BC Visitor Network Statistics Program Year Over Year Report 2014.

Visitor Centre Network Statistics Program Year Over Year Report 2015																	
NORTH VANCOUVER																	
Start Date: 01/01/2013																	
End Date: 31/12/2014																	
Parties Per Hour	January	February	March	Q1 Total	April	May	June	Q2 Total	July	August	September	Q3 Total	October	November	December	Q4 Total	TOTAL
2014	0.60	0.54	0.35	0.50	0.77	0.95	0.87	0.86	0.97	0.74	0.72	0.81	0.68	0.39	0.60	0.57	0.69
% change 2014 - 2013	11.2%	-3.4%	-50.9%	-17.7%	10.8%	-30.2%	7.1%	-3.3%	27.3%	-22.9%	-21.1%	-7.7%	0.1%	-22.0%	15.2%	1.5%	-5.6%
2013	0.54	0.56	0.71	0.60	0.70	1.36	0.81	0.89	0.76	0.96	0.91	0.88	0.66	0.50	0.52	0.56	0.73
Total Hours	January	February	March	Q1 Total	April	May	June	Q2 Total	July	August	September	Q3 Total	October	November	December	Q4 Total	TOTAL
2014	170.00	152.00	166.00	488.00	160.00	165.00	168.00	493.00	176.00	160.00	167.00	503.00	176.00	109.00	146.00	431.00	1,915.00
% change 2014 - 2013	-3.4%	0.0%	3.8%	0.0%	-4.8%	70.1%	5.0%	16.0%	4.8%	-8.1%	4.4%	-0.2%	0.0%	-31.9%	1.4%	-10.2%	0.9%
2013	176.00	152.00	160.00	488.00	168.00	97.00	160.00	425.00	168.00	176.00	160.00	504.00	176.00	160.00	144.00	480.00	1,897.00
Total Parties	January	February	March	Q1 Total	April	May	June	Q2 Total	July	August	September	Q3 Total	October	November	December	Q4 Total	TOTAL
2014	102	82	58	242	123	158	146	425	170	118	120	408	117	42	88	247	1,322
% change 2014 - 2013	7.4%	-3.5%	-49.1%	-17.7%	5.1%	18.2%	12.3%	12.1%	32.8%	-30.2%	-17.8%	-7.9%	0.9%	-47.5%	17.3%	-8.9%	-4.7%
2013	95	85	114	294	117	132	130	379	128	169	146	443	116	80	75	271	1,387
Total Parties-Event/Roaming	January	February	March	Q1 Total	April	May	June	Q2 Total	July	August	September	Q3 Total	October	November	December	Q4 Total	TOTAL
2014	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% change 2014 - 2013	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
2013	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Visitors	January	February	March	Q1 Total	April	May	June	Q2 Total	July	August	September	Q3 Total	October	November	December	Q4 Total	TOTAL
2014	109	91	73	273	137	175	167	479	237	173	170	580	137	47	100	284	1,616
% change 2014 - 2013	0.9%	-8.1%	-46.7%	-20.6%	-4.2%	2.9%	2.5%	0.6%	48.1%	-22.1%	-13.3%	0.3%	-5.5%	-48.4%	12.4%	-12.6%	-6.2%
2013	108	99	137	344	143	170	163	476	160	222	196	578	145	91	89	325	1,723

Visitor Centre Network Statistics Program Year Over Year Report 2015																	
N VANCOUVER - LONSDALE																	
Start Date: 01/01/2013																	
End Date: 31/12/2014																	
Parties Per Hour	January	February	March	Q1 Total	April	May	June	Q2 Total	July	August	September	Q3 Total	October	November	December	Q4 Total	TOTAL
2014	0.00	0.00	0.00	0.00	0.00	4.74	5.66	5.35	6.08	6.00	4.75	6.02	0.00	0.00	0.00	0.00	5.74
% change 2014 - 2013						36.6%	62.7%	54.0%	35.0%	21.1%	-11.6%	26.8%					35.6%
2013	0	0	0	0.00	0	3.47	3.48	3.48	4.50	4.95	5.38	4.75	0	0	0	0.00	4.23
Total Hours	January	February	March	Q1 Total	April	May	June	Q2 Total	July	August	September	Q3 Total	October	November	December	Q4 Total	TOTAL
2014	0.00	0.00	0.00	0.00	0.00	120.00	240.00	360.00	248.00	246.00	8.00	502.00	0.00	0.00	0.00	0.00	862.00
% change 2014 - 2013						-9.1%	-18.6%	-15.7%	-19.7%	-20.1%	-50.0%	-20.7%					-18.7%
2013	0.00	0.00	0.00	0.00	0.00	132.00	295.00	427.00	309.00	308.00	16.00	633.00	0.00	0.00	0.00	0.00	1,060.00
Total Parties	January	February	March	Q1 Total	April	May	June	Q2 Total	July	August	September	Q3 Total	October	November	December	Q4 Total	TOTAL
2014	0	0	0	0	0	569	1,358	1,927	1,508	1,475	38	3,021	0	0	0	0	4,948
% change 2014 - 2013						24.2%	32.4%	29.9%	8.3%	-3.3%	-55.8%	0.6%					10.2%
2013	0	0	0	0	0	458	1,026	1,484	1,392	1,526	86	3,004	0	0	0	0	4,488
Total Parties-Event/Roaming	January	February	March	Q1 Total	April	May	June	Q2 Total	July	August	September	Q3 Total	October	November	December	Q4 Total	TOTAL
2014	0	0	0	0	0	0	9	9	32	1	0	33	0	0	0	0	42
% change 2014 - 2013							0	0	0	0	0	0	0	0	0	0	0
2013	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Visitors	January	February	March	Q1 Total	April	May	June	Q2 Total	July	August	September	Q3 Total	October	November	December	Q4 Total	TOTAL
2014	0	0	0	0	0	953	2,380	3,333	2,716	2,737	63	5,516	0	0	0	0	8,549
% change 2014 - 2013						25.6%	35.9%	32.8%	11.9%	-0.1%	-58.0%	3.7%					13.0%
2013	0	0	0	0	0	759	1,751	2,510	2,428	2,741	150	5,319	0	0	0	0	7,829

Study Limitations

- 1) The 2013 Value of Tourism model is only relevant for the 12-month period from January to December 2013. The results should not be applied to any other time frame.
- 2) The accuracy of the model depends on how well the tourism characteristics and expenditure data represent actual spending and behavior patterns in the City and District of North Vancouver during the time of the study.
- 3) The model is dependent upon the data provided by local commercial accommodations and top two attractions, therefore is based on the quality of this data.
- 4) The model results for North Vancouver should not be applied to other areas.
- 5) The Value of Tourism model provides an estimate of direct visitor spending only and does not estimate the economic impact (GDP, employment, etc.) of tourism within a community.
- 6) Results from the Value of Tourism model are to be used with caution, due to the stated limitations of the model.

Noteworthy

The Value of Tourism Model uses visitor information from a variety of sources, including Destination BC's two national visitor studies (International Traveller Survey and Traveller Survey of Residents of Canada), BC Visitor Centres data, Visitor Centre studies, and other research sources to develop ratios that are used to estimate both volume and revenue. *There is a caveat in the analysis that indicates that **the value of tourism model is not an economic impact**; economic impact examines the effect on the economy in a specified area, ranging from a single neighbourhood to the entire globe. Economic impacts use business revenue, business profits, personal wages, visitor collection, etc. to estimate the change in economic activity if it did not occur. This can be increasingly difficult to complete due to data access and the ability to obtain local/small area economic multipliers.*

Bibliography

Government of Canada Census info 2011 City of North Vancouver

<http://www12.statcan.gc.ca/census-recensement/2011/as-sa/fogs-spg/Facts-csd-eng.cfm?LANG=Eng&GK=CSD&GC=5915051>

Government of Canada Census info 2011 North Vancouver District:

<http://www12.statcan.gc.ca/census-recensement/2011/dp-pd/prof/details/page.cfm?Lang=E&Geo1=CSD&Code1=5915046&Geo2=PR&Code2=59&Data=Count&SearchText=North%20Vancouver&SearchType=Begin&SearchPR=01&B1=All&Custom=>

Destination BC Value of Tourism for British Columbia 2013:

<http://www.destinationbc.ca/getattachment/Research/Industry-Performance/Value-of-Tourism/Value-of-Tourism-in-British-Columbia-A-Snapsho-%281%29/2014-Value-of-Tourism-Snapshot.pdf.aspx>

Destination BC The Value of Tourism Trends 2002-2012 dated Feb. 2014

Destination BC. (2014). Visitor Network Statistics Program (VNSP) Year Over Year Report 2014. Retrieved from the Tourism BC's Network Stats website

<https://www.networkstats.tourismbc.com/reportdefinition.aspx>

DBC Regional Publication

http://www.destinationbc.ca/getattachment/Research/Research-by-Region/All-Regional-Profiles/Vancouver,-Coast-and-Mountains-Regional-Profile/RegionalProfiles_Vancouver-Coast-and-Mountains_2014.pdf.aspx

Appendix

The following was used in the development and completion of the study:

- 1) Destination BC's "Estimating Tourism's Contribution to the Local Economy" Document
- 2) Template for Final Results provided by Destination BC
- 3) 2013 Fixed Roof Accommodations Form

Exhibits on following pages.

EXHIBIT I

Estimating Tourism's Contribution to the Local Economy

Having defensible estimates of tourism's contribution to a community is central to developing an informed tourism planning approach, strengthening the support of community stakeholders and local government, and encouraging an appreciation of tourism's economic development potential.

The Objective

To provide a conservative and credible estimate of visitor volume and direct tourism expenditures within a community.

The *Value of Tourism* model is a project that provides a structured, consistent approach to estimate the volume and value of tourism. The core data collection component of the model is an accommodation survey to gather the most recent annual performance data from commercial accommodation operators (including campgrounds). This information is then applied to local, regional, provincial and national tourism indicators to provide an informed estimate of the total number of visitors (overnight, day and visiting friends and relatives) and their expenditures in the community.

Confidentiality

Information provided by accommodation businesses will:

- only be used for the purposes of estimating tourism contribution to the community;
- be aggregated with other operators data to ensure anonymity;
- remain confidential and will not be released in any form that enables individual operators to be identified; and
- be submitted directly to either Research, Planning and Evaluation or the Project Coordinator.

Participation

Please review the *Tourism Contribution Survey* included in this package. Your facility's statistics are required complete the survey. Please complete the accommodation survey and return the form to Research, Planning and Evaluation

For More Information

If you have further questions please contact:

Research, Planning and Evaluation
Destination British Columbia

EXHIBIT II

An Estimate of XXXX Tourism Volume and Direct Expenditures in XXXX

Estimates of Visitors

Type of Visitor	Volume of Visitors
Fixed Roof (Hotels, Motels, B&B)	
Leisure	0
Business	0
RV & Campground	0
Total Visitors in Commercial Accommodations	0
Visiting Friends & Relatives (VFR)	0
Day Visitors	0
Overall Visitation	0

Estimates of Direct Visitor Expenditures

Type of Visitor	# Visitors	Average length of stay (days)	Average total spend (\$) per visitor per trip	Totals (\$)
Leisure (commercial accommodation)	0	0.0	\$0	\$0
Business	0	0.0	\$0	\$0
Visiting Friends & Relatives (VFR)	0	0.0	\$0	\$0
Day Visitors	0		\$0	\$0
Annual Direct Visitor Expenditures				\$0

NOTE: The Contribution of Tourism figures presented above provide estimates of the size and value of the local tourism industry. They are prepared largely from information collected from tourism businesses within the community and are limited to the quality of that data collection. These estimates are only applicable to the time period and community for which the data was collected for. These values should not be applied to other years or communities.



Fixed Roof Accommodation - Tourism Contribution Survey

Please complete the following information on your property for **the most recent year** and return the completed sheet to:

Tourism BC Research & Planning fax: 250-356-8246 or XXX, project coordinator fax: XXX-XXX-XXXX.

The information provided will be kept CONFIDENTIAL and only used by Tourism BC and the project coordinator for this project. Data will only be reported in aggregate and only where one cannot infer from the data the performance of individual properties.

Property Name

Property type

(e.g. Hotel, Motel, Lodge, B&B, Resort)

All Guests	Jan-08	Feb-08	Mar-08	Apr-08	May-08	Jun-08	Jul-08	Aug-08	Sep-08	Oct-08	Nov-08	Dec-08
Average Achieved Daily Rate												
Number of Available Rooms Per Night												
Total Occupied Room Nights												
Business Guests												
Occupied Room Nights												
Average Length of Stay												
Average Party Size												
Leisure Guests												
Occupied Room Nights												
Average Length of Stay												
Average Party Size												

****** If your business includes **more than one** type of accommodation (e.g. Motel & RV sites), please complete a separate form for each "type" of accommodation.

END OF DOCUMENT

MINUTES

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**DISTRICT OF NORTH VANCOUVER
REGULAR MEETING OF COUNCIL**

Minutes of the Regular Meeting of Council for the District of North Vancouver held at 7:03 pm on Monday, July 6, 2015 in the Council Chamber of the District Hall, 355 West Queens Road, North Vancouver, British Columbia.

Present: Mayor R. Walton
Councillor R. Bassam
Councillor M. Bond
Councillor J. Hanson
Councillor R. Hicks
Councillor D. MacKay-Dunn
Councillor L. Muri

Staff: Ms. C. Grant, Acting Chief Administrative Officer
Mr. B. Bydwell, General Manager – Planning, Properties & Permits
Mr. G. Joyce, General Manager – Engineering, Parks & Facilities
Mr. D. Milburn, Deputy General Manager – Planning, Properties & Permits
Mr. S. Bridger, Acting Manager – Engineering Services
Ms. L. Brick, Deputy Municipal Clerk
Mr. D. Allan, Planner
Ms. A. Mauboules, Social Planner
Ms. T. Smith, Project Manager, External Projects
Ms. C. Archer, Confidential Council Clerk

1. ADOPTION OF THE AGENDA

1.1. July 6, 2015 Regular Meeting Agenda

MOVED by Councillor MURI

SECONDED by Councillor BOND

THAT the agenda for the July 6, 2015 Regular Meeting of Council for the District of North Vancouver be adopted as circulated, including the addition of any items listed in the agenda addendum.

CARRIED

2. PUBLIC INPUT

2.1. Mr. Hazen Colbert, 1100 Block East 27th Street:

- Thanked Council for flying the rainbow flag; and,
- Commented on municipal procedures involved in the preliminary application for Belle Isle Place.

3. PROCLAMATIONS

Nil

4. RECOGNITIONS

Nil

5. DELEGATIONS

5.1. Mr. Peter Thompson, Community Monitoring Advisory Committee

Re: Highlights from the 2014/15 Annual Report.

Mr. Peter Thompson, Chair, Community Monitoring Advisory Committee, spoke regarding the Committee's work, including the Seymour–Capilano Filtration Plant and Capilano Water Main No. 9. Mr. Thompson thanked District Council and staff for their support of the Committee.

MOVED by Councillor HICKS

SECONDED by Councillor MURI

THAT the delegation of the Community Monitoring Advisory Committee be received for information.

CARRIED

6. ADOPTION OF MINUTES

6.1. June 1, 2015 Regular Council Meeting

MOVED by Councillor MURI

SECONDED by Councillor BOND

THAT the minutes of the June 1, 2015 Regular Council meeting be adopted.

CARRIED

6.2. June 15, 2015 Regular Council Meeting

MOVED by Councillor MURI

SECONDED by Councillor BOND

THAT the minutes of the June 15, 2015 Regular Council meeting be adopted.

CARRIED

6.3. June 16, 2015 Public Hearing

MOVED by Councillor MURI

SECONDED by Councillor BOND

THAT the minutes of the June 16, 2015 Public Hearing be received.

CARRIED

6.4. June 23, 2015 Public Hearing

MOVED by Councillor MURI

SECONDED by Councillor BOND

THAT the minutes of the June 23, 2015 Public Hearing be received.

CARRIED

7. RELEASE OF CLOSED MEETING DECISIONS

Nil

8. COMMITTEE OF THE WHOLE REPORT

Nil

9. REPORTS FROM COUNCIL OR STAFF

MOVED by Councillor MURI

SECONDED by Councillor BOND

THAT items 9.2, 9.4, 9.5, 9.6, 9.7 and 9.8 be included in the Consent Agenda and be approved without debate.

CARRIED

9.1. Bylaws 8103 and 8104: 1203 and 1207 Harold Road

File No. 08.3060.20/030.14

At the request of Mayor Walton, the following resolution from the June 15, 2015 Regular Meeting of Council is put before Council for reconsideration pursuant to s. 131 of the *Community Charter*.

MOVED by Councillor BASSAM

SECONDED by Councillor BOND

THAT "The District of North Vancouver Rezoning Bylaw 1325 (Bylaw 8103)" is given SECOND and THIRD Readings;

AND THAT "Housing Agreement Bylaw 8104, 2014" is given SECOND and THIRD Readings.

MOVED by Mayor WALTON

SECONDED by Councillor BASSAM

THAT "The District of North Vancouver Rezoning Bylaw 1325 (Bylaw 8103)" is amended as attached to the June 26, 2015 report of the Community Planner entitled "Reconsideration of Bylaws 8103 and 8104: 1203 and 1207 Harold Road."

CARRIED

Opposed: Councillors HANSON, MACKAY-DUNN and MURI

The question was called on the amended main motion as follows:

THAT "The District of North Vancouver Rezoning Bylaw 1325 (Bylaw 8103)" is given SECOND Reading as amended.

THAT "The District of North Vancouver Rezoning Bylaw 1325 (Bylaw 8103)" is given THIRD Reading.

THAT "Housing Agreement Bylaw 8104, 2014" is given SECOND and THIRD Readings.

CARRIED

Opposed: Councillors HANSON, MACKAY-DUNN and MURI

9.2. 2015 Community Service Grants Recommendations

File No. 10.4792.01/007

MOVED by Councillor MURI

SECONDED by Councillor BOND

THAT Council approve the 2015 Community Service Grants allocations of \$269,454 in accordance with Attachment A of the June 23, 2015 report of the Social Planner entitled "2015 Community Service Grant Recommendations";

AND THAT staff be directed to refer the request for an inflation adjustment to the Community Service Grant budget to the 2016 financial planning process.

CARRIED

9.3. 2015 Childcare Grants

File No. 10.4750.20/010.000

MOVED by Councillor MURI

SECONDED by Councillor BASSAM

THAT Council approve funding in the amount of \$50,880 for the twenty-three childcare grant applications listed in Table 1 of the June 22, 2015 report of the Social Planner entitled "2015 Childcare Grants" with funds allocated from the Childcare Grant account;

AND THAT staff be directed to refer the request for an inflation adjustment to the Childcare Grant budget to the 2016 financial planning process.

CARRIED

9.4. Final Instalment of 2015 Core Funding Grants

File No. 05.1930/Grants and Sponsorships/2015

MOVED by Councillor MURI

SECONDED by Councillor BOND

THAT Council approve the \$469,080 final instalment of the \$770,673 core grant previously approved in the 2015 budget.

Capilano Community Services Society	\$59,234
North Shore Neighbourhood House	\$57,388
Boys' and Girls' Clubs of Greater Vancouver (Norvan)	\$13,610
Parkgate Community Services Society (youth)	\$103,480
Silver Harbour Centre Society	\$56,960
Family Services of the NS	\$34,680
North Shore Childcare Resources Program	\$36,343
North Shore Restorative Justice Society	\$24,401
Lynn Valley Services Society (Mollie Nye House)	\$22,984
Hollyburn Family Services Society	\$10,000
Parkgate Community Services Society (operating)	\$50,000

TOTAL **\$469,080**

CARRIED

9.5. Sport and Recreation Travel Grant Recommendations

File No. 05.1930/Grants and Sponsorships/2015

MOVED by Councillor MURI

SECONDED by Councillor BOND

THAT the achievements of District of North Vancouver residents Anna Goodwin, Eliza Jane Kitchen, Katie Weaver, Jenna Sim, Kayden Sim and Brent Chapman be supported by awarding Sport and Recreation Travel Grants of \$200 each.

CARRIED

9.6. Bylaw 8125: Local Area Service Bylaw, Traffic Calming for 2900 Block Wembley Drive and 2800 Block Wembley Place

File No. 11.5320.20/064.000

MOVED by Councillor MURI

SECONDED by Councillor BOND

THAT "Speed Humps: 2700-2900 Block Wembley Drive and 2800 Block Wembley Place, Bylaw 8125, 2015" is ADOPTED.

CARRIED

9.7. Bylaw 8133 to Amend Street and Traffic Bylaw 7125 and Bylaw 8134 to Amend Fees and Charges Bylaw 6481

File No. 11.5245.01/000.000

MOVED by Councillor MURI

SECONDED by Councillor BOND

THAT "The District of North Vancouver Street and Traffic Bylaw 7125, 2004, Amendment Bylaw 8133, 2015 (Amendment 14)" is ADOPTED.

THAT "The District of North Vancouver Fees and Charges Bylaw 6481, 1992, Amendment Bylaw 8134, 2015 (Amendment 46)" is ADOPTED.

CARRIED

9.8. Cates Park / Whey-ah-Wichen Canoe Festival July 10, 11 & 12, 2015

File No. 12.5810.01

MOVED by Councillor MURI

SECONDED by Councillor BOND

THAT the Parks Control Bylaw 2733 be relaxed to permit overnight camping in Cates Park Whey-ah-Wichen for the Tsleil Waututh Nation Canoe Festival on July 10th, 11th and 12th, 2015.

CARRIED

9.9. Bylaws 8101, 8102 and 8112: 1241 – 1289 East 27th Street – Mountain Court Rezoning Application

File No. 08.3060.20/048.14

MOVED by Councillor MURI
SECONDED by Councillor HANSON

THAT "District of North Vancouver Rezoning Bylaw 1324 (Bylaw 8101)" is given no further Readings;

AND THAT "Housing Agreement Bylaw 8102, 2014 (1200 Block East 27th St.)" is given no further Readings;

AND THAT "Housing Agreement Bylaw 8112, 2015 (1200 Block East 27th St.)" is given no further Readings.

DEFEATED

Opposed: Mayor WALTON, Councillors BASSAM, BOND and HICKS

MOVED by Councillor BASSAM
SECONDED by Councillor HICKS

THAT "District of North Vancouver Rezoning Bylaw 1324 (Bylaw 8101)" is given SECOND and THIRD Readings;

AND THAT "Housing Agreement Bylaw 8102, 2014 (1200 Block East 27th St.)" is given SECOND and THIRD Readings;

AND THAT "Housing Agreement Bylaw 8112, 2015 (1200 Block East 27th St.)" is given SECOND and THIRD Readings.

CARRIED

Opposed: Councillor HANSON, MACKAY-DUNN and MURI

MOVED by Councillor BOND
SECONDED by Councillor BASSAM

THAT the following item be added to the agenda:

THAT the General Manager of Planning, Properties and Permits and the General Manager of Finance and Technology evaluate the District's ability to provide affordable rental units through the direct purchase of residential property and report back to Council.

The report must highlight, but not be limited to:

- an overview of financial tools available to purchase residential properties (ie: Land Opportunity Fund, Capital Reserves, debt);
- a financial analysis of purchasing, owning, operating and disposing of residential properties using the available tools;
- an analysis of staff capacity to manage and maintain rental residential properties.

DEFEATED

Opposed: Mayor WALTON, Councillors HANSON, MACKAY-DUNN and MURI

The aforementioned item will be added to a future Regular Council agenda.

9.10. Bylaws 8122 and 8123: Rezoning and Housing Agreement Bylaws for a Mixed Commercial/Residential Project at 3260 Edgemont Blvd., 3230 Connaught Cres. and 1055-1073 Ridgewood Drive (Edgemont Village BT Limited)
File No. 08.3060.20/054.14

MOVED by Councillor MACKAY-DUNN

SECONDED by Councillor BASSAM

THAT "Housing Agreement Bylaw 8123, 2015 (3260 Edgemont, 3023 Connaught Crescent and 1055-1073 Ridgewood Drive)" is given SECOND Reading as amended.

THAT "Housing Agreement Bylaw 8123, 2015 (3260 Edgemont, 3023 Connaught Crescent and 1055-1073 Ridgewood Drive)" is given THIRD Reading.

THAT staff be directed to:

1. ensure that appropriate private dispute resolution mechanisms are in place for dealing with issues that may arise between the commercial and residential components of this proposed development; and,
2. include a clause in the Development Covenant restricting the operation of outdoor seating areas to the hours of 8:00a.m. to 11:00p.m.

THAT staff be directed to include a requirement in the Development Covenant that, prior to issuance of a building permit, a covenant be registered in favour of the District to establish the provision of commercial parking, free for the general public.

CARRIED

9.11. "Community Building Fund" & "Eligibility Criteria for Waiving Municipal Permit Application Fees" Corporate Policies
File No. 01.0115.30/002.000

MOVED by Councillor BASSAM

SECONDED by Councillor MACKAY-DUNN

THAT the Corporate Policy "Community Building Fund" as outlined in the report of the Social Planner dated June 23, 2015 entitled "Community Building Fund" and "Eligibility Criteria for Waiving Municipal Permit Application Fees" Corporate Policies is approved;

AND THAT the Corporate Policy "Eligibility Criteria for Waiving Municipal Permit Application Fees" as outlined in the report of the Social Planner dated June 23, 2015 entitled "Community Building Fund" and "Eligibility Criteria for Waiving Municipal Permit Application Fees" Corporate Policies is approved.

CARRIED

Councillor MURI left the meeting at 9:03 pm and returned at 9:04 pm.

9.12. Maplewood Village Centre Implementation Planning and Maplewood Employment Lands Study
File No. 13.6480.30/00.003

MOVED by Councillor BASSAM
SECONDED by Councillor MACKAY-DUNN

THAT staff be authorized to proceed with the Maplewood Village Centre Implementation Planning Process and Maplewood Employment Lands Study as outlined in the report of the Policy Planners dated June 17, 2015 entitled "Maplewood Village Centre Implementation Planning and Maplewood Employment Lands Study."

CARRIED

10. REPORTS

10.1. Mayor

Nil

10.2. Chief Administrative Officer

Nil

10.3. Councillors

Nil

10.4. Metro Vancouver Committee Appointees

Nil

11. ANY OTHER BUSINESS

Nil

12. ADJOURNMENT

MOVED by Councillor BASSAM
SECONDED by Councillor MACKAY-DUNN

THAT the July 6, 2015 Regular Meeting of Council for the District of North Vancouver be adjourned.

CARRIED
(9:20 pm)

Mayor

Municipal Clerk

COMMITTEE OF THE WHOLE REPORTS

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Memo

July 14, 2015
File: 01.0380.20/074.000

TO: Mayor and Council

FROM: Linda Brick, Deputy Municipal Clerk

SUBJECT: Names for the New Community Recreation Facility in Delbrook

At the July 13, 2015 Committee of the Whole meeting Council recommended that the following resolutions be brought forward to Council for approval:

THAT "Lions Gate" as the new name for the Lower Capilano Center is endorsed.

AND THAT "Lynn Creek" as the new name for Lower Lynn Town Centre is endorsed.

As consensus was not reached regarding the name for the new community recreation centre Council recommended:

THAT the discussion regarding the naming of the new community recreation centre in Delbrook be referred to a Regular meeting of Council.

Linda Brick
Deputy Municipal Clerk

Attached:
Staff Report dated July 6, 2015

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Memo

July 6, 2015
File: 01.0380.20/074

TO: Mayor and Council

FROM: Karen Rendek, Community Planner, Chair of the Place Naming Committee

SUBJECT: Names for Lower Capilano Village Centre, Lower Lynn Town Centre and the new Community Recreation Facility in Delbrook

Council has previously considered new names for Lower Capilano Village Centre, the new community recreation centre in Delbrook and Lower Lynn Town Centre. While Council has expressed a preference for Lions Gate to replace Lower Capilano Village Town Centre, decisions are needed for the other two as well as formal motions for all three.

It is recommended that Council endorse:

1. "Lions Gate" as the new name for the Lower Capilano Village Centre;
2. "Delbrook Community Recreation Centre" as the name for the new community recreation centre in Delbrook; and
3. "Lynn Creek" as the new name for Lower Lynn Town Centre.

New Community Recreation Centre

While the new community recreation centre is scheduled to open in the Fall of 2016, ongoing public communications regarding the status of the project and need for signage on the site has elevated naming of this facility to high priority.

The Naming Selection Criteria and Procedures Corporate Policy offers the following community facility specific selection criteria to guide the naming of a new community facility:

- reflect geographic location (e.g. well-known street, natural feature, neighbourhood);
- reflect or share the name of an adjacent park;
- commemorate an historic event or event of cultural significance;
- help create an identity appropriate to public communications and marketing as well as civic and community goals;

SUBJECT:

Names for Lower Capilano Village Centre, Lower Lynn Town Centre and the new Community Recreation Facility in Delbrook

Page 2

- reflect user programs and services as a secondary naming consideration; and
- generally avoid the use of honourific names.

In applying both the general and community facility specific naming selection criteria, the Place Naming Committee considered and evaluated a number of different names for this new community recreation facility including Queens, Edgemont, Mosquito Creek and Delbrook. "William Griffin" was excluded given that redevelopment permits an opportunity to find a new name that is not an honourific name. While all four names are consistent with the policy, the Place Naming Committee supported Delbrook. Council has been split between Mosquito Creek and Delbrook.

Recommendation:

That Council endorse "Delbrook Community Recreation Centre" as the name for the new community recreation centre in Delbrook.

Lower Lynn Town Centre

Staff need to complete planning work, centres branding and the new District web site's neighbourhood pages.

The Naming Selection Criteria and Procedures Corporate Policy offer the following place naming specific selection criteria:

- take cues from the local, geographic, historical, social or cultural context;
- have a strong positive connection to the local community;
- consider the emerging identity and character for town and village centres; and
- contribute to the establishment of community identity and sense of place.

In applying both the general and specific place naming selection criteria, as well as community and stakeholder input the Place Naming Committee recommended the name "Bridge District". Council has not supported "Bridge District" and instead suggested the name should reflect a geographic or historic reference such as Seylynn, Seven Bridges or Lynn Creek.

Recommendation:

That Council endorse "Lynn Creek" as the new name for Lower Lynn Town Centre.

Following the selection of a new name, District staff will notify relevant public utility organizations, government agencies, emergency responders, internal departments (for updates to databases and GeoWEB), Google Maps, and other groups as needed, of this change. Updates to various plans and/or bylaws may also be needed, as appropriate.

REPORTS

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AGENDA INFORMATION


☒ Regular Meeting


Date: JULY 20 2015

☐ Workshop (open to public)

Date: _____


 Dept.
Manager


 GM/
Director


 CAO

The District of North Vancouver REPORT TO COUNCIL

July 10, 2015

File: 08.3060.20/022.15

AUTHOR: Annie Mauboules, Social Planner
Natasha Letchford, Development Planner

SUBJECT: Bylaw 8124: Turning Point Men's Support Recovery House on Burr Place

RECOMMENDATION:

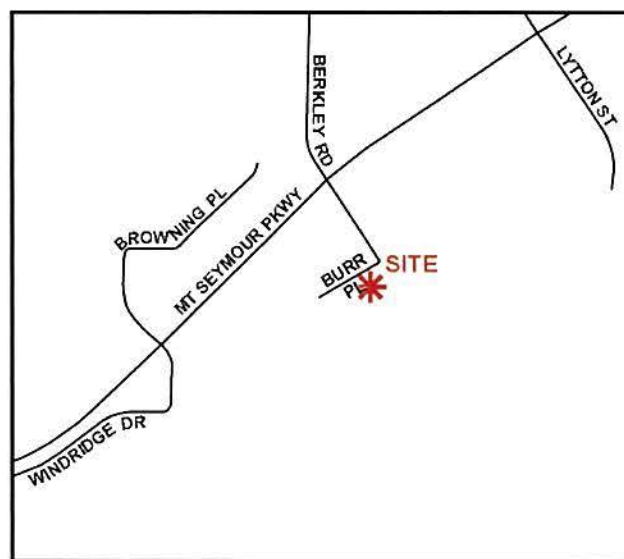
THAT "The District of North Vancouver Rezoning Bylaw 1331 (Bylaw 8124)" is given FIRST reading and is referred to a Public Hearing.

REASON FOR REPORT:

The proposal requires Council's consideration of Bylaw 8124 to amend the group home regulations specific to the Burr Place site to permit 10 persons (9 in care) in a single family home built under the site's single family RS3 zoning.

SUMMARY:

The site allows the group home use limited to 8 persons living in the home (6 in care) and the proposal is for 10 persons in the home (9 in care). An amendment to the group home regulations is required to permit the proposal.

**BACKGROUND:**

As a result of a needs assessment conducted by the North Shore Substance Abuse Working Group (SAWG) the District of North Vancouver, BC Housing and Turning Point Recovery Society have been exploring options to establish a nine- bed support recovery home for men within the municipality.

A District-owned lot adjacent to 2414 Windridge Drive was originally considered for this purpose. A public engagement process was undertaken which included meetings with the neighbours, community associations, service providers and a broad public meeting. Through that process staff heard there is need for such a home however residents have concerns

SUBJECT: Bylaw 8124: Turning Point Men's Support Recovery House on Burr Place

July 10, 2015

Page 2

regarding the Windridge site. These concerns included parking, traffic, safety and the overall tranquillity of the site for those in recovery.

As a result of the community consultation, the District withdrew its consideration of the property on Windridge Drive and offered an alternate site on Burr Place. Turning Point has now applied for a text amendment to the regulations for group homes to permit 10 persons (9 in care) at the Burr Place site.

Over the past several years the SAWG has identified the need on the North Shore for services for men and women recovering from drug and alcohol addiction. Turning Point Recovery Society has a long history of expertise and success in this field and is interested in supporting the SAWG goals in this area on the North Shore. Turning Point operates support recovery homes in suburban neighbourhoods in Richmond and Vancouver and most recently opened a 9 bed house for women on a District owned site on Lloyd Avenue in October 2014.



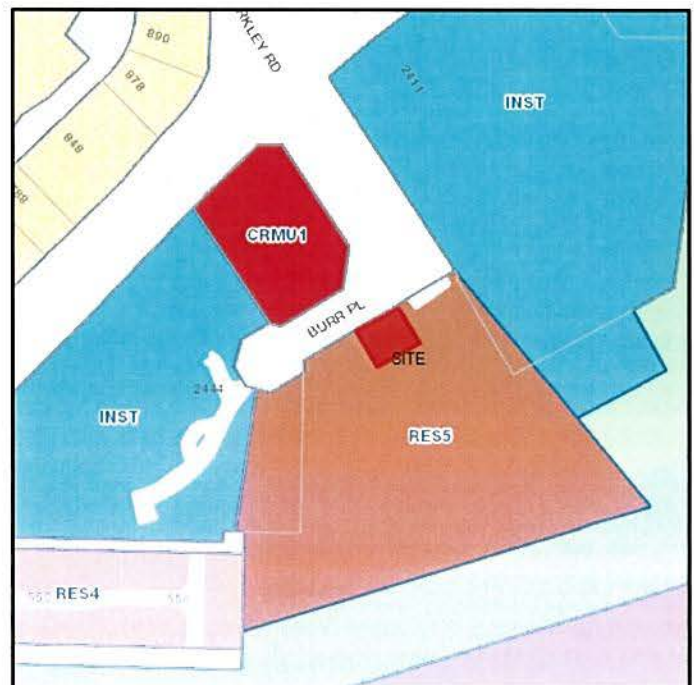
2670 Lloyd Avenue. Women's House

EXISTING POLICY:

Official Community Plan

The subject property on Burr Place is designated *Residential Level 5: Low Density Apartment* in the District Official Community Plan (OCP), and for reference is listed as "Family or Seniors Oriented Medium Density Apartment" in the Maplewood Local Plan.

This project supports the OCP policy objective to work with senior levels of government and social service providers to facilitate "the development of...supportive housing for those with mental health and/or addiction issues." (Bylaw 7900, Section 7.4). The District's legal department has confirmed that the proposed text amendment to group home regulations does not require an OCP amendment.



Zoning

The subject property is currently zoned Single Family Residential 7200 (RS3). The current zoning allows the group home use up to 8 persons (6 in care). The proposed text amendment will allow up to 10 persons (9 in care) in a group home on this site.

Development Permit

The site is located in the following Development Permit Areas (DPA):

- Protection of the Natural Environment
- Wildfire Hazard



Should zoning advance to adoption, Turning Point will require a development permit prior to issuance of any building permit.

ANALYSIS:

Site Selection

The early stages of this project involved an examination of all suitable District-owned sites. Social Planning and Real Estate staff met to review the possibilities and then shortlisted those sites based on a number of criteria including zoning, assessed land value and future highest and best use of the sites. The proponent reviewed the sites using criteria driven by the operational requirements of the project including character of the neighbourhood, proximity to transit, proximity to community services and the cost to build (site topography). The site on Windridge Dr. was the initial first choice. Subsequent to the March 4 community meeting, Council requested that Turning Point consider the District owned site on Burr Place as it addresses the concerns raised by the community about the Windridge site.

The site is located on the south side of the 2400 block Burr Place south of Mount Seymour Pkwy and west of Berkley Rd. Neighbouring uses include Canlan Ice Sports to the east, Kiwanis Parkway Place to the west and vacant District land adjacent and across the street. Should the proposal proceed the intention is to subdivide the subject property to create one single family residential lot.

Project Description

Support recovery homes provide those who are recovering from alcohol and drug addiction a safe and supported place to live for three to five months.

Residents receive clinical counselling, group counselling, health and nutritional care planning as well as life skills,

employment and educational support. The women's house was built in partnership with senior levels of government, each providing some capital and operational funding, to complement the District's contribution of land to make the project a reality. The project has been very successful to date. The project has not created any issues with the neighbourhood since opening and has the full support of the Edgemont and Upper Capilano Community Association.



Proposed House on Burr Place

The proposed house for men would provide men in recovery an identical model of service. The project consists of a 2 storey house with 4 surface parking spaces built in compliance with the RS3 Zone. The house would be licensed as a community care facility by Vancouver Coastal Health under the Community Care and Assisted Living Act. There are currently no similar facilities available on the North Shore dedicated to helping men in their efforts to stop using drugs and alcohol in a safe, supportive and abstinence-based environment. Like the women's house, the men's house would be built with funds from senior levels of government on land contributed by the District.

Partnership and Collaboration

This project is a partnership between all three levels of government and the non-profit sector. The District has been asked to provide the land for a nominal fee for a 60 year term. A future request for waiver of fees and a Permissive Tax Exemption will be forwarded for consideration should the project advance through the text amendment process. The District has a history of partnering in these ways with non-profit organizations such as Hollyburn Family Services Society, NS Connexions and NS Crisis Services Society.

The Provincial government, through BC Housing, will provide the capital to construct the house through a loan as well as a \$200,000 grant to Turning Point. Turning Point will oversee construction. BC Housing will own the house, be the lessee to the District and subsequently enter into an Operator Agreement with Turning Point Recovery Society.

The Provincial government will contribute annual operating funding to staff the house through a partnership with Vancouver Coastal Health.

SUBJECT: Bylaw 8124: Turning Point Men's Support Recovery House on Burr Place

July 10, 2015

Page 5

The Federal government, through Canadian Mortgage and Housing Corporation, will provide project development funding for the project.

Turning Point Recovery Society will be the operator. The society will apply for grants and engage in both operational and capital fundraising campaigns to address any budget shortfalls, as well as to enhance programming services available to the residents.

Public Input

A facilitated Community Information Meeting was held March 4, 2015 for the men's support recovery house previously proposed on Windridge Drive. District staff went door to door along Windridge Dr, the townhouse development on Browning Pl. and on Browning Pl. north of Mount Seymour Parkway to notify residents of the meeting and to provide summary information based on the early feedback from the community. Approximately 350 people attended the March 4 meeting. The meeting provided residents the opportunity to ask clarifying questions and to demonstrate either their support or opposition to the project. As a result of public input the site location was changed to Burr Place.

CONCLUSION

The Burr Place site is an appropriate location to construct a recovery house group home for men. The site has been carefully chosen to respect environmental and neighbourhood constraints. If the proposal is successful, the recovery group home will fill a need identified by the North Shore Substance Abuse Working Group and the North Shore Chief Medical Health Officer. Partnering with Turning Point Recovery Society and senior levels of government enables the District to facilitate the establishment of supportive/transition housing for local residents in need. The model proposed for this site has been demonstrated to be successful for those recovering from alcohol and drug addiction who seek to restore their health and return to being full and active members of our community.

Options:

The following options are available for Council's consideration:

1. Introduce Bylaw 8124 and refer the proposal to a Public Hearing (staff recommendation); or,
2. Defeat Bylaw 8124 at First Reading.

SUBJECT: Bylaw 8124: Turning Point Men's Support Recovery House on Burr Place

July 10, 2015

Page 6

Respectfully submitted,



Annie Mauboules
Policy Planner



Natasha Letchford
Development Planner

Attachments

1. Rezoning Bylaw 1331 (Bylaw 8124)

REVIEWED WITH:		
<input type="checkbox"/> Sustainable Community Dev. _____	<input type="checkbox"/> Clerk's Office _____	External Agencies:
<input type="checkbox"/> Development Services _____	<input type="checkbox"/> Communications _____	<input type="checkbox"/> Library Board _____
<input type="checkbox"/> Utilities _____	<input type="checkbox"/> Finance _____	<input type="checkbox"/> NS Health _____
<input type="checkbox"/> Engineering Operations _____	<input type="checkbox"/> Fire Services _____	<input type="checkbox"/> RCMP _____
<input type="checkbox"/> Parks _____	<input type="checkbox"/> ITS _____	<input type="checkbox"/> Recreation Com. _____
<input type="checkbox"/> Environment _____	<input type="checkbox"/> Solicitor _____	<input type="checkbox"/> Museum & Arch. _____
<input type="checkbox"/> Facilities _____	<input type="checkbox"/> GIS _____	<input type="checkbox"/> Other: _____
<input type="checkbox"/> Human Resources _____		

The Corporation of the District of North Vancouver

Bylaw 8124

A bylaw to amend The District of North Vancouver Zoning Bylaw 3210, 1965 to amend the text of the Zoning Bylaw for a portion of Lot B except portion in Plans 16298, LMP22002 AND LMP37723 Block X District Lots 469, 580 and 611 Plan 15231 shown outlined in bold on the sketch plan attached to this bylaw as Schedule A.

The Council for The Corporation of the District of North Vancouver enacts as follows:

1. Citation

This bylaw may be cited as "The District of North Vancouver Rezoning Bylaw 1331 (Bylaw 8124)".

2. Amendments

The District of North Vancouver Zoning Bylaw 3210, 1965 is amended as follows:

- a) Part 2 – "Interpretation" is amended by replacing the definition of "group home" with the following:

"group home" means a dwelling unit operated for persons in care or with special needs by either an incorporated non-profit society in good standing or a public body chartered by Federal or Provincial statute;"

- b) Part 2 – "Interpretation" is amended by replacing section (3) of "family" with the following:

"(3) a group of not more than eight unrelated persons occupying a group home, of whom no more than six can be persons in care or with special needs except on that portion of the parcel legally described as Lot B except portion in Plans 16298, LMP22002 AND LMP37723 Block X District Lots 469, 580 and 611 Plan 15231 outlined in bold and labelled as "Subject Area" on the sketch plan attached as Schedule A to Bylaw 8124" where a group of not more than ten unrelated persons, of whom no more than nine can be persons with special needs or in care;"

READ a first time

PUBLIC HEARING

READ a second time

READ a third time

ADOPTED

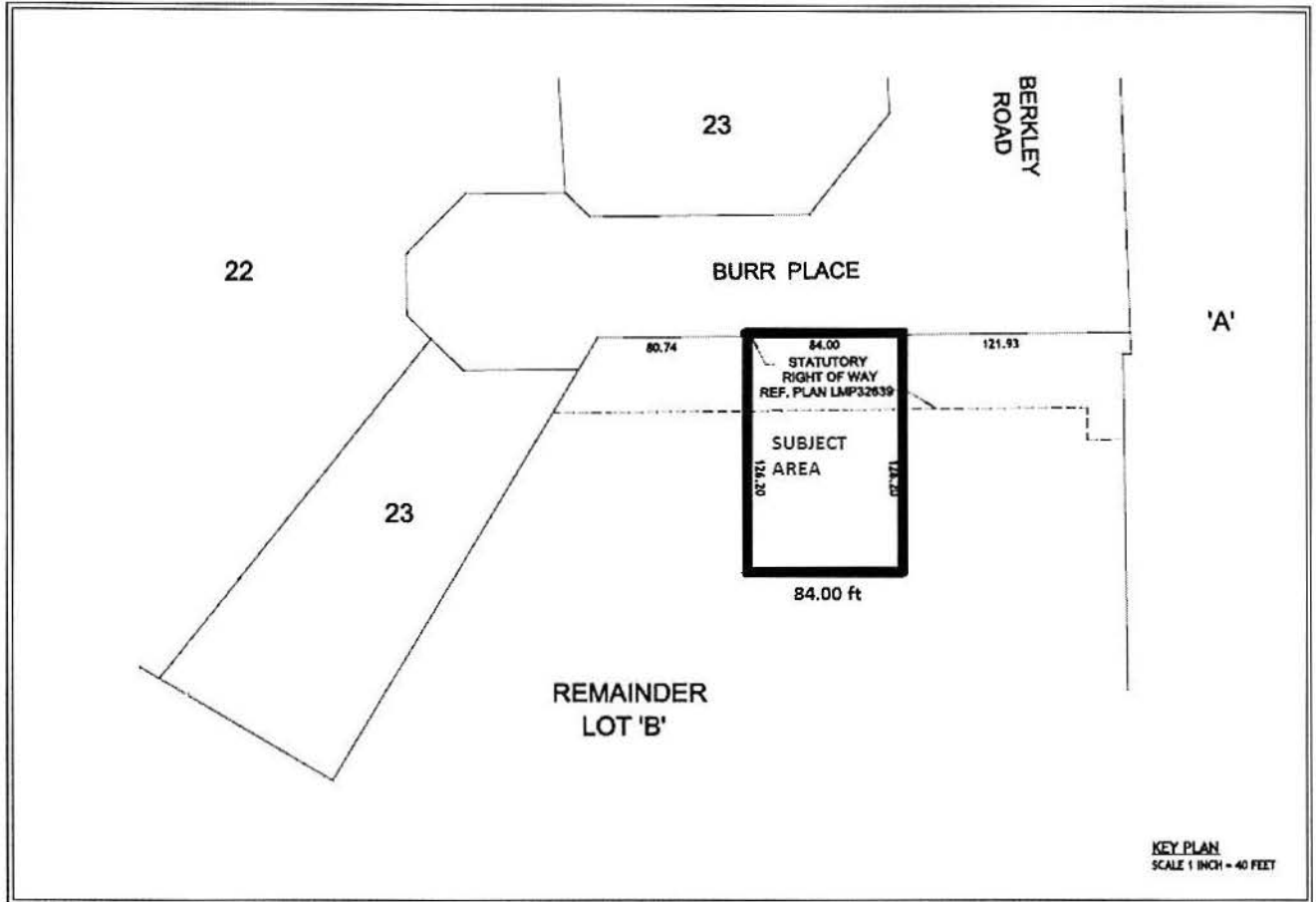
Mayor

Municipal Clerk

Certified a true copy

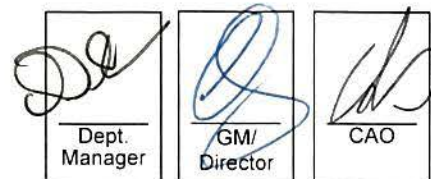
Municipal Clerk

Schedule A to Bylaw 8124



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AGENDA INFORMATION	
<input type="checkbox"/> Regular Meeting	Date: _____
<input type="checkbox"/> Workshop (open to public)	Date: _____



The District of North Vancouver REPORT TO COUNCIL

July 13, 2015
File: 5320-40

AUTHOR: David Desrochers, P. Eng., Manager Engineering Projects and Development

SUBJECT: **Local Area Service Bylaw 8137 - Lane Paving 100/200 Block Kensington Crescent, Southwest of Kensington Crescent and Northeast of Carisbrooke Crescent between West Braemar Road and Carnarvon Avenue**

RECOMMENDATION:

- (1) Pursuant to Section 211 of the Community Charter, a local area service project for the paving of the existing gravel lane located in the 100/200 block Kensington Crescent, be approved; and
- 2) Bylaw 8137 be now read a FIRST, SECOND, THIRD TIME.

REASON FOR REPORT:

A Local Area Service petition has been received and validated by the Municipal Clerk's office. Council approval is now required for the adoption of the construction bylaw.

SUMMARY:

A petition signed by ten (10) of the seventeen (17) or 59% of the benefiting property owners was received by Engineering Services to undertake paving and drainage works along the existing gravel lane located in the 100/200 blocks of Kensington Crescent, southwest of Kensington Crescent and northeast of Carisbrooke Crescent between West Braemar Road and Carnarvon Avenue. The Municipal Clerk has determined that this petition is valid. Sufficient funds are available to complete the works.

The benefiting properties and proposed Local Area Service works are shown on the appended Attachment 1. Properties which have signed the petition in favour of the lane paving are shown on Attachment 2. A construction bylaw is required in order to proceed with the works

BACKGROUND:

The petition is in compliance with Section 212 of the Community Charter and was certified by the Municipal Clerk on June 17, 2015.

The Municipal Clerk has determined that this petition is in accordance with Section 212 of the Community Charter; therefore, the works may proceed as a "Local Area Service Project" subject to Council's adoption of a construction bylaw.

SUBJECT: Local Area Service Bylaw 8137 - Lane Paving 100/200 Block Kensington Crescent, Southwest of Kensington Crescent and Northeast of Carisbrooke Crescent between West Braemar Road and Carnarvon Avenue

July 13, 2015

Page 2

The proposed lane improvements qualify for funding under the Local Area Service lane paving program. The paving work specified on the petition includes the installation drainage works, the repair and re-grading of the existing gravel base, asphalt paving having an average width of 3.4 metres (11.1 ft), and other related work as deemed necessary.

The Community Charter regulates Local Area Service projects, and as required under Section 212 of the Charter, Council shall, before adopting the construction bylaw, have a report stating:

- a) Description of the service;
- b) Definition of the boundaries of the local service area;
- c) Identification of the methods of cost recovery for the service, including the form of local service tax and the portion of the costs of service that are to be recovered by the local service tax;
- d) If applicable, identify the portion of the costs of the service that are to be recovered by a general property tax.

EXISTING POLICY:

Under Division 5 of the Community Charter, a Local Area Service Project may be achieved by Council Initiative (Section 213), or by Petition to Council (Section 212). This petition has met the criteria of Section 212, "Petition to Council for Local Area Service" which states in part:

1. The petition must be signed by the owners of at least 50% of the parcels that would be subject to the Local Area Service tax;
2. The persons signing must be the owners of parcels that in total represent at least 50% of the assessed value of land and improvements that would be subject to the Local Area Service tax.

Local Improvement Cost Sharing Bylaw 3711, a bylaw that establishes the owners' portion of the costs of certain classes of Local Area Service works, and provides for 50% cost sharing of paved surfacing on residential and commercial lanes. This lane paving project meets those conditions

ANALYSIS:

There are seventeen (17) parcels of real property abutting the proposed improvements. Of these parcels, ten (10) property owners or 59% signed the petition. The total assessed value of the properties owned by the owners that signed the petition represents 63% of the total value of all properties benefiting from the Local Area Service. As a result, conditions 1 and 2 comply with the Community Charter.

Timing/Approval Process:

The Municipal Clerk certified the Petition on June 17, 2015.

Financial Impacts:

The owners' share of the cost is \$32,500.00 and the District's share is \$32,500.00 for an estimated total of \$65,000.00. Funds for Local Area Service paving have been approved through the District's 2015 Financial Plan. The local improvement charges can be paid by the owners at the conclusion of the work, either in full without interest or by annual instalments over a five (5) year period at the prevailing market interest rate. The proposed lane paving is expected to reduce long term maintenance costs such as grading, pothole repairs and will also reduce dust problems for the residents during the summer months.

SUBJECT: Local Area Service Bylaw 8137 - Lane Paving 100/200 Block Kensington Crescent, Southwest of Kensington Crescent and Northeast of Carisbrooke Crescent between West Braemar Road and Carnarvon Avenue

July 13, 2015

Page 3

Environmental Impact:

Minor landscaping encroachments into the lane allowance may need to be trimmed back to facilitate construction. Otherwise, no significant environmental impacts are foreseen as a result of the proposed improvements to the lane.

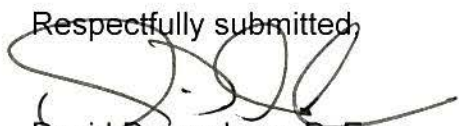
Public Input:

To our knowledge, the petitioner has made an effort to contact all property owners who potentially benefit from the improvements. Ten of the seventeen owners have signed the petition and wish to proceed with the paving as soon as possible.

Options:

1. Council may adopt Bylaw 8137 as proposed, and the project will proceed.
2. Alternatively, Council may, at their discretion, not enact the Bylaw.

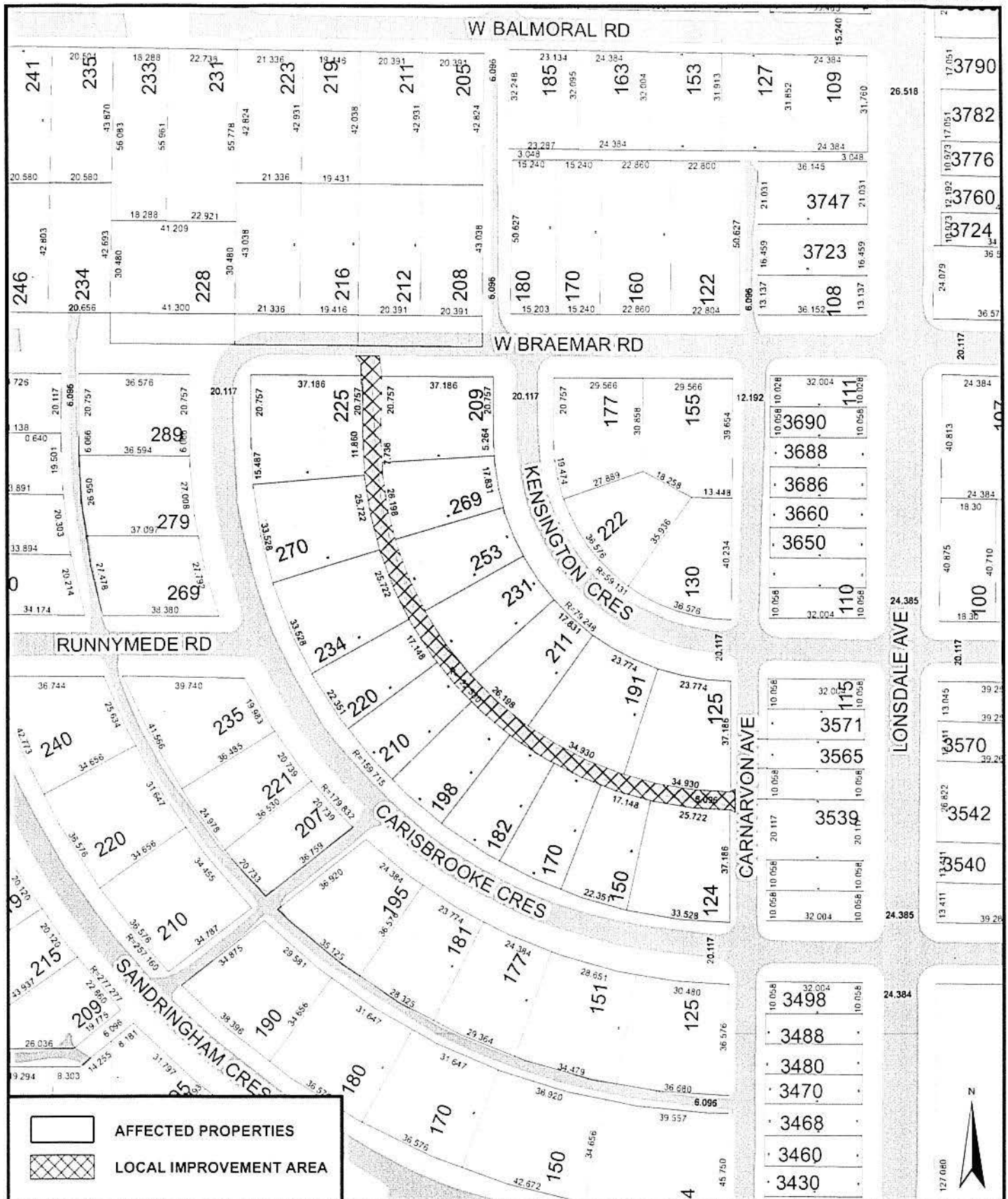
Respectfully submitted,



David Desrochers, P. Eng.

Manager Engineering Projects and Development Services

REVIEWED WITH:		
<input type="checkbox"/> Sustainable Community Dev. _____	<input type="checkbox"/> Clerk's Office _____	External Agencies:
<input type="checkbox"/> Development Services _____	<input type="checkbox"/> Communications _____	<input type="checkbox"/> Library Board _____
<input type="checkbox"/> Utilities _____	<input checked="" type="checkbox"/> Finance <i>RD</i> _____	<input type="checkbox"/> NS Health _____
<input type="checkbox"/> Engineering Operations _____	<input type="checkbox"/> Fire Services _____	<input type="checkbox"/> RCMP _____
<input type="checkbox"/> Parks _____	<input type="checkbox"/> ITS _____	<input type="checkbox"/> Recreation Com. _____
<input type="checkbox"/> Environment _____	<input type="checkbox"/> Solicitor _____	<input type="checkbox"/> Museum & Arch. _____
<input type="checkbox"/> Facilities _____	<input type="checkbox"/> GIS _____	<input type="checkbox"/> Other: _____
<input type="checkbox"/> Human Resources _____		



GIS DEPARTMENT
GEOGRAPHIC INFORMATION SYSTEMS
604-950-2311 www.geoweb.dnv.org gis@dnv.org

Lane Paving 100/200 blk Kensington
Between W Braemar and Carnarvon

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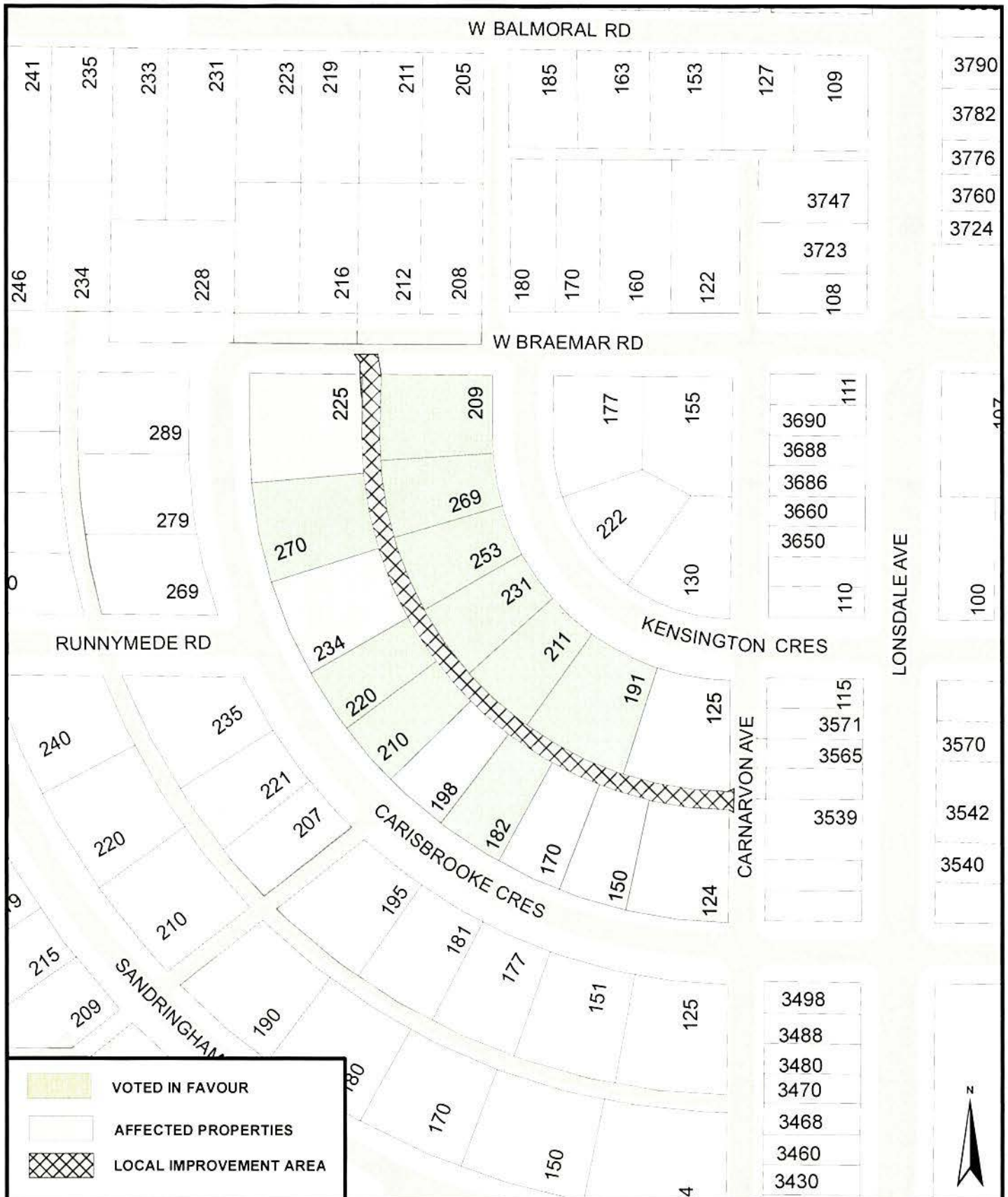
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LOCAL IMPROVEMENT PROJECT

Scale 1:6250

Published Date: May 7, 2015

LIP #
2015-03



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	<p>LIP # 2015-03</p>	

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The Corporation of the District of North Vancouver

Bylaw 8137

A bylaw to establish a local area service of generally constructing lane improvement works for the benefit of a part of the District of North Vancouver (LIP2015-03: Lane Paving 100/200 Block Kensington Crescent)

WHEREAS section 210 of the *Community Charter* authorizes a municipality to establish a local area service for the particular benefit of a part of the municipality, to be paid for in whole or in part by a property value tax or a parcel tax, or both, imposed only within the Local Service Area;

WHEREAS pursuant to section 212 of the *Community Charter*, the owners of parcels within the Local Service Area (as defined in this bylaw) have submitted a sufficient and valid petition proposing the service of the construction of road improvements on the lane located west of the 100/200 Block Kensington Crescent, south-west of Kensington Crescent and north-east of Carisbrooke Crescent between West Braemar Road and Carnarvon Avenue as shown in Attachment "1" appended to this bylaw, including the installation of storm sewers and appurtenances, the repair and re-grading of the existing gravel base, asphalt paving, and other works as necessary ("The Works"); and,

WHEREAS the Council for the District of North Vancouver wishes to provide the local area service contemplated in this bylaw and considers that such service will provide particular benefit to the Local Service Area.

The Council for The Corporation of the District of North Vancouver, in open meeting assembled, enacts as follows:

1. Citation

This bylaw may be cited as "Lane Paving – 100/200 Block Kensington Crescent Bylaw 8137, 2015".

2. Establishment of Local Area Service

The District hereby establishes the service of the construction and installation of the Works in accordance with the District's current engineering standards and specifications for the benefit of the local service area described in section 3 of this bylaw. The following will be included in the cost of the Works as necessary:

- i. all hard construction costs;
- ii. engineering and administration expenses;

- iii. cost of advertising and mailing of notices;
- iv. interest on temporary loans and discount and expenses relating to security issuing bylaws;
- v. compensation for land taken for the purpose of the Works or injuriously affected by it and for the expenses incurred by the corporation in connection with determining such compensation;
- vi. cost to acquire all such property, easements, rights-of-way, licences, rights or authorities that may be requisite or desirable for and in connection with the construction of the Works.

3. Local Area Service Boundaries

The local service area includes all of the parcels within the area outlined in bold on the plan attached to and forming part of this bylaw as Attachment "1" (the "Local Service Area").

4. Proportion of the Cost of the Works to be Specially Charged

The share or proportion of the total cost of the Works which will be specially charged against the parcels in the Local Service Area is 50%.

5. Cost Recovery Method

The total cost of providing the service established under section 2 of this bylaw is estimated to be \$65,000.00 and the portion of the cost to be charged against the parcels in the Local Service Area is estimated to be \$32,500.00 and will be recovered by means of a parcel tax imposed for a five (5) year period only on the parcels within the Local Service Area based on a single rate per taxable frontage of each parcel.

6. Frontage

The total actual frontage of the parcels in the Local Service Area is 415.816 metres and the total taxable frontage is 415.816 metres.

7. Frontage Tax Assessment Roll

A parcel tax roll shall be prepared for the purpose of imposing the parcel tax on each of the parcels in the Local Service Area based on the taxable frontage of the parcels determined in accordance with Local Improvement Cost Sharing Bylaw 3711.

READ a first time

READ a second time

READ a third time

ADOPTED

Mayor

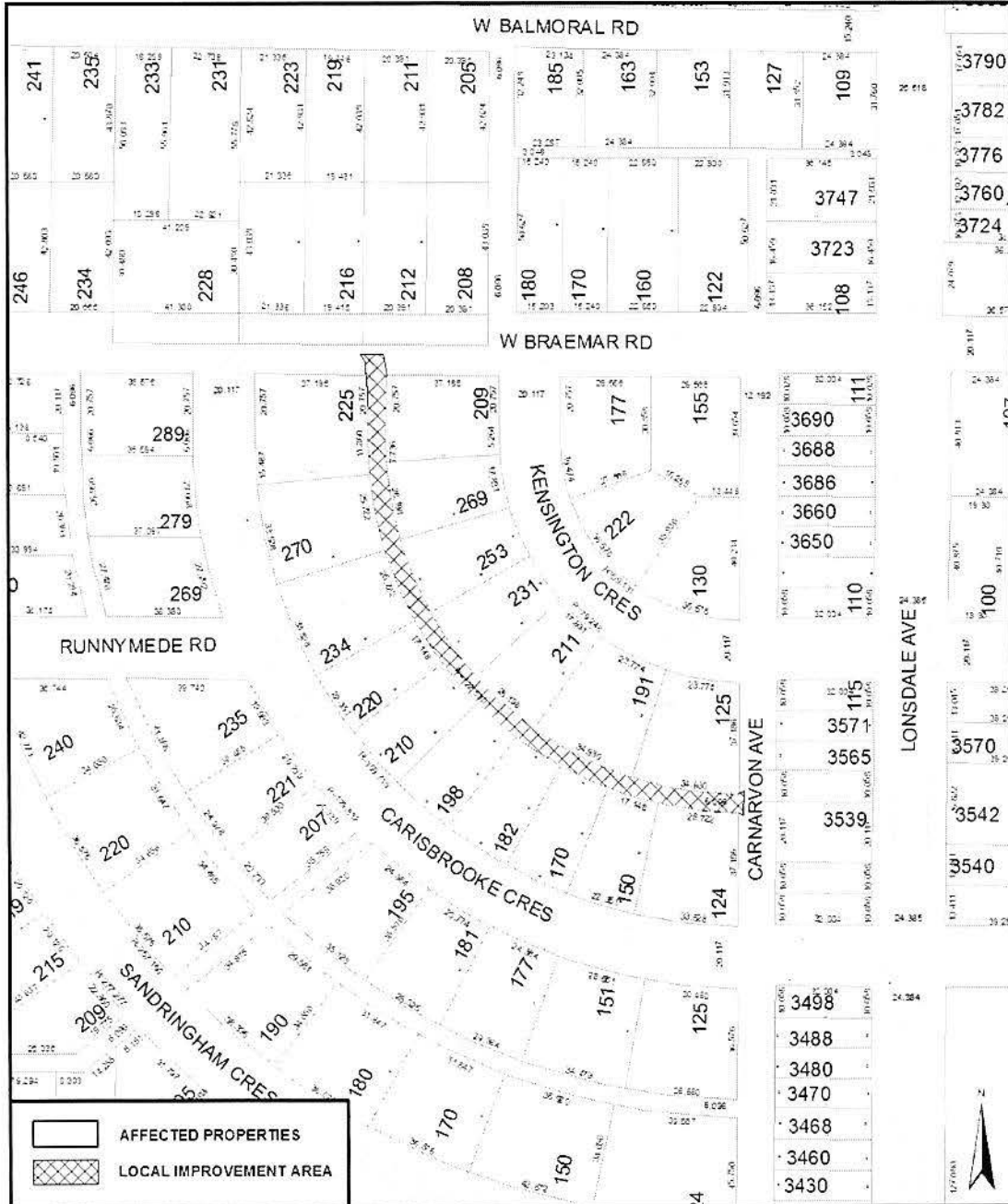
Municipal Clerk


Certified a true copy

Municipal Clerk

Attachment 1 to Bylaw 8137 Local Service Area

Attachment 1




 <p>GIS DEPARTMENT GEOGRAPHIC INFORMATION SYSTEMS 204-833-0211 FAX 204-833-0212 PAGE 1 OF 2</p>	<p>Lane Paving 100/200 blk Kensington Between W Braemar and Carnarvon</p>	
<p>LOCAL IMPROVEMENT PROJECT</p> <p>Scale 1:1,750 Published Date May 7, 2015</p>		<p>LIP # 2015-03</p>

AGENDA INFORMATION

- ☒ Regular Meeting
☐ Workshop (open to public)

Date: JULY 20, 2015
 Date: _____


 Dept.
 Manager


 GM/
 Director


 CAO

The District of North Vancouver REPORT TO COUNCIL

July 6, 2015
 File: 08.3060.20/053.14

AUTHOR: Erik Wilhelm, Planner

SUBJECT: Development Variance Permit 53.14 – 170 East Osborne Road

RECOMMENDATION:

It is recommended that Council issue Development Variance Permit 53.14 (Attachment A) to enable subdivision of 170 East Osborne Road.

REASON FOR REPORT:

The applicant has applied for a variance that requires Council's approval in order to allow for the subdivision of the subject property.

SUMMARY:

The applicant has applied for a lot width variance in order to enable a two lot subdivision. The variance relates to only one of the proposed lots. Both proposed lots comply with the minimum lot area requirements for the RSQ zone. The proposed subdivision is generally in keeping with the character of the neighbourhood.

BACKGROUND:

The subject property is located at 170 East Osborne Road. Carisbrooke Park is located west of the subject property. The site and surrounding residential neighbourhood is zoned Single-Family Queensdale (RSQ).



Below is a context map and air photo of the site. The existing residence and detached garage, situated on the northern/central portion of the site, have vehicle access via the lane on the eastern side of the property. There is an aging private tennis court adjacent to East Osborne Road on the southern portion of the property.



On February 17, 2014, Council considered Development Variance Permit 47.13 which would have facilitated subdivision of the property and allowed retention of the existing house and detached garage. There were five variances involved with the application. Four of the five variances involved zoning compliance variances in order to retain the existing residence and detached garage. A lot depth variance also formed part of the application due to the proposed panhandle lot configuration. Council did not issue DVP 47.13.

The applicant has now revised the subdivision design and variance application to address Council concerns raised as part of the previous application. The application has been modified in the following manner:

- Proposed demolition of the existing residence and detached garage (to remove 4 of the 5 previous setback variances);
- Implementation of a Section 219 covenant to ensure the new residence will not exceed the height of the existing house (to help protect existing views);
- Completion of comprehensive house designs with improved siting (to improve the view corridor for the neighbour to the north); and
- Amended subdivision design requiring a minor lot width variance of 0.43m (1.41 ft).

PROPOSAL:

The proposed subdivision will create two lots as seen in the adjacent plan.

Lot 1 will be accessed from the existing municipal lane on the eastern side of the site where vehicle access is currently located.

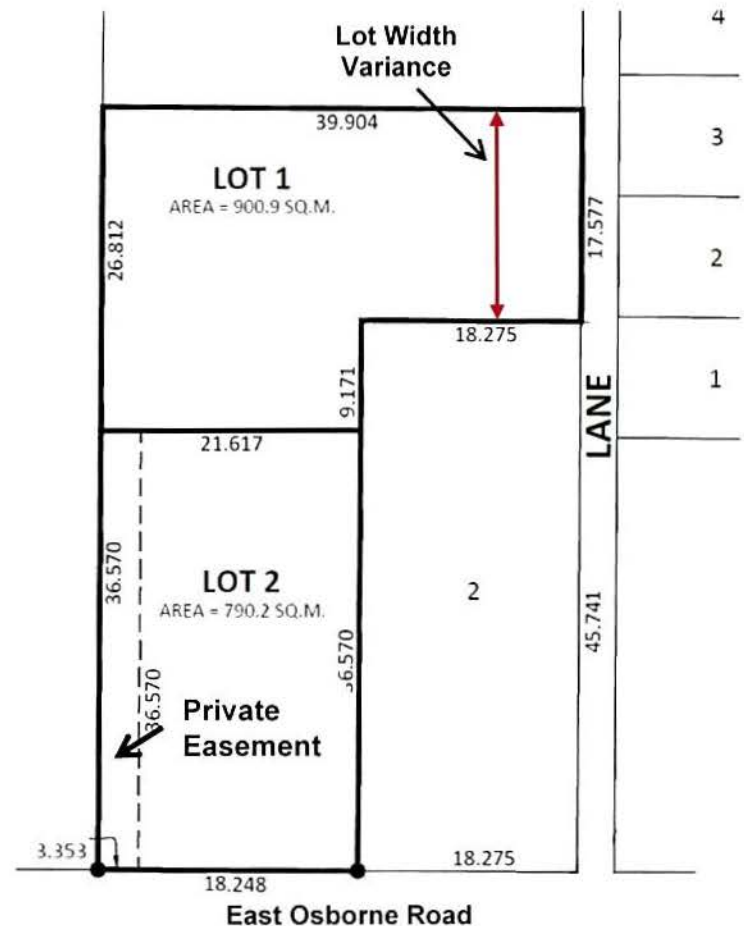
Lot 2 will be accessed off of East Osborne Road. A private easement along the west side of Lot 2 will be registered on title to provide Lot 1 with a secondary pedestrian connection to East Osborne Road.

The minimum lot area for a lot within this area of the RSQ zone is 660 m² (7104 sq ft). Both lots are oversized for the RSQ zone given that Lot 1 is approximately 900m² (9688 sq. ft.) and Lot 2 is approximately 790 m² (8503 sq. ft.).

New residences are to be built on each of the lots. In order to address existing sight line concerns of property owners to the north and northeast, the applicant's architect provided house plans as part of the subdivision proposal. The house plans form part of the development variance permit and will be registered on title (see Attachment A). Additionally, the applicant has volunteered to register a Section 219 covenant which limits the new house on Lot 1 to the same height as the existing house.

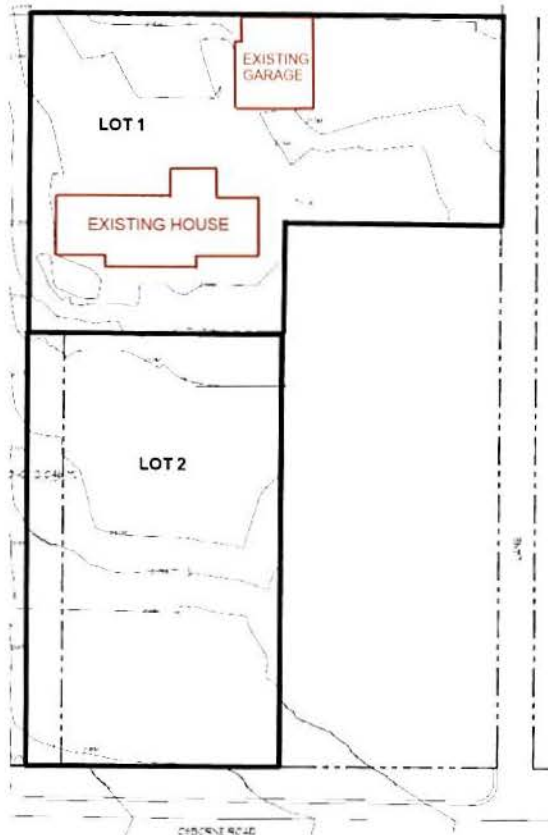
The house designs have aimed to preserve existing sightlines of neighbours to the northeast and to maintain an ample view corridor for the neighbour directly to the north by:

1. Introducing flat roof designs to produce the least height possible for a two storey residence;
2. Offsetting the uppermost storey of the home on Lot 1 to the east (allowing the northerly neighbour an improved view to the south/southwest); and
3. Limiting the height to the same height as the existing house onsite

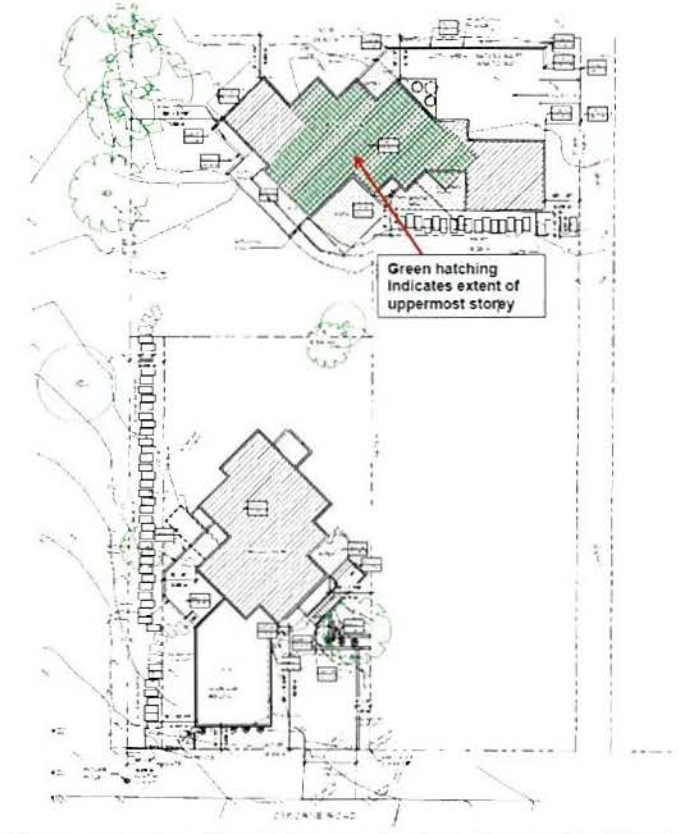


Height:

The siting of existing house and detached garage can be seen in Plan 1 below. The applicant has agreed to registration of a Section 219 covenant that limits the height of the new house to the same height as the existing house. The siting of the proposed house and the extent of the uppermost storey on Lot 1 can be seen in Plan 2 below. The siting of the uppermost storey further east onsite will expand the existing view corridor (towards the south/southwest) for the neighbour to the north.



Plan 1 - "Existing House Location"

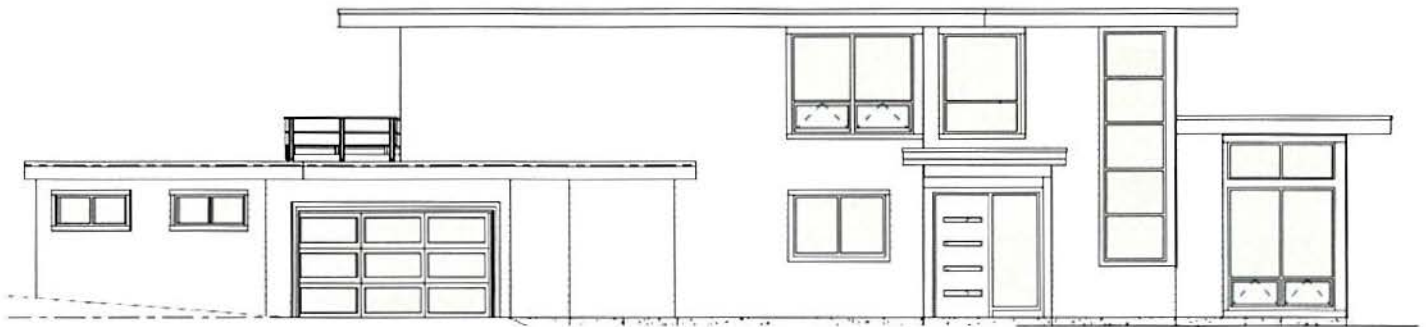


Plan 2 - "Proposed House Location"

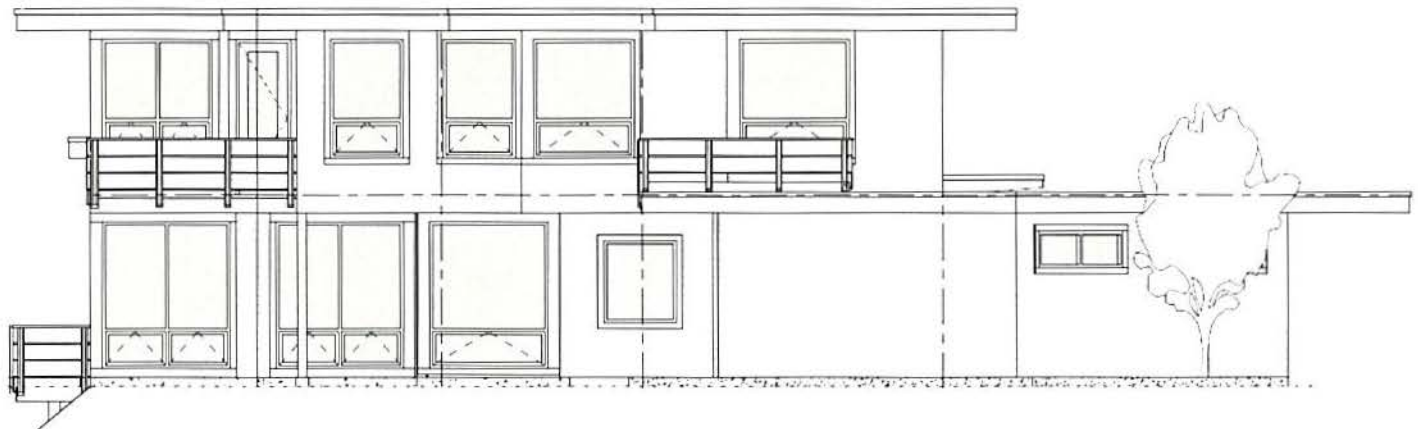
The RSQ zone allows house height maximums ranging from 6.71m (22 ft) (flat roof) to 9.75m (32 ft) (9 in 12 or greater roof slope). The flat roof design was chosen in order to provide the least total height possible. Variance approval would necessitate the houses to be built in accordance with the permit (i.e. flat roof designs); otherwise, the existing zoning could theoretically allow a house 32 ft. tall which could adversely affect existing views.

As the view corridor for the neighbour to the north has been improved, the neighbour has provided staff two separate letters of support for the application which commend the effort to preserve their views.

In order to provide additional context, the following drawings indicate the general architectural design of the proposed house on Lot 1. Additional drawings of the proposed houses for both lots are provided within the Development Variance Permit (Attachment A).



Lot 1 - "North Elevation" - as viewed looking southward



Lot 1 - "East Elevation" - as viewed from lane looking westward

Zoning Bylaw Compliance:

The variance required relates to lot width; the following table outlines the extent of the variance required as part of this application:

	Required (RSQ Zone)	Lot 1	Variance
Lot Width	18m (59.06 ft)	17.57m (57.64 ft)	0.43m (1.41 ft)

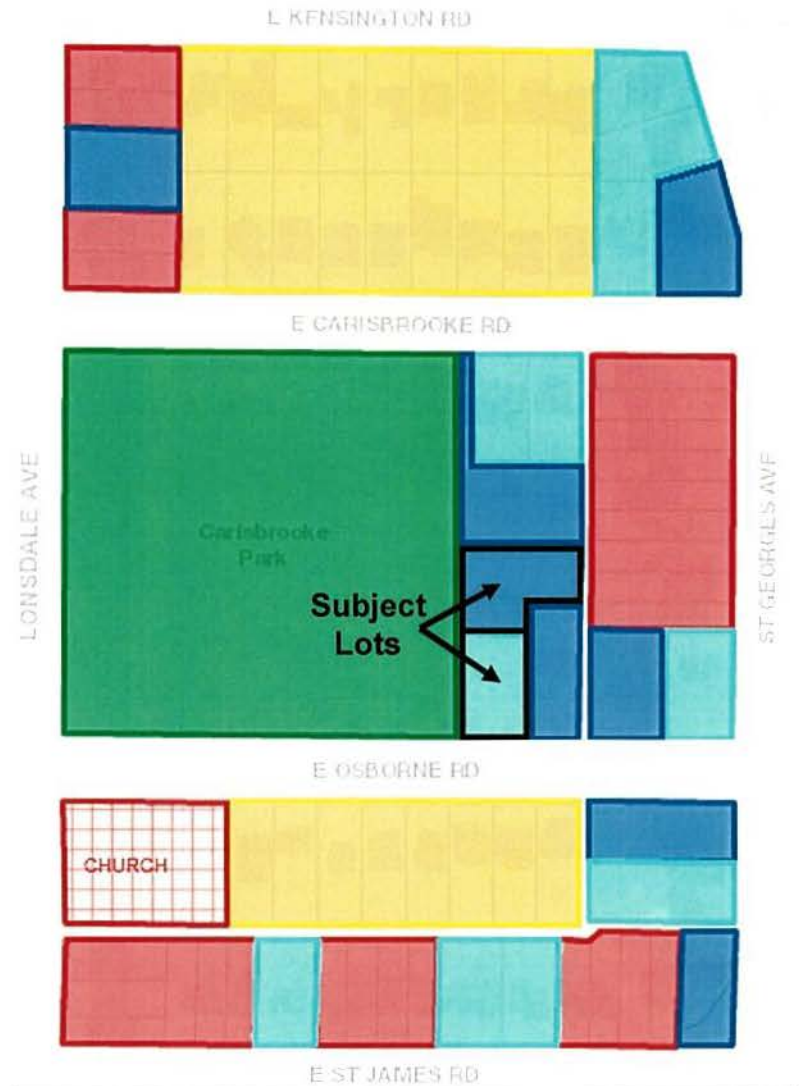
The rationale for lot width within the bylaw is to ensure a proper building envelope for housing. Given the minor nature of the variance proposed, and that Lot 1 will be approximately 900m² (9688 sq ft) in area, the variance will still allow for a proper building envelope and not adversely affect neighbourhood character.

ANALYSIS

Neighbourhood "Fit"

The adjacent map highlights the diverse lot sizes found in the local neighbourhood. Using the categories provided within the map, the subdivision would create a "Medium/Large" and "Large" lot within the neighbourhood.

The areas and configuration of the two lots proposed to be created will be compatible with the existing surrounding lots in the area and provide a similar housing form as seen in the immediate area.



Legend:

- "Small" Lots = 350 m² – 510 m²
- "Medium" Lots = 511 m² – 620 m²
- "Medium/Large" Lots = 621 m² – 800 m²
- "Large" Lots = 801+ m²

Sidewalk and Park Improvements

As part of the subdivision process, the applicant is required to provide sidewalk upgrades along the frontage of the property along East Osborne Road. In addition to required sidewalk upgrades, the applicant has volunteered to provide a sidewalk extension to the existing 'southeast' entrance to Carisbrooke Park and to arrange for new entrance fencing in that portion of the park. The extent of sidewalks to be provided is demarcated by the red arrow in the adjacent site plan.



Concurrence:

The proposal has been reviewed by staff from Planning, Parks, Engineering and Environmental Departments.

Parks Comments:

An existing fence and retaining wall supporting the tennis court were found to be located on District lands within Carisbrooke Park. The fence and retaining wall are required to be removed to the satisfaction of the Parks Department. A new fence, demarcating the boundary of Lot 1 and Carisbrooke Park, will be installed on Lot 1 as a condition of subdivision. The fence and sidewalk improvements to be provided by the applicant are considered beneficial to the park.

Arborist Comments:

The District Arborist reviewed an arborist report submitted by the applicant. The report outlines that there are two District trees, deemed in poor condition, proposed for removal which are located within Carisbrooke Park (west of the existing residence). The arborist supports removal of the two trees given that trees are not healthy. These trees will be replaced within Carisbrooke Park at a ratio of 2:1 through the subdivision process.

There are 11 trees located south of the existing residence (mainly within Lot 2) proposed for removal. The District Arborist accepts the rationale for removal given the need for services, building envelope and the poor health of the trees involved. Replanting of 11 new trees will be completed onsite and within Carisbrooke Park (at the discretion of Parks Department staff) through the subdivision process.

Given neighbourhood comments related to view reduction produced by tall trees within Carisbrooke Park, staff will ensure careful tree species selection during replanting onsite and within the park.

Public Input:

On January 21, 2015, an information letter outlining the application was mailed to 40 addresses within 75 metres of the subject property. Five comments noting objection to the application were received at that time which included the neighbour directly to the north.

Given the feedback garnered from the notification letter, the applicant held an informal "Question and Answer" open house on May 6, 2015, to further discuss with neighbours the details of the subdivision proposal. The open house was attended by 8 people.

At present, based on all comments received, there are five nearby households (including the neighbour to the north) in support of the application, one household with general questions and two households noting opposition to the application.

Support:

Neighbours were supportive of the architectural design proposed as part of the project and felt that the flat roof design maintained the view corridors and sight lines in the area. The neighbour directly to the north also supported the house design on Lot 1 which provides (a) the flat roof design; (b) an offset uppermost storey in order to maintain his primary view towards the south/southwest; and (c) presents a better alternative to a house that could be theoretically higher in accordance with the RSQ zoning.

Non-Support:

Some neighbours cited concerns with the perceived change in neighbourhood character that would be facilitated by subdivision of the property. Additionally, the grade of the existing lane, loss of treed character adjacent to Carisbrooke Park and concern that approval of the variance would set a precedent allowing further subdivision in the neighbourhood were other points raised.

With respect to concerns raised:

- **Neighbourhood Character:** The neighbourhood character will remain intact if the property is subdivided. As outlined within this report, the lot sizes to be created will be comparable (if not larger) to other lots in the area. Furthermore, the conventional fronting Lot 2 will provide a residential lot width and streetscape already seen on East Osborne Road.
- **Laneway:** The laneway will not be receiving additional density through the subdivision and the existing grade of the lane is not proposed to be changed. The Engineering Department has no immediate concerns related to the laneway. If the property is not subdivided, a new single family home would still be required to access the property via the laneway and not from East Osborne Road.
- **Park Character:** Two trees are proposed for removal within Carisbrooke Park because the trees have been deemed 'unhealthy' and four trees are to be replanted

within the park. There are 11 'unhealthy' trees slated for removal onsite; these 11 trees will be replanted both onsite and within Carisbrooke Park to ensure the continued treed character in the area.

- **Setting of Precedent:** Variances and other decisions of Council are decided on individual merit and Council is not bound by previous decisions.

Municipal notification advising that Council will be considering whether to issue a Development Variance Permit will be circulated. Response to the notification will be provided to Council prior to consideration of this application.


Conclusion:

Proposed Development Variance Permit 53.14 will enable subdivision of the property while protecting view corridors for surrounding properties and provide practical building envelopes on both lots within the subdivision; therefore, it is recommended that Council issue DVP53.14.

Options:

The following options are available for Council's consideration:

1. Issue Development Variance Permit 53.14 (Attachment A) to enable subdivision at 170 East Osborne Road. (staff recommendation); or
2. Deny Development Variance Permit 53.14.



Erik Wilhelm
Planner

Attach

Attachment A – DVP 53.14

REVIEWED WITH:		
<input type="checkbox"/> Sustainable Community Dev. _____	<input type="checkbox"/> Clerk's Office _____	External Agencies:
<input type="checkbox"/> Development Services _____	<input type="checkbox"/> Communications _____	<input type="checkbox"/> Library Board _____
<input type="checkbox"/> Utilities _____	<input type="checkbox"/> Finance _____	<input type="checkbox"/> NS Health _____
<input type="checkbox"/> Engineering Operations _____	<input type="checkbox"/> Fire Services _____	<input type="checkbox"/> RCMP _____
<input type="checkbox"/> Parks & Environment _____	<input type="checkbox"/> ITS _____	<input type="checkbox"/> Recreation Com. _____
<input type="checkbox"/> Economic Development _____	<input type="checkbox"/> Solicitor _____	<input type="checkbox"/> Museum & Arch. _____
<input type="checkbox"/> Human resources _____	<input type="checkbox"/> GIS _____	<input type="checkbox"/> Other: _____

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THE CORPORATION OF THE DISTRICT OF NORTH VANCOUVER**DEVELOPMENT VARIANCE PERMIT 53.14**

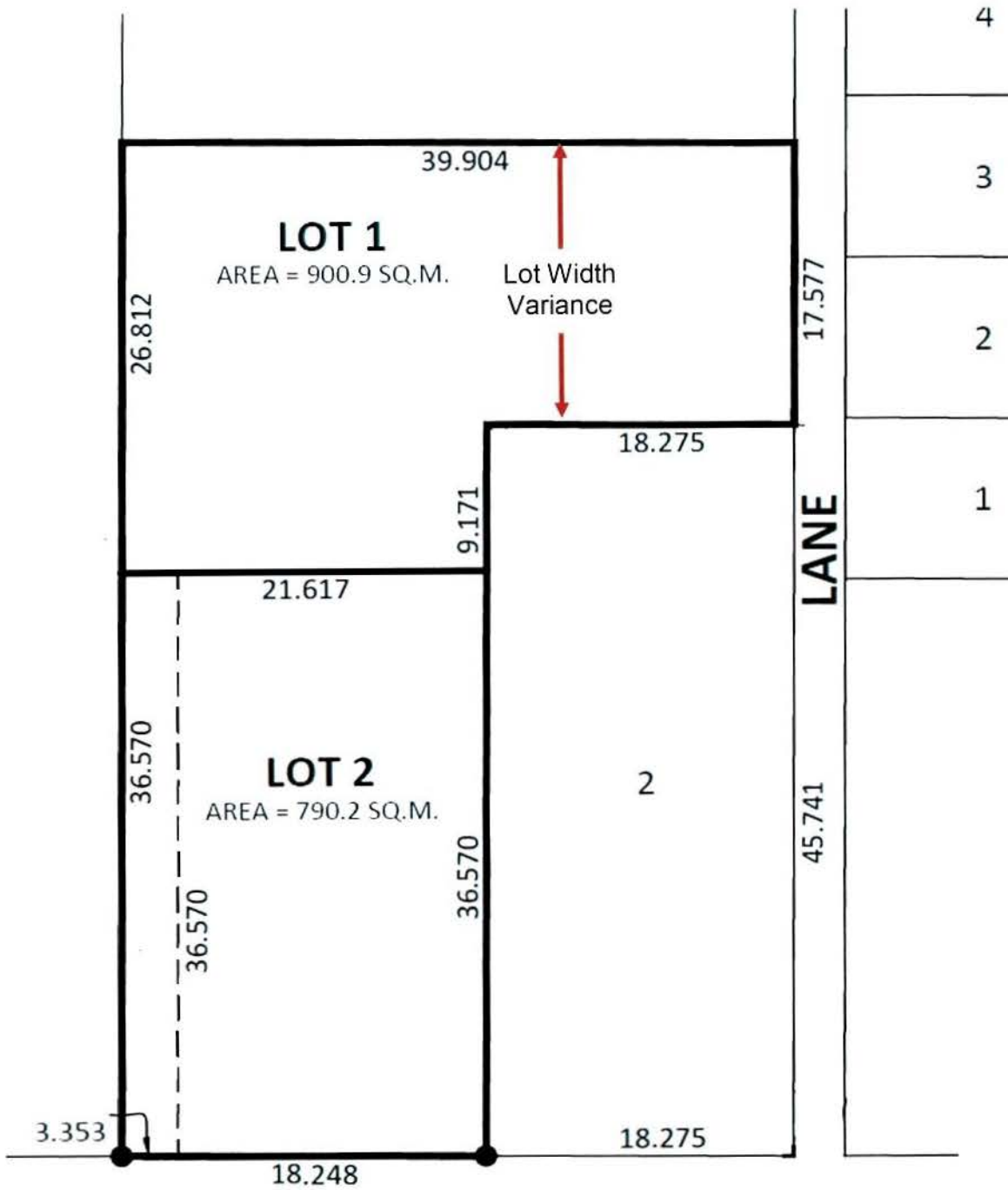
This Development Variance Permit 53.14 is hereby issued by the Council of The Corporation of the District of North Vancouver to Iris Yee Ming Chiu and Eve Yi Yin Chiu to facilitate a subdivision located at 170 East Osborne Road legally described as Lot 1, Blocks 19 and 20, District Lot 2026, Plan 12751 (PID: 008-829-187), subject to the following terms and conditions:

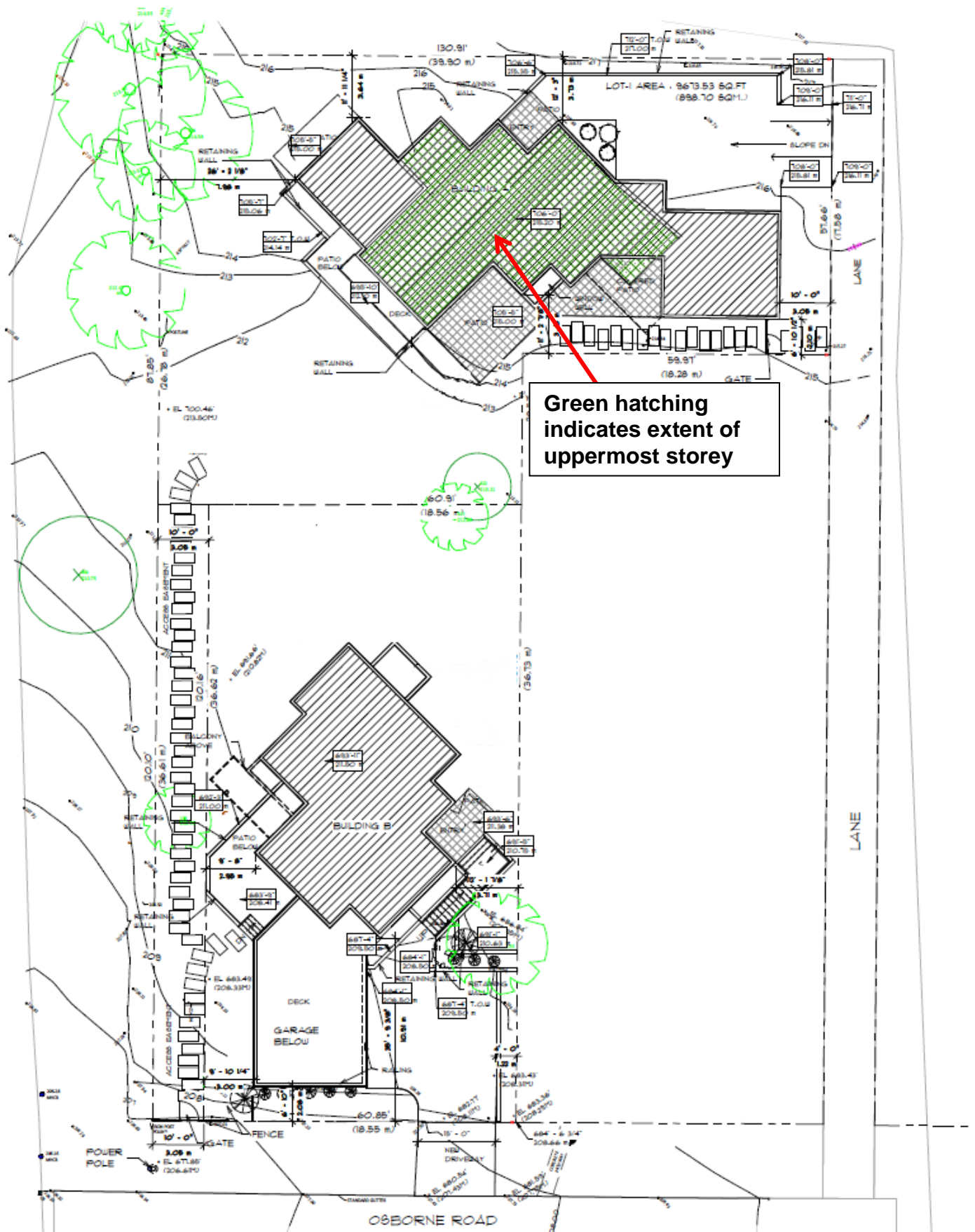
- A. The following Zoning Bylaw regulation is varied under subsection 922(1)(b) of the Local Government Act:
1. The minimum lot width is decreased from 18 metres (59.06 feet) to 17.57 metres (57.64 ft.) and applies only to the lot layout as illustrated in the attached drawing (DVP53.14A);
 2. The property must be developed in accordance with attached drawings DVP53.14B-DVP53.14H; and
 3. The above variance is granted subject to registration of a Section 219 Covenant registered on the property in favour of the District in priority of all financial charges to:
 - ensure that the house designs outlined in attached drawings DVP53.14B-DVP53.14H are built on the lands; and
 - the height of the house on Lot 1 is limited to a total height no greater than 220.44 ASL (Above Sea Level).
- B. The following requirement is imposed under subsection 926(1) of the Local Government Act:

Substantial completion of subdivision as determined by the Approving Officer Licences shall commence within two years of the date of this permit or the permit shall lapse.

Mayor

Municipal Clerk





DVP53.14F

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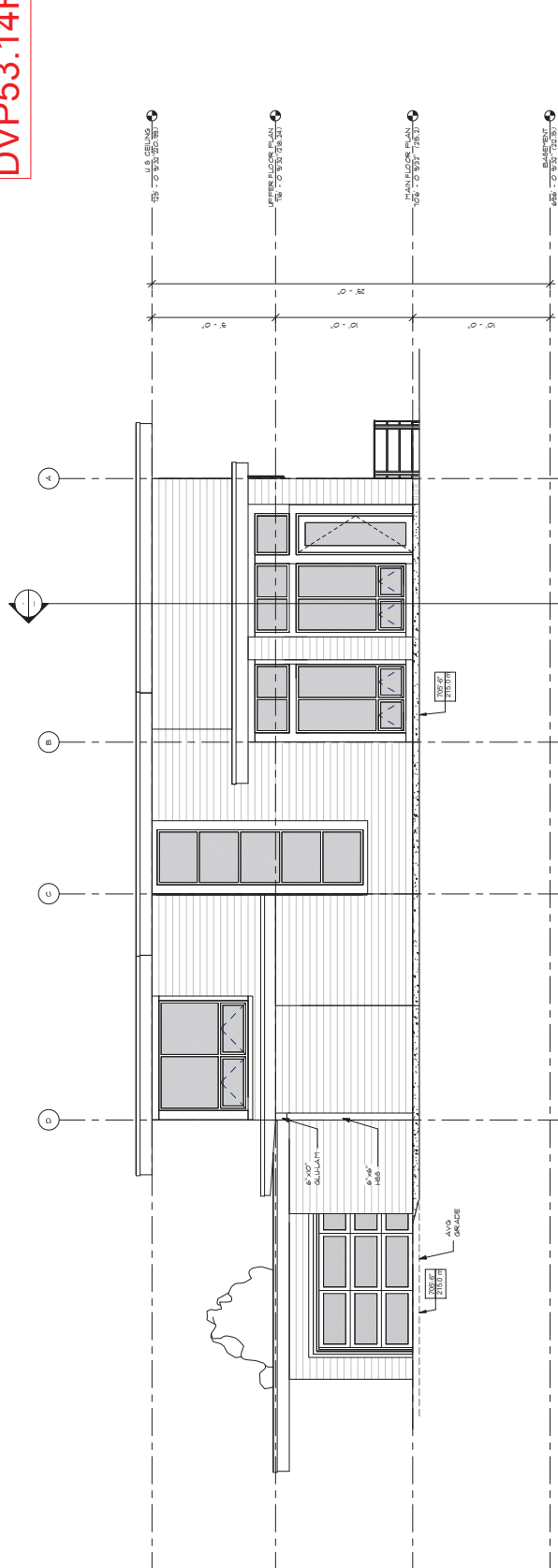
DATE	2013.02.28
BY	DOUGLAS R. JOHNSON
FOR	DOUGLAS R. JOHNSON ARCHITECT LTD.
PROJECT	1144 WEST 3RD ST. VANCOUVER, BC V6P 3P9
SCALE	1/4" = 1'-0"

TWO SISTERS
1144 East Osborn North
Vancouver BC
SHEET NO. 0000000000
BUILDING & ELEVATIONS

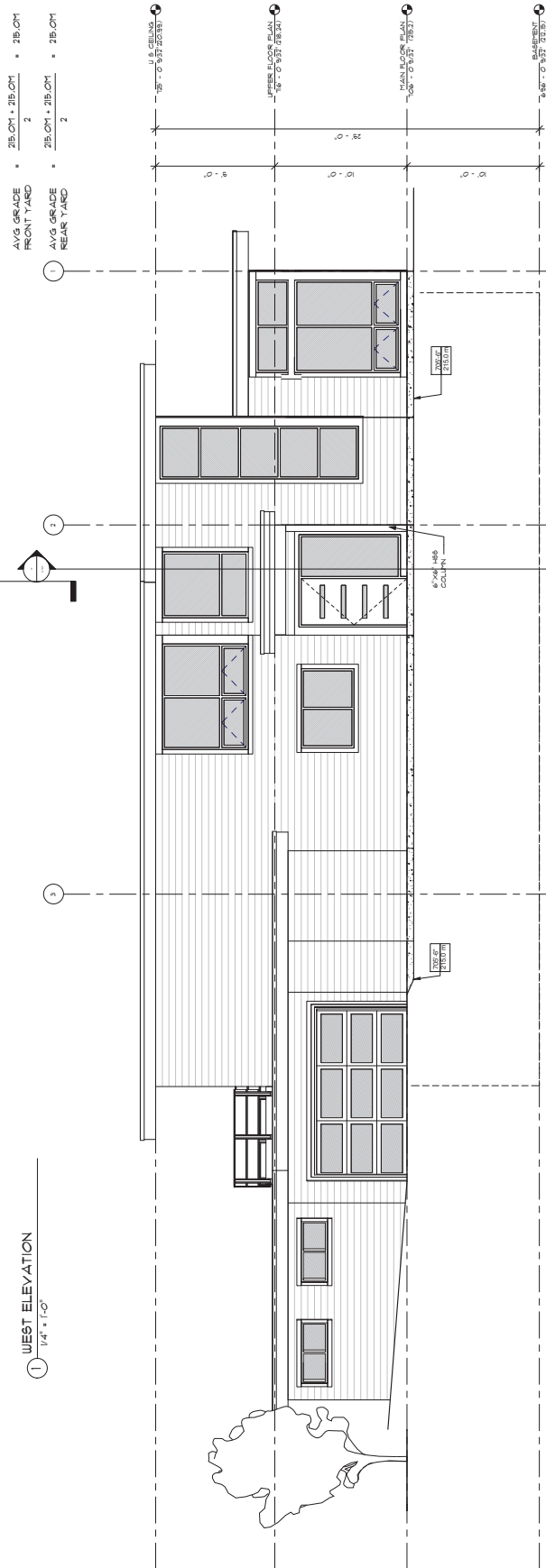
DOUGLAS R. JOHNSON
ARCHITECT LTD.
#374-901 WEST 3RD ST.
NORTH VANCOUVER, BC V6P 3P9
PH: (604) 998-3381
FAX: (604) 998-0217

SCALE	As indicated
PROJECT NO.	---
DATE	28 FEB 2013
BY	DOUGLAS R. JOHNSON
FOR	DOUGLAS R. JOHNSON ARCHITECT LTD.
PROJECT	1144 WEST 3RD ST. VANCOUVER, BC V6P 3P9
SCALE	1/4" = 1'-0"

DP-5.11
Appr
1/4" = 1'-0"



1 WEST ELEVATION
1/4" = 1'-0"



2 NORTH ELEVATION
1/4" = 1'-0"

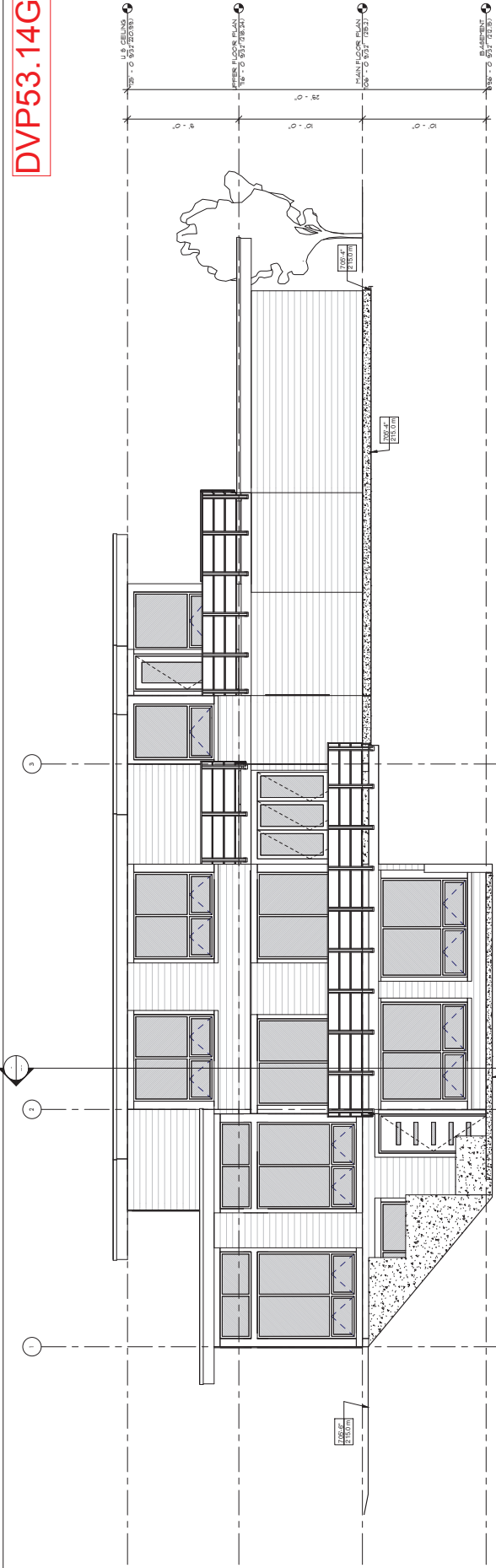
DVP53.14G

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DATE	19 MAY 2014
DESIGNER	DOUGLAS R. JOHNSON
PROJECT	170 East Osborn North Vancouver BC
PROJECT NO.	170 EAST OSBORN NORTH
PROJECT NAME	170 EAST OSBORN NORTH
PROJECT ADDRESS	170 EAST OSBORN NORTH
PROJECT CITY	VANCOUVER BC
PROJECT STATE	BC
PROJECT COUNTRY	CANADA
PROJECT PHONE	(604) 998-3381
PROJECT FAX	(604) 998-0217
PROJECT EMAIL	DOUGLAS@DOUGLASRJOHNSON.COM
PROJECT WEBSITE	WWW.DRJOHNSON.COM
PROJECT SOCIAL MEDIA	FACEBOOK: DRJOHNSONARCHITECT TWITTER: DRJOHNSONARCHITECT PINTEREST: DRJOHNSONARCHITECT

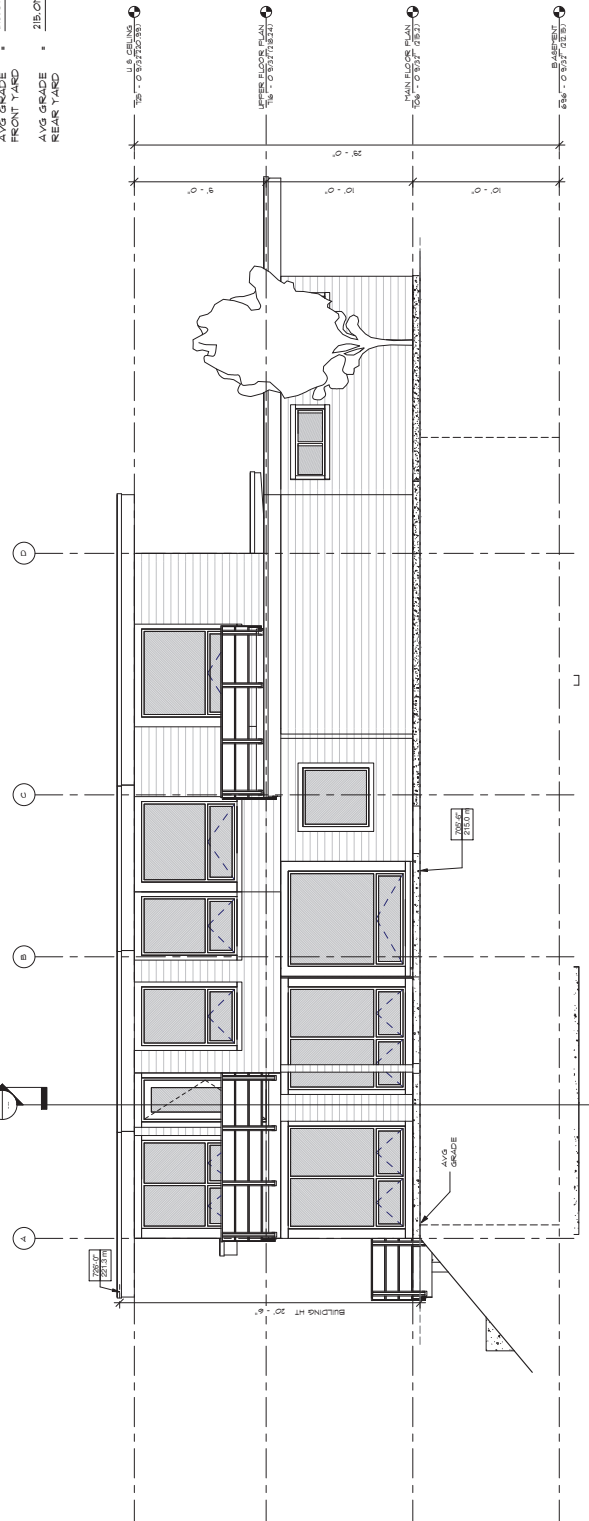
TWO SISTERS
170 East Osborn North
Vancouver BC
SHEET NO. 22 OF 22
BUILDING A ELEVATIONS

DOUGLAS R. JOHNSON ARCHITECT LTD. #374-901 WEST 3RD ST. NORTH VANCOUVER BC V7P 3P9 PH: (604) 998-3381 FAX: (604) 998-0217	SCALE: As indicated DATE: 28 FEB 2013 BY: [Signature] APPROVER: [Signature] PROJECT NO.: 170 EAST OSBORN NORTH SHEET NO.: 22 OF 22 PROJECT NAME: 170 EAST OSBORN NORTH PROJECT ADDRESS: 170 EAST OSBORN NORTH PROJECT CITY: VANCOUVER BC PROJECT STATE: BC PROJECT COUNTRY: CANADA PROJECT PHONE: (604) 998-3381 PROJECT FAX: (604) 998-0217 PROJECT EMAIL: DOUGLAS@DOUGLASRJOHNSON.COM PROJECT WEBSITE: WWW.DRJOHNSON.COM PROJECT SOCIAL MEDIA: FACEBOOK: DRJOHNSONARCHITECT TWITTER: DRJOHNSONARCHITECT PINTEREST: DRJOHNSONARCHITECT
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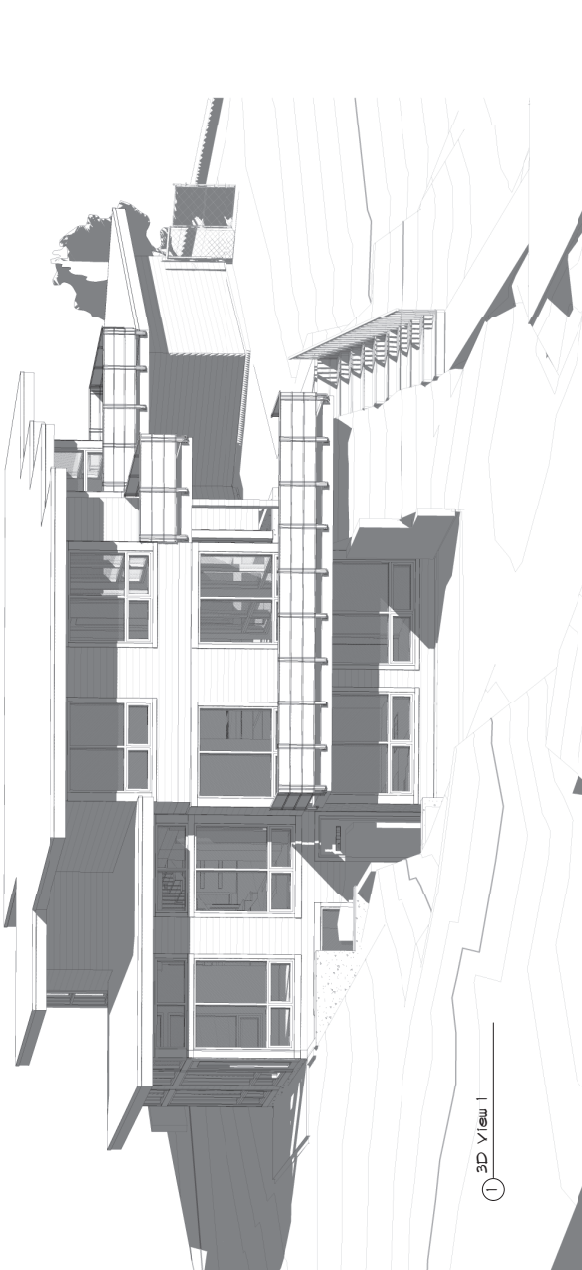
AVG GRADE FRONT YARD $\frac{215.01 + 215.01}{2} = 215.01$
AVG GRADE REAR YARD $\frac{215.01 + 215.01}{2} = 215.01$

1 SOUTH ELEVATION
1/4" = 1'-0"

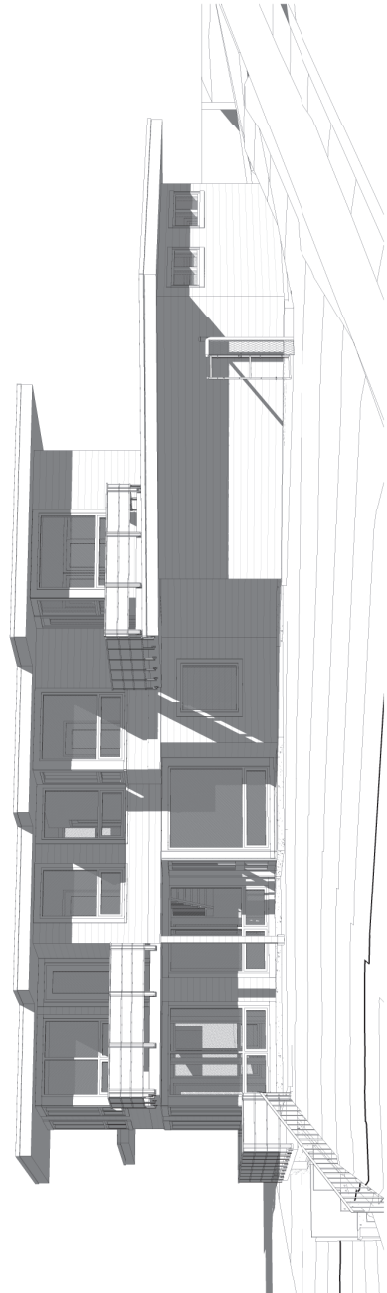


2 EAST ELEVATION
1/4" = 1'-0"

DVP53.14H



① 3D View 1



② 3D View 2

TWO SISTERS
170 East Osborn North
Vancouver BC
SHEET: 02 DESCRIPTION


BUILDING A VIEWS

PROJECT: 170 East Osborn North
DRAWN: 1
DATE: 28 FEB 2013
SCALE: 1/8" = 1'-0"

DOUGLAS R. JOHNSON
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#374-901 WEST 3RD ST.
NORTH VANCOUVER BC V7P 3P9
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SCALE: 1/8" = 1'-0"

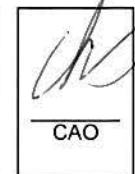
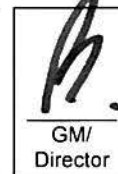
DATE: 28 FEB 2013

APPROVER: 
DP-5.31

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AGENDA INFORMATION	
<input checked="" type="checkbox"/> Regular Meeting	Date: <u>JULY 20/2015</u>
<input type="checkbox"/> Workshop (open to public)	Date: _____



The District of North Vancouver REPORT TO COUNCIL

July 7, 2015
File: 13.6680.20/005.000

AUTHOR: Suzy Lunn, Policy Planner

SUBJECT: Delbrook Community Recreation Centre Site Planning

RECOMMENDATION:

1. THAT Council direct staff to proceed with a planning and public engagement process for the Delbrook site based on the scope and objectives as outlined in this report.

REASON FOR REPORT:

Redevelopment of the new community recreation centre on the William Griffin site is anticipated to be complete by the fall of 2016, at which time the North Vancouver Recreation and Culture Commission offices and programs will be transferred from the Delbrook site to the new facility.

The purpose of the report is to seek Council's direction on the scope and objectives for the proposed planning and public engagement process. By defining the parameters up front, our intention is to embark on a proactive and collaborative planning and public engagement process, to inform the future of the Delbrook site.

SUMMARY:

The Delbrook site provides an opportunity to create housing choices in close proximity to schools, the new recreation centre, child care, shopping and bus routes, while continuing to protect and enhance Mission Creek and create a neighbourhood park. Partial development of the Delbrook site also has the potential to generate revenue for the District and to have positive implications for District tax payers.

Staff are proposing a planning and District-wide public engagement process to establish a concept plan for the Delbrook site. This planning process would also include opportunities to retain a portion of the site for parks and open space, a child care facility and Mission Creek habitat enhancement.

BACKGROUND:

Site Information

The Delbrook site, located at 600 West Queens Road, is approximately 17,607m² (4.3 acres) in area and is currently designated Institutional in the Official Community Plan and zoned Public Assembly. It includes the Delbrook North and South buildings, two parking lots, three lit tennis courts, a public children's play area, and a child care facility. Mission Creek marks the western boundary of the site.

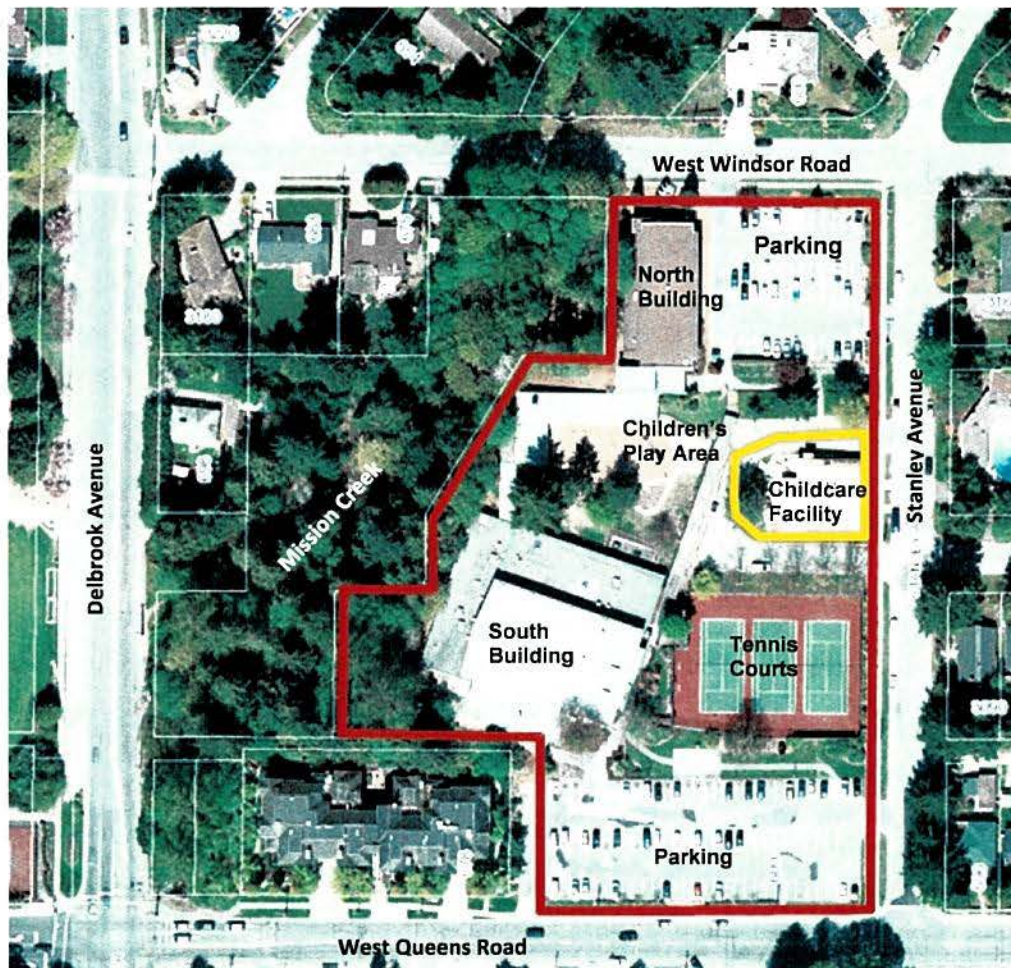


Figure 1. Delbrook Site Map

History

The Delbrook site was first occupied by the Delbrook High School between 1956 and 1977. The District of North Vancouver (District) bought the site from the North Vancouver School District in 1981. Since that time, the site and remaining buildings have been operated by the North Vancouver Recreation and Culture Commission (NVRC) as a community recreation centre. In 2006 an Indoor Recreation Facility Plan commissioned by the Recreation Commission recommended the consolidation of the William Griffin and Delbrook Community Recreation Centres.

Existing Buildings

William Griffin Community Recreation Centre closed in December, 2013 with the new facility anticipated to open in the fall of 2016. Once the programs transfer to the new community recreation centre, the buildings on the Delbrook site will be underutilized. Both the north and south recreation buildings on the Delbrook site are in poor condition and at the end of their useful lives. Since the decision to rebuild the consolidated community recreation centre, major component renewals and capital investments to the Delbrook recreation buildings have been deferred. Re-purposing the buildings is not recommended as they are in poor condition and the District Facilities Department estimate the preliminary costs to extend the life of these buildings by 25 years is \$11.3 million.

Tenants

Capilano Community Services Society and its partner organizations currently occupy the bottom floor of the north building. Staff are currently exploring the potential for relocating this society into a temporary home at the Capilano United Church (recently acquired by the District), until their new home in the Capilano recreation centre is built.

A privately operated child care facility, Little Rascals, runs out of a separate building on this site, adjacent to Stanley Avenue. This operation has a ground lease which expires in 2023.

Report Back on Preliminary Community Discussions

In October 2013, Council decided to defer the commencement of the formal public engagement to consider options for future use for the Delbrook site until 2015. In the meantime, staff has held informal discussions with the Delbrook Community Association Executive, attended Delbrook Dialogues sessions, reviewed the research reports authored by their planning students, and had preliminary, one-on-one discussions with residents and stakeholders.

Preliminary input from community members indicates a keen interest in engaging in a meaningful consultation process. Issues raised for consideration include: potential for increased traffic, environmental protection, retention of some parkland, opportunities for revenue generation, consideration of continued community uses on the site, potential retention of the tennis courts and children's play area, need for affordable housing, and opportunities to improve connectivity and circulation through the site. Further input will be sought on these issues through the planning process.

EXISTING POLICY:

Official Community Plan Bylaw 7900 (OCP)

The OCP identifies two key issues that relate to Delbrook site planning: the challenging demographic profile and the lack of housing diversity and affordability. The population pyramid in the OCP illustrates the missing generation, people aged 20 to 40 years in the District of North Vancouver. The Delbrook site is a strategic location for providing a diversity of housing form, type and tenure that may accommodate the missing generation as well as seniors looking to downsize.

While the OCP directs the majority of new growth into the four key town and village centres, Neighbourhood Infill Plans can be undertaken for smaller areas outside of centres where a neighbourhood may be in transition. Neighbourhood Infill Planning process can also be used to assess sensitive densification of appropriate scale at strategic locations along transit corridors (OCP, Section 2.4).

Meaningful public and stakeholder consultation is an integral component of this planning exercise. Neighbourhood Infill Plans are expected to use the portfolio of land use designations provided by the OCP and may lead to an amendment of the OCP land use map.

Public Assembly Land Strategy

The Public Assembly (PA) Land Strategy provides principles and criteria to be used as a framework to evaluate proposed changes to PA lands. This framework supplements the evaluation that is already undertaken as part of a rezoning or OCP amendment. The criteria are not intended to prevent changes to PA lands from taking place, but to help ensure that any change is in the public interest and provides an overall benefit to the community.

Parks Open Space and Strategic Plan

The Parks and Open Space Strategic Plan indicates that the neighbourhood surrounding the Delbrook site is well served by district and community parks and school outdoor play areas. Improvements to the site provides an opportunity to add needed neighbourhood park space.

ANALYSIS:

Proposed Planning Process

Subject to Council's direction, staff recommends proceeding with a planning process, including local and District-wide public engagement to develop a concept plan for the Delbrook site. The following factors render the site as suitable for a Neighbourhood Infill Planning process:

- size of the Delbrook site and range of other potential uses (residential, institutional and parkland);
- location on an existing transit network and a future frequent transit network;
- proximity to the Queensdale and Edgemont Village Centres;
- location near the intersection of two arterial roads (Queens Road and Delbrook Avenue/Westview Drive); and
- proximity to areas in transition (nearby Cypress Gardens and Westview Shopping Mall are identified as a Special Study Areas in the City of North Vancouver's OCP).

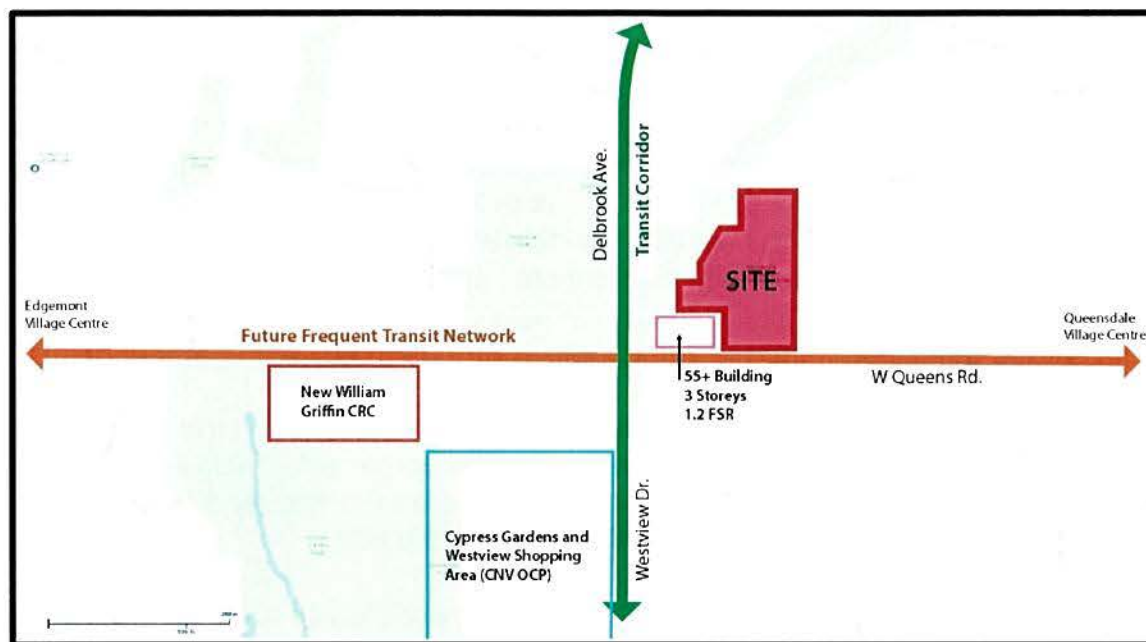


Figure 2. Delbrook geographical context map

The proposed neighbourhood planning process would involve working with the local and the broader District-wide community to:

- identify potential issues and opportunities for the Delbrook site;
- establish a vision and set of principles to guide the planning process;
- explore conceptual planning options (with varying built forms, densities, and building heights); and
- establish a recommended concept plan for the site that meets broad community needs and respectfully considers the local context and character.

As a starting framework, it is anticipated that the planning process will be guided by the following objectives:

- Protect and enhance the Mission Creek riparian corridor;
- Retain a portion of the site for parks and open space;
- Seek to enhance the pedestrian connectivity and network through the site;
- Retain space for a child care facility and children's play area;
- Seek to include a diversity of housing choices, including affordable housing, to meet a range of community needs;
- Consider opportunities to sensitively integrate redevelopment within the neighbourhood context; and
- Consider opportunities for responsible use of the site to generate revenue and to benefit District tax payers.

With the aid of an urban land economist, the planning process will include a comparison of the revenue generation potential arising under the different conceptual planning options.

Proposed Public Engagement Process

Meaningful engagement with the community will be critical to the success of this planning exercise. Staff recommends striking a working group representing diverse community interests, to provide advice to the District and consultants in planning for the future of the Delbrook site. The working group will be comprised of stakeholders from the immediate neighbourhood as well as across the District. The group is anticipated to:

- act as a sounding board to evaluate key deliverables at each step in the process;
- provide input on conceptual planning options; and
- connect the District with their networks to help disseminate information and to encourage participation from diverse interest groups at key engagement points.

Staff will also consult with the broader community at key milestones in the planning process. This may include seeking input on opportunities to incorporate affordable / rental housing on the site, to consider the amount and type of open space on the site, and to comment on the planning options and recommended conceptual plan for the site.

Planning and Communications will work together using a variety of tools to engage the wider audience such as the District's web site and social media channels, media relations, open houses and other face-to-face input opportunities, as well as conducting a District-wide professional phone survey to ensure statistically relevant data is gathered.

Preliminary Economic Analysis

Residential development of the Delbrook site has the potential to generate revenue for the District through the potential sale of District lands and increased taxes, while achieving the previously noted OCP objective of encouraging a diversity of housing.

The planning process will consider the implications of the built form and density for the housing, type and number of affordable housing units, size and features of parks and open space, location of the child care, land value and tax levy implications.

Staff conducted a preliminary financial analysis on the potential gross land value of the site to assess the potential revenue generated under a range of redevelopment options. Assuming redevelopment (of approximately 43% of the site) based on a mix of town homes and low rise apartments, the potential land value is estimated at approximately \$14.1 million. If 10% of the units are provided as affordable (less than market rental units), then the potential land value is estimated at approximately \$12.7 million.

Assuming redevelopment (of approximately 70% of the site) based on a mix of low and mid-rise apartments, the potential land value is estimated at approximately \$25.2 million. If 10% of the units are provided as affordable (less than market rental units), the potential land value is estimated at approximately \$23.1 million. Note that while the supported value suggested by low and mid-rise apartments is relatively high, mid-rise concrete buildings limited to less than 12 stories in height may not be economically viable. With this in mind, the planning process will explore the economic and land value implications for varying building heights, built forms, densities and site coverage.

In both the above cases, potential development costs for the site (which may include demolition costs, parks improvements costs, water/ sewer line upgrades, costs to relocate the child care facility onsite if needed, etc.), which are estimated at approximately \$5 million, are not included in the potential land values as indicated above. These costs, some or all of which may need to be borne by the developer, may impact the net revenues that can be generated from the site.

Given the opportunity for revenue generation with redevelopment, an assessment of the financial implications for various conceptual options for this site will be considered within the scope of the planning process. Consideration will also be given to the implications for the existing tax levy for each scenario.

Timing/Approval Process:

Planning and public engagement for the Delbrook site is anticipated to take place from the fall of 2015 to the late spring/early summer of 2016 with updates to Council at key milestones. Towards the end of the process staff will present a number of concept planning options and a recommended preferred approach, as informed by community feedback, for Council's consideration. Subject to Council direction, a final site concept plan, and supporting OCP amendments, will be drafted for Council's consideration.

This timetable will allow for proactive planning and the delivery of a vision and conceptual plan for the future of the Delbrook site in advance of the completion of the new Community Recreation Centre on the William Griffin site.

Concurrence:

This report has been reviewed by Real Estate and Properties, Finance and Corporate Communications.

Financial Impacts:

Financial impacts related to the two options identified in the economic analysis on page 6 are shown in Table A on the next page.

The percentage of the 4.35 acre site to be developed ranges from 43% - 70% with the remaining land designated park. Land value realized through the development of the site ranges from \$14.1m - \$25.2m and net proceeds after investing in affordable housing and added costs (offset by DCC revenue) ranges from \$9.4m - \$20.0m. The developer will bear some of the added costs so the net proceeds shown are considered conservative. The allocation to affordable rental housing is included under each of the options as the site presents a unique opportunity to realize related community wide OCP objectives

Ongoing taxation revenue, based on estimated sales values, ranges from \$170k - \$390k and \$110k - \$340k after direct service impacts. A net ongoing contribution towards the water and sewer and drainage utilities is also anticipated and will be confirmed in the future.

One time and ongoing estimates provided will be refined during the planning process.

TABLE A: Option Impacts (thousands \$)

	Townhouse & Low Rise Apartments	Low Rise and Mid Rise Apartments
One Time		
Land Value	\$14,130	\$25,200
Affordable Housing	(1,410)	(2,100)
Added Costs ⁽¹⁾	(4,500)	(5,500)
DCC Revenue ⁽²⁾	1,190	2,410
Net Proceeds	\$9,410	\$20,010
Ongoing		
Tax Revenue	\$167	\$386
Direct Service Impacts ⁽³⁾	(60)	(45)
Net Revenue ⁽⁴⁾	\$107	\$341
Land and Units		
% Site developed	43%	70%
New Units	161	368
Notes:		
1) Includes demolition, servicing, relocation of Little Rascals, and park improvements		
2) DCC revenue shown as an offset to "Added Costs"		
3) Operating & maintenance costs for new park (estimated at 2% of park capital costs)		
4) To be contributed to tax growth reserve to offset future service impacts		

One-time net proceeds of \$9.4m - \$20.0m can be used for retiring debt, replenishing reserves or funding future capital. The early retirement of debt would generate \$1.4 million in avoided debt servicing costs which is the equivalent of almost 2% on the tax levy.

Liability/Risk:

Once the North Vancouver Recreation and Culture Commission moves from the Delbrook Community Centre the south buildings will be empty. Having the buildings sit empty is a liability for the District as they may fall into disrepair and may be the target of vandalism.

Social Policy Implications:

Informed by Public Assembly Land Strategy the Delbrook site planning process will consider: opportunities for an interim space for the Capilano Community Services Society and their partner organizations if needed, retention of the Little Rascals Day Care facility, and opportunities to retain some portion of the site as parks and open space to enable public use of the Delbrook site. Redevelopment may also include new rental and affordable housing options for District residents.

Environmental Impact:

Redevelopment of the site provides an opportunity to protect and enhance Mission Creek through adherence the Streamside Protection Development Permit Area guidelines.

Conclusion:

Given the proximity of this site to nearby schools, the new recreation centre, child care, shopping and bus routes, the District has the opportunity to create much needed housing for young families and seniors on the site. By ensuring an appropriate mix, type, tenure and size of housing and open space, this development has the potential to add housing diversity to our community, and at the same time provide positive benefits for District tax payers.

A comprehensive public engagement process is recommended to ensure the conceptual plan for the Delbrook site meets the community's needs.

Respectfully submitted,

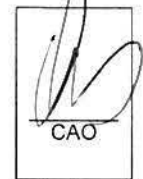
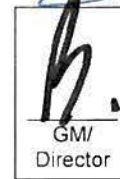
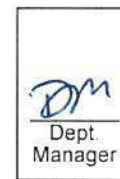


Suzy Lunn, Community Planner

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<input type="checkbox"/> Facilities _____	<input type="checkbox"/> GIS _____	<input type="checkbox"/> Other: _____
<input type="checkbox"/> Human Resources _____		

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AGENDA INFORMATION	
<input type="checkbox"/> Regular Meeting	Date: <u>July 20/2015</u>
<input type="checkbox"/> Workshop (open to public)	Date: _____



9.5

The District of North Vancouver REPORT TO COUNCIL

June 24, 2015

File: 13.6480.30/002.004.000

AUTHOR: Sarah Dal Santo, Section Manager Policy Planning
Ross Taylor, Community Planner
Jessie Gresley-Jones, Planning Assistant 2

SUBJECT: Lions Gate Public Realm Strategy

RECOMMENDATION:

THAT Council approve the "Lions Gate Public Realm Strategy" attached to this Council report; and

THAT Council rescind the "Lower Capilano Marine Village Centre Design Guidelines and Streetscape Details" Schedule of the Lower Capilano Marine Village Centre Implementation Plan.

REASON FOR REPORT:

The purpose of this report is to inform Council of the results of the recent public engagement process and to seek Council approval of the "Lions Gate Public Realm Strategy". (See Attachment 1 of this report.)

Please note that the name "Lions Gate" now refers to the area formerly referred to as the "Lower Capilano/Marine" or "Capilano Village" Centre.

SUMMARY:

The Lions Gate Public Realm Strategy guides the design of streetscapes, parks and plazas to help create a pedestrian oriented, walkable neighbourhood with well-designed, diverse and inviting social spaces that are inspired by a unique Capilano River theme. In this manner the design guidelines promote consistency through a distinct character and identity, while at the same time providing flexibility to enable creativity and innovation as the centre redevelops over time.

The Public Realm Strategy has been informed by a series of public consultation events in 2012 to 2013, as well as throughout the village centre implementation planning process. A co-design workshop held in April 2014 focussed on the village heart and was instrumental in shaping the shared use "Woonerf" and "Crossroads" streets as well as the central and

"Crossroads" plazas. More recently, an estimated 70 people attended a public open house on May 21, 2015 to view and comment on the Draft Public Realm Strategy. Results of the open house and online survey indicated that the Public Realm Strategy is supported by the majority (78%) of respondents.

BACKGROUND:

Community engagement on the public realm design for this centre began in 2012 and coincided with engagement on the Lower Capilano Marine Village Centre Implementation Plan (approved by Council in 2013). The following is a summary of community engagement events on the planning and the design framework:

- Apr 2012:** Open House/Workshop – Update on technical analyses, plan directions, table discussions on public realm design, village heart etc.
- Oct 2012:** Open House/Workshop – Open House display of design concept and public realm interactive display boards, urban design "tour" of precincts, etc.
- Mar 2013:** Open House – Key elements of the implementation plan, concept plan and design framework for the public realm.
- May 2013:** Series of peripheral/transitional area planning meetings.
- Feb 2014:** Open House – public realm design.
- May 2014:** Co-Design Session – Full day workshop with residents, land owners' consultants and staff focussing on the design of the proposed new community street or "Woonerf" road, the "Crossroads", and key public plazas.
- May 2014:** Open House/Workshop – Peripheral area housing options.

On **22 July 2014**, the Committee of the Whole received a presentation from staff that summarized key sections in the draft design guidelines for the Lions Gate centre. The Committee of the Whole report was forwarded to the **8 September 2014**, Regular Council meeting where Council approved the following resolution:

THAT staff be directed to complete the community consultation program and finalize the public realm guidelines for Council consideration in the fall of 2014.

Subsequently, further refinements to the design guidelines were made in response to community and Council feedback, and internal interdepartmental review. Staff also held design specific workshops with Grouse Inn and LARCO development applicants to discuss how the proposed design elements would apply to their respective projects.

Then on **21 May 2015**, an additional public open house was held at the Grouse Inn to gather input and comment on the consolidated draft Public Realm Strategy. Feedback from this open house is summarized in detail in the Public Input section of this report. Overall, results showed strong general support for the Public Realm Strategy (attached) that is presented for Council's consideration.

EXISTING POLICY:

The District Official Community Plan (OCP) identifies the Lower Capilano Marine Village Centre as one of four key growth centres within the network of centres concept. The OCP establishes land uses and densities throughout the District, and OCP Schedule B: Development Permit Areas provides a series of form and character design guidelines for commercial, industrial and multi-family development.

The Lower Capilano Marine Village Centre Implementation Plan (approved by Council in 2013) builds on the land use, growth management and urban structure policy framework in the OCP and provides more detailed policies to guide growth and change in this centre to 2030. Should Council approve the Lions Gate Public Realm Strategy, the preliminary and outdated "Lower Capilano Marine Village Centre Design Guidelines and Streetscape Details" (Attachment 2) that was approved as a Schedule to the Lower Capilano Marine Village Centre Implementation Plan, would need to be rescinded as a housekeeping measure.

If approved by Council, the Lions Gate Public Realm Strategy will supplement the Centre Implementation Plan and the OCP Form and Character Design Guidelines by providing detailed streetscape, parks and plaza design elements to guide the realization of a distinct sense of place and well-designed, inviting public realm spaces in the village centre.

ANALYSIS:

The design guidelines attached to this report reflects and responds to the community vision from the Lower Capilano Marine Village Centre Implementation Plan. It also reflects the same design principles, inspiration from "river flows" that the community and Council has seen in early consultation and reporting updates. While the basic framework and intent of the guidelines remains the same as presented to Council last July, the following updates to the document have been made in response to community and stakeholder input, Council feedback and internal interdepartmental review:

- Consolidation and tightening of the text for clarity and legibility,
- Distinction between intent/programming and design elements,
- New design detail, explanatory notes, and cross-sections for the hierarchy of streets,
- Improved parks/open space and cycling network,
- Additional illustrations and precedent images,
- Updates to lighting typology and lighting strategy, and
- Appendix for material and street details.

Highlights of key sections in the design guidelines are provided below. Additional detail will be provided in the staff presentation to Council.

Design Principles and Inspiration for Theme

The inspiration for the public realm design guidelines is the Capilano River and flowing water. The unique treatment of key public spaces embraces this river theme and promotes a distinct identity and character through continuity of design elements in paving materials, planting, street furniture, lighting and public art. Angular sidewalk and boulevard design with irregularly shaped planting islands are evocative of the meanders of a natural river system. Water features are proposed at key park and plaza locations.

The Public Realm Strategy is shaped by eight overarching principles:

1. Create forms inspired by river systems;
2. Provide distinct and diverse social spaces;
3. Establish naturalized, intensive, and concentrated planting areas;
4. Emphasize a strong, pedestrian-oriented focus;
5. Provide support for pedestrians, cyclists, transit users as preferred transportation modes;
6. Allow for integrated uses of public spaces by creating flexible spaces;
7. Ensure universal accessibility throughout; and
8. Integrate sustainable features.

Key Public Spaces and Character Areas

The Public Realm Strategy identifies a number of key character areas that are defined by their role in the neighbourhood, intent and programming, and adjacent land uses. Key areas are identified in Figure 1 and described more fully below.



Figure 1: Key Public Spaces in Lions Gate

The Village Heart and Key Plazas

The central "Village Plaza" will be the key social gathering space within the village heart. Prominently located and visible from the "Crossroads" this plaza will be urban in form with spaces that can be adapted to meet a variety of community activities including farmer's markets, concerts, community barbecues and other local events. The proposed new community centre and local serving retail will help establish this area as the centre of activity and the most pedestrian oriented portion of the village centre.

The "Crossroads Plaza" marks the eastern gateway to the village heart and is a key transition area and link from the heart to the neighbourhood east of Capilano Road. It will serve as a secondary gathering place for the community and entry point to the transit routes on McGuire Avenue.

The "Gateway Plaza" is the southern gateway to the village centre. This plaza draws pedestrians from Marine Drive toward the village heart. Retail uses around the edge of the plaza will help to animate and program the space.

The "Village Green" complements the "Village Plaza", linking it to the park network and the residential neighbourhoods to the west. It has a softer, greener expression than the plaza with less hard surface and more planting.

Neighbourhood Parks and Open Space

Belle Isle Park is an existing flat and open grassed area that allows for informal activities with passive open spaces. Over time, Belle Isle Park should be expanded to provide a direct connection to the Village Green and the overall parks and open space network.

The proposed new Curling Road Park is envisioned as a new adventure play park with some passive space. The park marks the southern tip of the new greenway connection leading to the "Crossroads" and village heart.

The "Greenway" is envisioned as a wide pedestrian priority connection from Fullerton Avenue to Curling Road. The Greenway provides connectivity throughout Lions Gate, reinforcing it as a walkable community and connecting park and plaza spaces.

The Village Streets

Capilano Road is a major arterial road that supports vehicle, transit and cycling modes and is also the primary retail street in the village centre. A key objective is to achieve an interesting and safe pedestrian condition that enables access to the street retail environment. The pedestrian realm will be comprised of significant sidewalk, eddy zones for seating and resting and substantial planting to buffer from adjacent traffic. Capilano Road features the "river village" sidewalk and boulevard design.

Marine Drive is a prominent gateway and entry point to both Lions Gate and the District of North Vancouver. West of Capilano Road, a key public realm objective is to present an attractive and bold tree-lined pedestrian greenway that is buffered from high traffic volumes on Marine Drive, and which connects to the open space plaza network in the village heart.

Marine Drive also features the “river village” sidewalk with boulevards on both sides of the sidewalk.

The “Woonerf” is envisioned as a special destination within the core of Lions Gate serving several purposes beyond the typical functions of a street. It provides a strategic north/south connection through the neighbourhood and will be a shared, multi-modal space with the highest priority given to pedestrians. Curbless paving treatment will facilitate pedestrian accessibility and use of this area as a flexible community space that can be closed off for large scale community events. Angled or parallel parking alternated with irregular and substantive landscaping will serve to slow vehicle traffic and provide a rich canopy for this pedestrian oriented street.



Figure 2: Illustrative sketch of the “Woonerf” Street

The “Crossroads” provides a primary east-west connection through the village heart providing improved pedestrian and vehicular access in and out of the heart and across to the neighbourhood east of Capilano Road. Like the “Woonerf”, the “Crossroads” may be closed off occasionally to expand the central plaza for community events. The paving treatment will include use of the same pavers as the “Woonerf” road and Village Plaza as well as a river themed band of exposed aggregate.

McGuire Avenue combines many uses: pedestrians, bikes, cars, and bus, and is intended to function as a “triple A” separated bike facility and major bus route for the village centre. A safe commuter corridor is created with the use of wide, planted boulevards and bike path

separation. Proper separation between all uses is provided through the use of boulevard planting, grade separation, or median zone with seating and planting.

Fullerton Avenue and Curling Road are neighbourhood streets providing local access to the peripheral area west of the village centre. Curling Road spans the village centre and links the community across Capilano Road. Design guidelines for these residential streets include a conventional sidewalk and boulevard configuration with trees in groupings and a lush understorey with seating at appropriate intervals. Fullerton Avenue offers a shared lane cycling facility.

Site Furnishing and Lighting

The design guidelines call for high quality furnishings to provide durability over time and to reduce maintenance needs. Street furniture including benches, bike racks, trash and recycling containers are proposed to be located in the “amenity” zones of sidewalks to reduce clutter and to enable sufficient room for clear pedestrian movements. The lighting strategy includes street and pedestrian oriented lighting from the same family of products to create a consistent look. Special feature catenary lighting and bollard lighting in key locations in the village heart help create a sense of gateway and arrival and establish a warm ambient street character.

Landscaping

The proposed planting strategy reinforces the theme of the river system within the village centre. The proposed planting list includes plants that are drought tolerant, low maintenance and beneficial to rainwater management.

Public Art Strategy

Public art is integrated into the village centre in a variety of different forms such as: bold and iconic stand-alone pieces located in key open space areas, functional art and interpretive way-finding elements on pedestrian routes, gateway features to mark the entrances to key destinations and features integrated into the design of landmark buildings.

Timing/Approval Process:

The Lions Gate Public Realm Strategy is needed to inform development proposals and guide the review of development applications in this centre.

Concurrence:

The Lions Gate Public Realm Strategy has been reviewed by inter-departmental staff in Planning, Engineering, Transportation Planning and Parks. The guidelines have also been reviewed by the Advisory Committee on Disability Issues (ACDI), the Advisory Design Panel (ADP), the Transportation Consultation Committee, and Vancouver Coastal Health.

Financial Impacts:

The proposed design guidelines identify a range of opportunities to enhance the village centre. Improvements would be funded through development, as and when it occurs. Operating and maintenance budgets to meet the desired level of service for some features (furnishings, water features, paving) will be monitored and may need to be increased over time. Where feasible, the District will encourage opportunities for private and community programming and maintenance of public realm components.

Liability/Risk:

The proposed Public Realm Strategy is consistent with the OCP and is not anticipated to expose the District to any liability or risk.

Social Policy Implications:

The Public Realm Strategy identifies opportunities to enhance active transportation (walking, cycling) and foster community interaction through an integrated network of public plazas, parks, plazas and greenways. These design measures are supported by District social well-being and other policies, and by Vancouver Coastal Health objectives.

Environmental Impact:

The Public Realm Strategy enhances ecological performance with the integration of open space, landscaped boulevards and rainwater management features.

Public Input:

Since 2012, community feedback has played a vital role in informing the general direction and successive refinements to the planning and design for the Lions Gate.

OCP Implementation Planning Committee (OCPIC) members have participated in a number of Lions Gate public engagement processes and events.

Most recently, on May 21, 2015, approximately 70 people attended an open house at the Grouse Inn to review and provide comment on the Draft Lions Gate Public Realm Strategy. A total of 40 feedback forms (including 17 open house forms and 23 online forms) were received. A detailed summary and analysis of survey responses is provided in Attachment 3. Overall, 78% of survey respondents indicated that they strongly or somewhat agree with the Lions Gate Public Realm Guidelines. This overall support was consistent with the input that staff received from participants at the open house.

Public and stakeholder feedback has contributed in significant ways to the development of the Public Realm Strategy at multiple stages of the project. One participant expressed appreciation for the engagement process and felt that they could see their participation had made a difference. Another commented that the design guidelines are an opportunity to create an exceptional space in this centre.

The HUB (Cycling Group) North Shore Committee commented that they support the network connections and proposed street designs as they will allow residents of all ages and abilities to cycle all streets in the village centre.

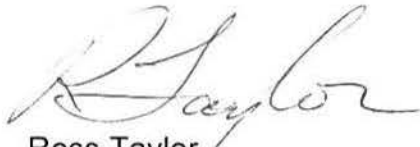
Conclusion:

The Lions Gate Public Realm Strategy provides a design framework to guide the delivery of a unified and distinct system of plazas, parks, open space and streetscapes that supports the community vision. Based on strong support expressed by the community and stakeholder groups, staff recommends that Council approve the Lions Gate Public Realm Strategy as a necessary tool to guide future revitalization of public spaces in Lions Gate.

Respectfully submitted,



Sarah Dal Santo
Section Manager Policy Planning



Ross Taylor
Community Planner



Jessie Gresley-Jones
Planning Assistant 2

Attachments:

1. Lions Gate Public Realm Strategy.
2. Lower Capilano Marine Village Centre Design Guidelines and Streetscape Details – Schedule attached to the Lower Capilano Marine Village Centre Implementation Plan.
3. Summary and Analysis of Feedback on the Lions Gate Public Realm Strategy – May 21, 2015 Public Open House.

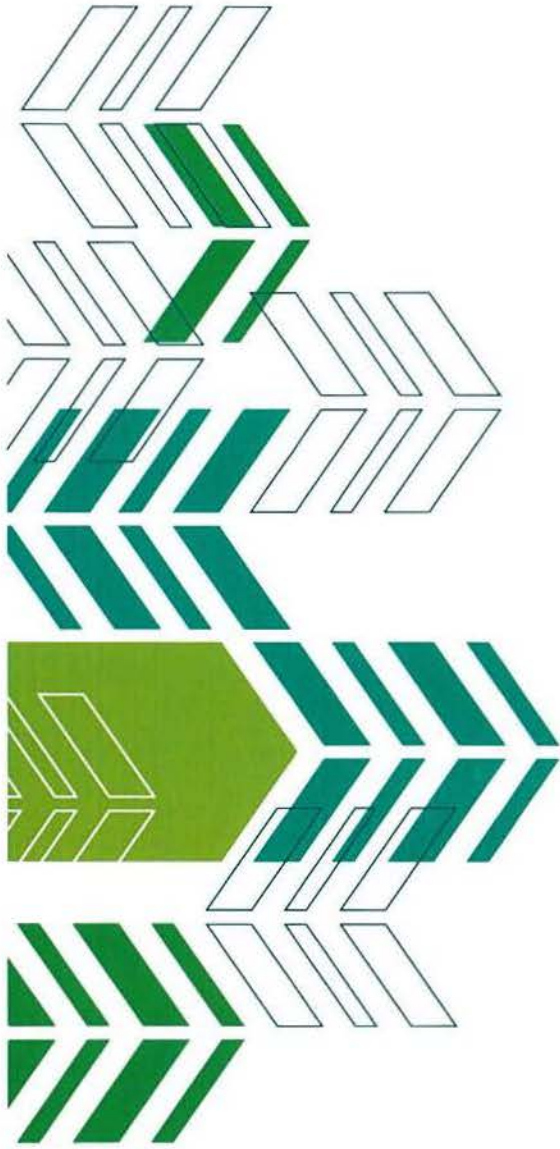
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DISTRICT OF
NORTH VANCOUVER
2015.07.13

LIONS GATE PUBLIC REALM STRATEGY

DESIGN GUIDELINES FOR PUBLIC REALM & OPEN SPACE



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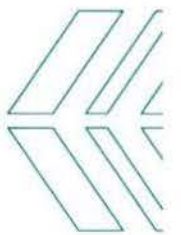
These design guidelines have been developed by the District of North Vancouver in collaboration with the consultant team of PWL Partnership Landscape Architects Inc. and the Office of McFarlane Biggar Architects + Designers, as informed and refined by community and stakeholder feedback.

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1.0 INTRODUCTION





INTRODUCTION

Background

The Lions Gate neighbourhood is located on the North Shore close to the Lions Gate Bridge and immediately north of the intersection of Marine Drive and Capilano Road. Set at the foot of the North Shore Mountains and beside the Capilano River, the area is rich with natural landscape, habitats, and views of downtown and the mountains.

Looking to 2030, Lions Gate will create a sense of excitement through its creative and quality public realm and architecture. The Lions Gate Centre Implementation Plan (Formerly the Lower Capilano Marine Village Centre Implementation Plan),

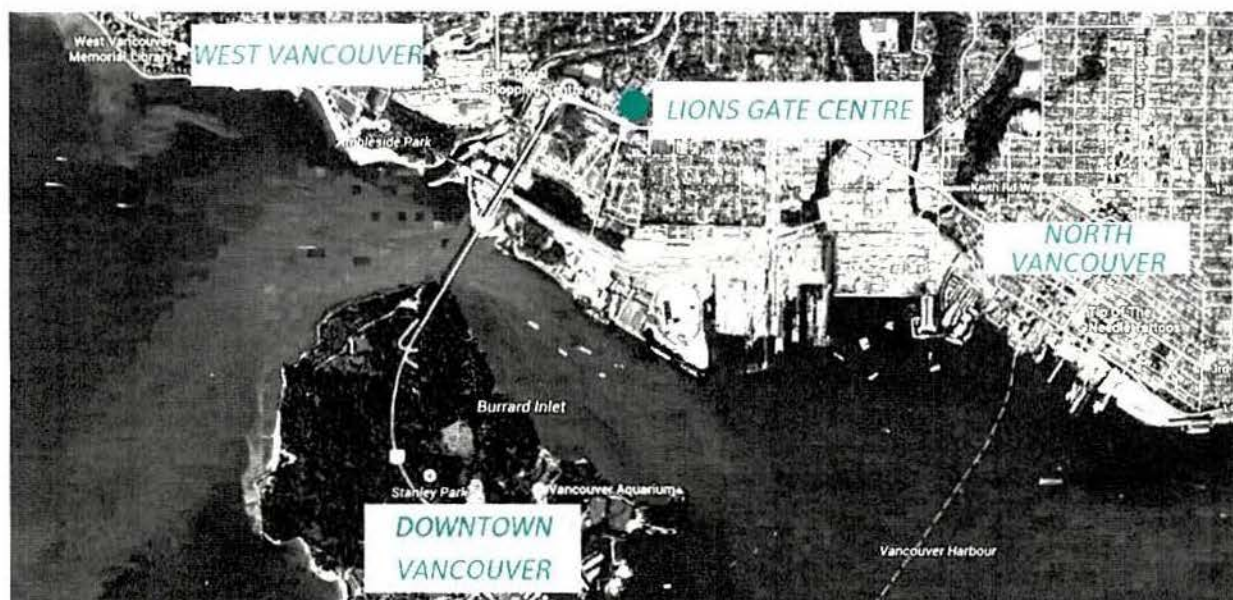
envisioned this area as: “a gateway to the District (that) will function as a vibrant, walkable neighbourhood with local serving businesses, jobs, community recreation opportunities and a range of housing options”.

This **Lions Gate Public Realm Strategy** recognizes the value of this centre as a gateway to the District and draws inspiration from its proximity to the Capilano River to create a strong design theme that is carried throughout the public realm to encourage a unified and distinct sense of place. A key objective is to develop a quality public realm that complements the natural beauty of the North Shore and the Capilano

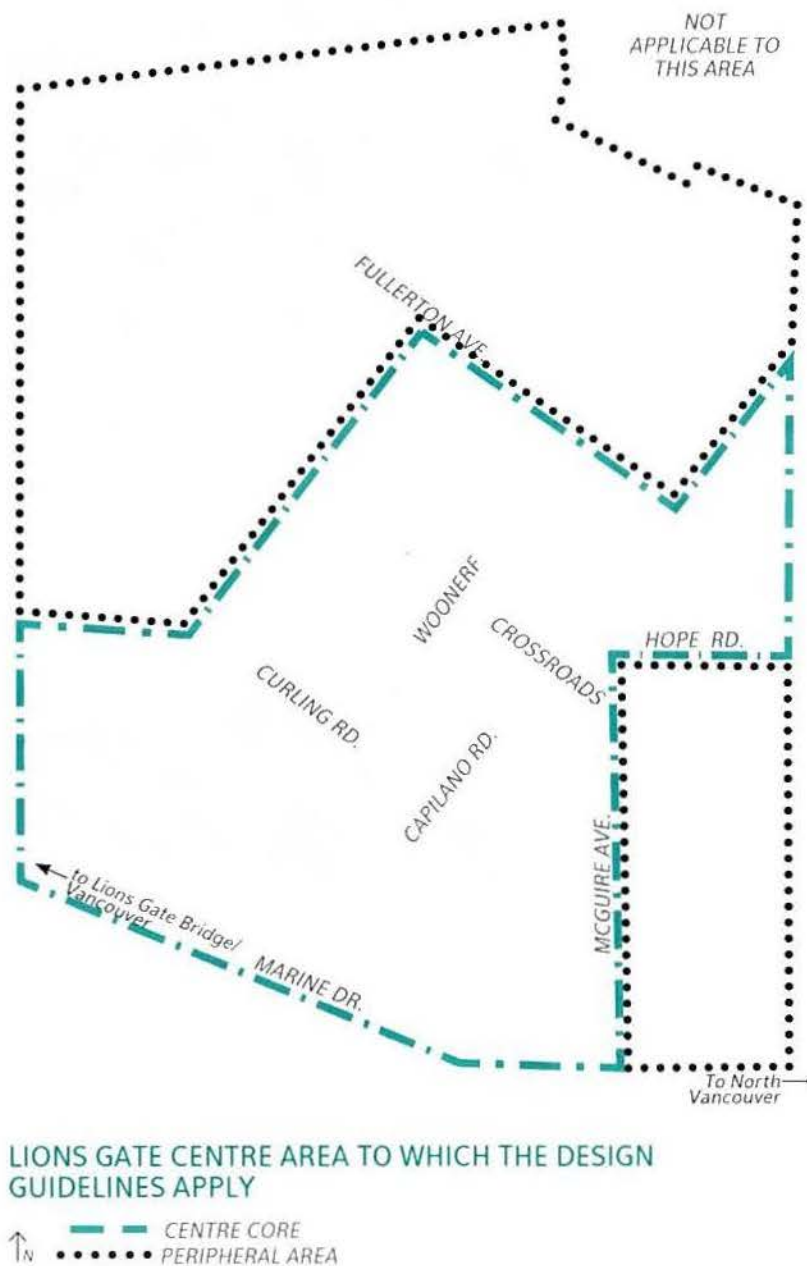
River, while supporting the social, commercial and recreational life of this local community.

Consultation to Inform these Guidelines

The preparation of this document has been informed by community and stakeholder input involving a series of engagement events, design workshops, meetings and online forums with local residents, members of the general public, business owners, the Advisory Committee on Disability Issues (ACDI), Vancouver Coastal Health, the Advisory Design Panel, as well as development and consulting teams. Through these



CONTEXTUAL AERIAL PHOTO



diverse engagement events held at key milestones throughout the planning and design process, a wide group of participants were able to provide input to help shape the design guidelines.

Purpose of the Guidelines

This Public Realm Strategy provides a design framework for the plazas, open spaces, parks, greenways, streetscapes and other public realm spaces within the centre of Lions Gate; and is intended to guide property owners, developers, design consultants, District staff and Council towards the achievement of the community vision for Lions Gate. While these design guidelines seek to create a consistent look and feel for the centre; they also enable flexibility, creativity and innovation as the centre develops and grows over time.

These design guidelines are applicable to the centre of Lions Gate as identified in the Official Community Plan and the Lions Gate Centre Implementation Plan (see map on previous page). The centre is comprised of the mixed use central core and the surrounding lower density multi-family residential peripheral area. The majority of the design elements apply to the central core area with some basic design elements (overall design theme, material choices etc.) continued into the peripheral area, (with the exception of the single family area in the northeast corner), to ensure a consistent identity and character throughout this centre.





How to Use this Document

The District of North Vancouver's Official Community Plan (OCP, Bylaw 7900, 2011) and Lions Gate Centre Implementation Plan (2013) establish the land use and policy context for this centre. In addition, OCP Schedule B: Development Permit Areas includes a series of Form and Character Design Guidelines that apply to all commercial, industrial and multi-family development in the District including the Lions Gate Centre. The Lions Gate Public Realm Strategy supplements, and must be read in conjunction, with these policy documents.

Other applicable policies and plans include:

- Development Services Bylaw 7388 (2005) and other supplementary Municipal Standards
- Lions Gate Peripheral Housing Policy (Formerly the Lower Capilano Peripheral Housing Policy) - 2014
- Lions Gate Centre Lower Transportation Study (formerly the Capilano Marine Village Centre Transportation Study) -2013
- Lions Gate Centre Public Art Plan (formerly the Lower Capilano Marine Village Public Art Plan) -2012)

The public realm guidelines are based on the land use concept plan as shown in the Lions Gate Centre Implementation Plan. The locations of new roads and buildings shown on the concept plan are approximate and may be adjusted during the course of the municipal review process of rezoning and development permit applications depending on

the timing of development, land assemblies and other factors.

As part of development application review, applicants should provide plans for maintaining all public realm components.

Importantly, this document is to be used to guide decisions. It does not represent final decisions in themselves. Decisions on specific development applications and civic improvements will be made by Council, with full public input, on a case-by-case basis. This guiding document is neither prescriptive nor exhaustive, but rather illustrates the anticipated key directions for the centre.

Overarching Design Principles

Overarching principles to guide the public realm design for the Lions Gate Centre are as follows:

1. Create forms inspired by river systems
2. Provide distinct and diverse social spaces
3. Establish naturalized, intensive, and concentrated planting areas
4. Emphasize a strong pedestrian-oriented focus
5. Provide support for pedestrians, cyclists, and transit users as preferred modes of transportation
6. Allow for integrated uses of public spaces by creating flexible spaces
7. Ensure universal accessibility throughout
8. Apply the use of quality, durable and natural materials
9. Integrate sustainable features throughout



INSPIRATION: THE CAPILANO RIVER SYSTEM

A Public realm shaped by the flow of water

The public realm and architectural expression for the Lions Gate draws upon the Capilano River system, the key geographic feature of the area, and reflects its importance in the residents' identity of their community.

The key inspirational element is the flowing water of the river. It is the most significant element that shapes the character, functions and all aspects of the river system.

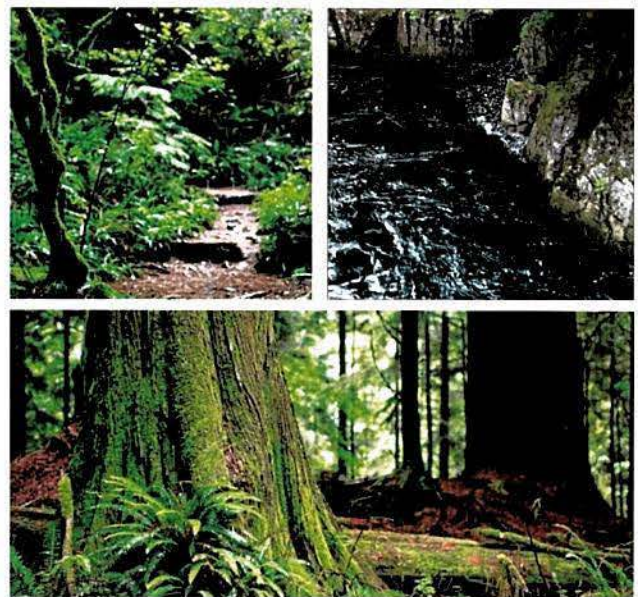
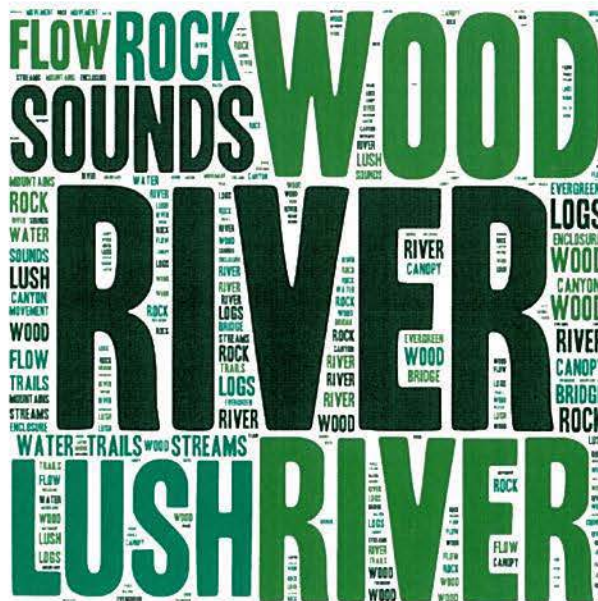
Architecture informed by natural landscapes

The forest canopy and the river's edge quietly support and frame the river. Drawing from this inspiration, the architecture of Lions Gate should strive to mimic the river bank and forest system with simple forms that are timeless, and ultimately support the enduring vibrancy of the public realm.

A Diverse landscape

The diverse riparian landscape of the Capilano watershed has many unique species and forms.

These guidelines seek to infuse the public realm with the experience of being in different areas of this natural landscape and to reflect the diversity of the Capilano river system.





Applying the Metaphor

The river concept applies the river system, and water flows, as a metaphor for the circulation routes in the centre.

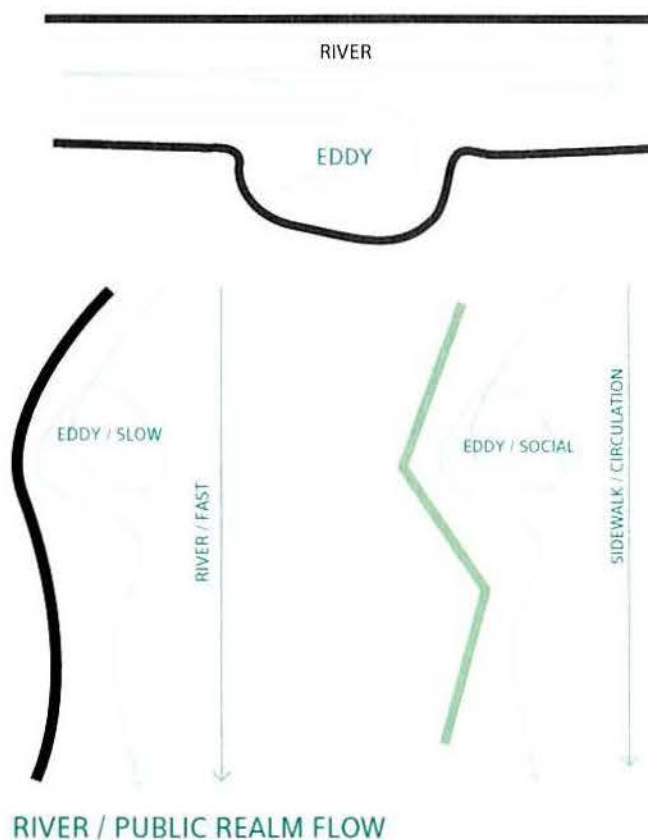
Capilano Road with the highest traffic volumes in the north-south direction is like the river. Its forms and scales are designed to accommodate higher volumes and speeds.

The "Crossroads" acts as a major connecting element and bridge between communities on the east and west sides of Capilano Road.

McGuire Avenue and the "Woonerf" with lower traffic volumes are like streams. Their scale is slower and more intimate, reflecting the different land uses and traffic speeds.

Curling Road and Fullerton Avenue are like tributaries through the forest further interconnecting flows of pedestrians, cyclists and vehicles into the larger circulation routes of the other streets.

Marine Drive on the southern limit of the project boundary represents the Burrard Inlet shoreline. It has the highest volumes of traffic and connects Lions Gate Centre to the wider North Shore.



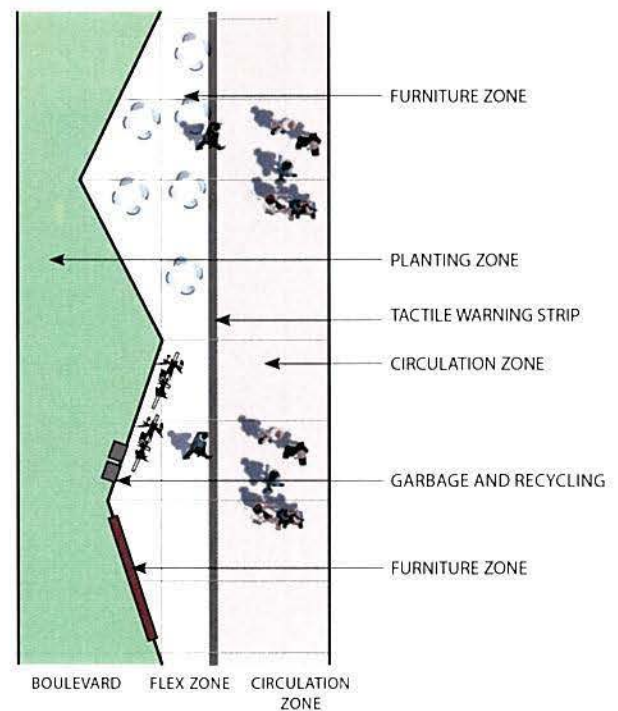
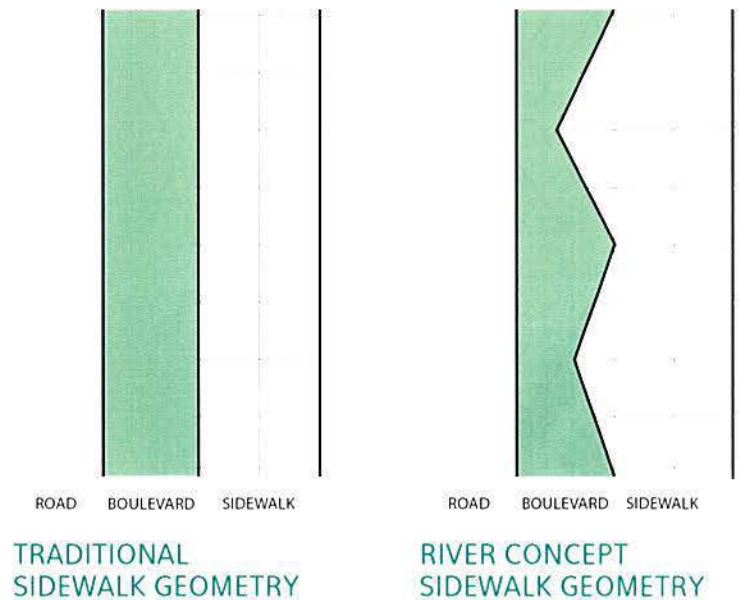
Flow

Water flows at different speeds across the width of a river; faster in the middle, slower at the edges and still slower in eddies along recesses of the shoreline or behind objects that block the water's path.

Applying this metaphor to the streetscape, the faster pedestrian circulation zone is placed adjacent to the buildings and has a simple paving pattern free of obstacles. Fast zones have the least pavement texture to facilitate unimpeded pedestrian movement. Rain protection is provided by the building cantilever or other cover above.

Eddies are quieter spaces placed adjacent to the sidewalks. The eddy zones are slower, flexible spaces with places to sit and where street furniture can be located without impacting the faster flow of pedestrians in the main sidewalk. Paving within the flex zones will have more texture suggesting a place to slow down and be more aware of the surroundings and activities.

Trees and other landscaping representing the riparian edge of the river, are located in the boulevard zone. The boulevard acts as a buffer to the vehicle traffic, creating a pleasant environment for the seating areas.



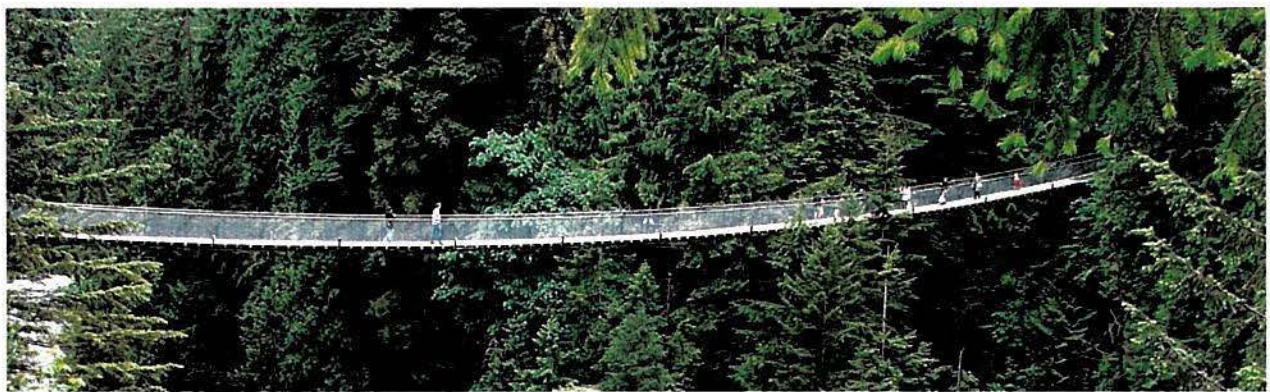
ILLUSTRATED RIVER CONCEPT





2.0 CHARACTER AREAS





LIONS GATE CHARACTER AREAS

The centre of Lions Gate has three distinct character areas that are defined by their role in the neighbourhood, and the adjacent land uses. They are the Village Heart, the Village Streets, and the Neighbourhood Parks and Open Space. Design Guidelines for each of these distinct character areas are outlined in the following sections.

Overarching in all three areas is the sense of being in the natural landscape, but in a new and urban way. Robust, naturalized planting areas with informal tree layouts connect people to the forest. The indented edges of the sidewalk and the social eddies mimic the sunny rock outcroppings along the river that so many enjoy; and the clear and obvious routes for circulation match the natural efficiencies of the water flowing in the river.



KEY CHARACTER AREAS IN LIONS GATE

- » Village Heart and Plazas
- » Neighbourhood Parks and Open Space
- » Village Streets
- » Commercial Frontage
- » Choice of Commercial or Residential Frontage





CHARACTER AREAS: VILLAGE HEART & KEY PLAZAS

This section focuses on four of the main areas that together comprise the key public realm spaces in the village heart and at the Capilano / Marine Gateway to the centre.

1 The Village Green

The Village Green is a key element of the heart that connects to residential neighbourhoods to the west and Belle Isle Park, and serves as an area for more informal use.

2 Village Plaza

The Village Plaza will be the key social gathering space within the village heart. It will be urban in form and flexible to accommodate events and other community needs.

A new community center and local serving retail will establish this area as the center of activity and the most pedestrian oriented portion of the Village.

3 "Crossroads" Plaza

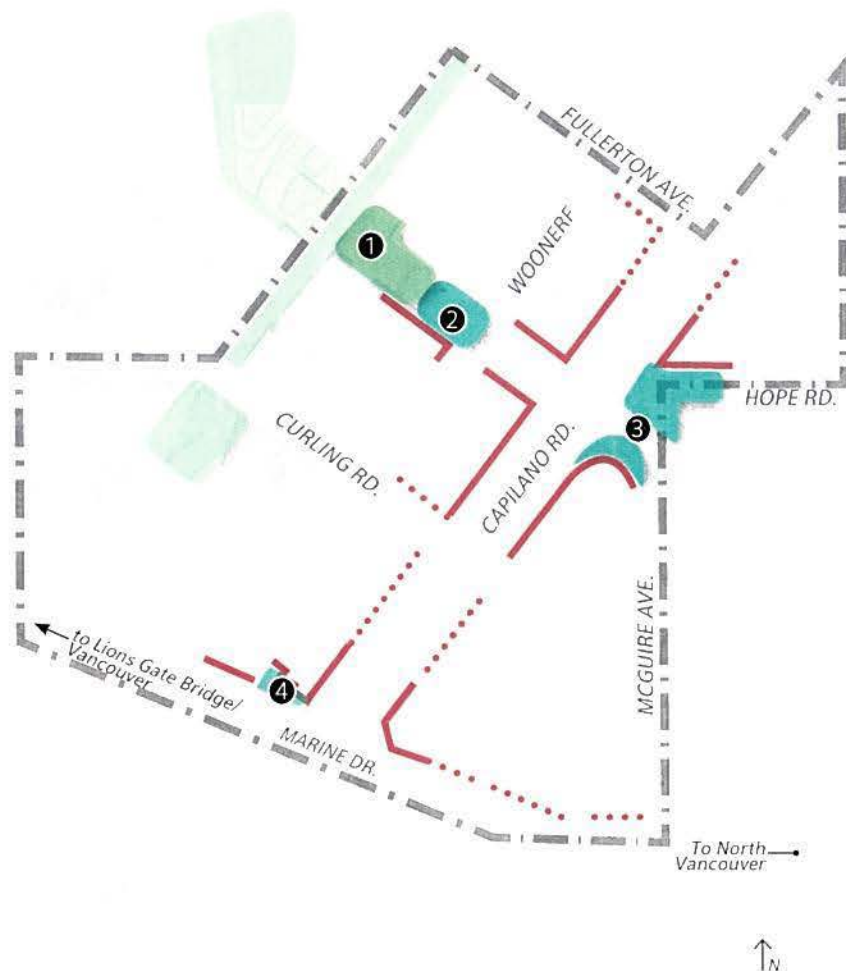
The "Crossroads" plaza is a key link to the eastern portions of the neighbourhood. It will be a critical connection to transit and a focal point along busy vehicular corridors.

4 Gateway Plaza

The Gateway Plaza will act as the southern entrance to the village centre drawing people into the village centre from the intersection of the two major vehicular routes.

General Character

1. Unique features such as special paving to emphasize pedestrian priority, lighting and the careful inclusion of cycling and vehicle connections.



2. An abstract river flowing across the community from east to west visually linking the adjoining neighbourhoods to the Village Heart
3. Angular design style – modern interpretation of meandering river
4. Planting islands in the angular design style – unified but diverse

- in character - should be of varied raised topography (bermed)
5. Community markers – welcoming and guiding through the neighbourhood
6. Unique lighting – a variety of lighting types which include overhead (catenary), street, pedestrian and bollard lighting.



VILLAGE HEART CONCEPT SKETCH SHOWING RELATIONSHIP AND CONNECTIONS OF KEY SPACES WITHIN THE HEART



ILLUSTRATIVE IMAGES



VILLAGE PLAZA

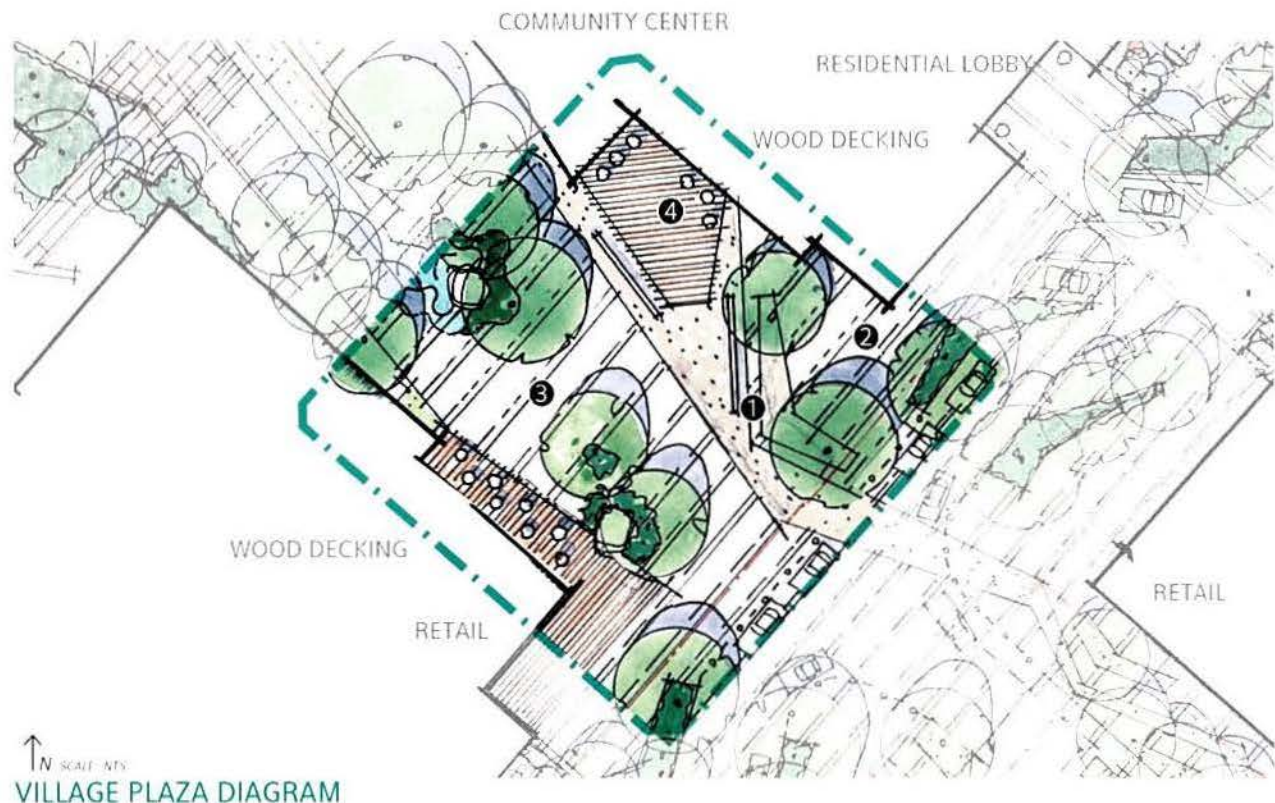
This central plaza will be the most important public space in the centre of Lions Gate. It is prominently located, and visible from the "Crossroads" and new north-south "Woonerf". It will become a focal point for most community activities and events within the Centre. The plaza will be a place to gather, play, and socialize. It will be oriented to community activities and needs to be flexible enough to accommodate farmer's markets, concerts, community barbecues, and other events.

The design and use of the central plaza must be closely integrated with the adjacent community centre, the Village Green to the west and the "Woonerf" to the east. The plaza

will serve as a spill-out area for the community centre.

Program Intent

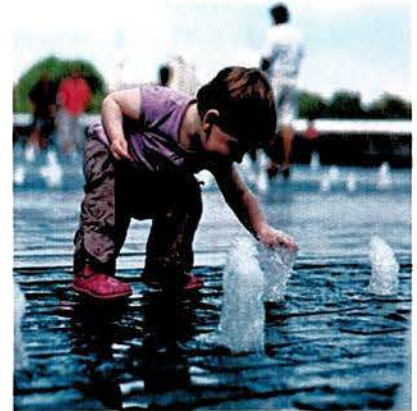
- » A variety of covered areas that extend from the building edges into the plaza
- » Strong indoor-outdoor relationship for community services areas and retail spaces facing into the plaza
- » Ability to host community events
- » Retail edges at corners facing the plaza with spill out space for merchandise or seating
- » Easy and inviting access for people arriving by foot, bicycle and car
- » Clear visual connections and design relationship/unity with the "Woonerf" and "Crossroads"
- » Water feature to be flexible and invite play with the ability to be turned off to enlarge available hard surface area for events
- » Significant public art/water feature as community marker connecting to the Village Green
- » Mix of low walls for built-in seating and standard benches near the community centre
- » Drop off areas close to community centre entry
- » Provide electrical outlets with sufficient power for special events and at benches or seating areas



↑ N SCALE: NTS
VILLAGE PLAZA DIAGRAM

Design Elements

- 1 Abstract river flow theme through the space on the north, directs people to the community centre doors and beyond to the greenway
 - » The plaza space and Village Green should be designed as one unified space with different, but complimentary characteristics, which in turn connects to the Greenway and Belle Isle Park.
- 2 Flexible community outdoor room
- 3 Urban with extensive hard surfacing with some shade tree
 - » The design should accommodate occasional closure of portions of the "Woonerf" and "Crossroads" to expand the available area for larger scale community events.
- 4
 - » Mainly hard surface (same pavers as "Woonerf" and "Crossroads") with ability to close off areas for events or games
 - » Islands of trees planted in crushed stone. Angular shaped planter concept mimicking the island concept
 - » Catenary lighting, irregularly shaped, over plaza space
 - » Wood decking at community centre entrance and adjacent to retail use



VILLAGE PLAZA: ILLUSTRATIVE IMAGES

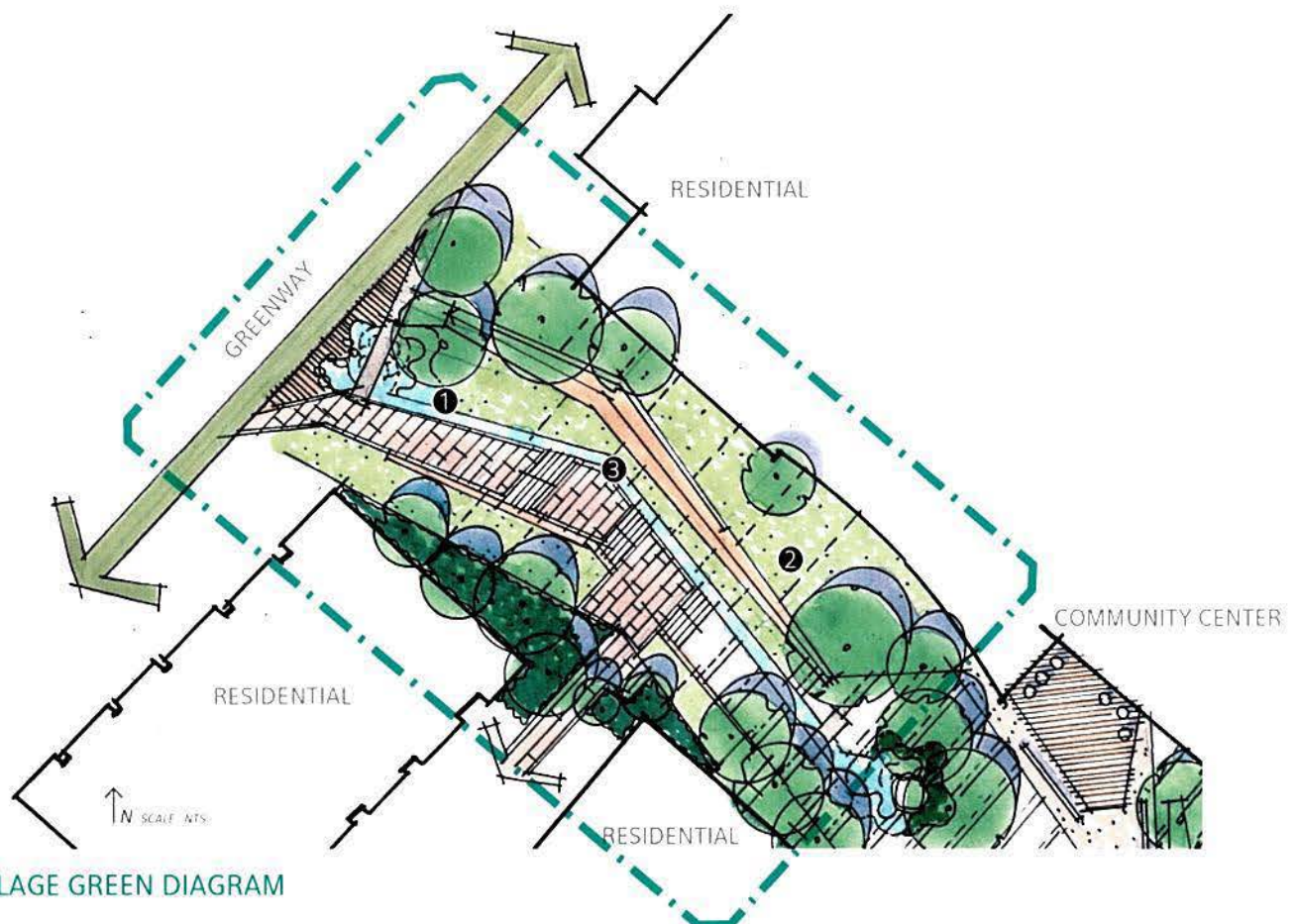


VILLAGE GREEN

The Village Green complements the Village Plaza and will link to the rest of the park network. It has a softer, greener expression than the plaza with less hard surface and more planting. The Village Green should provide opportunities for active children's play (near the community centre) as well as quieter places to relax, read a book and socialize. A water channel connects the plaza water feature to its terminating element close to the Greenway.

Program Intent

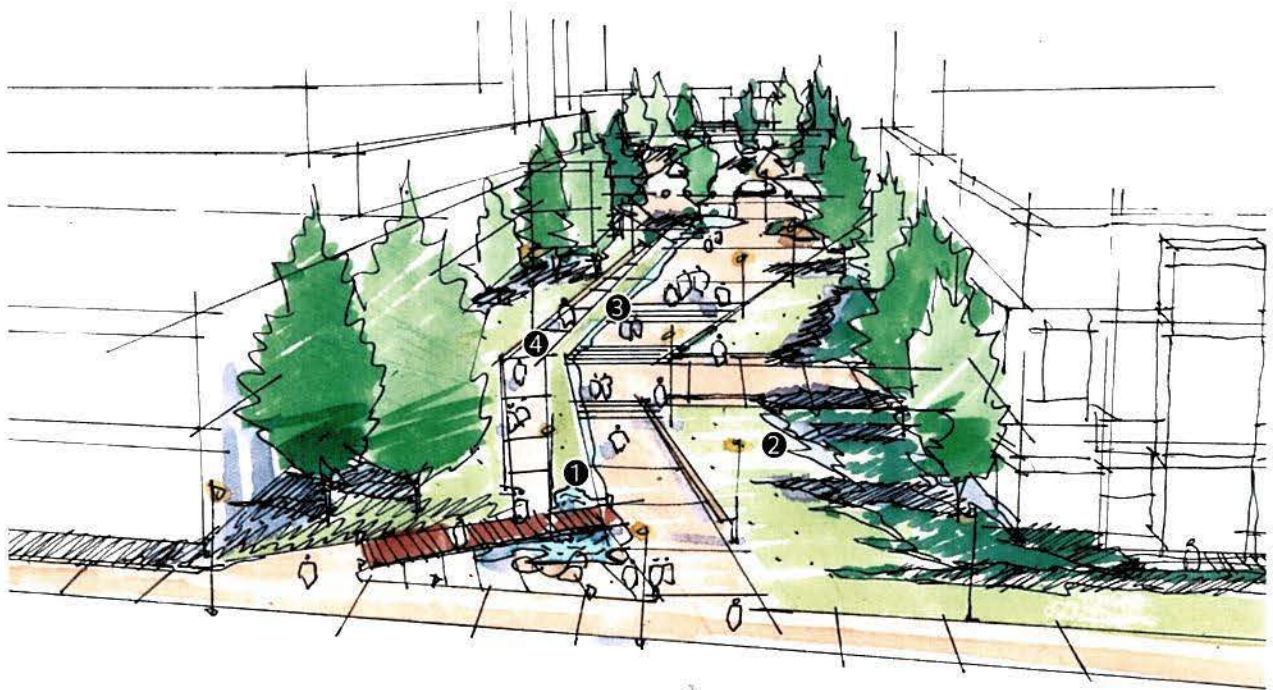
- » Primary Function: Community open space and green connector
- » Play elements integrated throughout the space that engage active upper body movements
- » Visual and physical connections from the surrounding residential units to the park space
- » Pedestrian connections to adjacent residential development to the north and south
- » Open lawn areas with flat areas for picnicking etc, some sloped lawn areas for relaxing
- » Active childrens play area adjoining community centre, designed for diverse play activities.
- » Residential edges with patios facing Village Green
- » Future extension to connect with Belle Isle Park



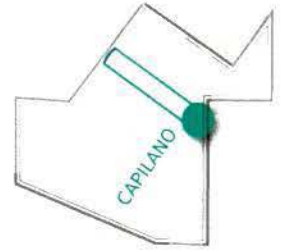
VILLAGE GREEN DIAGRAM

Design Elements

- 1 » Integrated natural play areas and features combining water and landscape features
- 2 » Soft and green landscape expression with bigger open lawn areas
- 3 » Abstracted river flows through the space connecting to the greenway from the plaza
 - » Naturalized rainwater management features integrated with pockets of soft landscape
 - » Open lawn areas with large trees in irregular groupings provide a mix of sunny and shaded areas.
 - » Water feature as play element, associated with central walkway and stairs, channel leading to
- 4 » greenway originating in the Village Plaza
 - » Seating for a broad range of users including manufactured site furniture and utilizing the grade change for seating walls
 - » Universal accessibility directly integrated into the overall design with sloped walkway to be less than 5%
 - » Climbing nets attached to community centre building and climbing wall on building exterior to be considered
 - » Denser planting areas for residential screening and to define public / private spaces



VILLAGE GREEN: ILLUSTRATIVE PERSPECTIVE LOOKING EAST



"CROSSROADS" PLAZA

The "Crossroads" Plaza is the eastern gateway and act as a transition area from the Village Heart to the neighbourhood east of Capilano Road. Located at the east end of the "Crossroads". The "Crossroads" plaza is a secondary gathering place for the community. It will be programmed by surrounding retail uses and attract transit and other users.

Program Intent

- » Eastern gateway to centre and secondary plaza
- » Community hub, place of arrival and departure

- » Perimeter of plaza engaged with adjacent land uses for multiple "front door" opportunities

- » Possible patios for surrounding food service retail

Design Elements

- » Urban with extensive hard surfacing, substantial areas of soft landscape

- » Topographically diverse angular planters with a mix of deciduous and evergreen trees of varying height

- » Balanced hard and soft landscape expression

- 1 » Seating with rain protection
- » Potential integrated transit stop with weather protected seating for departure and arrival

- 2 » Feature pedestrian crossing at Capilano Road

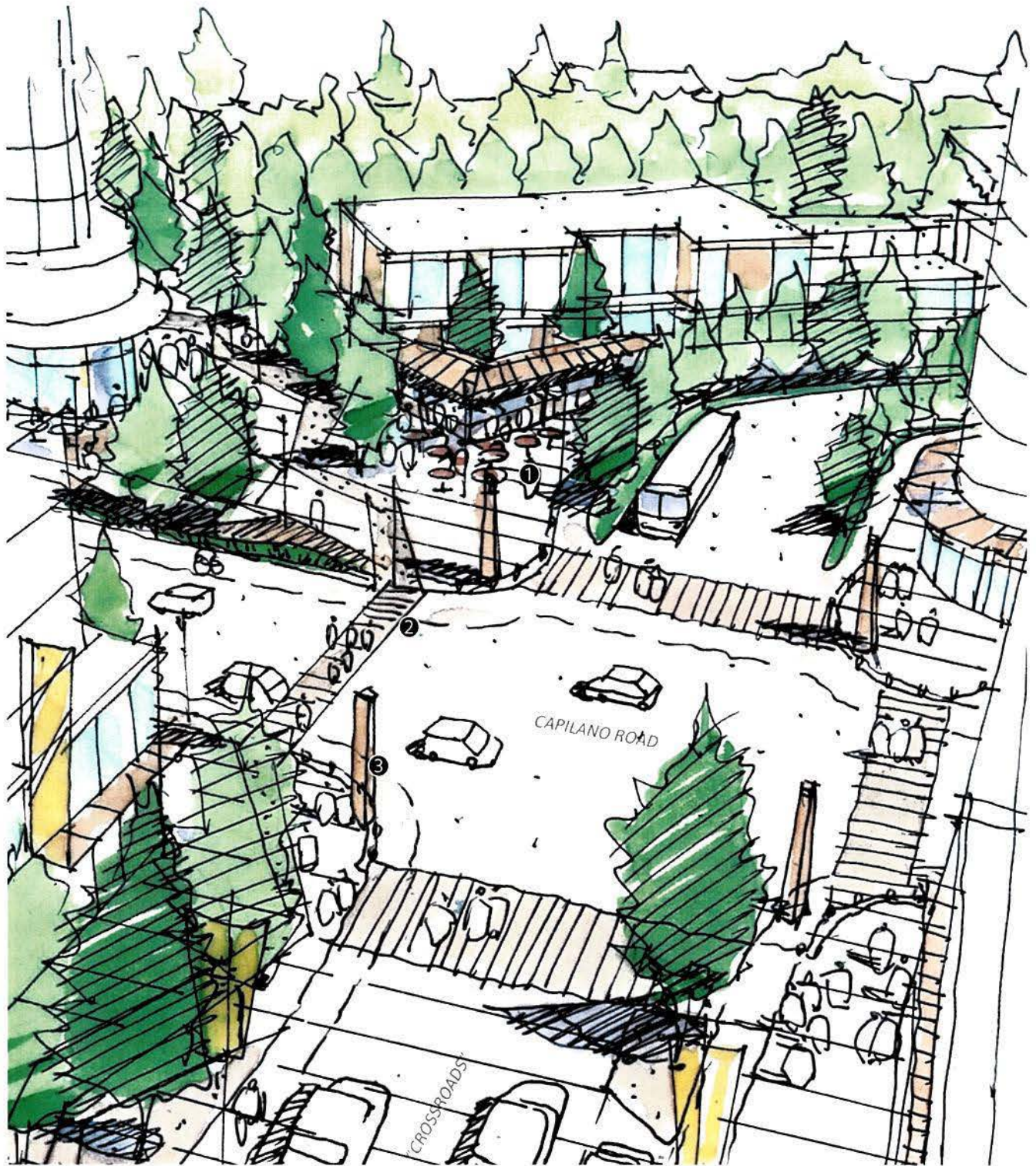
- » Integrated depressions for rainwater management and planting as buffer to Capilano Road

- 3 » Vertical element as community marker and public art at intersection

- » Islands of crushed stone with canopy and evergreen trees and understory vegetation



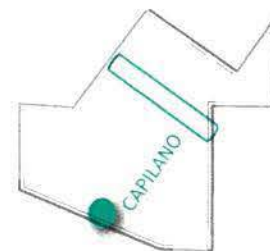
CROSSROADS PLAZA DIAGRAM



CROSSROADS PLAZA: ILLUSTRATIVE PERSPECTIVE LOOKING EAST

↑ N SCALE: 1/8"





GATEWAY PLAZA

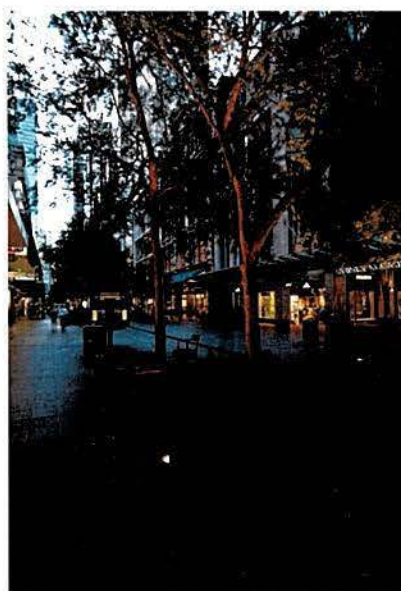
Located in the northwest corner of Capilano Road and Marine Drive, the Gateway Plaza is the southern gateway to the centre of Lions Gate. This plaza draws people in and transitions pedestrians from Marine Drive to the neighbourhood area. Retail uses around the edge of the plaza will help to animate and program the space.

Program Intent

- » Primary function: gateway to centre
- » Perimeter of plaza engaged with adjacent land uses for multiple "front door" opportunities
- » Seating with rain protection
- » Possible patios if there is surrounding food service retail
- » Vertical element as community marker and public art at Capilano Road / Marine Drive intersection

Design Elements

- » Should be urban in character with extensive hard surfacing
- » Areas of soft landscape included in topographically diverse angular design planters with a mix of deciduous and evergreen trees of varying height
- » Possible fountain for noise attenuation
- » Significant planting as buffer to Marine Drive
- » Islands of crushed stone with canopy and evergreen trees
- » Islands of understory vegetation with high deciduous canopy and evergreen trees



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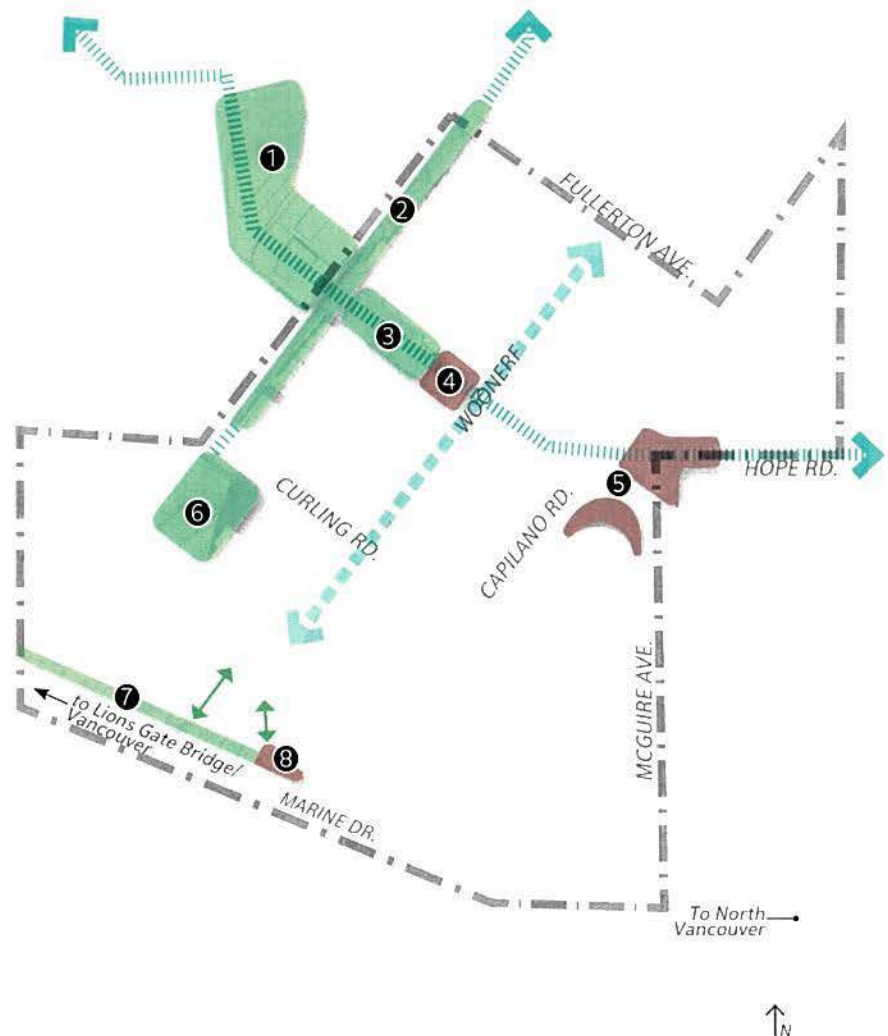
CHARACTER AREAS: NEIGHBOURHOOD PARKS, CONNECTIONS AND OPEN SPACES

Within the centre of Lions Gate, three neighbourhood parks, three plazas and two greenway connections are linked to form an internal network that serves to integrate the peripheral neighbourhoods into the village heart and facilitate future pedestrian connections to the Capilano River trail system and beyond.

The parks and plazas are collectively programmed as a well-connected hierarchy of park and open spaces serving the recreational and social needs of the community by providing programmed and informal play space, casual recreation, meeting places and green space for walking.

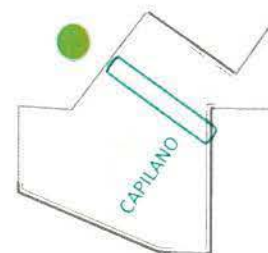
Parks and Open Space Network

1. Belle Isle Park
2. Greenway
3. Village Green
4. Village Plaza
5. "Crossroads" Plaza
6. Curling Road Park
7. Marine Greenway
8. Gateway Plaza



OPEN SPACE NETWORK MAP

-  » Neighbourhood Parks and Open Space
-  » Village Heart & Plazas
-  » "Woonerf"
-  » Major Pedestrian Connections
-  » Pedestrian Green Connections



BELLE ISLE PARK

Belle Isle Park is an existing flat and relatively open grassed area. Present use for informal and passive activities should be retained with some added seating and tables.

Over time the intent is to explore opportunities to expand Belle Isle Park to reduce the extent of under-utilized paved surfaces and more fully connect the Park into the open space network to be contiguous with the Village Green.

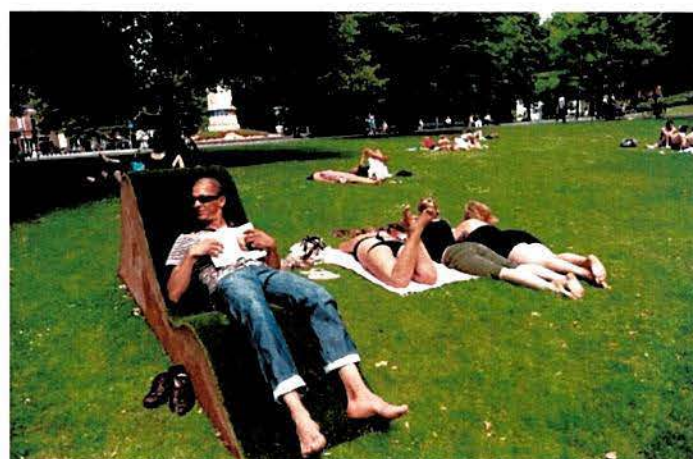
The design program outlined below is based on an expanded Belle Isle Park.

Program Intent

- » A variety of seating types including tables
- » Open and sunny space for informal play, picnicking, relaxing
- » Open area for active and passive recreation
- » Hard surface multi-sport court (i.e. half basketball court)
- » Swings and other play features
- » The park should be primarily open lawn areas with seating opportunities
- » Potential for daycare with outdoor play space to be located adjacent to the park

Design Character

- » Should include large open areas framed by deciduous and evergreen trees for sunlight exposure
- » Small undulations in the topography and berms for views over the space
- » Informal groupings of trees in moderate sized planted areas of soft landscape understory to frame and protect the park from surrounding streets, "forest pockets" with seating opportunities



BELLE ISLE PARK: ILLUSTRATIVE IMAGES





CURLING ROAD PARK

Curling Road Park is envisioned as a new adventure play park with the secondary purpose being passive use. The park marks the start of the new greenway connection leading to the "Crossroads" and Village Heart. A feature sculpted berm with water play elements and safety surfacing around the perimeter is proposed. There is some open lawn area for passive use and a variety of seating including benches, tables and chaises lounges.

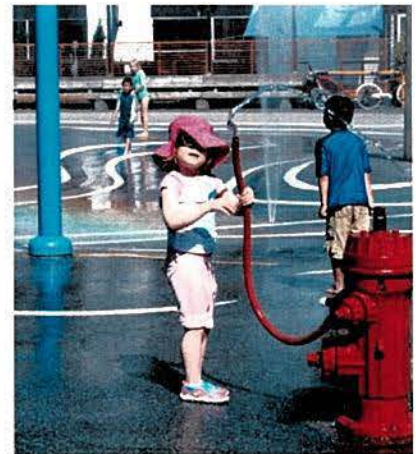
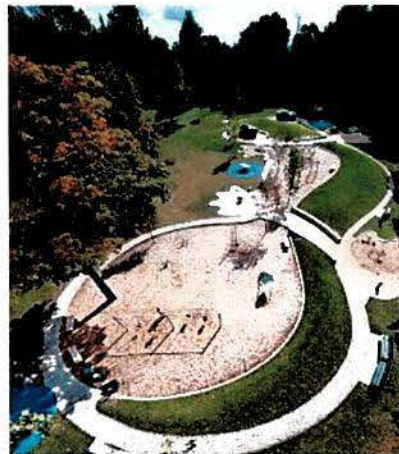
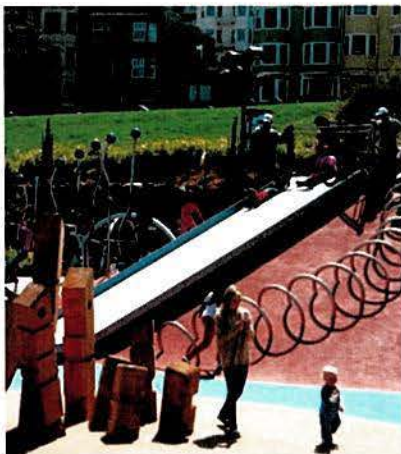
Program Intent

- » Adventure play on sculpted berm incorporating water play and / or slides
- » Some seating positioned for monitoring of play area

Design Character

- » Synthetic lawn surfacing on the berm, wide area of safety surfacing around the berm with variety of play elements
- » Planted berms around park perimeter to frame the park

- » Should be a unique mountain and forest inspired adventure play ground, surrounded by open lawn areas and berms to frame and protect the park from the surrounding streets, "forest pockets" with seating opportunities
- » The greenway character and expression should frame the west side of the park as it continues to connect with the crescent shaped street



CURLING ROAD PARK: ILLUSTRATIVE IMAGES



GREENWAY

The Greenway is envisioned as a 6.0 metre wide pedestrian priority connection from Fullerton Avenue to Curling Road. The Greenway provides connectivity throughout the centre of Lions Gate, reinforcing it as a walkable community and connecting park and plaza spaces.

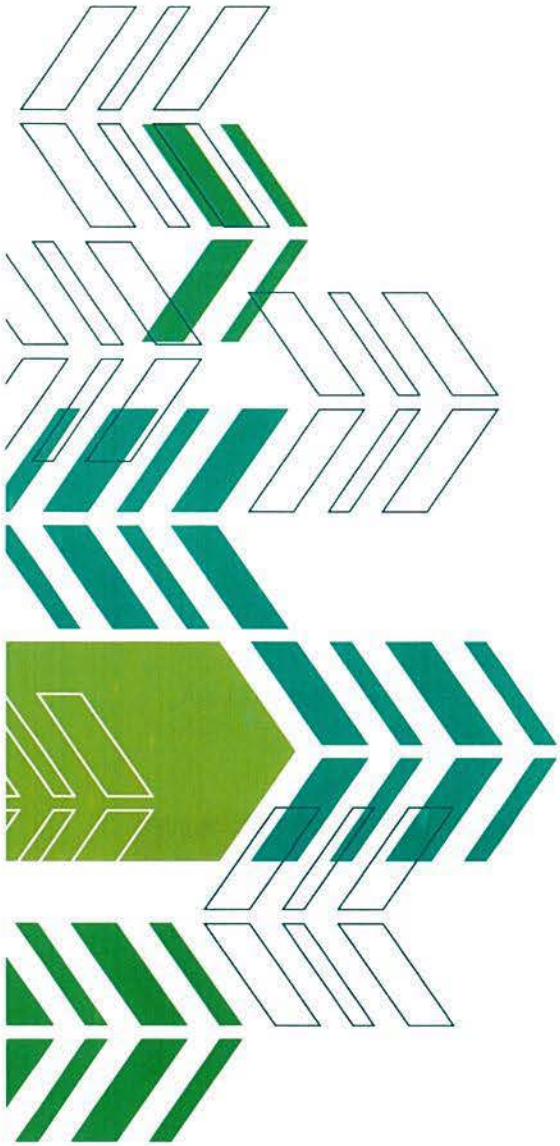
Design Elements

- 1 » Deciduous and evergreen trees should create an overhead canopy and sense of enclosure along the length of the greenway
- 2 » The plantings should have a lush understory beneath a naturalized arrangement of a mix of trees
- 3 » 4.0 metre pathway with 2.0 metre landscaping buffer on west side
- 4 » Concrete surface to comfortably accommodate pedestrians and cyclists
- 5 » Linear "river village" sidewalk form with seating clusters in the "eddies"
- 6 » Pedestrian lighting
- 7 » Way finding signage
- 8 » Opportunities for "front door" connections to adjacent residential units on east and west sides

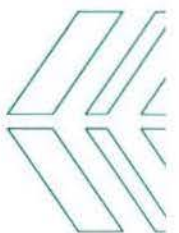


GREENWAY ILLUSTRATIVE PERSPECTIVE LOOKING NORTH





3.0 STREETS










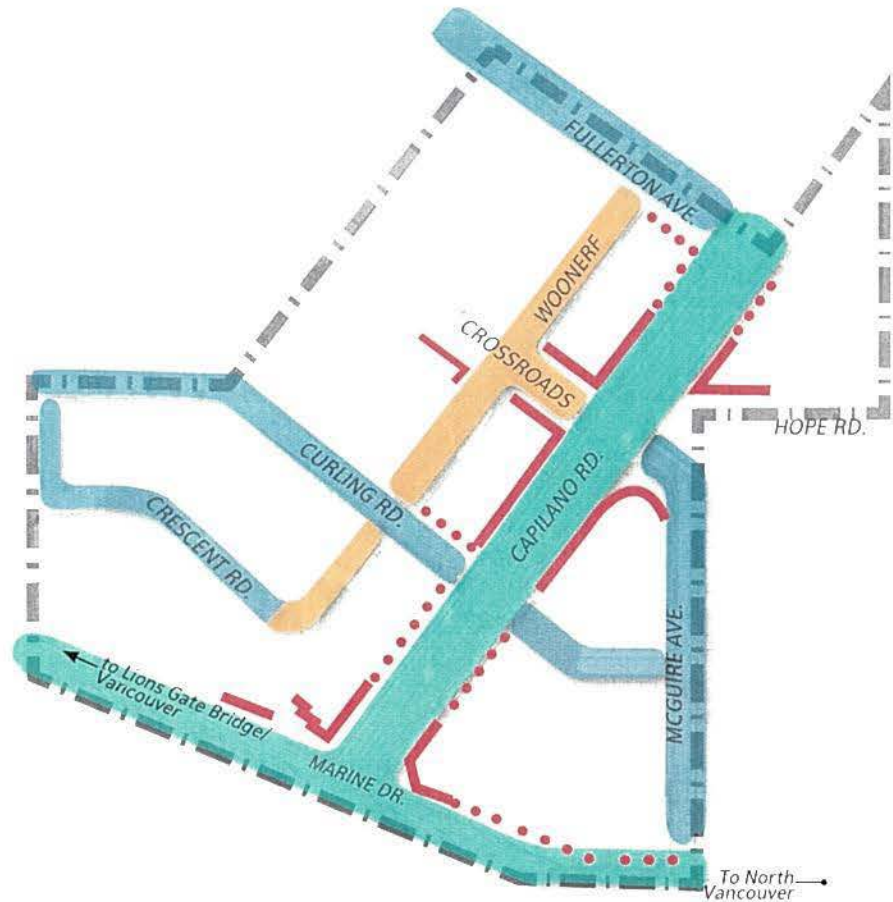
STREET NETWORK

This section describes the character of the key centre streets including:

1. Capilano Road
2. Marine Drive
3. Curling Road
4. Fullerton Avenue
5. McGuire Avenue
6. "Crossroads"
7. "Woonerf"
8. "Crescent Road"

Each street type offers unique elements relating to traffic volumes, adjacent building use and natural features. Peripheral neighbourhood streets should be designed with similar cross sections and elements as per the Neighbourhood Streets, however cross sections will vary when necessary to accommodate future conditions. All cross sections are subject to engineering and design review.

-  » Major Road Network
-  » Shared Use Streets
-  » Neighbourhood Streets
-  » Commercial Frontage
-  » Choice of Commercial or Residential Use



PROPOSED ROAD NETWORK



CYCLING NETWORK

Establishing a hierarchy of bike connections through the centre of Lions Gate aims to provide internal circulation, safe and viable commuting routes and the opportunity to increase cycling infrastructure and use. It is important to provide a variety of bike lane configurations that are suitable to the context, road configuration, land availability and safety considerations.

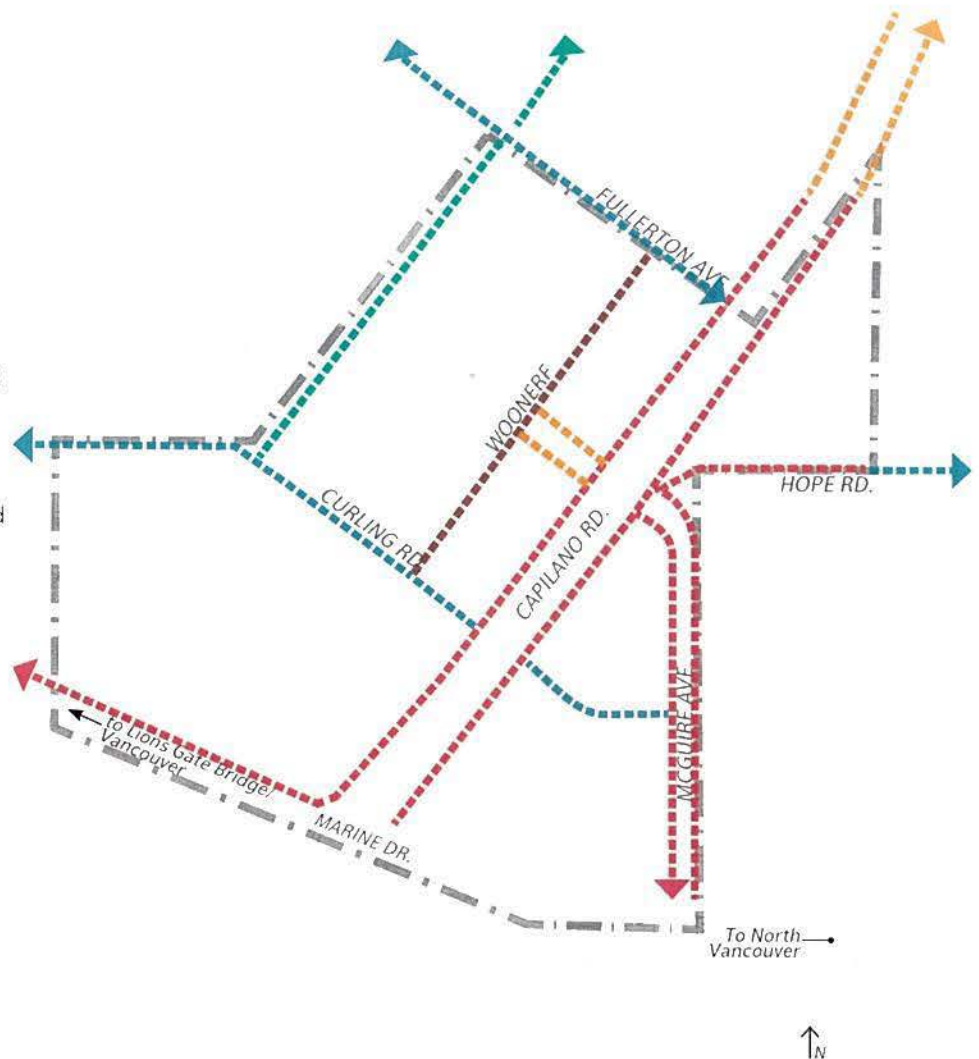
Multi-modal paths, including the greenway are to be shared environments between cyclists and pedestrians with ample space to accommodate mixing of all user types.

Sharrows or shared lanes are painted road marking to indicate a shared lane environment for cyclists and automobiles.

Conventional bike lanes offer greater separation of modes with the delineation of spaces with painted lines.

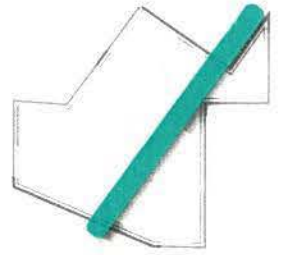
Separated bike lanes offer safety improvements by physically separating cyclists from automobile traffic. The use of concrete medians and vertical signage can be used to improve safety and make cycling lanes viable for users of varying ability.

- » Multi-modal Path
- » Sharrow
- » Shared Space
- » Conventional Bike Lane
- » Separated Bike Lanes



PROPOSED BIKE FACILITIES





CAPILANO ROAD

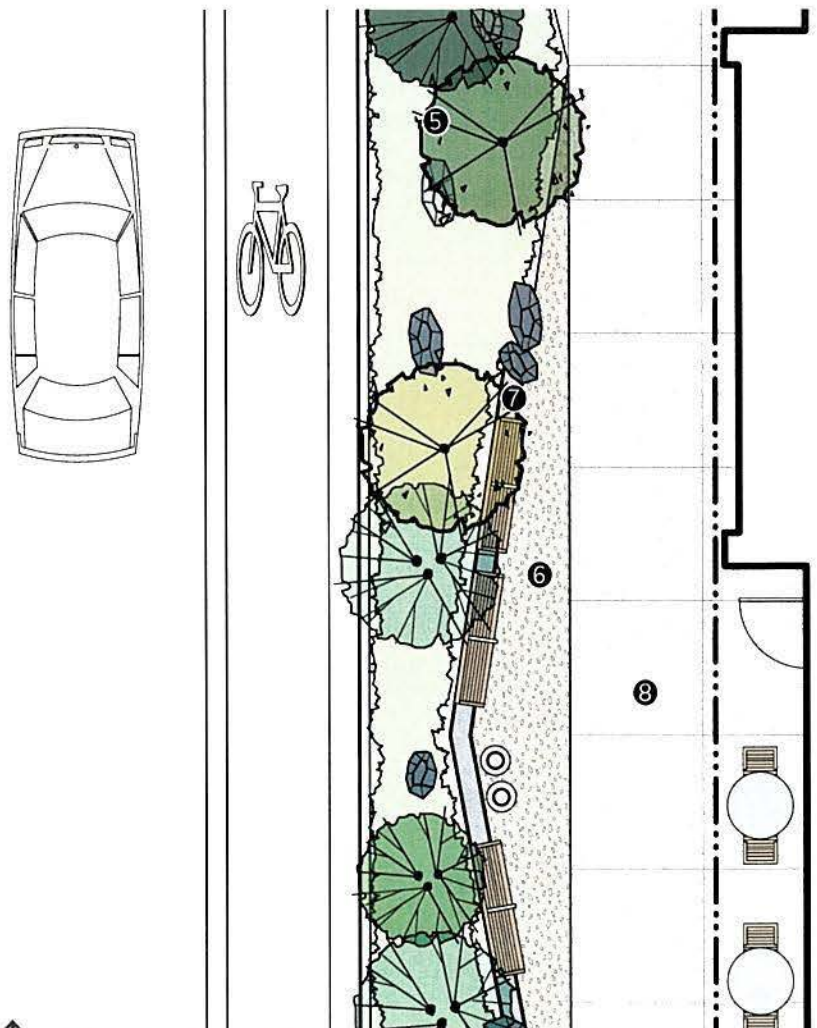
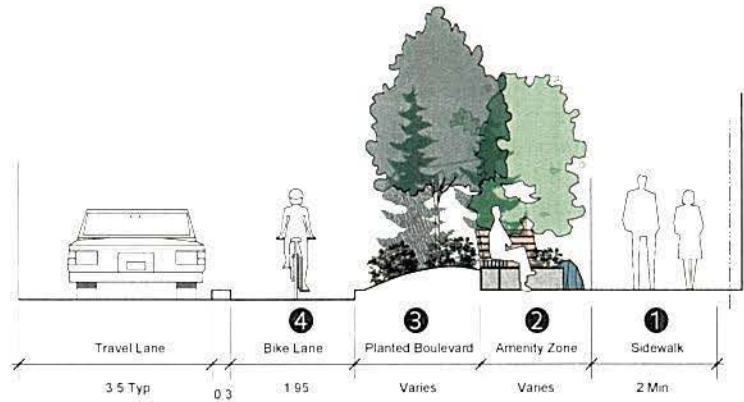
Major Road Network

Recognizing that this is a major arterial road, a key objective for Capilano Road is to achieve an interesting and safe pedestrian condition that enables access to the street retail environment. Capilano Road will be the main shopping street of the centre. The pedestrian realm will be comprised of a significant sidewalk, eddy zones for seating and resting and substantial planting to buffer from adjacent traffic.

Program Intent

Capilano Road features the "river village" sidewalk and boulevard design, as described on page 7.

- 1 A 2.0 metre concrete "fast" zone provides unimpeded pedestrian movement immediately adjacent to the building frontage. See page 66 for the grid pattern and dimensions of the river village sidewalk and boulevard design.
- 2 There are two zones within the boulevard. The first is the slow zone with exposed aggregate furniture / amenity zone accommodates seating, waste and recycling receptacles, newspaper boxes, street lighting etc.
- 3 The second is a bermed planting zone at the curb which provides a buffer from traffic. Each of these two zones is 1.5 to 3.0m wide and offset so that the combined width is 3.0m. In the narrower portions of the planting zone a 0.45m concrete retaining wall with a wood top can provide seating. The furniture



PARTIAL CROSS SECTION OF CAPILANO ROAD (EAST SIDE)

zones are located in consideration of the opposite retail frontage. For example, larger furniture zones are appropriately located opposite a coffee shop or similar type of use. Tree groupings are located in the alternating wider planting zones in order that retail signage is more visible from the street.

- 4 A protected bike lane will provide for safe cycling movements along this busy arterial road adjacent to the travel lane. The bike lane will be separated with a 0.3m extruded concrete median with appropriate signage at intersections.

Design Elements

- » Protected bike lane at curb
- 5 » Mix of deciduous trees and ground cover in bermed planting zone with a mix of ground covers and shrubs
- » Trees in groupings: 2-3 min.
- 6 » Slow Zone with exposed aggregate material
- 7 » Low concrete seating walls with timber tops.
- 8 » Minimum 2.0m unobstructed concrete sidewalk from building face with saw cuts, 2.0m pattern (see pg. 66)
- » Boulders as accent elements combined with integrated seating which acts to retain planting beds



ILLUSTRATION OF PROPOSED CONDITIONS





MARINE DRIVE

Major Road Network

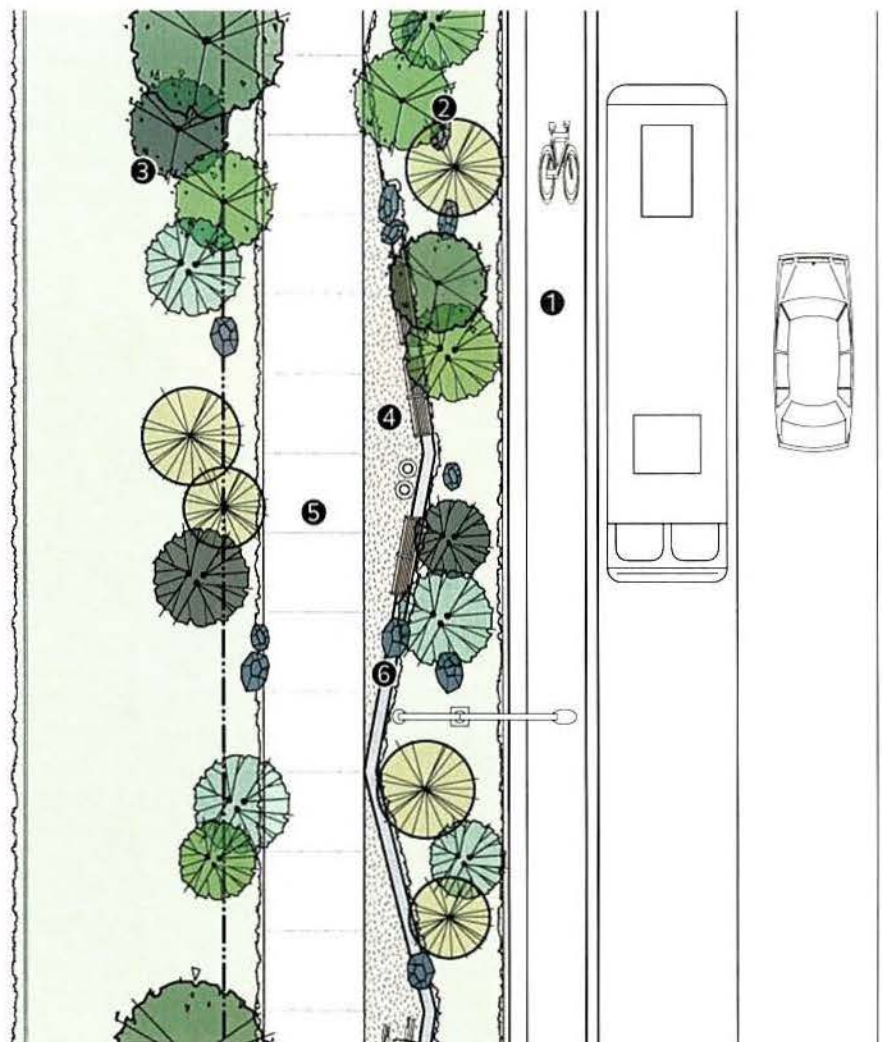
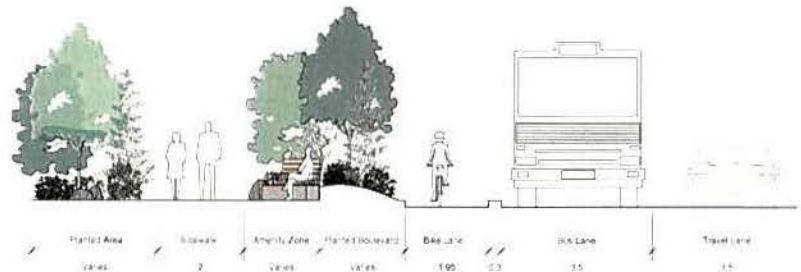
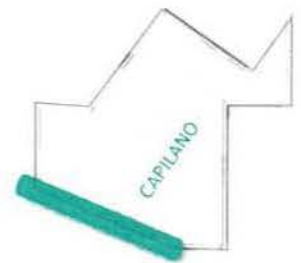
Marine Drive is a prominent gateway and entry point to both the centre of Lions Gate and the District of North Vancouver. This will be a critical location for defining the expression of the river village. West of Capilano Road a key public realm objective is to present an attractive and bold tree-lined pedestrian greenway that is buffered from high traffic volumes on Marine Drive, and which connects to the open space plaza network in the centre.

Program Intent

Marine Drive features the "river village" sidewalk and boulevard design, but it differs from Capilano Road in that there are boulevards on both sides of the sidewalk. A dense primary boulevard is located next to the curb. This will follow the angular slow zone/boulevard scheme similar to that described in the Capilano Road section as well as page 7. It is to be planted with deciduous trees and includes a lush understory.

The boulevard will be utilized as a transit hub just west of Capilano Road. At this transit location the pedestrian waiting area and shelter will be positioned closest to the curb with the bike lane between sidewalk and bus waiting zone. This will offer safe movements for cyclists, pedestrians and commuters.

Driveway accesses will be maintained until such time as they can be closed off and a complete boulevard installed.



N → PARTIAL CROSS SECTION OF MARINE DRIVE (NORTH SIDE)

Next to the primary boulevard is a 2.0m unobstructed concrete sidewalk, behind which is the secondary boulevard that is planted with evergreens. These are representative of the natural forested character of the North Shore. This planting will be significant, acting as a buffer between Marine Drive and adjacent buildings as well as a dramatic gateway feature to the neighbourhood.

Design Elements

- ① » Protected bike lane at curb
- ② » Deciduous trees in groupings, dense ground cover in primary boulevard (south side)
- ③ » Evergreens in north boulevard, planting zone extends into private realm to accommodate growth
- » Trees in minimum groupings of 3
- ④ » Exposed aggregate in furniture zones and primary boulevard openings
- ⑤ » Minimum 2 metre unobstructed concrete sidewalk with saw cuts, 2m pattern in between the boulevards (see pg. 67)
- ⑥ » Use of large boulders as accent elements and to demarcate bus stops
- » Bus stop accommodated within boulevard with demarcated, at-grade pedestrian crossing of bike lane



ILLUSTRATION OF PROPOSED BOULEVARD AND SIDEWALK CONDITIONS ON THE NORTH SIDE OF MARINE DRIVE



BUS STOP AND BIKE LANE CONFIGURATION



"WOONERF" STREET

Shared Use

The "Woonerf" is envisioned to be a pedestrian first, special place within the heart of Lions Gate serving several purposes beyond the typical functions of a street. It provides a strategic north/south connection through the neighbourhood and will be a shared, multi-modal space with the highest priority given to pedestrians.

The "Woonerf" will be a busy mix of activities shaped by irregular planting islands alternating with parking pockets to create meanders in the roadway, slow vehicle traffic and reflect the river theme.

The road will be designed and surfaced as an extension of the central plaza so that it can be closed off for large scale community events. Pavers will be used in the central section of the "Woonerf" road as in the central plaza and the "Crossroads" to visually connect these important public realm spaces.



Program Intent

- » Central roadway supporting a wide variety of functional activities that is able to become a flexible plaza and open space when closed to vehicle traffic
- » Curbless paving treatment from the west to east property lines
- » Features to define the sidewalk area from the parking aisles and roadway for the visually impaired
- » Underground parking access to east buildings should be limited to one location north and one south of the Crossroads



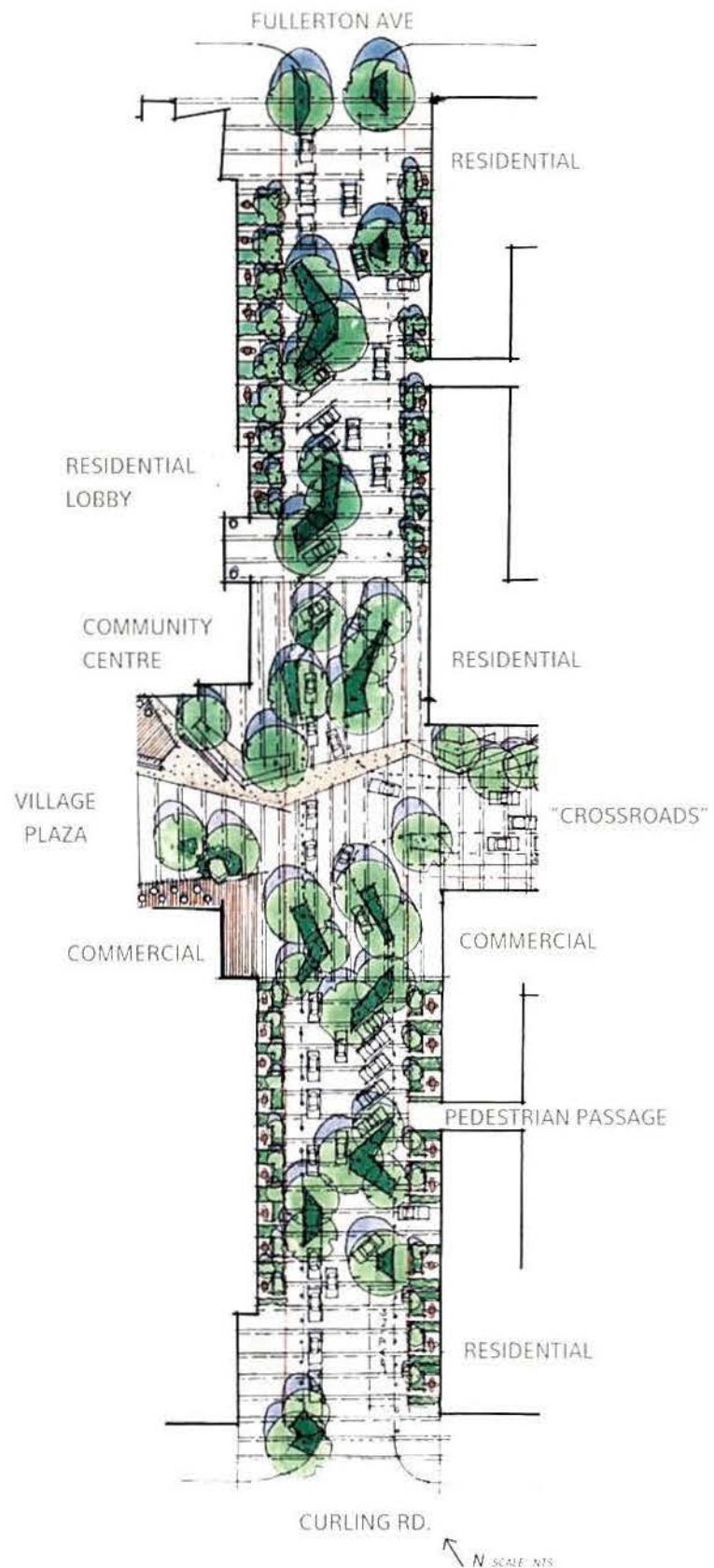
CONCEPTUAL DRAWING OF "WOONERF"

- » Avoid underground parking to buildings on the west side
- » Central crown with drainage to concrete gutters and directed to integrated rain gardens in the planter areas
- » Parallel or angled parking with drop off stalls close to the community centre
- » On street loading bays may be considered that can potentially be used as parallel parking at non-loading hours

Design Elements

- » Extensive hard surfacing to allow for flexible movement
- » Substantial areas of soft landscape in topographically diverse angular design planters with a mix of deciduous and evergreen trees of varying height
- » Same pavers as for central plaza and "Crossroads" applied across full width of road
- » 16.0m width of paving edge to edge
- » Large canopy trees, evergreen trees, lush understory in planting islands

Note: Developer of adjacent land to follow design intent and prove out technical aspect such as vehicle travel route and parking.





"CROSSROADS"

Shared Use

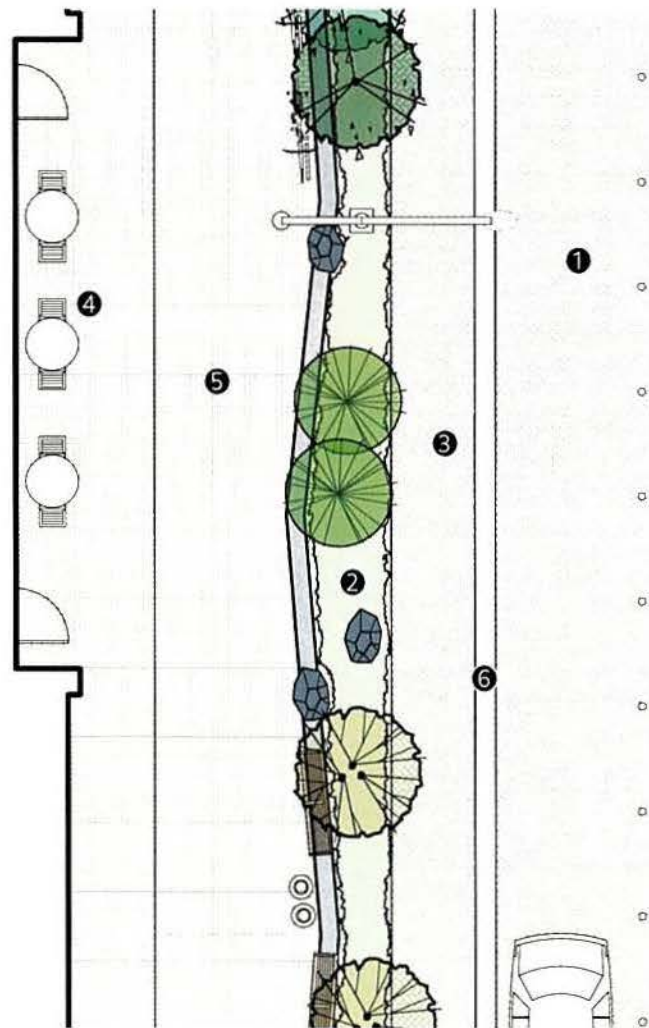
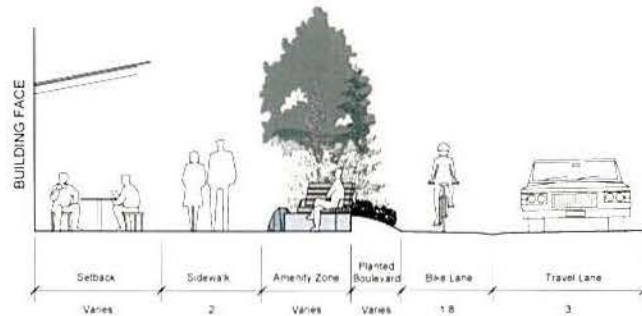
The "Crossroads" is a significant addition to the local road network providing residents with better vehicular access in and out of the neighbourhood and an improved pedestrian connection to the neighbourhood east of Capilano Road.

Like the "Woonerf", the "Crossroads" may be closed off occasionally to expand the central plaza for community events, allowing crowds to spill out into both spaces. The "Crossroads" is a secondary gateway location as the entrance to the Village Heart and will be recognized as such through public art installations at the intersection including feature crosswalk treatment and vertical elements framing the intersection.

The paving treatment will include the use of the same pavers as the "Woonerf" road and Village Plaza as well as a river themed angular band of exposed aggregate.

Program Intent

- » Local vehicle access with safe pedestrian movement taking priority
- » Two-way vehicle access and demarcated bike lanes with concrete banding
- » On-street parking may be provided (south side) if possible
- » Shade trees within islands of crushed stone
- » 2m setback at ground level on private properties for retail spill out and enhanced sidewalk condition
- » Catenary lighting leading to Village and "Crossroads" plazas



←N PARTIAL CROSS SECTION OF "CROSSROADS" (NORTH SIDE)

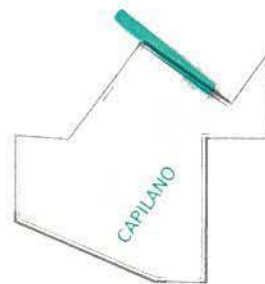
Design Elements

- » Visual and physical gateway to the Village Plaza from Capilano Road - extension of the Village Plaza
- » Urban with extensive hard surfacing with some shade trees
- 1 » Street with special paving (matching pedestrian areas, Woonerf and Village Plaza) creating a seamless treatment from the north to south building frontages
- 2 » Continuation of angular river planting on north side
- 3 » Bike paths with special demarcation
- 4 » Retail edges along both sides to promote on street activity
 - » Rain protection
 - » Feature pedestrian crossing at Capilano Road
- 5 » 2.0m sidewalks utilizing pavers consistent with the "Woonerf"
- 6 » concrete gutter curb for stormwater management and bike lane demarcation

Note: Developer of adjacent land to follow design intent and prove out technical aspect such as vehicle travel route, parking, and bike lane.



CONCEPTUAL DRAWING OF "CROSSROADS" LOOKING WEST



FULLERTON AVENUE

Neighbourhood Street

Fullerton Avenue is a neighbourhood street providing access to the lower density residential developments in the Peripheral Area of the centre of Lions Gate. Fullerton has one shared travel/bike lane in each direction plus a turn lane at Capilano Road. Some on-street parking is provided west of the "Woonerf".

Design Elements

» The public realm of the south side of Fullerton Avenue needs to transition to the angular eddy treatment in the "Woonerf" and Capilano Road public realm

» Seek to retain existing Plane trees on south side

1

» 2.0m boulevard planted with naturalized groupings of trees with lush understory planting of ground covers, low shrubs, perennials, ferns and ornamental grasses

2

» Trees in minimum groupings of 3

» 2.0m concrete sidewalk with 2.0m grid saw cuts (see pg. 69)

3

» Aggregate let down with bollards at entry point to Greenway

» Aggregate openings in boulevard for driveway accesses

4

» Boulders in boulevard as accent items

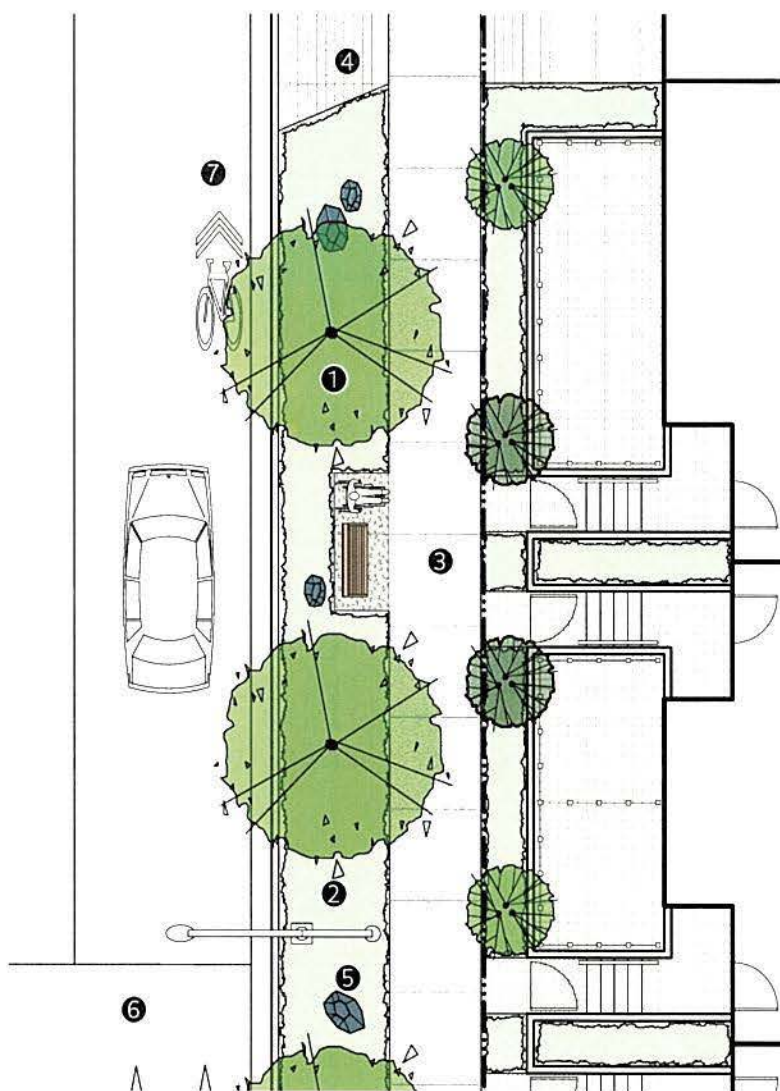
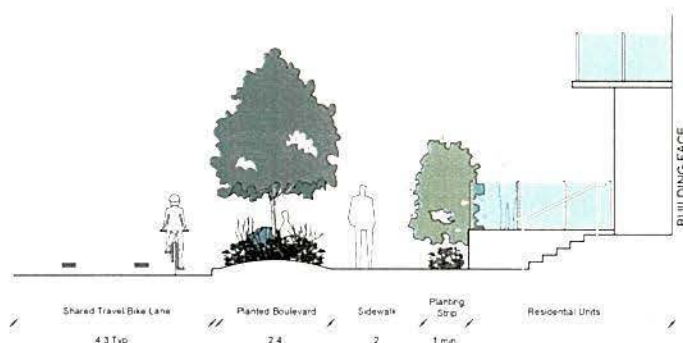
5

» Speed humps to provide traffic calming

6

» Sharrow demarcation for cyclists

7



←N PARTIAL CROSS SECTION OF FULLERTON AVENUE SOUTH SIDE

CURLING ROAD

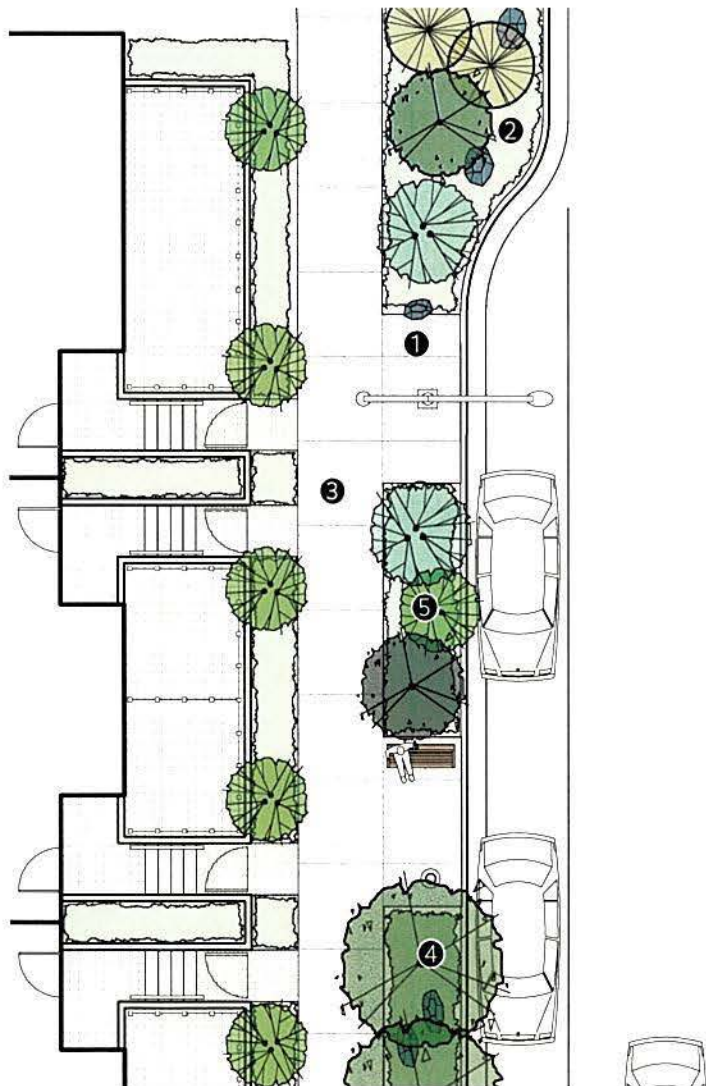
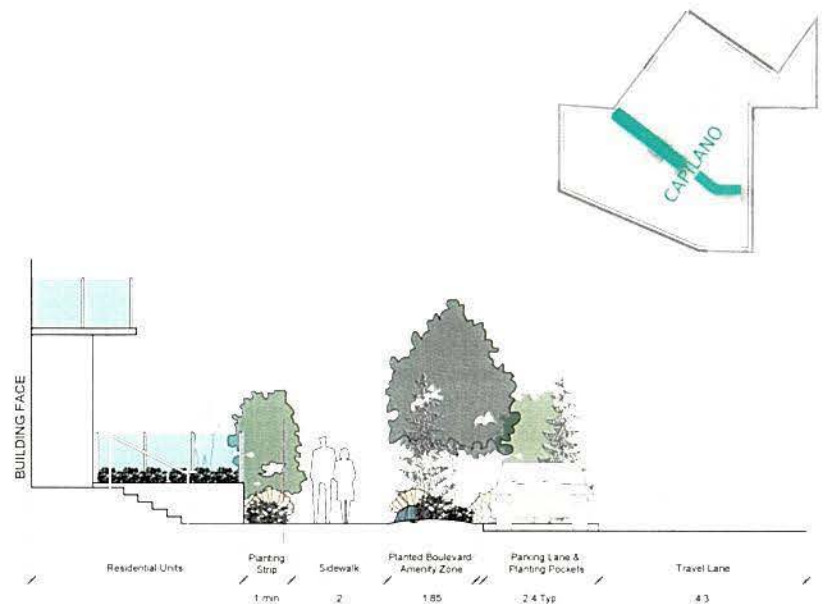
Neighbourhood Street

Curling Road is the only east-west road spanning the centre of Lions Gate and linking the community across Capilano Road. Curling Road is primarily a residential street west of Capilano Road and mixed use east of Capilano Road. There is one shared bike and travel lane in each direction and left turn lanes at Capilano Road and McGuire Avenue. Curling Road also provides access to the properties in West Vancouver that are east of the Capilano River.

Boulevard and sidewalk treatment is similar to that of Fullerton Avenue, which is a 2.0m concrete sidewalk behind a 2.0m boulevard with trees in grouping with a lush understory of ground cover of low shrubs, perennials, ferns and ornamental grasses.

Design Elements

- 1 » Aggregate openings in boulevard for driveway accesses or parallel parking pockets
- 2 » Boulders and logs in boulevard as accent items
 - » Public realm must transition to the angular eddy treatment on Capilano Road
- 3 » 2.0m wide concrete sidewalk with 2.0m grid saw cuts (see pg. 70)
- 4 » 2.0m boulevard, lush planting mix of ground covers, low shrubs, perennials, ferns and ornamental grasses
- 5 » Trees in minimum groupings of 3
 - » Boulders as accent items



←N PARTIAL CROSS SECTION OF CURLING ROAD NORTH SIDE



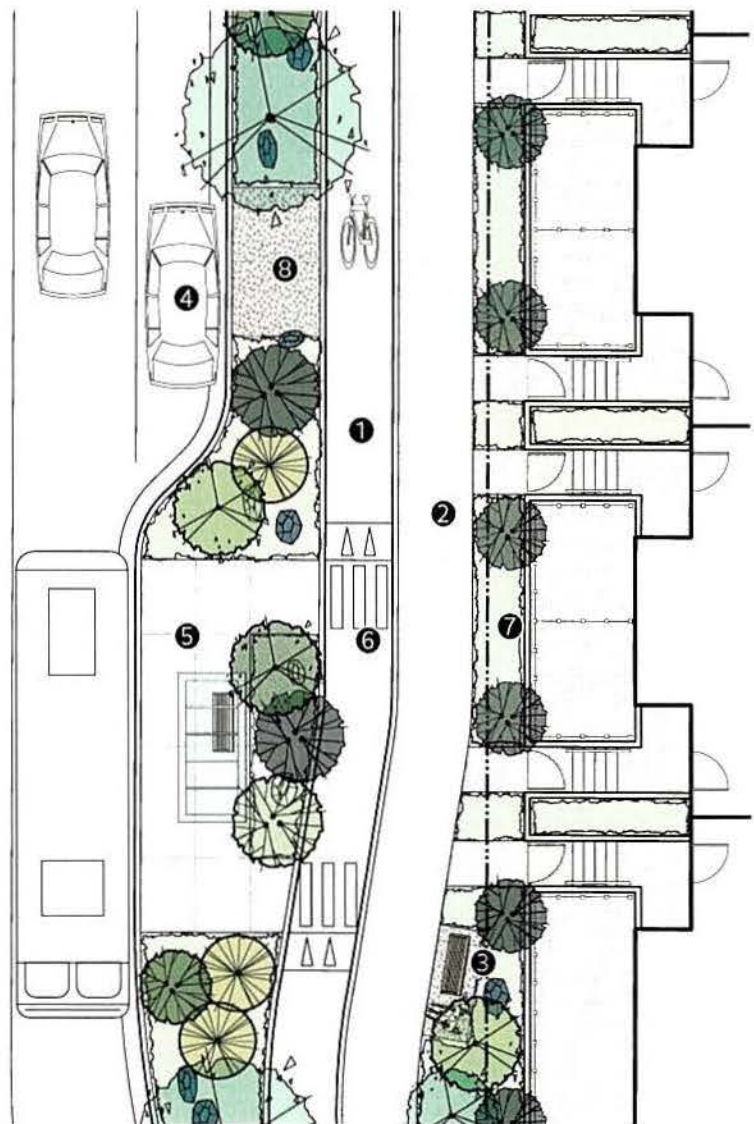
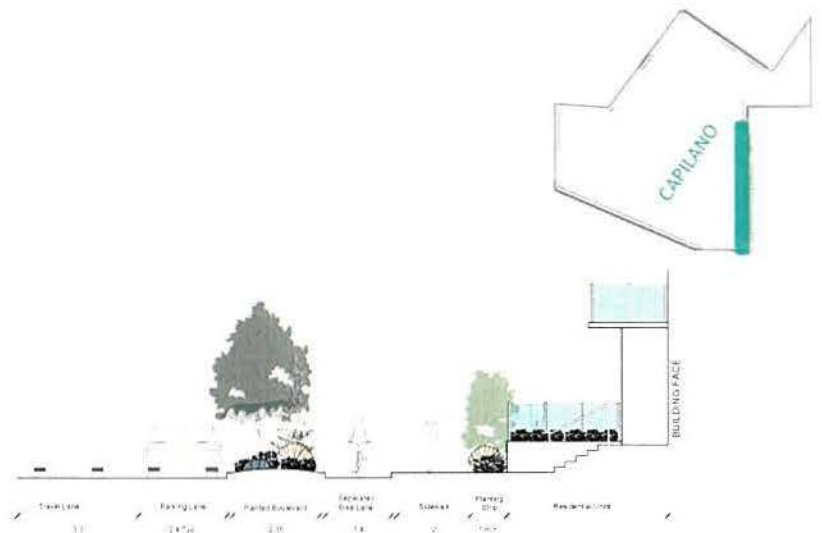
MCGUIRE AVENUE

Neighbourhood Street

McGuire Avenue combines many uses: pedestrians, bikes, cars, and bus, and is intended to function as a triple A bike route and major bus route. A safe commuter corridor is created with the use of wide, lush boulevards and bike path separation. Proper separation between all uses is provided with boulevard planting, grade separation, or median zone with seating and planting. When needed, openings in boulevard are created with exposed aggregate material to provide for clear and safe bus stop access.

Design Elements

- » Naturalized groupings of trees with lush understory planting interspersed by turf openings in the boulevard
- » Increased separation from the roadway to the existing and future residential units
- » Bike lanes grade separated from the sidewalks
- 1 2.0m wide concrete sidewalk with
- 2 2.0m grid saw cuts (see pg. 71) extending into the "Crossroads" to the north.
- 3 Integrated street furniture including seating
- 4 Integrated on-street parking
- 5 Bus stops integrated into boulevard
- 6 Raised pedestrian crossing to bus stop
- 7 Lawn or understory plantings on property side of sidewalk
- 8 Aggregate parking access across boulevard



↓ N PARTIAL CROSS SECTION OF MCGUIRE AVENUE WEST SIDE

CRESCENT ROAD

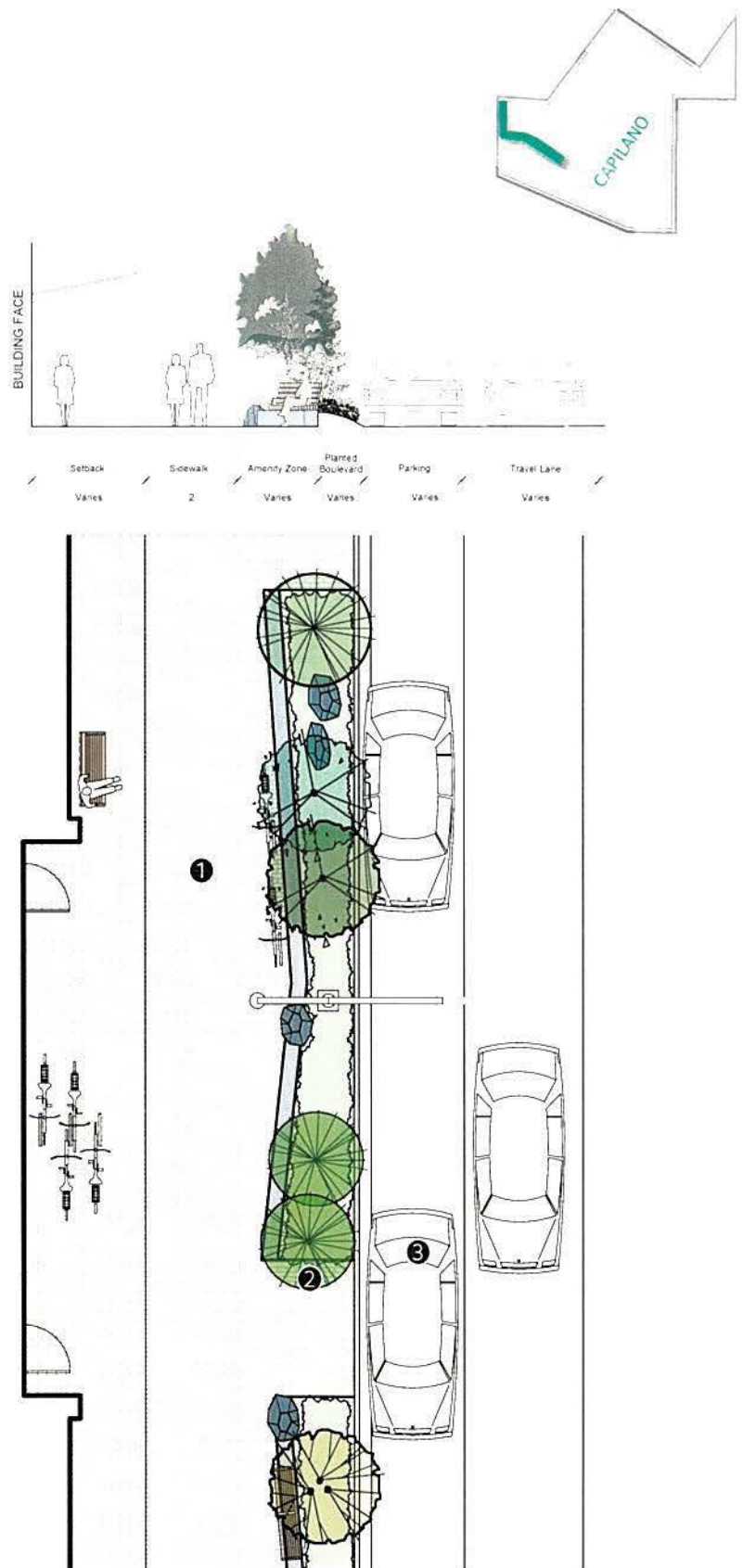
Neighbourhood Street Hybrid

Crescent Road will be a hybrid condition connecting a residential street typology to the shared conditions at the southern extension of the "Woonerf" street. This more urban expression of a residential street will utilize materials consistent with the "Woonerf" in the pedestrian spaces while using typical asphalt travel lanes. Parking will be provided as appropriate.

Boulevards will be consistent with the "Crossroads" design, integrating the angular boulevard pattern. Planting beds should be bermed with trees in grouping and a lush understory of ground cover of low shrubs, perennials, ferns and ornamental grasses. Spaces will be provided to allow more pedestrian porosity onto and off of the residential street.

Design Elements

- ❶ 2.0m clear pedestrian sidewalk utilizing pavers.
- ❷ Openings in boulevard for greater porosity of movement.
- ❸ Parallel parking where necessary and suitable.
 - » Boulders and logs in boulevard as accent items
 - » Public realm should transition to the angular eddy treatment on the "Woonerf".

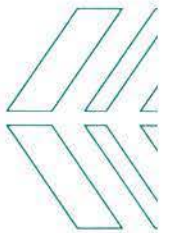


← N PARTIAL CROSS SECTION OF CRESCENT ROAD NORTH SIDE





4.0 PUBLIC ART





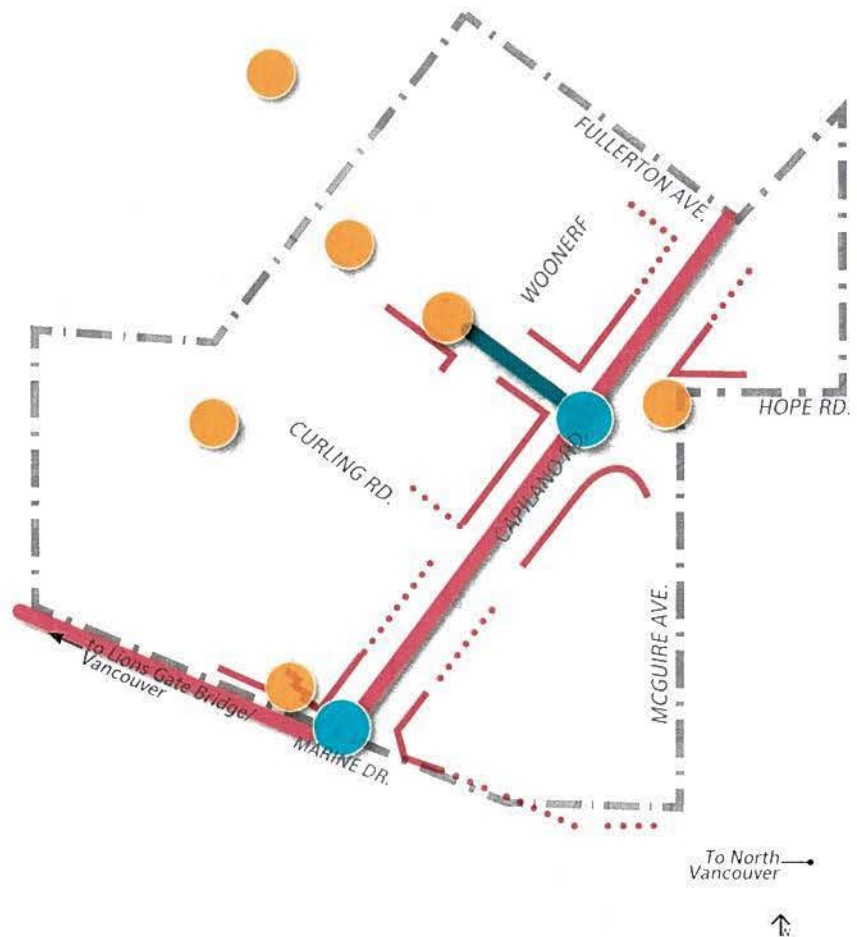
PUBLIC ART STRATEGY

Funding and implementation procedures for public art, including site selection criteria and other guidelines are outlined in the District's "Public Art Policy". See also the "Lions Gate Centre Public Art Plan" (Formerly the Lower Capilano Marine Village Centre Public Art Plan) for a description of the overarching goals and guiding principles of the public art program as applied to the Centre.

Public art can play an important role supporting the "river village" public realm theme and helping to achieve a community identity and vision of the future. Public art should creatively explore and interpret the natural environment of the Capilano River system while being meaningful within the setting of its specific location.

Public art should be seamlessly integrated throughout and may take the form of bold and iconic stand-alone pieces located in key open space areas; art features integrated into the design of landmark buildings; interpretive way-finding elements on pedestrian routes and other functional art; as well as recurring markers on both private land and the public realm.

- » Gateways (welcoming features)
- » Land mark buildings
- » Plazas, parks and gathering places
- » Way-finding or guiding elements on pedestrian routes



Public art/ Village Markers Diagram:

- 1. Gateway Features
- 2. Plazas and Gathering Spaces
- 3. River Village Sidewalk
- 4. Overhead catenary lighting

Direction:

- » Gateway locations at Marine Drive and the "Crossroads" should be acknowledged with large scale, vertical elements recognizing that they will be viewed by vehicular traffic.
- » A unique thermoplastic crosswalk design should be commissioned for application at crosswalk locations throughout the village centre.
- » Feature catenary lighting will be designed to link the Village Plaza, "Crossroads" and "Crossroads" plaza.
- » Public art installations should foster awareness of the surrounding environment, ecosystem and greater context
- » Elements should include a variety of scales and expressions that are both verticle and horizontally integrated within the ground plain.
- » Use of nurse logs and boulders within landscaping areas on both public and private land is encouraged as a unifying feature for the Village Centre.





5.0
SITE
FURNISHINGS +
LIGHTING





SITE FURNISHINGS

High quality durable furnishings are proposed to provide the best value over time and reduced maintenance. In addition, it is proposed that all street furniture is placed in the exposed aggregate areas of the sidewalks to reduce clutter and provide clear movement zones.

Seating elements should include a combination of off shelf products identified below and custom made, built in place seating solutions that will provide sufficient seating for all age groups, needs and configurations.

Waste Receptacle

- » Manufacturer: Victor Stanley
- » Model: SDC-36
- » Material / Colour: Titanium Finish
- » 36 gal (136 L)
- » Side-Deposit
- » Side-Door Opening



Recycling Receptacle

- » Manufacturer: Victor Stanley
- » Model: RSDC-36
- » Material / Colour: Titanium Finish
- » two half moon plastic liners
- » Side-Deposit
- » Side-Door Opening



Bike Rack

- » Manufacturer: Landscape Forms
- » Model: Ring
- » Material / Colour: Titanium colour powdercoated metal
- » Frame made of aluminum, painted
- » Bike rack holds 2 bikes and can be arranged in groups



BENCHES

Backless Park Bench

- » Manufacturer: Equiparc
- » Model: EP 1990 Outdoor park bench
- » Supports made of hot dipped galvanized steel, painted
- » Covering available in wood
- » This bench can be assembled in sequence with intermediate supports.

COVERINGS:

- » Wood: Jatoba and Ipe

OPTION:

- » R - Top-of-wall mount



000101A



000101B



000101C



000101D

Outdoor Park Bench

- » Manufacturer: Equiparc
- » Model: EP 1990 Outdoor park bench
- » Park bench with backrest and optional seating arms
- » Supports made of hot dipped galvanized steel, painted
- » Covering available in wood
- » This outdoor park bench can be assembled in sequence with intermediate supports and is also available with a top-of-wall mount.

COVERINGS:

- » Wood: Jatoba and Ipe

OPTIONS:

- » A - Set of 2 painted armrests
- » R - Top-of-wall mount

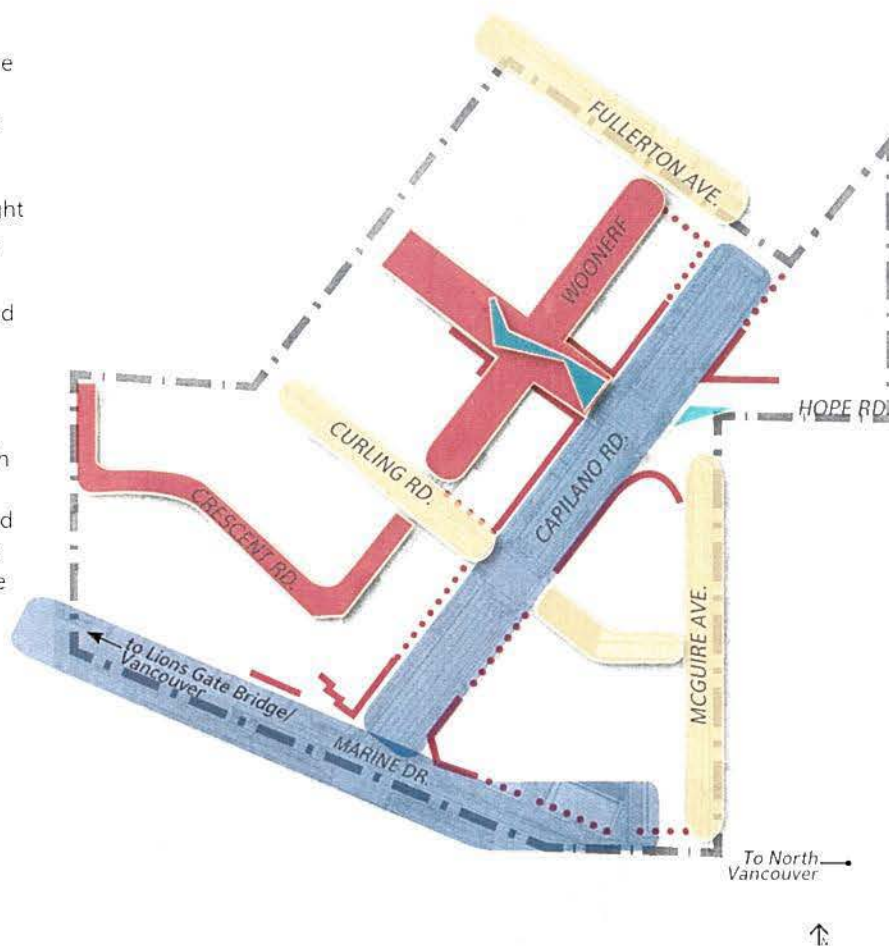




LIGHTING STRATEGY

Lighting of the public realm is an essential element to provide safe spaces and to improve and enhance the experience of the streets while bringing unique character to Lions Gate.

- » Provide consistency of street light typologies by providing options within a family of products
- » Ensure light levels meet required standards for safety
- » Aim to use most compatible energy efficient light sources
- » Provide feature lighting through bollards and catenary systems to create a sense of gateway and arrival as well as warm ambient street character throughout the heart
- » Provide low lighting such as wall lights, bollard lights when possible to improve pedestrian experience, atmosphere and safety



Lighting Diagram:



1. Feature Catenary Lighting - "Crossroads" Plaza, "Crossroads", Village Heart Plaza



2. Low pole pedestrian lighting and bollards with lighting where appropriate - Woonerf



3. Street and Pedestrian Lighting - Curling Rd. Fullerton Ave., McGuire Ave.



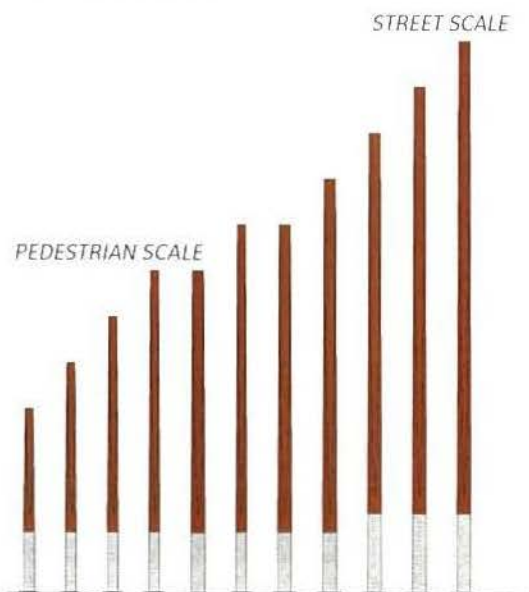
4. Major Street lighting - Capilano Road and Marine Dr. - Street lights as per District standard (not in these guidelines), but should include pedestrian scale lighting as needed

LIGHT TYPOLOGIES:

Pedestrian and Street Lights:

Proposed pedestrian and street light system is "Bol-RSQ" from Structura Inc. If not available, similar light pole should be used.

- » Unique round cross section at base with tapering to square cross section at top of pole
- » Solid glulam wood design
- » Round extruded aluminum powder coated pole base
- » Wood finish: S3 - Cumaru
- » Metal finish: CHRTX Charcoal
- » Available in 8" to 24" heights to provide variety of form and function
- » Variety of mounting brackets and mast arm luminaires to provide a range of light coverage options based on need
- » Ability to be used as pedestrian and street lighting





Fixture Mounting Arms:

Proposed mounting arms including "CENO-SPGT, CENO-SGNL, CENO-TRK, API, PORTO, ISIS, RA, CANTELAM" from Structura Inc. Other Structure mounting arms will be considered based on lighting needs. If not available, similar fixture should be used.

- » Range of options should address specific nature of space and lighting needs
- » Metal finish: CHRTX Charcoal
- » CENO range includes options for event lighting and specific needs associated with programatic elements of a plaza
- » Accept most slip mount luminaires



Feature Catenary Lighting:

Proposed catenary light is "Delta Star" from B-K Lighting. Other compatible products will be considered based on unique design needs and lighting requirements. If not available, similar fixture should be used.

- » Simple shape
- » Colour: mat black
- » Irregular pattern imitating "river band" in the sky
- » Installed on cables suspended from posts (not buildings)
- » For pattern and location, see lighting diagram



Ring Mount
Delta Star™

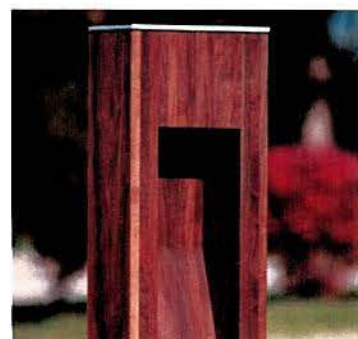


EXAMPLES OF MOUNTING ARM OPTIONS

Bollard:

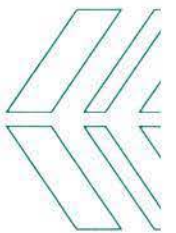
Proposed fixed bollard with lighting is "Mac LED and Mac II LED".
Proposed removable bollard for locations of fire access or temporary use is "Spar TB bollards" from Structura Inc. If not available, similar bollard light should be used.

- » unique solid glulam wood product design
- » geometric forms
- » High power, low voltage LED light source
- » Wood finish: S3 - Cumaru
- » Metal finish: CHRTX Charcoal
- » 32' or 42" standard heights
- » Concealed anchor bolt base and mounting hardware





6.0 PLANTING





STREET TREES

Tree Planting Guidelines:

The following species collectively help to achieve the "river village" theme. They provide a range of forms, colour and species that are suited to a variety of growing conditions. Plantings should utilize an assortment of the below species consistent with the intent and preferred locations as outlined.

Fagus sylvatica 'Dawykiei', Dawykii European Beech

- » Large tree best for locations with 4-5m setback from building face
- » Large planting pit required
- » Good as a feature tree or focal point
- » Not a typical street tree.
- » Should be considered in parks or large planting conditions

Acer rubrum 'Bowhall', Bowhall Red Maple

- » Wonderful fall colour
- » Columnar shape ideal for small setback areas
- » 3-4 meter canopy
- » Suitable for most street conditions

Picea omorika, Serbian Spruce

- » Best for a 3m boulevard condition
- » Can be in smaller confined spaces as well
- » A multi-purpose tree, could be included in many group plantings especially at key points

- » Good tree to provide a buffer between spaces / vehicular traffic

Carpinus betulus 'Fastigiata', Fastigate Hornbeam or *Carpinus betulus* 'Frans Fontaine', Frans Fontaine Hornbeam

- » Columnar shape
- » Good for confined setbacks
- » Dense canopy for shade
- » Forms can be quite low – use with wide boulevard but narrow building setback
- » Resilient species good adjacent to travel lanes / salt

Tilia tomentosa 'Green Mountain', Green Mountain Linden or *Tilia x euchlora*

- » Large canopy tree
- » Wide boulevard settings
- » Consistent with existing Fullerton Street trees

Acer x fremanii 'Jeffers Red', Autumn Blaze Maple

- » Good fall colour
- » Columnar form
- » 3-4 meter canopy
- » Suitable for most street conditions

Acer circinatum, Vine Maple

- » Good for group plantings
- » Wide locations
- » Good typical street tree

Cornus Mas, Cornelian Cherry

- » Wide locations
- » Good for park settings

Liquidambar styraciflua 'Worplesdon', Worplesdon Sweetgum

- » Good fall colour
- » Holds leaves late into fall
- » Should be included in park settings
- » Risk of branch dropping

Amelanchier x grandiflora, Hybrid Serviceberry

- » Very small species
- » Good to tuck into a group planting
- » Provides berries and flowers for wildlife / interest
- » Easy upkeep
- » Do not grow tall

Quercus palustris 'Pringreen', Green Pillar Oak

- » Very columnar form
- » Good for linear settings (allee)
- » Tall and suitable for confined setbacks

Acer cappadocicum 'Rubrum', Coliseum Maple

- » Yellow leaves
- » Tall and wide canopy
- » Good for wide streets
- » Suitable for parks or streets
- » Provide a mix of colour

Platanus x acerifolia 'Exclamation', Exclamation Planetree

- » Wide and tall species
- » Require large setbacks
- » Consistent with existing Fullerton Trees
- » Resilient species

Tree Species and Boulevard Planting Selection:

Healthy and majestic trees are an important environmental feature and element for the River Village.

Appropriate street tree selection is critical to support urban forest management and stormwater management strategies, as well as creating great streets. Ideally, selecting species that have good longevity and can be raised or pruned-up over time to reduce obstructions at the street and sidewalk level is preferable to short-lived trees or small trees that will have minimal impact or presence in the street. The street tree list included in this plan reflects the need for species that are long-lived, diverse, adaptable to climate change, and pest resistant.

Careful selection of planting within the public realm, such as in the

boulevard, will contribute to seasonal interest, biodiversity, colour, green character, and environmental benefits.

Intent

- » To have long-lived and healthy large-scale street trees.
- » To have street trees that may help capture rainwater and provide habitat for local fauna.
- » To diversify tree species in park areas and within the public realm.
- » To increase mature tree canopy size to help reduce potential urban heat island effect.

Other Considerations

- » Minimum caliper of 10cm
- » Sufficient growing medium to be installed to ensure healthy growth of street trees to maturity. Growing medium and soil cells should be utilized as per District of North Vancouver standards.
- » Trees along the street are to be in small groupings as per design guidelines.
- » Final selection and approval of street trees to be done by the District of North Vancouver.
- » Street trees are to be of the highest quality and uniform in shape and form.

Street Tree Spacing

The streetscape is to have a character evocative of the forest edge. Trees are to be arranged in groups rather than in rows as is typical for street trees. Trees are to be given adequate space to grow and express their natural form. Massing of trees - placing them in close groups that knit together over time as they would in a forest is encouraged.

The boulevard varies in width and tree species should be selected based on their suitability outlined in the Tree Planting Guidelines.



Acer rubrum
'Armstrong'



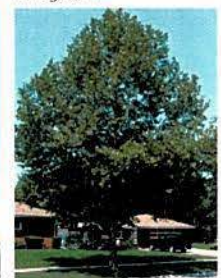
Quercus palustris
'Pringreen'



Tilia tomentosa 'Green Mountain'



Acer x freemanii
'Autumn Blaze'



Platanus x acerifolia
'Exclamation!'



UNDERSTOREY PLANTING

Planting proposed for the "river village" brings lush and green character to the area. Proposed plants are drought tolerant and low maintenance to provide minimum maintenance and contribute to the storm water management.

When appropriate, boulevard planting and planting island should be considered as rain gardens to improve storm water collection.

Boulevard Plant Selection List Shrubs

SCIENTIFIC NAME	COMMON NAME
<i>Cornus sericea</i> 'Kelsey'	Kelsey's Dwarf Dogwood
<i>Sarcococca hookeriana</i> 'humilis'	Dwarf Sweet Box
<i>Senecio greyi</i>	Daisy Bush
<i>Symphoricarpos chenaultii</i> 'Hancock'	Hancock Trailing Snowberry



Gaultheria shallon



Polystichum munitum



Carex morrowii 'Ice Dance'



Luzula sylvatica



Cornus sericea 'Kelseyii'



Rosa 'Gourmet Popcorn'

Ground Covers

SCIENTIFIC NAME	COMMON NAME
<i>Arctostaphylos uva-ursi</i>	Bearberry
<i>Erica carnea</i> 'Springwood Pink'	Springwood Pink Winter Heath
<i>Erica carnea</i> 'Springwood White'	Springwood White Winter Heath
<i>Gaultheria shallon</i>	Salal
<i>Mahonia repans</i>	Creeping Oregon Grape
<i>Mahonia media</i> 'Charity'	Hybrid Mahonia
<i>Oxalis oregana</i>	Evergreen Redwood Sorrel
<i>Sedum oreganum</i>	Oregon Stonecrop
<i>Spiraea japonica</i> 'Little Princess'	Japanese Spirea
<i>Vaccinium ovatum</i>	Winter Huckleberry
<i>Waldsteinia ternata</i>	Barren Strawberry

Perennials, Ferns and Ornamental Grasses

SCIENTIFIC NAME	COMMON NAME
<i>Blechnum spicant</i>	Deer Fern
<i>Carex flagellifera</i> 'Kiwi'	Weeping Brown Sedge
<i>Carex morrowii</i> 'Ice Dance'	Variegated Sedge
<i>Carex pendula</i>	Great Drooping Sedge
<i>Echinacea purpurea</i>	Purple Cone Flower
<i>Hemerocallis</i> 'Stella d'Oro'	Stella d'Oro Daylily
<i>Liriope muscari</i>	Lilyturf
<i>Nepeta</i> 'Walker's Low'	Walker's Low Catmint
<i>Polystichum munitum</i>	Western Swordfern
<i>Rudbeckia fulgida</i> var. <i>fulgida</i>	Black-eyed Susan
<i>Sedum</i> 'Autumn Joy'	Autumn Joy Sedum





PLANTING CONDITIONS

Growing Medium:

The performance and long-term survival of urban trees is heavily dependent upon the provision of adequate volumes of growing medium. This is particularly true for street tree and boulevard plants that grow in harsh urban environments with little maintenance.

Intent

- » To ensure there is adequate soil/ growing medium to produce healthy large-scale street trees to maturity.
- » To ensure there is adequate above ground and below ground space for the species selected and its expected mature size and form.

There are a number of options for achieving adequate soil volume which can be used individually or in combination:

- » The use of soil cells under paved area adjoining tree pits
- » The use of structural soil under paved area adjoining tree pits
- » Engineered suspended sidewalk systems

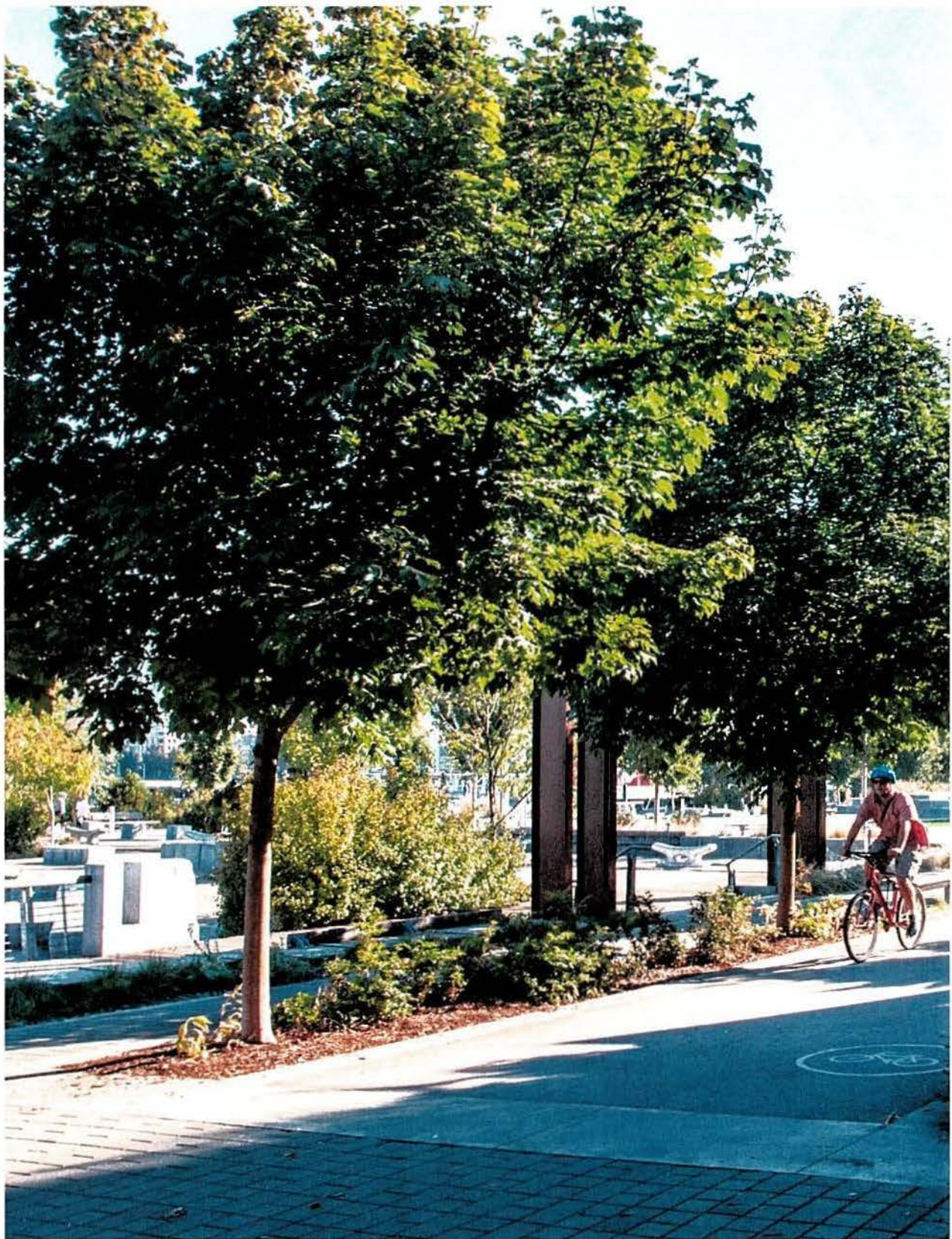
Typical soil cells and structural soil details refer to the District of North Vancouver Engineering department. Soil cells and engineered suspended sidewalks provide the most growing medium within a given volume but carry the highest construction costs. Structural soil is a simpler and less expensive solution but only provides

approximately 30% growing medium by volume of structural soil installed. The remaining 70% is rock. Soil cells and engineered suspended sidewalks are preferred.

Soil quality and volume are two aspects that are critical contributing factors to the health of street trees. Without either, the health, functionality and lifespan street trees is dramatically reduced as are the benefits they bring to the city and neighbourhood.

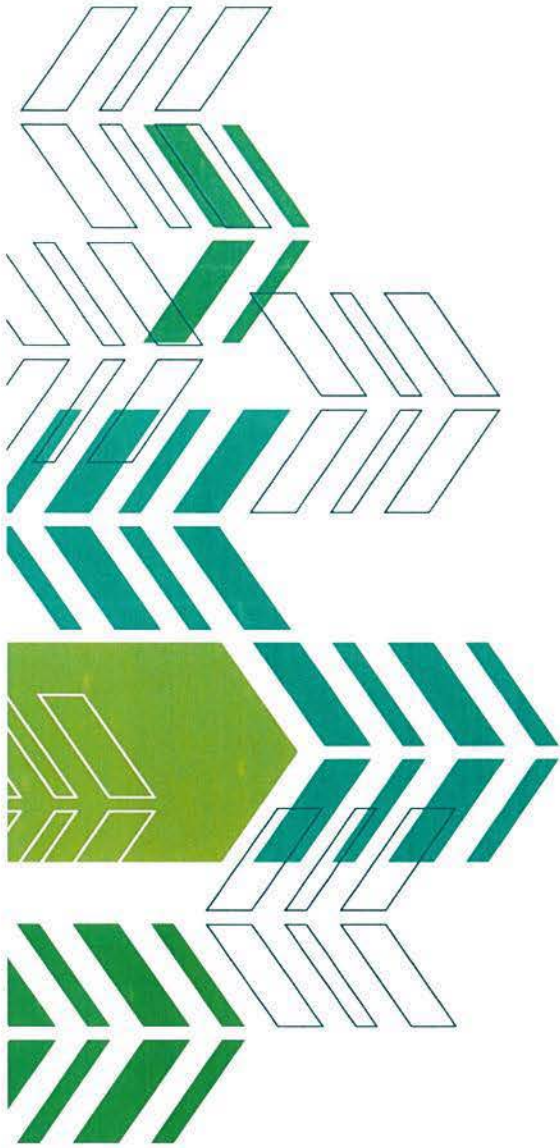
Planting guidelines:

- » Ensure appropriate soil volumes and soil quality; refer to the District standards.
- » Where possible, ensure street trees are planted in a continuous planting trench to allow for greater access to soil/growing medium volume and water.
- » Work with the third party utility providers to ensure there are adequate spaces for trees and their soil volume. Create a tree corridor as well as utility corridors and give them all the same importance.

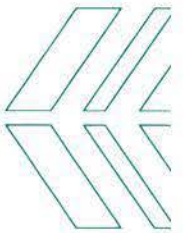


CONTINUOUS PLANTING BED WITH SUFFICIENT GROWING VOLUME SPACING





7.0 APPENDIX





PAVING

The paving strategy proposes a palette of paving materials that address simplicity, variety, robustness, and permeability (where suitable), that reflect a river character theme.

Character:

Materials chosen are durable, strong but easy to maintain and replace with time.

- » Simple and strong
- » Unique pattern application

Patterns:



Stack bond

This pattern is appropriate for pedestrian spaces which are not primarily for vehicular movements.

Application:

- » Pedestrian portion of "Crossroads"
- » "Crossroad" Plaza
- » Village Heart Plaza



Herring bone:

This pattern is appropriate for travel lanes and supports turning movements of vehicle traffic.

Application:

- » Travel lanes
- » Road section of "Crossroads"
- » "Woonerf"

Concrete Unit Pavers Mix

1. Paver one:
 - » Size: 150x300x108mm
 - » Colour: charcoal
2. Paver two:
 - » Size: 150x300x108mm
 - » Colour: mid grey
3. Paver three:
 - » Size: 150x300x108mm
 - » Colour: natural

UNIT PAVERS MIX 1:

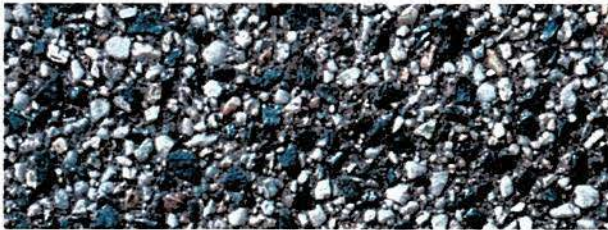
Application: "Woonerf" and other travel lanes

- » Mix: 3 shades of grey
 1. Charcoal - 15% of the mix
 2. Medium Grey - 35%
 3. Natural - 50%

UNIT PAVERS MIX 2:

Application: "Crossroads" plaza, "Crossroads", Village Heart Plaza

- » Mix: 3 shades of grey+warm tone
 1. Charcoal - 10% of the mix
 2. Medium Grey - 30%
 3. Natural - 50%
 4. Beige(sandy) - 10%



Exposed Aggregate

Application:

- » Slow zones along sidewalks, boulevard and for bus stop areas, dividing strip at McGuire Ave.
- » "River Band" - feature paving strip connecting "Crossroads" Plaza and Community Centre



Crushed Basalt

- » Colour: dark grey

Application:

- » Soft islands under trees at "Crossroads" Plaza, "Crossroads", Village Heart Plaza
- » Key zones within boulevards at Curling Road and Fullerton



Concrete

Application:

- » Sidewalks - fast zone
- » Pedestrian crosswalk at intersection of Capilano Road and Crossroads - size: 3m wide
- » Finish: light sandblast with saw cut pattern



Composite Decking

- » Composite wood such as Resysta or similar

Application: To create a notion of a boardwalk at the Crossroads, Community centre and Plaza

- » Retail edges/ patios
- » Community Centre Plaza



Tactile Warning Surfaces

- » Stainless steel or equivalent

Application:

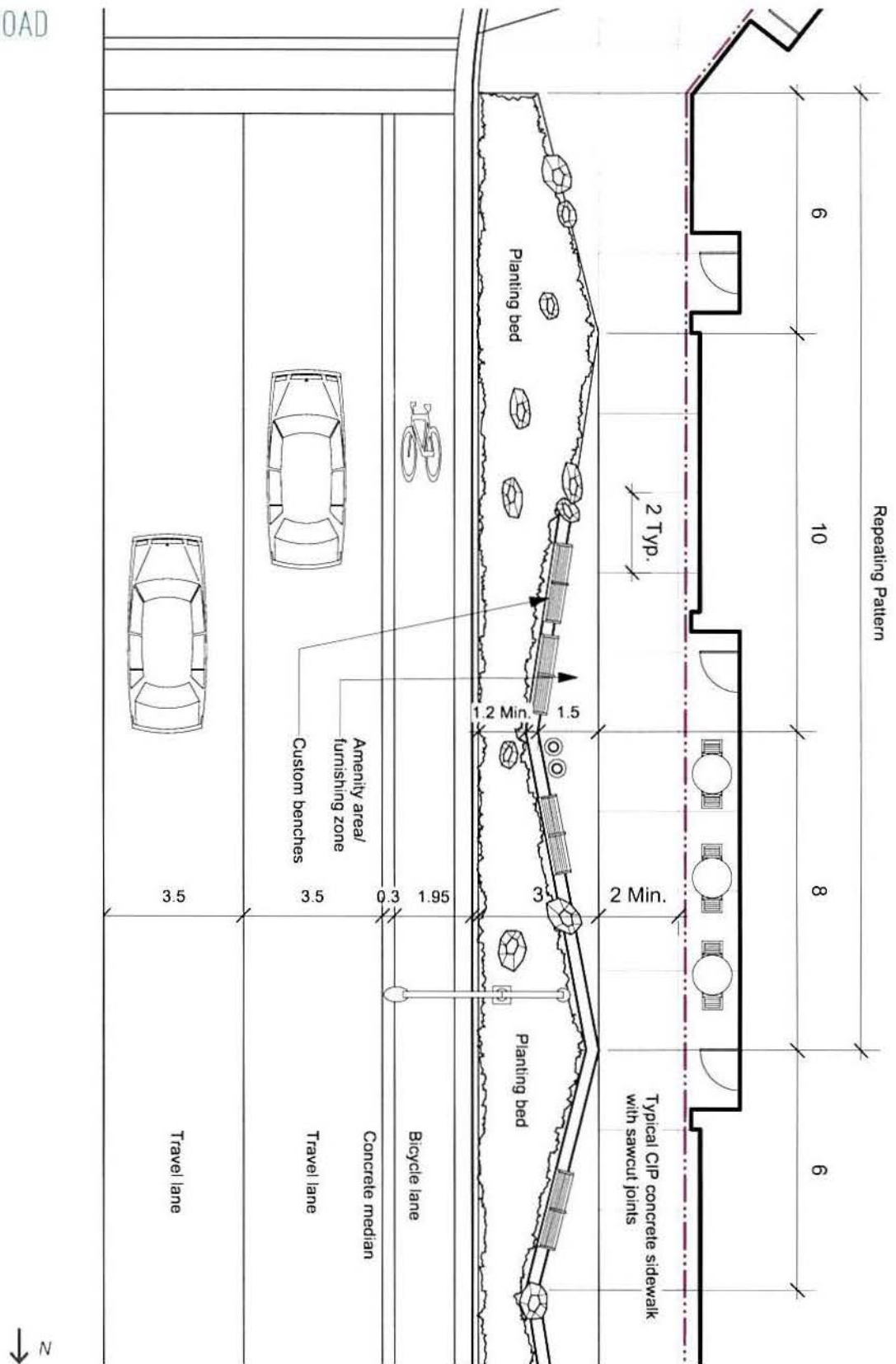
- » To be used when other physical queues are limited, when necessary in open plazas and at the tops of stairs.



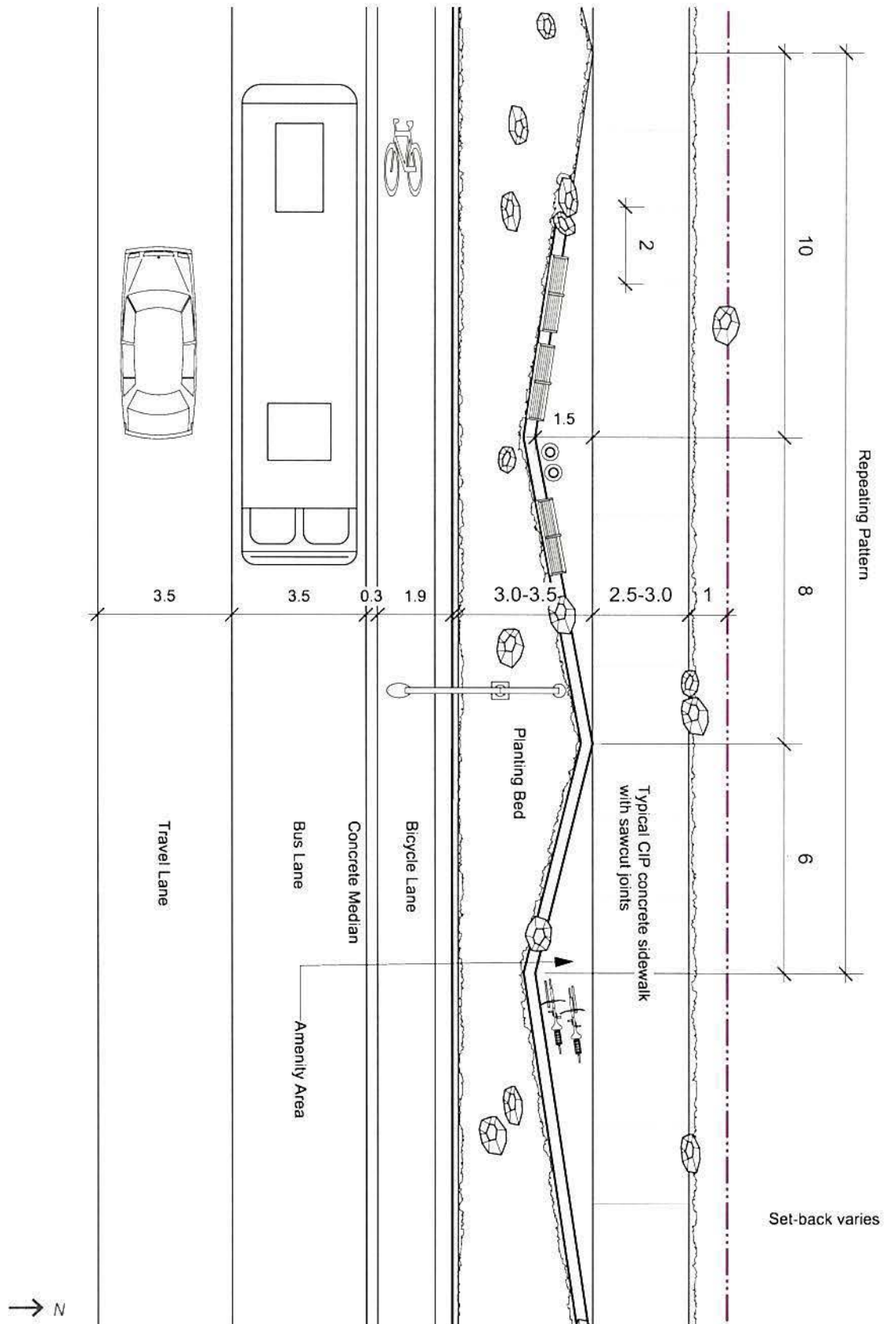


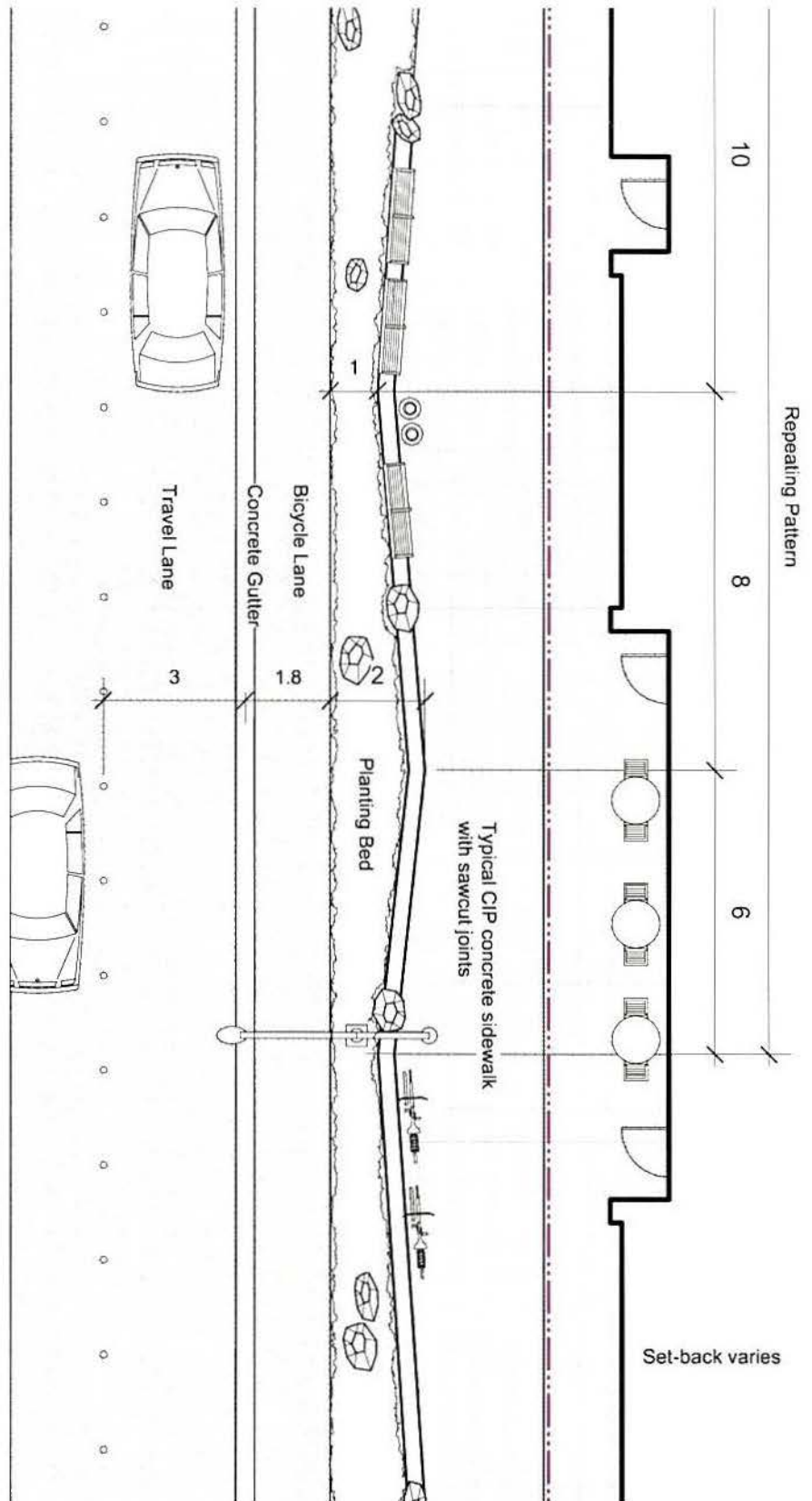
STREET DETAILS

CAPILANO ROAD

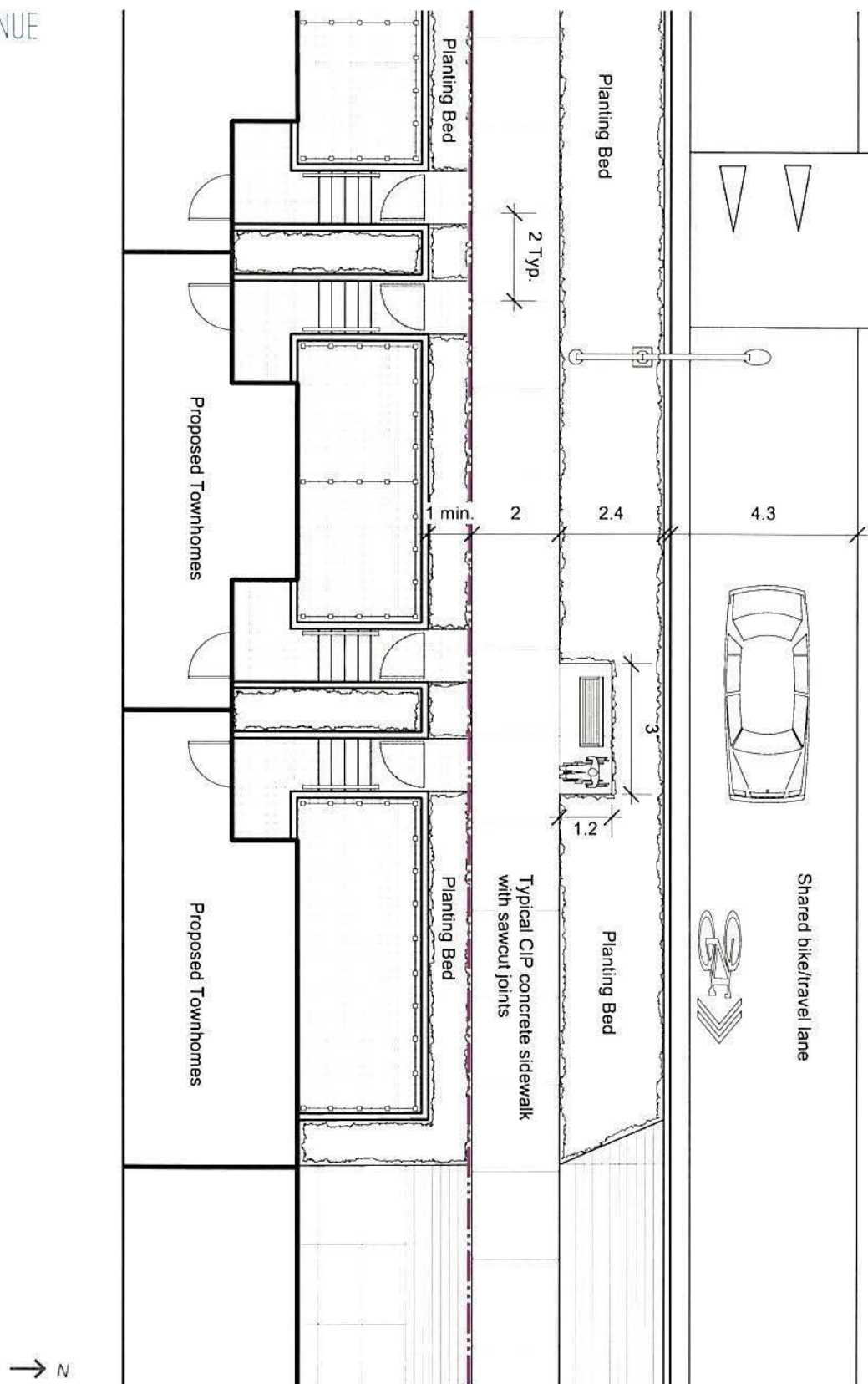


MARINE DRIVE



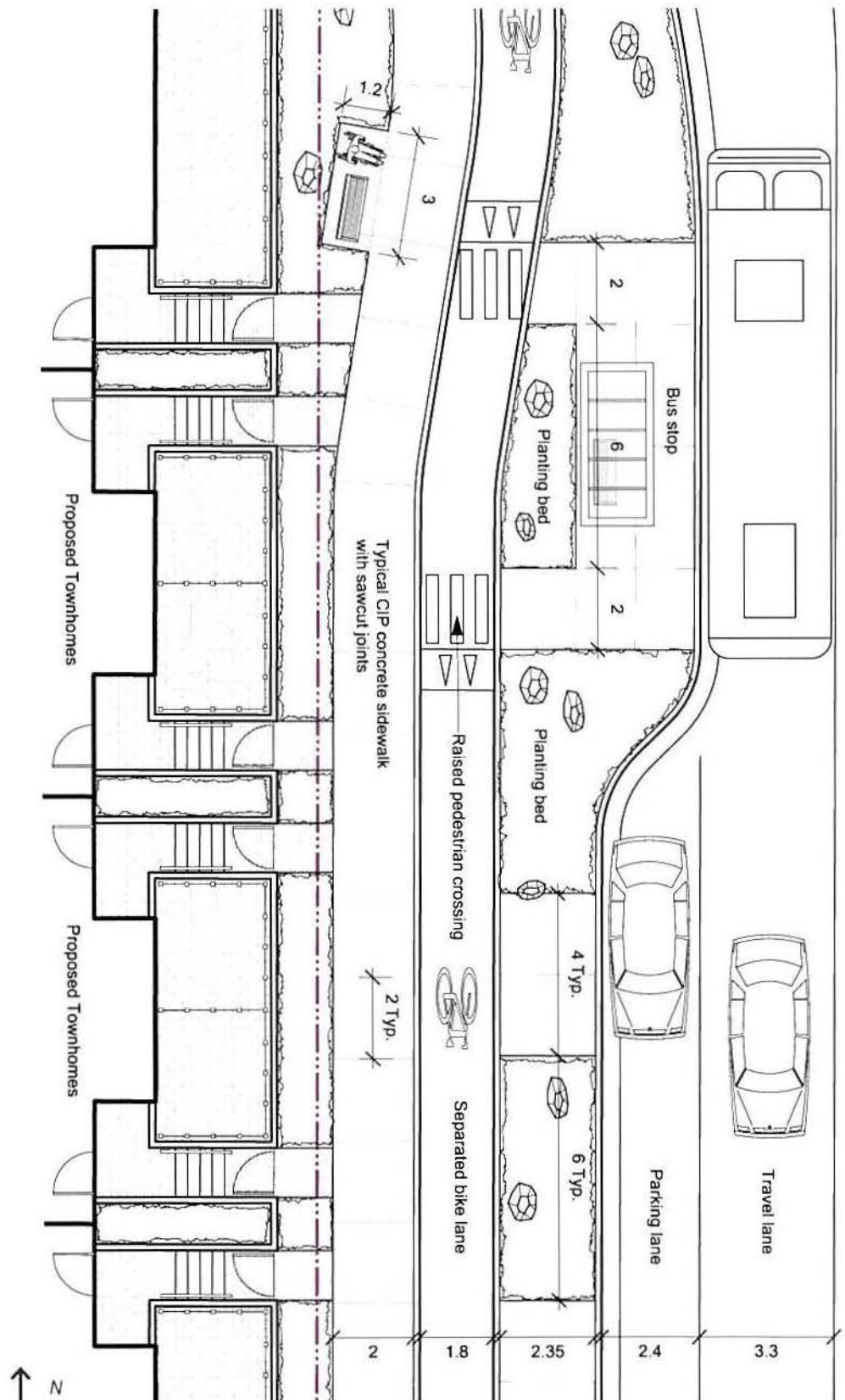


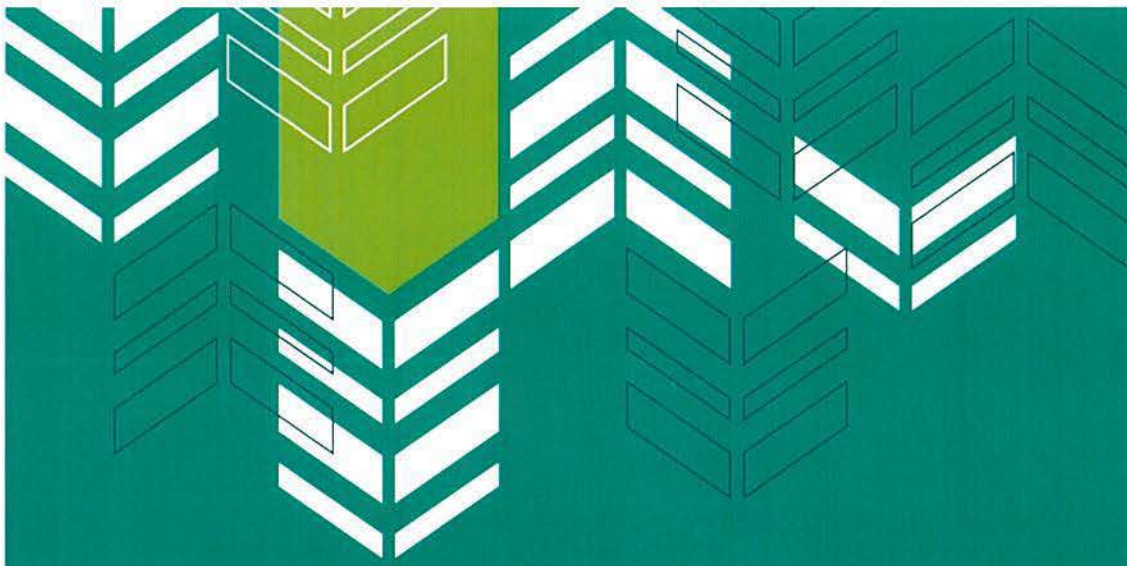
FULLERTON AVENUE





MCGUIRE AVENUE





Lower Capilano Marine Village Centre Design Guidelines and Streetscape Details

These guidelines supplement the District-wide Form and Character Design Guidelines found in Schedule B of the Official Community Plan, and provide more specific direction to guide redevelopment towards the expression of a unique community identity for the Lower Capilano Marine Village Centre.

These design guidelines may be supplemented from time to time, to provide further clarity and direction for the Village Centre.



A Distinct Community:

In the village of Lower Capilano Marine consider design styles, and features that are distinct from those used along Marine Drive, to help set the village apart from the Marine Drive Corridor, and help build the village's unique character.

The Capilano River:

In the Lower Capilano Marine Village Centre, celebrate the importance of the Capilano River through the use of water, and in particular:

- Consider including water features in landscaping and plaza areas;
- Include public art that references the river; and
- Use glass and other materials to mimic the feel of water reflecting light, and in other ways celebrate the Capilano River.



Lush Landscaping:

In the Village Centre, celebrate the culture of gardens and landscaping in the community by ensuring that new development maximizes the opportunity for lush landscaping. In particular, new projects should consider including:

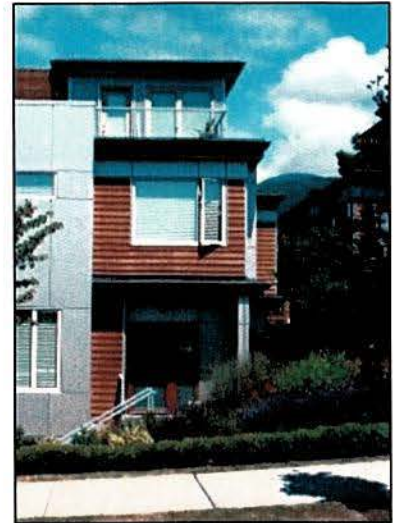
- Ample at-grade landscaping;
- Rooftop gardens;
- Planters on balconies and decks;
- Green walls;
- Garden Plots; and
- Private outdoor recreational space.



Building Materials:

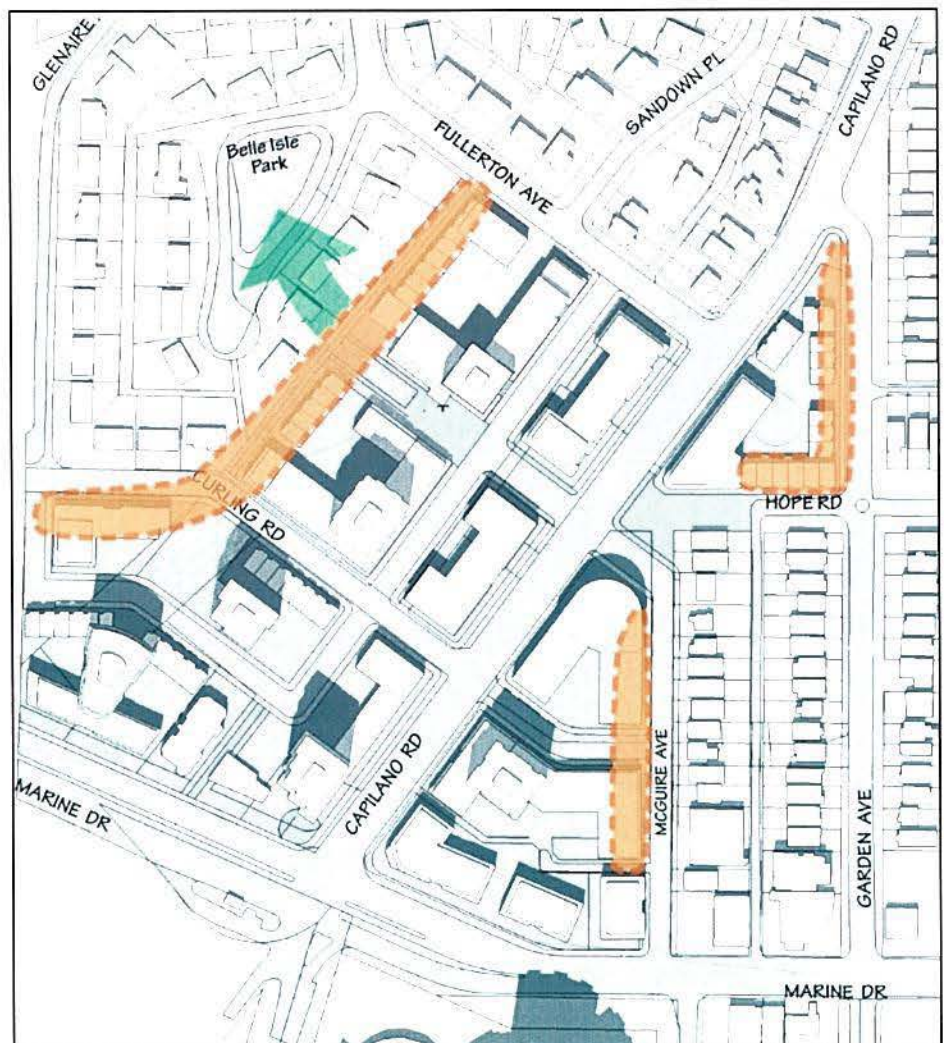
Encourage the use of building materials that connect with the natural setting and in particular consider:

- The use of natural (unpainted) wood as a feature element on all buildings;
- The use of warm tones in the building colour palette;
- The use of natural materials and those traditional to construction on the North Shore including rock, wood siding and brick; and
- While other materials including siding and metal may be used, the materials should complement the natural wood, and the water elements, celebrating the setting in Lower Capilano.



Neighbourly Design and Interface Areas

In keeping with the District's urban design principles of neighbourly design, new development in the areas shown in orange on the map below must sensitively transition to the surrounding lower density housing forms, with building heights stepping down to 2-4 storeys.

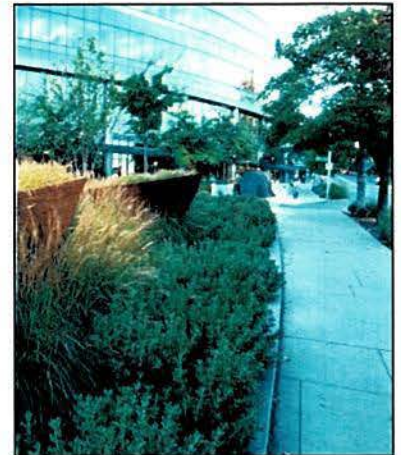


Streetscape Details:

New development must contribute towards the development of pedestrian oriented streets, in accordance with the following street cross sections, which are tailored to the needs of the road network in the Lower Capilano Marine Village Centre.

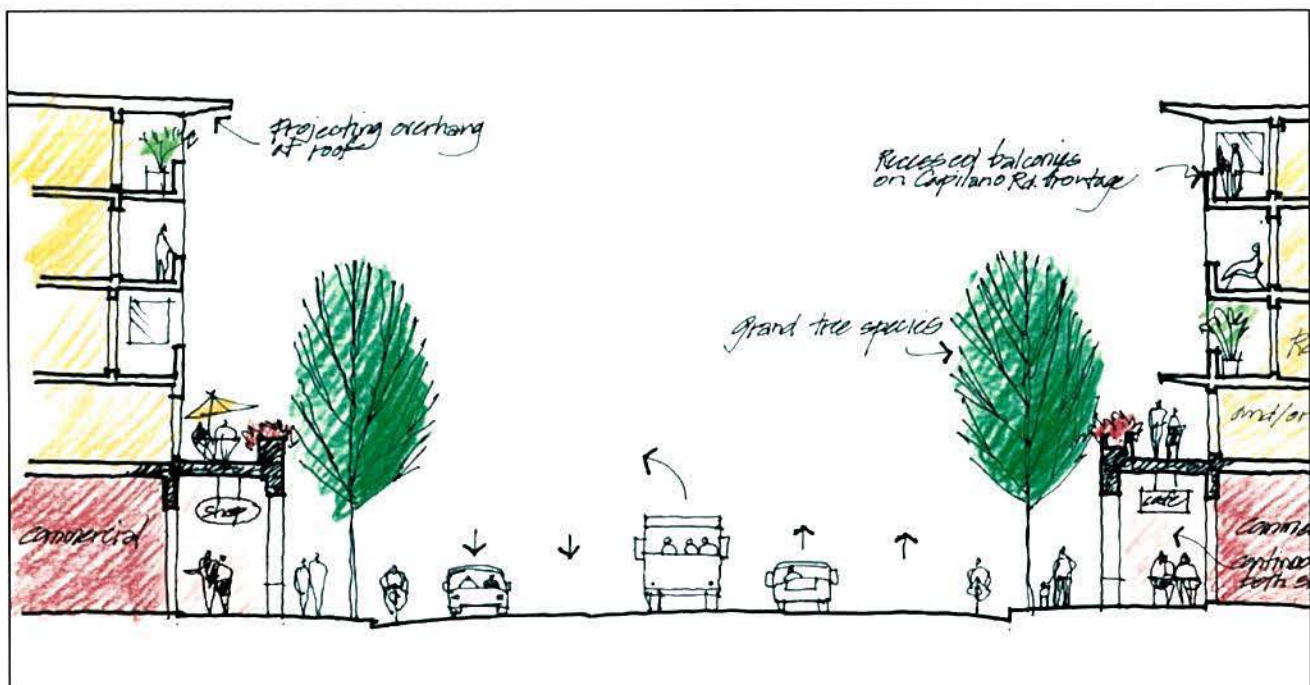
In the Village Centre all streets will include:

- Distinct street trees to be determined for the Lower Capilano Village Centre by the District Parks Department.
- Wide sidewalks to accommodate pedestrians, including those using strollers, walkers, and in wheelchairs.
- Landscaping pockets, hanging baskets, planters, green walls and other means will be explored to make these streets as lush as possible.
- The use of public art in the street to celebrate the Lower Capilano location.



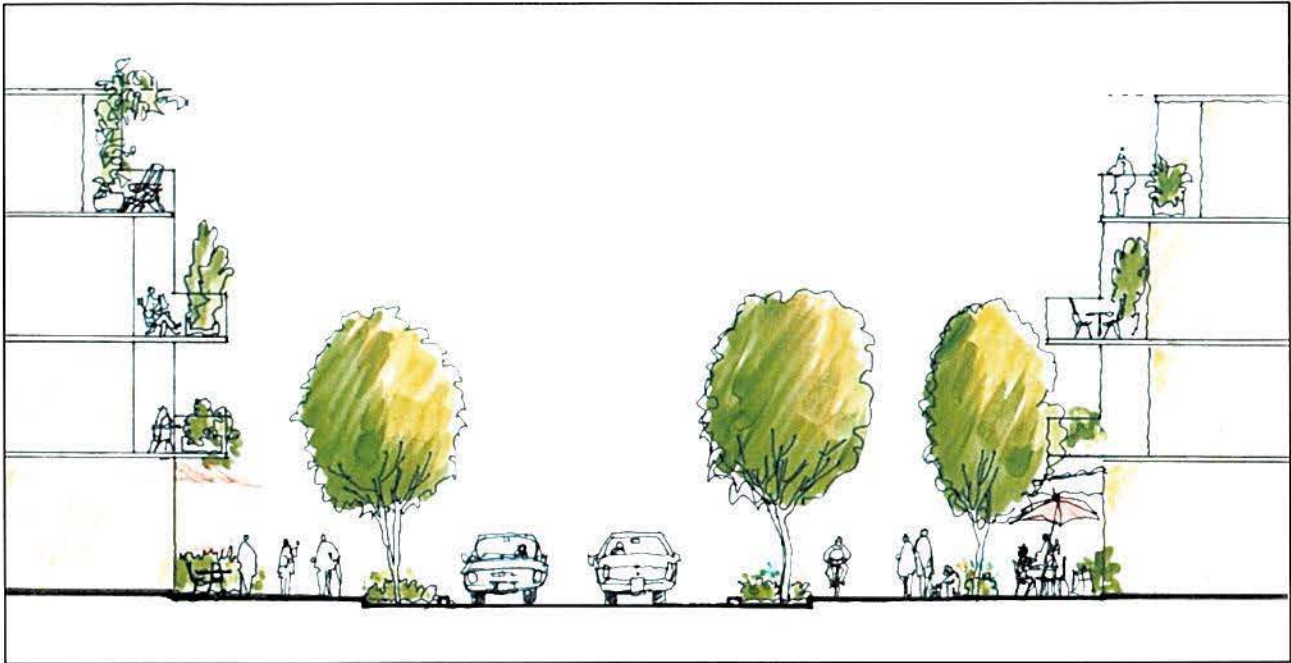
Capilano Road:

Capilano Road will include improved travel lanes, widened sidewalks, boulevard street trees, and additional public space next to the developments that may occur within an arcade or open area, but that can serve as a widened sidewalk suitable for retail displays and café seating.



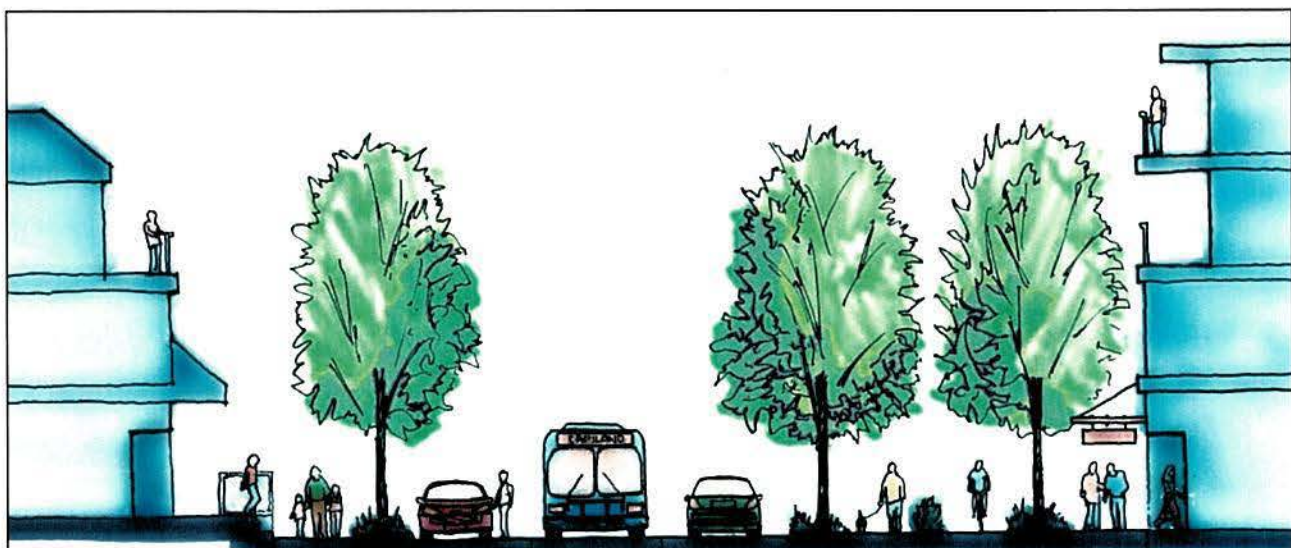
The Cross Roads

McGuire Avenue will extend west across Capilano Road, into the heart of the Village Centre. This new road extension is being called the Cross Roads and will create both a new connection into and out of the neighbourhood but also a new meeting place that highlights the location of this new Village Centre for everyone passing along Capilano Road. Special paving may be merited for this roadway.



McGuire Avenue

McGuire Avenue is proposed to become the new transit road from the Cross Roads south to Marine Drive. This road will include two travel lanes, a parking lane, street trees, and a pedestrian and cycle path to link to the Spirit Trail and the Village Centre.



Where McGuire Avenue meets Capilano Road and the new Cross Roads, the sidewalks will widen into small plazas to create opportunities for pleasant waiting areas for transit riders, and places where neighbours can meet.

New North-South and Crescent Road

To facilitate the redevelopment of the Village Centre and the widening of Capilano Road a new north-south road is proposed. As this new road heads south of Curling Road it will curve into a new crescent road that will provide safe access to the Marine Drive properties. This road will be the most pedestrian oriented of the local roads, but as the primary access to the community centre it will have two way traffic, and the potential for parking pockets. This road will have wide landscaped pockets that will be deeper and wider than typical street tree planting areas, and these pockets of landscaping will be staggered along the road allowing the traffic to meander gently down the street.



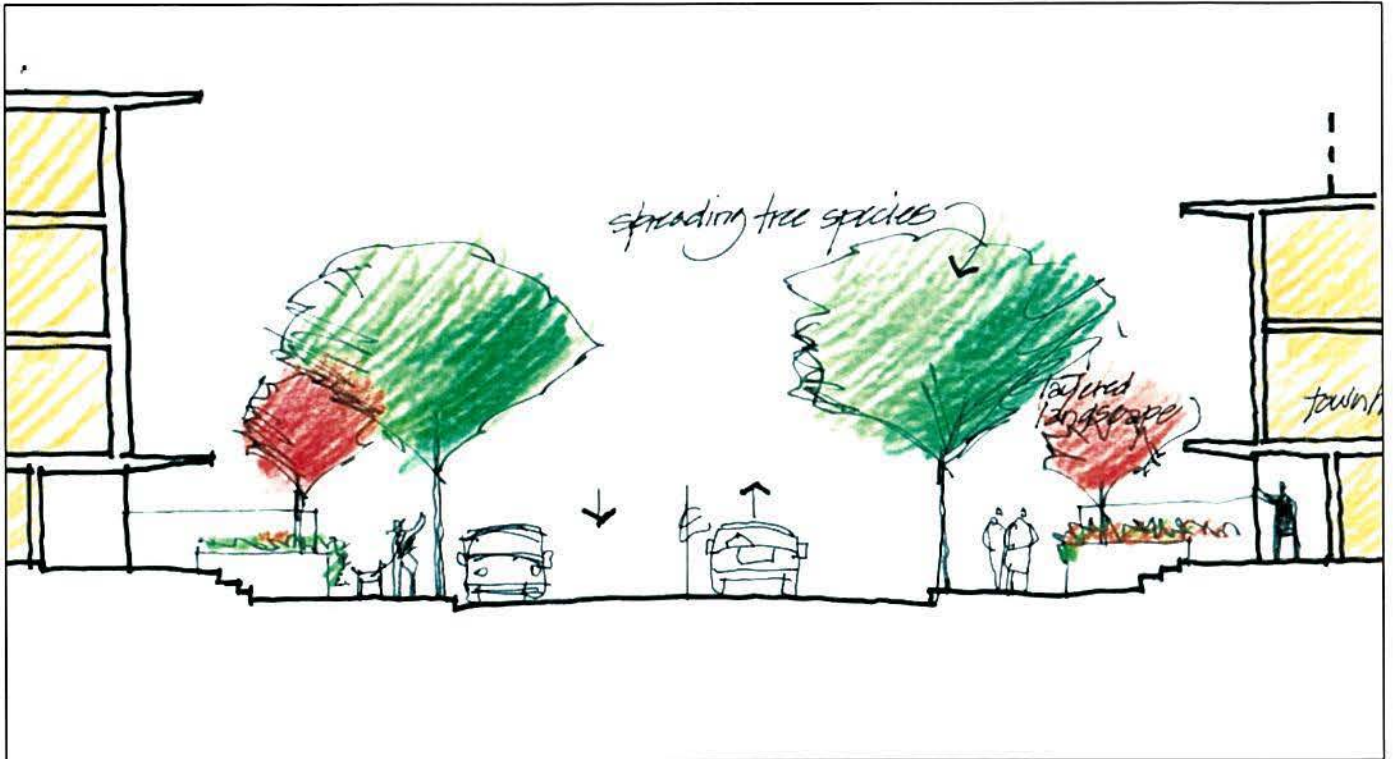
Fullerton Avenue:

Fullerton Avenue will be widened to accommodate a better pedestrian environment with wider sidewalks, street trees and the opportunity for landscaping. The street will continue to include two travel lanes, and parking.

Residents of the area are also exploring a beautification and traffic calming plan for Fullerton Avenue that, if adopted by Council, may further enhance the pedestrian experience.

Curling Road

Curling will be widened, and extend east to McGuire Avenue, creating a new linkage for residents. Curling Road will include two travel lanes that will be shared with bikes, a parking lane, street trees, and wide sidewalks.



Greenway Trail



Improved pedestrian trails are proposed in the Village Centre to create more options and better linkages for people moving through the area and recreationally walking around the neighbourhood.

To this end, a new greenway trail network will run north-south between Fullerton Avenue and Marine Drive connecting Belle Isle Park, the proposed Village Green and the new Curling Road Park. The greenway will continue south of the new crescent road connecting to Marine Drive.

ATTACHMENT 3

SUMMARY AND ANALYSIS OF FEEDBACK ON THE LIONS GATE PUBLIC REALM STRATEGY

Overall

- Number of people attending May 21 Open House: approximately 70
- Number of completed feedback forms received:
 - 17 forms submitted at open house
 - 23 on-line responses
 - **40 feedback forms received in total**

In addition, the District received feedback from Vancouver Coastal Health and HUB (cycling) group. Comments from these groups are summarized below along with community feedback, but are not reflected in the survey analysis %.

1. In general, do you support the Capilano Village Centre Public Realm Guidelines?

Strongly or somewhat support:	78%
Somewhat or strongly support:	20%
Not sure:	2%

Summary of comments received:

- The Public Realm design guidelines have strong overall support.
- Several people expressed concerns related to traffic especially on Capilano Road and Marine Drive, increased density and construction.
- One respondent commented that this will be one of the busiest areas in the District to undertake construction work, and hence will require traffic and congestion management, especially for the people who live in the area.
- One respondent called for traffic calming measures on Garden Avenue and the removal of the 4-way stop and roundabout.
- Another respondent was looking forward to more community pockets where cycling and pedestrian traffic are encouraged and prioritized, and to revitalization of this community.
- One respondent suggested naming the centre as “Lions Gate” and dropping reference to “Village”.
- One respondent asked how the Design Guidelines relate to the peripheral area surrounding the core.
- Another respondent commented that they couldn’t wait to enjoy the new community space with their kids.
- One respondent commented on the need for affordable rental housing, sighting renters as people who are most likely to walk, bike and take transit.
- Another expressed appreciation for the engagement process and that they can see their participation made a difference.
- Another commented that the Design Guidelines finally come close to achieving what the community has sought to bring forward for some time, and that there is an opportunity to create an exceptional space in this centre.

- Overall, the HUB (Cycling Group) North Shore Committee supports the network connections and proposed street designs for Lower Capilano as they will allow residents of all ages and abilities to cycle all streets in the Village Centre. Some additional suggestions were offered for Curling Road.
- Vancouver Coastal Health (VCH) supports increased public space, natural elements, multi-use paths and walkability. VCH also suggest that features of the (design guidelines) will improve accessibility, promote social connectedness and increase physical activity, all of which are linked to improved physical and mental health. VCH also encouraged strengthening of connections to parks and other areas outside of the village centre.

Design Guideline response:

- It should be noted that the Design Guidelines do not direct the land use, density or height of buildings in the Capilano Village Centre. Building on existing policies in the Official Community Plan, the Lower Capilano Marine Village Centre Implementation Plan and other relevant policy documents, the design guidelines are intended to guide the design of plazas, parks, greenways and other public open spaces between the buildings in this centre.
- The majority of design elements apply to the central core area, with some basic design elements (overall design theme, materials choices etc.) continued into the peripheral area.

2. Capilano River system as inspiration

Do the design guidelines effectively reflect the river theme and promote a distinct identity and character for the Capilano Village?

Strongly or somewhat agree: 72%

Somewhat or strongly disagree: 18%

Not sure: 10%

Summary of comments received:

- Strong support for the connection to the Capilano River that reflects the context/history of the area.
- Water features and rainwater management should be visible in the centre.
- A natural river system is more undulated, curved than is suggested by the angular sidewalk design.
- Consider more opportunities to integrate water flow and rainwater management into the design.

Design Guidelines response:

- Water and landscape features have been incorporated with natural play areas in the Village Green and the Curling Road Park.
- Naturalized rainwater management features are integrated into the village centre.
- The angular design geometry is intended to be evocative of a river system, not a literal depiction. The angular design approach is straightforward, flexible and can be readily applied under a variety of redevelopment scenarios including multiple ownerships and variable project timing occurring on any given block face.

3. Village Heart and Key Plazas

Do you think the proposed design guidelines for the Village Plaza, Village Green and “Crossroads” plaza will enable flexible and interesting social spaces to support a variety of community events and functions?

Strongly or somewhat agree: 80%

Somewhat or strongly disagree: 10%

Not sure: 10%

Summary of comments received:

- Strong agreement for the Village Heart and Plaza design that will support community events, provide interesting social spaces for people to gather, and opportunities for outdoor entertainment.
- Support for Village Green, park connection and angled parking (Woonerf) that introduces opportunities for community events.
- Comment that interesting social spaces need retail shops, cafes and restaurants (to animate and help program these spaces).
- The need for good lighting and more on-street parking within the heart was suggested.
- Comment that with proper landscape components and surface treatments, there may be no need for bollards which may define exclusivity for cars.

Design Guidelines response:

- The Design Guidelines build on the land use concept plan in the Lower Capilano Marine Village Centre Implementation Plan that indicates at-grade commercial/retail uses along the “Crossroads”, wrapping the edges of the Village Plaza and along Capilano Road.
- Bollards, along with paving patterns, do serve to help delineate different travel paths especially at turning points and at the entry to the plaza. Bollard design will help add character to the Village Heart, allow for pedestrian and planting understory lighting, and provide visual interest along the length of the “Woonerf”.
- Benches, bike racks and other street elements may be used as another way of limiting vehicular movement, where necessary.

4. Parks and Open Space

Do you think the proposed parks and open space will serve the recreational, play and leisure needs of the Capilano Village?

Strongly or somewhat agree: 72%

Somewhat or strongly disagree: 8%

Not sure: 20%

Summary of comments received:

- General support for parks, open space, greenways and pedestrian network, and enhanced access to Capilano River.
- Some respondents called for more park space.
- Additional explanation on the proposed programming of Belle Isle and Curling parks would be useful.
- The expanded Belle Isle Park is needed, and perhaps more planting should be considered in this park.
- Note that Curling Park is small and should not be over-subscribed with different uses and activities.
- Curling Park should provide a play area for young children.

Design Guidelines response:

- The Design Guidelines provide a vision and future programming objectives for the expansion of Belle Isle Park and the creation of a new Curling Road Park.
- Curling Park is envisioned as a new adventure play park incorporating a water feature and child’s play area. Further detailed park design will be undertaken, at an appropriate time, to ensure that community and programming needs are addressed.

5. Capilano Road and Marine Drive

Do the proposed design guidelines for Capilano Road and Marine Drive promote pedestrian and cycling safety and mark the gateway to the Village Centre?

Strongly or somewhat agree: 67%

Somewhat or strongly disagree: 18%

Not sure: 15%

Summary of comments received:

- Respondents strongly support the focus of a pedestrian oriented Village Centre and proposed bike facilities on Capilano Road and Marine Drive.

- The importance of safe pedestrian crossings of Marine Drive/Capilano Road to better connect to other neighbourhoods was noted. A pedestrian overpass was suggested to improve safety for pedestrians crossing Capilano Road.
- Several respondents were encouraged to see protected bike lanes in the Village Centre and expressed interest in seeing these come to reality.
- HUB supports the proposed street designs which provide cycling facilities for all ages and abilities.
- There was a concern that development will bring too much traffic and density to this area. Proximity to the Lions Gate Bridge ensures that this area will continue to experience high traffic flows that need to be addressed.
- Others suggested the need for more roads, on-street and underground parking and traffic calming.

Design Guidelines response:

- With input from HUB, the Design Guidelines include separated bike lanes on the north side of Marine Drive, both sides of Capilano Road, and both sides of McGuire. These separated bike lanes will be supported by a hierarchy of shared space roads and greenway connections.
- The Design Guidelines support functional design and capital works to improve road and intersection design, add signalization and integrate the bus network.
- Timed signals and clear, regularized intersections along Capilano aim to provide safer pedestrian crossing points. No pedestrian overpass is contemplated at this time.
- As per the Implementation Plan, several new roads will be added to the road network: new “Woonerf”, “Crossroads”, new “Crescent Road” and extension of Curling Road.

6. “Woonerf” and “Crossroads”

Do you think the proposed design guidelines for “Woonerf” and “Crossroads” promote inviting, pedestrian-oriented environments that will meet a range of community needs?

Strongly or somewhat agree:	73%
Somewhat or strongly disagree:	12%
Not sure:	15%

Summary of comments received:

- Support for the pedestrian oriented focus of these streets, outweighed comments that these streets should be used for primarily vehicle use and not for community events.
- Respondents indicated support for the curb-less design, decorative pavers and angled parking pockets. Some felt that the “Woonerf” should be slightly wider to accommodate angled parking on both sides.
- Others felt that there should be retail uses along the “Woonerf” Street to make them interesting and lively.
- The need for good lighting and more on-street parking within the heart was suggested.

Design Guideline response:

- The “Crossroads” is anticipated to have local serving retail shops from the “Woonerf” Street through to Capilano Road. Retail uses will also be integrated along the edge of the Village Plaza. The “Woonerf” is intended to support a variety of activities and is able to be closed to vehicle traffic for community events.
- On-street parking is provided, where possible, to service the area; while balancing the design objectives of achieving a pedestrian first environment, and working within the constraints of available space.
- Angle parking along the “Woonerf” allows for efficient use of space in a pedestrian first environment and helps realize the greatest number of spaces achievable within the right-of-way.
- On-street parking will be supported by underground parking provided for residential and commercial uses.

7. Residential Streets – McGuire, Fullerton and Curling

Do the proposed design guidelines for these residential streets complement the vision for the Capilano Village and provide attractive residential streetscapes?

Strongly or somewhat agree:	78%
Somewhat or strongly disagree:	12%
Not sure:	10%

Summary of comments received:

- Several respondents indicated overall support for a “triple A” bike facility on McGuire Avenue.
- A couple of respondents commented on the need to address existing traffic in and through this area.
- Another felt that McGuire Avenue should be for local traffic only.
- Another wanted to make sure there would be no buses on the “Woonerf” Street.
- Other respondents called for more detailed plans for Fullerton Ave as it receives traffic flows from Woodcroft.
- HUB supported the integration of triple A bike facilities along McGuire Avenue, and made suggestions to improve cycling facilities along Curling Road.

Design Guideline response:

- McGuire Avenue is proposed support “triple A” bike facilities as well as an integrated transit network system with transit stops.
- Locating the transit system on McGuire Avenue helps unclutter Capilano Road, while still providing ready access to transit in the Village Heart. There will be no transit buses on the “Woonerf” Street.
- Curling Road is a relatively quiet local road that can support a shared lane environment for cars and cyclists, and forms part of the hierarchy of cycling lanes, shared spaces and greenway connections to meet the needs of all users and abilities.
- The Fullerton Avenue, and other roads in this area, will be guided by these Design Guidelines, as well as other functional engineering design plans and adopted plans.

8. Street Furnishings and Lighting

Do you think the street lighting and furnishings provide distinctive, special and consistent elements to help define the public realm of the Capilano Village?

Strongly or somewhat agree:	78%
Somewhat or strongly disagree:	10%
Not sure:	12%

Summary of comments received:

- General support for the use of natural materials (wood) in street furniture, bollards and lighting choices.
- Support for limited/appropriate use of bollards being careful to avoid potential conflicts with cyclists.
- Another commented that low level lighting is important to creating a comfortable, warm atmosphere.
- It was suggested that hanging flower baskets should be considered to add colour and vibrancy.
- One person wondered if local wood could be used. Another commented that the District should not opt for cheap lighting.

Design Guideline response:

- Street and pedestrian scale lighting will be integrated into planting berms along each of the streets. As such, hanging baskets may be obscured by street trees and other vegetation. Hanging baskets also represent a high level of maintenance and have, therefore, not been recommended. However, a wide variety of street trees and

understorey planting should provide visual interest and a lush, green condition along streets and key public spaces.

- High quality lighting and street furnishings have been selected for their aesthetic value, durability, lower maintenance and ability to support community needs and use of civic spaces in this community.

9. Planting and Materials

Does the proposed planting strategy and choice of paving materials achieve a unified sense of place and effectively express the river theme?

Strongly or somewhat agree: 78%

Somewhat or strongly disagree: 8%

Not sure: 14%

Summary of comments received:

- General support for the planting strategy.
- A preference for evergreen trees over deciduous was noted as being more consistent with the river theme.
- Some comments suggested the use of evergreen bushes, landscaping with biodiversity value and colour.
- Another comment called for more trees and ensuring enough soil volume for healthy tree growth.
- Another commented on the need to ensure that textured materials do not hinder access to people in wheelchairs or those with limited vision.

Design Guideline Response:

- Some evergreen trees are contemplated in the Design Guidelines, and these will be integrated within boulevards, where appropriate. In particular, feature evergreen trees will highlight significant arrival points along Capilano Road and Marine Drive.
- Understorey planting includes some native species, providing a variety of colours and textures.
- Pavers will be installed in compliance with District standards to mitigate any surface disturbances and prevent issues with accessibility.
- Specified pavers have narrow bevelled edges, reducing gaps between pavers and providing a level accessible surface.

10. Public Art Strategy

Does the proposed public art strategy relate to and support the other elements of the public realm to create unique spaces and a consistent river theme?

Strongly or somewhat agree: 68%

Somewhat or strongly disagree: 10%

Not sure: 22%

Summary of comments received:

- Overall support for the public art strategy in the Design Guidelines. Some respondents were not sure.
- A respondent recognized the need for an appropriate signature art feature at the Capilano Village/Marine Drive gateway to the Village Centre.
- One respondent expressed concern for the cost of public art.
- A couple of respondents felt that a First Nations inspired art theme would complement the river theme.
- Another respondent suggested avoiding already abundant “salmon” sculptures and other literal interpretations of the river theme.

Design Guideline response:

- The Design Guidelines enable public art to take on a variety of different forms, including iconic stand-alone pieces, art integrated into the design of landmark buildings, interpretive way-finding elements on pedestrian routes, recurring markers and other functional art.
- First Nations art can be considered within the public art strategy and should be complimentary to the natural setting and river theme.
- Public art should consider creative interpretations of the natural environment and foster awareness of the ecosystem and context.

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Memo

July 15, 2015

File:

TO: Mayor Richard Walton and Council

FROM: Councillor Mathew Bond

SUBJECT: Affordable Housing

RECOMMENDATION:

Further to Council's June 15, 2015 direction to staff to prepare a Green Paper on affordable housing outlining regulatory and policy tools to ensure a future supply of affordable housing in the District, staff be further directed to include options such as property acquisition, utilization of existing District properties, rental property management and partnership models, considering the District's financial and human resource capacity to support such initiatives. The options should include opportunities that may exist to provide temporary relief for those renters being displaced as early as the spring of 2016.

DISCUSSION:

Council had a wide ranging discussion on the issue of affordable housing in June and directed staff to prepare a Green Paper on this issue for Council consideration. It is my understanding that Council will have a workshop in September to better assist staff in preparing the paper which will likely be available in October.

On July 6 Council considered a development application which proposed to replace existing rental units on a 1 for 1 basis with smaller, likely more expensive units. While the rental units are being replaced numerically (at a cost to the CAC contribution normally provided), much of the discussion centred around the displacement of the existing tenants. Because of the dearth of affordable rental units in the District, this displacement is likely to be permanent rather than temporary. We were also advised by staff that there are other pending development applications which will likely have an impact on existing affordable rental units resulting in further displacement.

In recognition of this, I am proposing that the Green Paper include analysis on a possible direct role the District may have in providing affordable rental stock (at this point left primarily to the not for profit and private sectors) and in particular to deal with short term displacement. At this point I cannot speak with confidence on the business or economic case for providing this new service, nor anticipate the many financial,

social or operational issues. That being said, given the importance of the issue and the impact on our community, I want to make sure that Council has an opportunity to consider all possible options, including those that have not traditionally been on the table, including more direct involvement at the municipal level.

Mathew Bond
Councillor

AGENDA INFORMATION	
<input checked="" type="checkbox"/> Regular Meeting	Date: <u>June 15, 2015</u>
<input type="checkbox"/> Workshop (open to public)	Date: _____

Dept. Manager	GM/ Director	CAO
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The District of North Vancouver REPORT TO COUNCIL

June 8, 2015

AUTHOR: Councillor Lisa Muri

SUBJECT: Affordable Rental Housing

RECOMMENDATION:

THAT Staff be directed to begin a discussion with Council about the retention of existing affordable housing and to prepare a green paper outlining regulatory and policy tools to ensure a future supply of affordable housing in the District.

REASON FOR REPORT:

To initiate a response to housing affordability in North Vancouver.

DISCUSSION:

Redevelopment is putting pressure on our older, affordable multi-family housing stock. While new housing is adding supply, it is typically more expensive than comparable, existing housing stock of the same size, type and location.

OCP supported growth within centres is putting additional pressure on existing older multi-family projects. The current DNV approach, while supporting one-for-one replacement of rental housing does not address "like-for-like" replacement. New replacement rental housing is typically smaller and more expensive.

Given low vacancy rates, displaced residents are potentially forced to look for comparable housing outside their existing neighbourhoods, the District and the North Shore.

Council should consider:

1. Making a clear statement that existing affordable rental housing stock should be protected
2. Phasing rental redevelopment in centres to avoid a large displacement of renters
3. Revising the one for one rental replacement policy to a "like-for-like" policy;
4. Adopting mandatory tenant relocation plans that allow residents to stay in the community.
5. Advising landlords and stratas that Council will not accept age or condition of existing multi-family structures as a rationale for redevelopment;
6. Direction to seek fewer replacement units but at a more affordable price point.

Regulatory and Policy Tools

OCP objectives include policy 7.2.3. Encourage retention of existing and the development of new units through development, rezoning and other incentives; 7.2.7. develop a rental and affordable housing strategy through Housing Action Plan(s) and/ or Centres Implementation Plans. 7.3.4 Require where appropriate, that large multifamily developments contribute to the provision of affordable housing by, but not limited to; a) including a portion of affordable rental or ownership units as part of the project, b) providing land dedicated for affordable housing c) providing a payment in lieu to address affordable housing. Not addressing affordability, may be a roadblock to a successful implementation of the OCP.

It is requested that staff prepare a green paper for Council discussion outlining policy and regulatory tools Council could consider to incentivize a diversity of affordable housing. The paper should include, for discussion, options such as:

1. Creating centres housing action plans, particularly for Lynn Valley Town Centre and Maplewood Village Centre, to address rental, rental replacement and affordability;
2. Adopting inclusionary zoning to require a percentage of new development be designated and/or transferred to the District for affordable housing;
3. Requiring developer contributions to an affordable housing fund;
4. Reducing parking requirements where appropriate
5. Creating a housing corporation, retaining a property manager or partnering with a not-for-profit to manage an affordable / rental housing inventory;
6. Waiving all, or a portion of, DCC's for affordable housing units;
7. Updating the District's Standards of Maintenance Bylaw to provide better clarity and stricter sanctions against landlords who fail to maintain their rental buildings (including secondary suites) in acceptable condition;
8. Encouraging innovation and flexibility;
9. Leveraging the District's land assets to facilitate affordable and/or rental housing;
10. Identifying opportunities to maintain as well as add, affordable ground oriented family housing options near schools and transit; and
11. Directing CAC's from one project to another to allow for affordable price point based on the affordability equation, and

a process to achieve a more fulsome affordable housing strategy.

Recognizing the District's budgetary and land use tools are limited senior governments need to be engaged in addressing the affordability crisis. Through lobbying actions as well as partnerships with government, agencies and non-profits the District needs to be at the forefront of change.

To that end, the District should create and maintain an active inventory of all types of housing in the District, to actively monitor that inventory and report on key housing metrics in order to facilitate prioritization and decision making.

Conclusion:

The OCP identified a need to grow and change, to diversify its housing base and to respond to a changed, and changing demographic. To be successful as a community, to be socially diverse and be respectful of those who both currently live here and future citizens who may not be as fortunate as others we need to include an affordability lens to all our decision making.

Respectfully submitted,

Lisa Muri
Councillor

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AGENDA INFORMATION	
<input checked="" type="checkbox"/> Regular Meeting	Date: <u>July 20, 2015</u>
<input type="checkbox"/> Workshop (open to public)	Date: _____

[Signature]
Dept.
Manager

[Signature]
GM/
Director

[Signature]
A/CAO

The District of North Vancouver REPORT TO COUNCIL

July 7, 2015

File: 11.5460.01/000.001

AUTHOR: Daniel Cifarelli - Transportation Engineering

SUBJECT: Request for Noise Bylaw Variance - 2151 Front Street - Dollarton Road works

RECOMMENDATION:

That Council relax the provision of Noise Regulation Bylaw 7188, which regulates construction noise during the night to complete the off-site works for the new development at 2151 Front Street.

BACKGROUND:

Dollarton Hwy. is a major east/west route, connecting Maplewood, Roche Point, and Dollarton Neighbourhoods to Highway 1 and the rest of the District of North Vancouver. There is also significant commercial and industrial truck activity in and around the 2100 block of Dollarton Hwy.

In determining the best time for conducting this work along Dollarton Hwy, a vehicle study was conducted capturing volumes, classifications and speeds. Based on the results from this study we have been able to determine that the midday traffic volumes exceed the capacity of a single lane of traffic in each direction through the signals at the intersections of Amherst and Riverside. If the work and required lane closures were permitted to occur during typical construction hours, significant traffic delays and major disruptions for commuters would be experienced.

REASON FOR REPORT:

The Noise Regulation Bylaw prohibits any construction-related noise after 8:00 pm Monday to Friday and after 5:00 pm on Saturdays, except if such construction activities would be significantly more disruptive to vehicular traffic during the day time hours.

In an effort to minimize the disruption to traffic on Dollarton Hwy, staff recommend a variance to the District's Noise Regulation Bylaw to complete the road works along this section of Dollarton Hwy.

**SUBJECT: Request for Noise Bylaw Variance
2151 Front Street - Dollarton Road works**

July 7, 2015

2

EXISTING POLICY:

Under Section 6(b) of Noise Regulation Bylaw 7188 Council may, by resolution, relax the regulation that prohibits construction noise during the night (8 pm to 7 am) and on weekends when such activities would be more disruptive to vehicular traffic if carried out during the day.

SUMMARY OF WORKS:

The following is a summary of the off-site work to be done on Dollarton Highway at night;

- Install two new driveway let-downs
- Remove two existing driveway let-downs.
- Install a left turn lane through the centre median between Amherst Ave. and Riverside Dr.
- Paving between Amherst Ave. and Riverside Dr.

All work will be subject to a traffic control plan acceptable to transportation staff. Traffic advisory signage will be installed at key locations in the District and Highway 1 to alert road users of the construction work ahead and will advise the use of alternate routes.

The work is anticipated to take place over 3 weeks in mid July / August , 2015; however the exact dates have not yet been confirmed. The construction crews will generally be working between 7 pm and 6 am. Commercial properties and local residents who are potentially affected will be notified in writing in advance of the work. See Attachment 1 for the proposed notification area. Noise will be generated from the operation of construction equipment and vehicle traffic and it will be monitored by the District.

Timing/Approval Process:

The exact date of this work is not known at this time as it is dependent on Council approval, dry weather and contractor availability. With Council's current schedule, Noise Bylaw variances cannot be considered after July 20 or before September 14, 2015. For these reasons we are requesting a 3 week window of time to take place in July and/or August in which to schedule this night work.

Subject to the approval of the noise variance, a Highway Use Permit will be issued for this work.

Liability/Risk:

Limited risk to the District provided adequate traffic management protocols are followed.

Respectfully submitted,

Daniel Cifarelli - Transportation Engineering

SUBJECT: Request for Noise Bylaw Variance
2151 Front Street - Dollarton Road works
 July 7, 2015

3

REVIEWED WITH:		
<input type="checkbox"/> Sustainable Community Dev.	<input type="checkbox"/> Clerk's Office	External Agencies:
<input type="checkbox"/> Development Services	<input type="checkbox"/> Communications	<input type="checkbox"/> Library Board
<input type="checkbox"/> Utilities	<input type="checkbox"/> Finance	<input type="checkbox"/> NS Health
<input type="checkbox"/> Engineering Operations	<input type="checkbox"/> Fire Services	<input type="checkbox"/> RCMP
<input type="checkbox"/> Parks & Environment	<input type="checkbox"/> ITS	<input type="checkbox"/> Recreation Com.
<input type="checkbox"/> Economic Development	<input type="checkbox"/> Solicitor	<input type="checkbox"/> Museum & Arch.
<input type="checkbox"/> Human resources	<input type="checkbox"/> GIS	<input type="checkbox"/> Other:

Attachment 1 – Work Limits and Notification Area



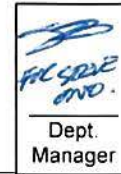
Work Limits

Notification Area

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**SUBJECT: Request for Noise Bylaw Variance
Seylynn Paving Mountain Hwy**

July 7, 2015 *Regular Meeting Date: July 20, 2015*



9.8

The District of North Vancouver REPORT TO COUNCIL

July 7, 2015

File: 11.5460.01/000.001

AUTHOR: Daniel Cifarelli - Transportation Engineering

SUBJECT: Request for Noise Bylaw Variance - Paving Mountain Hwy between Keith Road Extension and the lane south of Fern St., and Fern St. from Mountain Hwy to cul-de-sac East

RECOMMENDATION:

That Council relax the provision of Noise Regulation Bylaw 7188, which regulates construction noise during the night, to complete the paving of Mountain Hwy from the Keith Road extension to the lane south of Fern St., and Fern St. from Mountain Hwy to the cul-de-sac east.

BACKGROUND:

Mountain Hwy is a major north/south route, connecting Keith Rd to Main St. west of Highway #1. There are several large construction projects in the area including the Keith Road Bridge Project and Phase 2 of the Seylynn Village development, both of which are underway.

If conducted during daytime hours, paving Mountain Hwy. will result in significant traffic delays and major disruptions for commuters. In addition, this work will impact other construction activities in the area. Allowing paving on Mountain Hwy. to be carried out at night will minimize traffic disruption to the majority of commuters, increase the efficiency of construction, and thereby shorten the work duration.

REASON FOR REPORT:

The Noise Regulation Bylaw prohibits any construction-related noise after 8:00 pm Monday to Friday and after 5:00 pm on Saturdays, except if such construction activities would be significantly more disruptive to vehicular traffic during the day time hours.

In an effort to minimize the disruption to traffic on Mountain Hwy and reduce congestion with the construction taking place along East Keith Road, staff recommend a variance to the District's Noise Regulation Bylaw to complete the paving of Mountain Hwy. and Fern St. at night.

EXISTING POLICY:

Under Section 6(b) of Noise Regulation Bylaw 7188 Council may, by resolution, relax the regulation that prohibits construction noise during the night (8 pm to 7 am) and on weekends when such activities would be more disruptive to vehicular traffic if carried out during the day.

**SUBJECT: Request for Noise Bylaw Variance
Seylynn Paving Mountain Hwy**

July 7, 2015

2

SUMMARY OF WORKS:

The following is a summary of the paving work to be done at night;

- Mountain Hwy. from Keith Rd. extension to the Lane south of Fern St.
- Fern Street from Mountain Hwy. to the cul-de sac to the east.

All work will be subject to a traffic control plan acceptable to transportation staff. Traffic advisory signage will be installed at key locations in the District and the City of North Vancouver to alert road users of the construction work ahead and will advise the use of alternate routes.

The work is anticipated to take place on one night once scheduled in July / August , 2015. The construction crews will generally be working between 7 pm and 6 am and to minimize disruption to local residents, the work on Fern St. will be scheduled to be completed first. Commercial properties and local residents who are potentially affected will be notified in writing in advance of the work. See Attachment 1 for the proposed notification area. Noise will be generated from the operation of construction equipment and vehicle traffic and it will be monitored by the District.

Timing/Approval Process:

The exact date of the paving is not known at this time as it is dependent on dry weather and on the removal of third-party utility poles. The removal of these poles has been requested for several months and is expected to take place in July however dates have not been confirmed. The paving will take place once the poles are removed. With Council's current schedule, Noise Bylaw variances cannot be considered after July 20 or before September 14, 2015. For these reasons we are requesting a window of time in which to schedule this one night of work.

Subject to the approval of the noise variance, a Highway Use Permit will be issued for this work.

Liability/Risk:

Limited risk to the District provided adequate traffic management protocols are followed.

Respectfully submitted,

Daniel Cifarelli - Transportation Engineering

**SUBJECT: Request for Noise Bylaw Variance
Seylynn Paving Mountain Hwy**

July 7, 2015

3

REVIEWED WITH:		
<input type="checkbox"/> Sustainable Community Dev.	<input type="checkbox"/> Clerk's Office	External Agencies:
<input type="checkbox"/> Development Services	<input type="checkbox"/> Communications	<input type="checkbox"/> Library Board
<input type="checkbox"/> Utilities	<input type="checkbox"/> Finance	<input type="checkbox"/> NS Health
<input type="checkbox"/> Engineering Operations	<input type="checkbox"/> Fire Services	<input type="checkbox"/> RCMP
<input type="checkbox"/> Parks & Environment	<input type="checkbox"/> ITS	<input type="checkbox"/> Recreation Com.
<input type="checkbox"/> Economic Development	<input type="checkbox"/> Solicitor	<input type="checkbox"/> Museum & Arch.
<input type="checkbox"/> Human resources	<input type="checkbox"/> GIS	<input type="checkbox"/> Other:

Attachment 1 – Work Limits and Notification Area



Work Limits

Notification Area

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3075 Fromme Road

Materials to be circulated via agenda addendum.

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MAYOR REPORTS

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Memo

July 10, 2015

TO: Council

FROM: Mayor Richard Walton

SUBJECT: Mayor's Special One Time Contingency Fund

At Mayor Walton's request, \$510.72 from the Mayor's Special One Time Contingency Fund has been used to frame a new photo of Karen Magnussen (see attached).

This framed photo will be hung at the Karen Magnussen Arena replacing the old picture of Karen that was removed a few years ago due to the poor shape of the photo.

Richard Walton

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