

AGENDA

REGULAR MEETING OF COUNCIL

Monday, January 18, 2016

7:00 p.m.

Council Chamber, Municipal Hall

355 West Queens Road,

North Vancouver, BC

Council Members:

Mayor Richard Walton

Councillor Roger Bassam

Councillor Mathew Bond

Councillor Jim Hanson

Councillor Robin Hicks

Councillor Doug MacKay-Dunn

Councillor Lisa Muri



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REGULAR MEETING OF COUNCIL

7:00 p.m.
Monday, January 18, 2016
Council Chamber, Municipal Hall,
355 West Queens Road, North Vancouver

AGENDA

BROADCAST OF MEETING

- Live broadcast on Shaw channel 4
- ReBroadcast on Shaw channel 4 at 9:00 a.m. Saturday
- Online at www.dnv.org

CLOSED PUBLIC HEARING ITEMS NOT AVAILABLE FOR DISCUSSION

- Bylaw 7984 – Rezoning 3568-3572 Mt. Seymour Parkway
- Bylaw 8138 – Rezoning 756 and 778 Forsman Avenue
- Bylaw 8149 – Rezoning 115 and 123 West Queens Road

1. ADOPTION OF THE AGENDA

1.1. January 18, 2016 Regular Meeting Agenda

Recommendation:

THAT the agenda for the January 18, 2016 Regular Meeting of Council for the District of North Vancouver be adopted as circulated, including the addition of any items listed in the agenda addendum.

2. PUBLIC INPUT

(limit of three minutes per speaker to a maximum of thirty minutes total)

3. PROCLAMATIONS

4. RECOGNITIONS

5. DELEGATIONS

- 5.1. Joy Hayden, Hollyburn Family Services Society**
Re: Hollyburn Family Services Society – An Overview.

p. 9-10

6. ADOPTION OF MINUTES

6.1. December 14, 2015 Regular Council Meeting

p. 13-18

Recommendation:

THAT the minutes of the December 14, 2015 Regular Council meeting be adopted.

7. RELEASE OF CLOSED MEETING DECISIONS

8. COUNCIL WORKSHOP REPORT

9. REPORTS FROM COUNCIL OR STAFF

With the consent of Council, any member may request an item be added to the Consent Agenda to be approved without debate.

If a member of the public signs up to speak to an item, it shall be excluded from the Consent Agenda.

Recommendation:

THAT items _____ be included in the Consent Agenda and be approved without debate.

9.1. Lynn Creek Public Realm Guidelines

p. 21-141

File No. 13.6480.30/002.001

Recommendation:

THAT the "Lynn Creek Public Realm Guidelines", as attached to the December 14, 2015 joint report of the Community Planners entitled Lynn Creek Public Realm Guidelines, are approved.

9.2. Bylaws 8138 and 8139: 756-778 Forsman Avenue

p. 143-204

File No. 08.3060.20/085.12

Recommendation:

THAT "The District of North Vancouver Rezoning Bylaw 1334 (Bylaw 8138)" is given SECOND and THIRD Readings.

THAT "Housing Agreement Bylaw 8139, 2015 (756 & 778 Forsman Avenue)" is given SECOND and THIRD Readings.

9.3. Bylaws 8149 and 8150: 14 Unit Townhouse Development at 115 and 123 West Queens Road

p. 205-259

File No. 08.3060.20/033.15

Recommendation:

THAT "The District of North Vancouver Rezoning Bylaw 1337 (Bylaw 8149)" is given SECOND and THIRD Readings.

THAT "Housing Agreement Bylaw 8150, 2015 (115 and 123 West Queens Road)" is given SECOND and THIRD Readings.

9.4. Bylaws 8159, 8160 and 8162: OCP Amendment, Rezoning and Housing Agreement for a 24 Unit Apartment Project – 1103, 1109, 1123 Ridgewood Drive and 3293 Edgemont Boulevard p. 261-305
File No. 08.3060.20/030.15

Recommendation:

THAT the “District of North Vancouver Official Community Plan Bylaw 7900, 2011, Amendment Bylaw 8159, 2015 (Amendment 16)”, to amend the Official Community Plan (OCP) land use designation for the affected parcels from Residential Level 2 (RES2) to Residential Level 4 (RES4), is given FIRST Reading;

AND THAT the “District of North Vancouver Rezoning Bylaw 3210 (Bylaw 8160)”, to rezone the subject parcels from Single Family Residential Edgemont (RSE) to Comprehensive Development Zone 92 (CD92), is given FIRST Reading;

AND THAT “Housing Agreement Bylaw 8162, 2015 (1103, 1109, 1123 Ridgewood Drive and 3293 Edgemont Boulevard)”, to authorize a Housing Agreement to prevent future rental restrictions on the subject property, is given FIRST Reading;

AND THAT pursuant to Section 879 of the *Local Government Act*, additional consultation is not required beyond that already undertaken with respect to Bylaw 8159;

AND THAT in accordance with Section 882 of the *Local Government Act*, Council has considered Bylaw 8159 in conjunction with its Financial Plan and applicable Waste Management Plans;

AND THAT Bylaw 8159 and Bylaw 8160 are referred to a Public Hearing.

9.5. Bylaws 8147 and 8148: OCP Amendment and Rezoning for a Four Lot Single-Family Residential Development: 3600 Mahon Avenue (Braemar Elementary School) p. 307-349
File No. 08.3060.20/026.15

Recommendation:

THAT “The District of North Vancouver Official Community Plan Bylaw 7900, 2011, Amendment Bylaw 8147, 2015 (Amendment 15)”, to amend the Official Community Plan for a portion of the subject site from Institutional to Residential Level 2: Detached Residential (RES2) to allow for a four lot detached residential development, is given FIRST Reading;

AND THAT “The District of North Vancouver Rezoning Bylaw 1336 (Bylaw 8148)” to rezone a portion of the subject site from Public Assembly (PA) to Comprehensive Development Zone 93 (CD93) to allow for a four lot detached residential development, is given FIRST Reading;

AND THAT pursuant to Section 879 of the *Local Government Act*, additional consultation is not required beyond that already undertaken with respect to Bylaw 8147;

AND THAT in accordance with Section 882 of the *Local Government Act*, Council has considered Bylaw 8147 in conjunction with its Financial Plan and applicable Waste Management Plans;

AND THAT Bylaws 8147 and 8148 are referred to a Public Hearing.

9.6. Community Amenity Contribution Policy Update

p. 351-378

File No. 08.3060.20/000.000

Recommendation:

THAT the Community Amenity Contribution Policy as attached to the January 7, 2016 report of the Deputy General Manager – Planning & Permits entitled Community Amenity Contribution Policy Update is endorsed for approval by the Chief Administrative Officer.

9.7. North Vancouver Recreation & Culture Commission Sport and Recreation Travel Grants Recommendations

p. 379-383

File No.

Recommendation:

THAT the achievements of North Vancouver resident Brendan Artley be recognized with a \$200 travel grant and the Capilano University Men's Soccer Team be granted a \$1,000 travel grant.

10. REPORTS

10.1. Mayor

10.2. Chief Administrative Officer

10.3. Councillors

10.4. Metro Vancouver Committee Appointees

11. ANY OTHER BUSINESS

12. ADJOURNMENT

Recommendation:

THAT the January 18, 2016 Regular Meeting of Council for the District of North Vancouver be adjourned.

DELEGATIONS

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Delegation to Council Request Form

District of North Vancouver
Clerk's Department

355 West Queens Rd, North Vancouver, BC V7N 4N5

Questions about this form: Phone: 604-990-2311

Form submission: Submit to address above or Fax: 604.984.9637

COMPLETION: To ensure legibility, please complete (type) online then print. Sign the printed copy and submit to the department and address indicated above.

Delegations have five minutes to make their presentation. Questions from Council may follow.

Name of group wishing to appear before Council: Hollyburn Family Services Society

Title of Presentation: Hollyburn Family Services Society - an overview

Name of person(s) to make presentation: Joy Hayden and TBD

Purpose of Presentation: Information only
 Requesting a letter of support
 Other (provide details below)

Please describe:

Attach separate sheet if additional space is required

We wish to update Council on our programs, services and outcomes over the past few years and to thank them for their ongoing support.

Contact person (if different than above): Joy Hayden

Daytime telephone number: 604-987-8211 (223)

Email address: jhayden@hollyburn.ca

Will you be providing supporting documentation? Yes No

If yes: Handout DVD
 PowerPoint presentation

Note: All supporting documentation must be provided 12 days prior to your appearance date. This form and any background material provided will be published in the public agenda.

Presentation requirements: Laptop Tripod for posterboard
 Multimedia projector Flipchart
 Overhead projector

Arrangements can be made, upon request, for you to familiarize yourself with the Council Chamber equipment on or before your presentation date.



Delegation to Council Request Form

Rules for Delegations:

1. Delegations must submit a Delegation to Council Request Form to the Municipal Clerk. Submission of a request does not constitute approval nor guarantee a date. The request must first be reviewed by the Clerk.
2. The Clerk will review the request and, if approved, arrange a mutually agreeable date with you. You will receive a signed and approved copy of your request form as confirmation.
3. A maximum of two delegations will be permitted at any Regular Meeting of Council.
4. Delegations must represent an organized group, society, institution, corporation, etc. Individuals may not appear as delegations.
5. Delegations are scheduled on a first-come, first-served basis, subject to direction from the Mayor, Council, or Chief Administrative Officer.
6. The Mayor or Chief Administrative Officer may reject a delegation request if it regards an offensive subject, has already been substantially presented to council in one form or another, deals with a pending matter following the close of a public hearing, or is, or has been, dealt with in a public participation process.
7. Supporting submissions for the delegation should be provided to the Clerk by noon 12 days preceding the scheduled appearance.
8. Delegations will be allowed a maximum of five minutes to make their presentation.
9. Any questions to delegations by members of Council will seek only to clarify a material aspect of a delegate's presentation.
10. Persons invited to speak at the Council meeting may not speak disrespectfully of any other person or use any rude or offensive language or make a statement or allegation which impugns the character of any person.

Helpful Suggestions:

- have a purpose
- get right to your point and make it
- be concise
- be prepared
- state your request, if any
- do not expect an immediate response to a request
- multiple-person presentations are still five minutes maximum
- be courteous, polite, and respectful
- it is a presentation, not a debate
- the Council Clerk may ask for any relevant notes (if not handed out or published in the agenda) to assist with the accuracy of our minutes

I understand and agree to these rules for delegations

Joy Hayden

October 2, 2015

Name of Delegate or Representative of Group

Date

Signature

For Office Use Only

Approved by:

Municipal Clerk



Deputy Municipal Clerk

Appearance date:

January 18, 2015

Receipt emailed on:

October 20, 2015

Rejected by:

Mayor

Applicant informed on:

CAO

Applicant informed by:

The personal information collected on this form is done so pursuant to the Community Charter and/or the Local Government Act and in accordance with the Freedom of Information and Protection of Privacy Act. The personal information collected herein will be used only for the purpose of processing this application or request and for no other purpose unless its release is authorized by its owner, the information is part of a record series commonly available to the public, or is compelled by a Court or an agent duly authorized under another Act. Further information may be obtained by speaking with The District of North Vancouver's Manager of Administrative Services at 604-990-2207 or at 355 W Queens Road, North Vancouver.

MINUTES

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**DISTRICT OF NORTH VANCOUVER
REGULAR MEETING OF COUNCIL**

Minutes of the Regular Meeting of the Council for the District of North Vancouver held at 7:03 p.m. on Monday, December 14, 2015 in the Council Chambers of the District Hall, 355 West Queens Road, North Vancouver, British Columbia.

Present: Mayor R. Walton
Councillor R. Bassam
Councillor M. Bond
Councillor J. Hanson
Councillor D. MacKay-Dunn

Absent: Councillor R. Hicks
Councillor L. Muri

Staff: Mr. B. Bydwell, Acting Chief Administrative Officer
Ms. C. Grant, General Manager – Corporate Services
Mr. G. Joyce, General Manager – Engineering, Parks & Facilities
Mr. A. Wardell, Acting General Manager – Finance & Technology
Mr. D. Milburn, Deputy General Manager – Planning & Permits
Mr. J. Gordon, Manager – Administrative Services
Ms. M. Welman, Manager – Strategic Communication & Community Relations
Ms. L. Brick, Deputy Municipal Clerk
Ms. S. Dale, Confidential Council Clerk

1. ADOPTION OF THE AGENDA

1.1. December 14, 2015 Regular Meeting Agenda

MOVED by Councillor BASSAM

SECONDED by Councillor BOND

THAT the agenda for the December 14, 2015 Regular Meeting of Council for the District of North Vancouver be adopted as circulated.

CARRIED

2. PUBLIC INPUT

2.1. Mr. Hazen Colbert, 1100 Block East 27th Street:

- Spoke regarding affordable housing; and,
- Commented that in 2009 there were 297 families on the social housing waiting list in North Vancouver and noted that today there are 603 families, growing twice as fast as the Metro Vancouver average.

2.2. Mr. John Harvey, 1900 Block Cedarvillage Crescent:

- Suggested that the November 16, 2015 Regular Council minutes be amended by revising his comments regarding the Hamersley House; and,
- Opined that the time allocated for delegations is not sufficient.

2.3. Ms. Sharee Dubouvites and Mr. Brian Smith, Seymour Salmonid Society:

- Provided history on the Seymour Salmonid Society;
- Provided an update on the Seymour River Rockslide Recovery project;
- Advised that no tagged juvenile fish released above the slide area or mature fish tagged below the slide area were able to traverse the section of the river blocked by the slide; and,
- Advised that a fence will be installed in the lower river which would allow a safe and relatively low-stress method of collecting fish before they become trapped below the slide debris.

3. PROCLAMATIONS

Nil

4. RECOGNITIONS

Nil

5. DELEGATIONS

5.1. Mr. Keith Fenton, Coho Society of the North Shore

Re: Update Regarding Coho Society and Coho Festival

Mr. Keith Fenton, Coho Society of the North Shore, provided an overview of the Coho Society mandate and the September 13, 2015 festival. On behalf of the Coho Society, Mr. Fenton thanked Council for their support.

MOVED by Councillor BASSAM

SECONDED by Councillor MACKAY-DUNN

THAT the delegation from the Coho Society of the North Shore is received.

CARRIED

6. ADOPTION OF MINUTES

6.1. November 23, 2015 Regular Council Meeting

MOVED by Councillor BASSAM

SECONDED by Councillor BOND

THAT the minutes of the November 23, 2015 Regular Council meeting be adopted.

CARRIED

6.2. December 1, 2015 Public Hearing

MOVED by Councillor BASSAM

SECONDED by Councillor BOND

THAT the minutes of the December 1, 2015 Public Hearing be received.

CARRIED

6.3. December 7, 2015 Regular Council Meeting

**MOVED by Councillor BASSAM
SECONDED by Councillor BOND**

THAT the minutes of the December 7, 2015 Regular Council meeting be adopted.

CARRIED

7. RELEASE OF CLOSED MEETING DECISIONS

7.1. November 23, 2015 Closed Special Meeting of Council

File No. 01.0360.20/076.000

7.1.1. Advisory Design Panel

THAT Mr. Tieg Martin be re-appointed to the Advisory Design Panel for a two year term ending December 31, 2017;

AND THAT Ms. Amy Tsang be re-appointed to the Advisory Design Panel for a two year term ending December 31, 2017;

AND THAT Mr. Laurenz Kosichuk be appointed to the Advisory Design Panel for a two year term ending December 31, 2017;

AND THAT Mr. Craig Taylor be appointed to the Advisory Design Panel for a two year term ending December 31, 2017;

AND THAT Mr. Steve Wong be appointed to the Advisory Design Panel for a two year term ending December 31, 2017;

AND THAT Mr. Stefen Elmitt be appointed to the Advisory Design Panel for a two year term ending December 31, 2017;

AND THAT Ms. Diana Zoe Coop be appointed to the Advisory Design Panel for a two year term ending December 31, 2017.

North Vancouver Recreation and Culture Commission

THAT Allison Rzen be re-appointed to the North Vancouver Recreation & Culture Commission for two years (the remainder of a three-year term) with the term ending December 31, 2017.

7.2. December 7, 2015 Closed Special Meeting of Council

File No. 01.0360.20/023.000

7.2.1. Municipal Library Board

THAT Mike Little, Kristine MacTaggart Wright, Alison Rzen and Phil Webber be appointed to the Municipal Library Board for the two year term January 1, 2016 to December 31, 2017.

8. COUNCIL WORKSHOP REPORT

Nil

9. REPORTS FROM COUNCIL OR STAFF

- 9.1. Bylaw 8151: Sewer Bylaw**
Bylaw 8152: Waterworks Bylaw
Bylaw 8153: Solid Waste Removal Bylaw
File No. 09.3900.20/000.000

MOVED by Councillor BOND
SECONDED by Councillor BASSAM

THAT "Sewer Bylaw 6656, 1994, Amendment Bylaw 8151, 2015 (Amendment 26)" is ADOPTED.

THAT "Waterworks Regulation Bylaw 2279, 1958, Amendment Bylaw 8152, 2015 (Amendment 60)" is ADOPTED.

THAT "Solid Waste Removal Bylaw 7631, 2007, Amendment Bylaw 8153, 2015 (Amendment 12)" is ADOPTED.

CARRIED

- 9.2. Bylaw 8143: Fees and Charges Bylaw 6481, 1992**
File No. 09.3900.20/000.000

MOVED by Councillor BASSAM
SECONDED by Councillor HANSON

THAT "District of North Vancouver Fees and Charges Bylaw 6481, 1992, Amendment Bylaw 8143, 2015 (Amendment 48)" is ADOPTED.

CARRIED

- 9.3. Bylaw 8154: Bylaw Notice Enforcement Bylaw 7458, 2004**
File No. 09.3900.20/000.000

MOVED by Councillor BASSAM
SECONDED by Councillor HANSON

THAT "Bylaw Notice Enforcement Bylaw 7458, 2004, Amendment Bylaw 8154, 2015 (Amendment 27)" is ADOPTED.

CARRIED

9.4. Bylaw 8155: Development Cost Charges Bylaw 7135, 2000
File No. 09.3900.20/000.000

MOVED by Councillor BOND

SECONDED by Councillor MACKAY-DUNN

THAT “Development Cost Charges Bylaw 7135, 2000, Amendment Bylaw 8155, 2015 (Amendment 5)” is ADOPTED.

CARRIED

9.5. Establishment of a New Transit Centre on the North Shore
File No. 16.8500.30/004.000

MOVED by Councillor BASSAM

SECONDED by Councillor MACKAY-DUNN

THAT a transit centre remain on the North Shore which is capable of supporting the operations of all or a portion of the North Shore fleet;

AND THAT staff be directed to search for any and all suitable lands for a new North Shore Transit Centre and report back to Council with the results;

AND THAT a copy of this resolution be forwarded to the Federal Government, Provincial Government, TransLink, City of Vancouver, City of North Vancouver and the District of West Vancouver.

CARRIED

10. REPORTS

10.1. Mayor

Mayor Walton wished everyone a safe and happy holiday season.

10.2. Chief Administrative Officer

Nil

10.3. Councillors

Councillor Bond reported on the Safe Routes Advocates Freedom Friday events which promote children to walk or bike to school.

Councillor Bassam reported on the North Vancouver Museum and Archives work plan and expressed concern that the District’s vision of the collection being displayed/presented in District facilities has not been included in the new museum report.

10.4. Metro Vancouver Committee Appointees

Nil

11. ANY OTHER BUSINESS

Nil

12. ADJOURNMENT

MOVED by Councillor BASSAM

SECONDED by Councillor HANSON

THAT the December 14, 2015 Regular Meeting of Council for the District of North Vancouver be adjourned.

CARRIED
(8:00 pm)

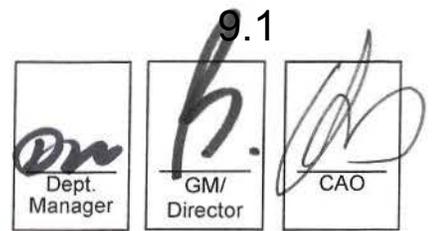
Mayor

Municipal Clerk

REPORTS

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AGENDA INFORMATION	
<input checked="" type="checkbox"/> Regular Meeting	Date: <u>JAN 18, 2016</u>
<input type="checkbox"/> Committee of the Whole	Date: _____



The District of North Vancouver REPORT TO COUNCIL

January 6, 2016
File: 13.6480.30/002.001.000

AUTHOR: Ross Taylor, Community Planner
Jessie Gresley-Jones, Community Planner

SUBJECT: Lynn Creek Public Realm Guidelines

RECOMMENDATION:

THAT the attached “Lynn Creek Public Realm Guidelines” are approved.

REASON FOR REPORT:

The purpose of this report is to inform Council of the results of the recent public engagement process and to seek Council approval of the “Lynn Creek Public Realm Guidelines” (attachment 1).

SUMMARY:

The public realm guidelines provide a design framework for the plazas, parks, open space, streetscapes and buildings within the Lynn Creek Town Centre. They will guide property owners, developers, design consultants, District staff and Council towards the achievement of the community vision for the Lynn Creek Town Centre.

Based on strong overall support for the design guidelines from the recent engagement, staff recommends Council approval of these public realm guidelines.

BACKGROUND:

Staff first presented a draft Lynn Creek Public Realm Guidelines (previously referred to as the Lower Lynn Streetscape Design Guidelines) to the Committee of the Whole on January 13, 2014. The current version of the public realm guidelines reflects Councillors’ comments from the Committee of the Whole meeting and has also been influenced by several related initiatives or projects within or adjacent to the Lynn Creek Town Centre. The more pertinent of these are outlined below.

- A Lynn Creek Town Centre “**branding**” workshop, held in 2014 explored the future experience and sense of place for this Town Centre. Insights and ideas gained from the branding session informed the inspiration, identity and character of the town centre.

- The **Phibbs Exchange Design** has progressed through a collaborative process with the District and this has assisted in better integrating Phibbs Exchange into the town centre, particularly the Oxford Street streetscape.
- **The Preliminary Lower Lynn Interchange** project has advanced since the earlier draft of the guidelines with confirmation of on and off ramp configurations and pedestrian crossing of Highway 1 at Crown Street.
- **The Keith Road bridge** replacement is well underway enabling cycling and pedestrian trail improvements which have been incorporated into the public realm guidelines.
- **The Lower Lynn Functional Design Study** evaluated right of way and major utility requirements for Mountain Highway and Crown Street, and this information has been incorporated into the guidelines.
- **The Seylynn and Bridgman Parks Master Plan** provides direction with respect to Lynn Creek trails and edge conditions along Mountain Highway and Hunter Street, which has informed the public realm guidelines.
- **The Lynn Creek Industrial Lands Strategy** has explored ways of enlivening this precinct while retaining the “industry first” focus. The design guidelines celebrate and promote the “gritty” and eclectic nature of the industrial lands.
- Reviews of several **major development applications** including phase two of Seylynn Village and Intergulf Development's application adjacent to Seylynn Park have informed streetscape refinements on Fern Street, Mountain Highway and Hunter Street.

EXISTING POLICY:

The District Official Community Plan (OCP) identifies the Lynn Creek Town Centre as one of four key growth centres within the network of centres concept. The OCP establishes land use and density throughout the District, and OCP Schedule B: Development Permit Areas provides a series of form and character design guidelines for commercial, industrial and multi-family development.

The Lower Lynn (now referred to as Lynn Creek) Town Centre Implementation Plan (approved by Council in 2013) builds on the land use, growth management and urban structure policy framework in the OCP and provides more detailed policies to guide growth and change within the Lynn Creek Town Centre.

If approved by Council, the Lynn Creek Public Realm Guidelines will supplement the Centre Implementation Plan and the OCP Form and Character Design Guidelines by providing detailed streetscape, parks and plaza design direction to guide the realization of a distinct sense of place and well-designed, inviting public realm spaces in the town centre.

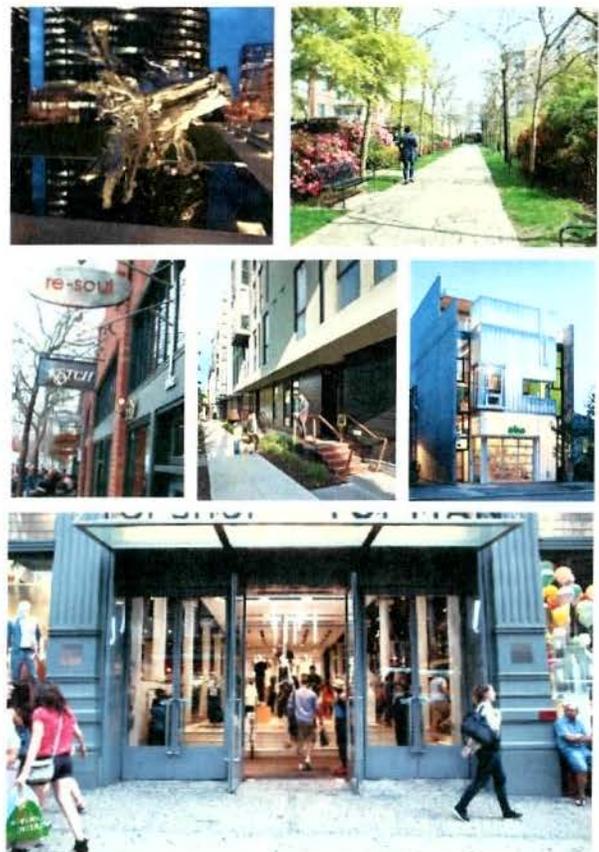
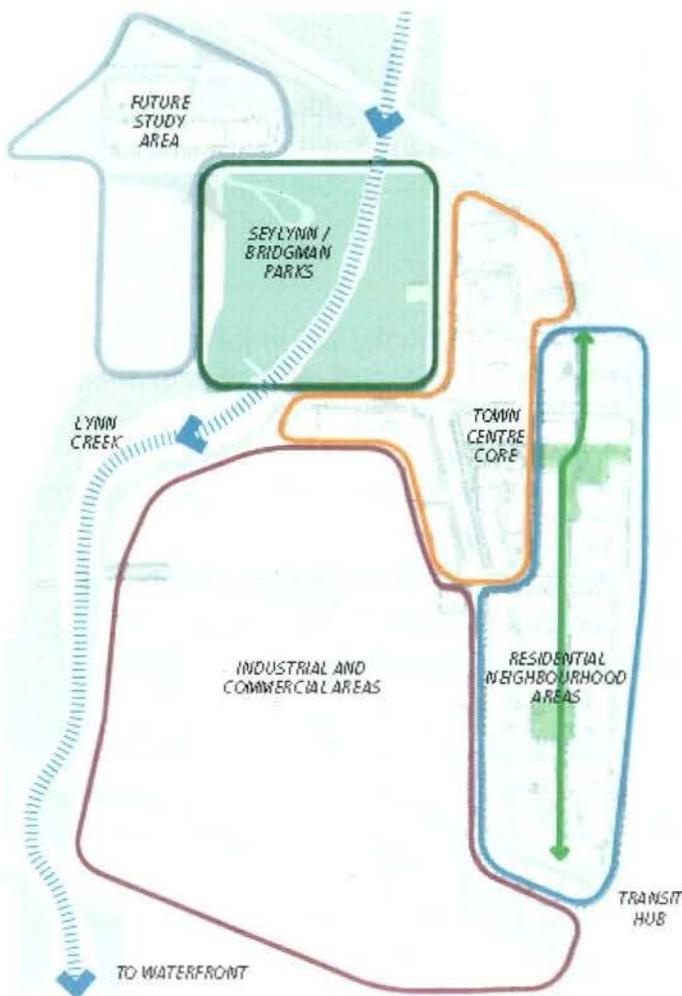
ANALYSIS:

Four Character Areas

The public realm guidelines identify four character areas within the town centre that are differentiated by land use type. These areas include the *town centre core*; the *residential neighbourhood*; the *industrial/commercial precinct*; and *parks and open space*. While the guidelines recommend a range of unifying design elements to establish a common identity and sense of place, each character area also exhibits a unique design response.

CHARACTER AREAS

Figure 1: Character Areas



- a) The *town centre core*, centred on Mountain Highway and Hunter Street, is the heart of the community where higher density, mixed-use development is focussed. Several key elements of the public realm including a community centre, central plaza and the High Street are located here. Notable design features for this area include, wide sidewalks, street trees, weather protection, street furnishings, public art and engaging building edges to create a lively pedestrian oriented shopping and important open

space that supports businesses, community needs and establishes an iconic central plaza shopping hub.

- b) The *residential neighbourhood* is situated east of Mountain Highway, adjacent to the town centre core and is organized around the “green spine”, which is a linear park connecting the neighbourhood from Phibbs Exchange, through the central plaza and on to Seylynn Park and the Lynn Creek trail system.
- c) Changes to the *industrial/commercial precinct* west of Mountain Highway and south of Main Street will happen gradually given the stability of land uses. The design guidelines propose few changes to the industrial lands, allowing these areas to maintain their gritty and eclectic nature.
- d) The *parks and open space network* connects Lynn Creek residents to a range of passive and active recreational opportunities and links the town centre to Park & Tilford, Inter-River and other key destinations.

Design Principles and Inspiration

The public realm guidelines guide the realization of the community vision for the look and feel of the Lynn Creek Town Centre. They strive to establish a sense of place that expresses a distinct character that is contemporary and innovative. The inspiration is drawn from the following experiences:

- Connects neighbourhoods to Lynn Creek and other key destinations - overcoming existing barriers with pedestrian crossings and bridges.
- Establishes a contemporary complete community – work places, amenities, shops, key destinations and transit within walking distance from homes.
- Promotes active living – moving people out of cars (reducing need for cars) and embracing active living including walking and cycling.
- Promotes social interaction and neighbourliness – social gathering spaces and community amenities and facilities become the “outdoor living rooms” for people to meet and get to know their neighbours.
- Embraces local business – small businesses as key to the local economy + playing an important role in energizing, animating, infusing new ideas & building excitement in this area.
- Celebrates eclectic, random, funky, organic and somewhat unruly nature of current and future development – different architectural forms, scales of development, lot sizes, types of uses etc.
- Encourages innovation and creativity - including smaller, fine grain lot patterns and affordable spaces for families and workers.

- Provides flexibility and choice – flexible spaces that can be adapted to residential and business needs, flexible zoning to promote innovation, opportunity and experimentation.
- Encourages seamless integration - Mountain Highway should connect and not “divide” the neighbourhoods and streets.

The following unifying design elements are intended to be expressed throughout the town centre to help establish a cohesive and integrated sense of place.

- Establishing the Green Spine as an important North-South linear park;
- Creating a cycling and pedestrian network to support all users, abilities and connections to key destinations within the town centre;
- Integrating functional stormwater management in a meaningful way that is both functional and aesthetic;
- Ensuring full and healthy growth of urban trees through innovative planting techniques and increased soil volumes.

Parks, Plazas & Open Spaces

The proposed open space network for Lynn Creek Town Centre is a series of parks, urban plazas, and natural areas that together provide a range of passive and active recreational opportunities, outdoor experiences, and other amenities connected by trails, multi-use paths, and greenways for the enjoyment of residents, employees, and visitors.

Town Centre Plaza and Interconnecting Mews

The Town Centre plaza on the east side of Mountain Highway is intended to be a focus of both daily use for sitting, outdoor seating, and people watching as well as a programmable space that can host community events. It will be designed to accommodate a wide variety of users, and promote accessibility and inclusivity.

Marie Place Neighbourhood Park

Marie Place Park is currently a neighbourhood pocket park with a children's playground. The Implementation Plan envisions expanding the size of this park and enhancing facilities suited to its role as a locally serving neighbourhood park. The park design should consider opportunities for expanded children's play, gathering and seating areas, and public art.



Mid-Block Pedestrian Spine and New Local Neighbourhood Park

The pedestrian green "spine" will run north south from Fern Street to Oxford Street and will be a significant new linear park in the Lynn Creek Town Centre. This aligned pathway will be

more than simply a pedestrian trail. It is envisioned as a continuous park providing connectivity with seating and opportunities for play, with stormwater elements integrated along its length.

New Neighbourhood Park

A new neighbourhood park created along the spine and between Rupert and Bond Streets will provide a more substantive area for play and act as a node along this linear park.

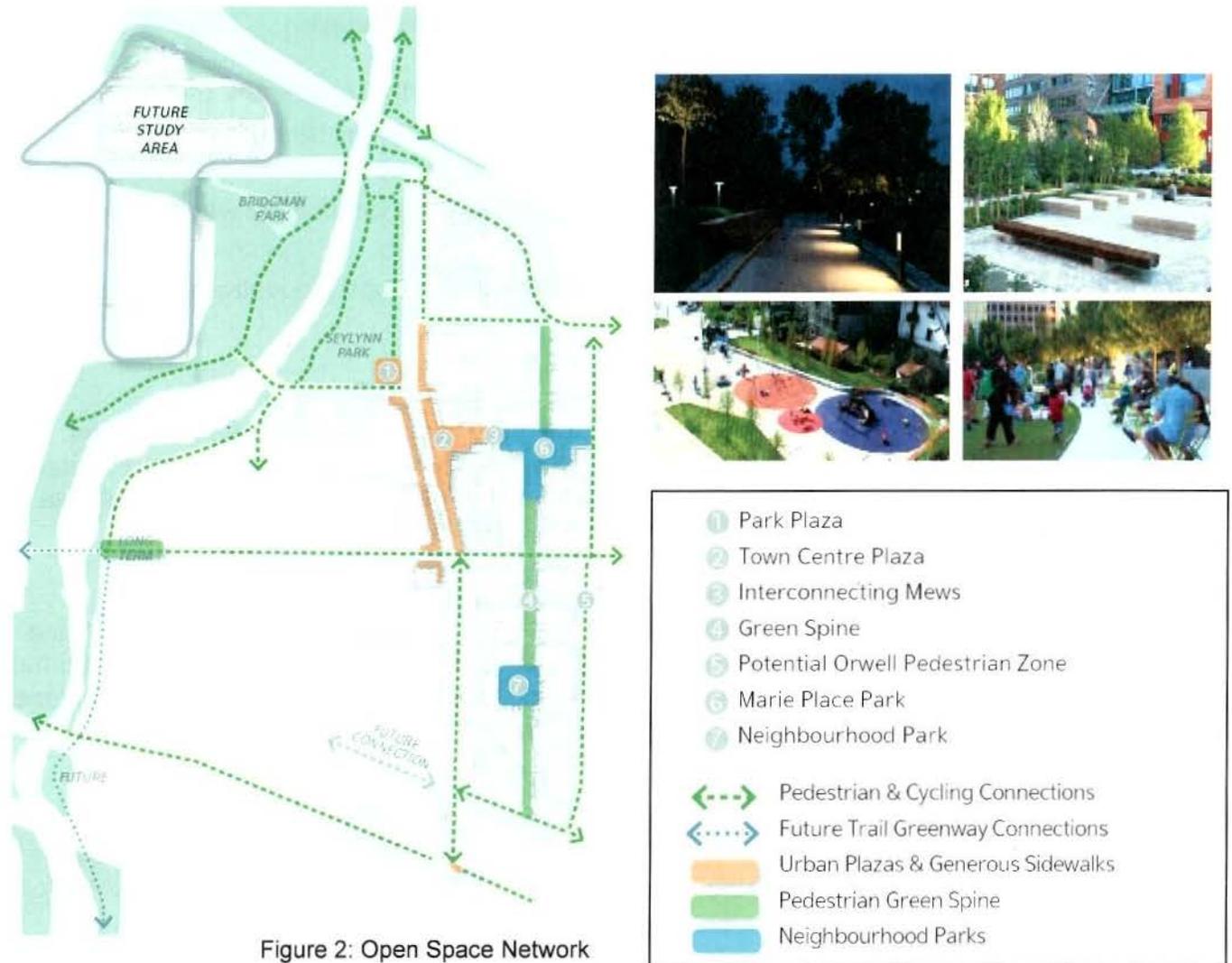


Figure 2: Open Space Network

Streets

Each street type offers unique elements relating to traffic volumes, adjacent building use and natural features. Consistent elements including stormwater management features and tree planting will be incorporated throughout. Another important goal will be establishing a hierarchy of bike connections through Lynn Creek that will facilitate internal circulation, provide a range of commuting routes and promote the use of cycling infrastructure by a variety of different users. Bike lane configurations should be suited to the context, road configuration, availability of land and safety objectives.

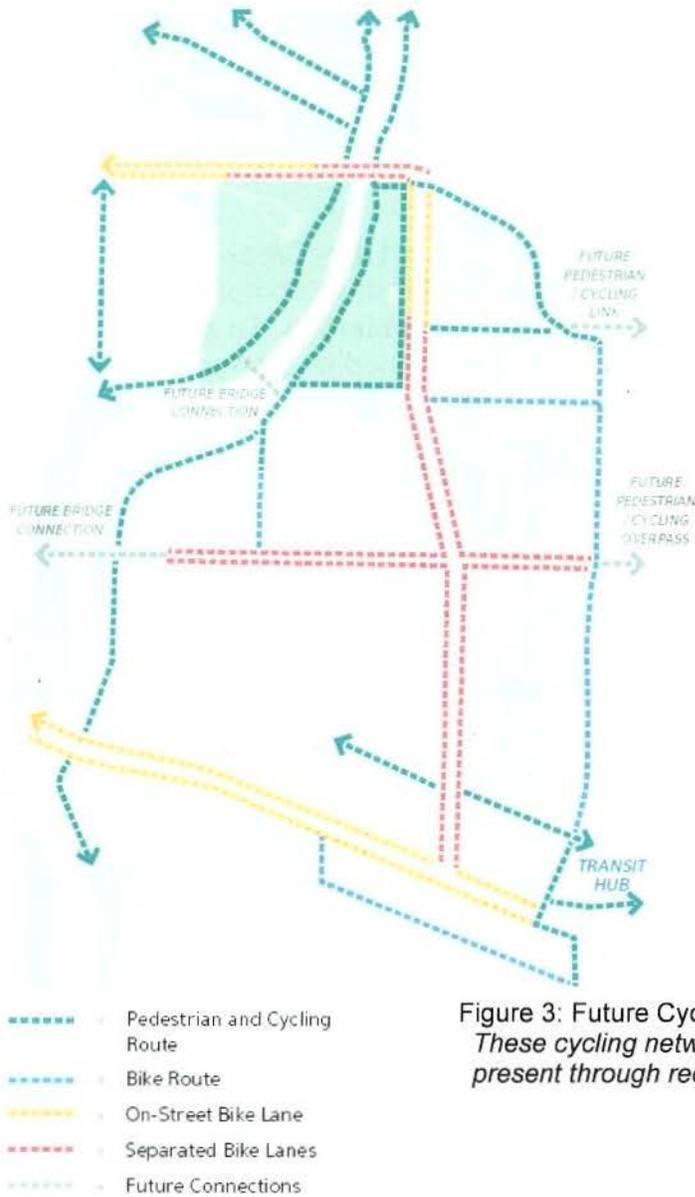


Figure 3: Future Cycling Network
These cycling networks will be achieved over time as opportunities present through redevelopment

Mountain Highway - High Street

The blocks of Mountain Highway between Fern Street and Crown Street are the heart of the Town Centre core. The core will include active street-fronting retail activity on both sides of the block and will form the "High Street" of Lynn Creek Town Centre intent. The streetscape will be characterized by large street trees on both sides as well as street trees in the median for a portion of the High Street. This will provide a buffer from vehicular traffic and a fuller, more continuous tree canopy. Grade separated bike lanes will be integrated along the length of Mountain Highway and provide easy access to arrival points along the High Street.

Crown Street

Crown Street will become an important east-west route for pedestrians and cyclists once the proposed pedestrian / cycle bridge over Lynn Creek and pedestrian overpass across Highway 1 are constructed. To meet the needs of this important pedestrian and cycling connection the street configurations will provide additional space for grade separated bike lanes in both directions.

Flood Resilience

The Lynn Creek Town Centre is located at the alluvial fan of Lynn Creek and has been identified as a Creek Hazard Development Permit Area. Site development has a key role to play in addressing flood risk and building flood resilient communities. To achieve these goals minimum flood construction levels (FCL) have been established ensuring that floor levels are raised above street level to mitigate potential flood issues.

Commercial space along the High Street requires an alternate approach to flood resiliency. To ensure universal access, storefronts should be accessed at grade from the sidewalk. In a significant departure from the previous design, the current design guidelines propose no exterior stairs within the sidewalk or setback space along commercial frontages. Instead, raised internal strategies or flood resistant construction are encouraged at the internal entrance to retail buildings to create a more desirable street condition with commercial display spaces positioned closer to the pedestrian sidewalk level. This supports a more active commercial frontage and builds a consistent neighbourhood character, while placing all damageable materials above the minimum FCL level.

Planting

The planting strategy for the Lynn Creek Town Centre will be based on a selection of resilient plant species that exemplify seasonal variations in colour. In all locations the planting strategy should focus on providing a mix of colours and seasonal variation. Groups of planting will create swathes of colour. This will create swathes of colour and the ability to feel a progression, particularly in linear conditions.

Rain gardens will play both an aesthetic and rainwater management function in the Lynn Creek Town Centre. Rain gardens should be integrated along major streets including Mountain Highway, on residential streets in curb bulges and along the length of the green spine.



Public Art

Public art can have an aesthetic and/or functional role and can be expressed in a variety of different forms. Gateways will play an important role in establishing the identity of the Lynn Creek Town Centre. Because of the town centre's unique location bounded by bridges and overpasses at all access points; gateways can establish a sense of arrival, a sequence of focal points and an overall distinct identity.

Site Furnishing and Lighting

Unique site furnishings include benches, bollards, tree grates and trench grate covers will establish a common language throughout the Town Centre. The use of robust, durable materials and the integration of stormwater management in these elements will relate closely to the “gritty” and eclectic aesthetic.

**Building Façade and Form**

The aim of the Form and Character Guidelines is to identify existing and emerging neighbourhood characteristics that are distinct and intrinsic to the Lynn Creek area and to propose design principles that reinforce this character. The character intended for Lynn Creek is derived from a combination of the natural setting, the historical context, existing built patterns (industrial and residential), a trend towards simple contemporary design, a local culture of outdoor recreation, and sustainable development of energy efficiency and rainwater management.

Timing/Approval Process:

The Lynn Creek Public Realm Strategy is needed to inform development proposals and guide the review of development applications in this centre.

Concurrence:

The Lynn Creek Public Realm Guidelines have been reviewed by inter-departmental staff in Planning, Engineering, Transportation Planning and Parks. The guidelines have also been reviewed by the Advisory Committee on Disability Issues (ACDI), the Advisory Design Panel (ADP), the Transportation Consultation Committee, and Vancouver Coastal Health.

Financial Impacts:

Infrastructure improvements will generally be funded through the development process, as and when it occurs. Operating and maintenance budgets to deliver desired service levels for some elements (e.g. furnishings, rain gardens, community facilities) will be monitored. New more sophisticated furnishings, infrastructure, plantings, pavement treatments, signs, markings, lighting, etc. constitute expanded inventory, and new and higher levels of service that will need higher budgets for operating, maintenance and eventual renewal. However, this enhanced public realm will provide value through economic activity, environmental benefits and social well-being.

Liability/Risk:

The proposed Lynn Creek Public Realm Guidelines are consistent with the OCP and are not anticipated to expose the District to any liability or risk.

Social Policy Implications:

The Public Realm Strategy identifies opportunities to enhance active transportation (walking, cycling) and foster community interaction through an integrated network of public plazas, parks and plazas. These design measures are supported by District social well-being and other policies, and by Vancouver Coastal Health objectives.

Environmental Impact:

The Public Realm Guidelines enhance ecological performance with the integration of open space, landscaped boulevards and rainwater management features including:

- Rain gardens incorporated into street design;
- Flood resilient construction practices on private property;
- Integrated stormwater management in public plazas and parks;
- Increased tree canopy throughout.

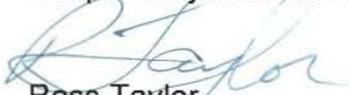
Public Input:

An estimated 45 people attended the December 3rd open house at the DNV Operations Centre. A total of 40 completed feedback forms were submitted. Of these, 69% strongly or somewhat supported the inspiration and overall design principles in the Lynn Creek Public Realm Guidelines and 71% believe that the guidelines support the vision for the town centre. A comprehensive summary of feedback is attached to this report as Attachment 2.

Conclusion:

The Lynn Creek Public Realm Guidelines provide a design framework to guide the delivery of a unified and distinct system of plazas, parks, open space and streetscapes that supports the community vision. Based on strong support expressed by the community and stakeholder groups, staff recommends that Council approve the Lynn Creek Public Realm Guidelines as a necessary tool to guide future revitalization of public spaces in Lynn Creek.

Respectfully submitted,



Ross Taylor
Community Planner



Jessie Gresley-Jones
Community Planner

Attachments:

1. Lynn Creek Public Realm Guidelines.
2. Summary and Analysis of Feedback on the Lynn Creek Public Realm Guidelines – December 3, 2015 Public Open House.

			REVIEWED WITH:			External Agencies:		
<input checked="" type="checkbox"/> Sustainable Community Dev.		_____	<input type="checkbox"/> Clerk's Office	_____	_____	<input type="checkbox"/> Library Board	_____	_____
<input type="checkbox"/> Development Services		_____	<input type="checkbox"/> Communications	_____	_____	<input type="checkbox"/> NS Health	_____	_____
<input type="checkbox"/> Utilities		_____	<input type="checkbox"/> Finance	_____	_____	<input type="checkbox"/> RCMP	_____	_____
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<input type="checkbox"/> Facilities		_____	<input type="checkbox"/> GIS	_____	_____			
<input type="checkbox"/> Human Resources		_____	<input type="checkbox"/> Real Estate	_____	_____			

LYNN CREEK PUBLIC REALM GUIDELINES



GUIDELINES FOR FORM, CHARACTER & STREETScape DESIGN



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Credits:

These design guidelines have been developed by the District of North Vancouver in collaboration with the consultant team of PFS Studio and Taylor Kurtz Architecture and Design, as informed and refined by community and stakeholder feedback.

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1 INTRODUCTION



Please note that the “Lynn Creek Town Centre” LCTC was formerly referred to as the “Lower Lynn Town Centre” LLTC.

Introduction

Centrally located at the Second Narrows bridgehead, bordering the Lynn Creek parks and trails system, and close to Phibbs Exchange, the Lynn Creek Town Centre (formerly referred to as Lower Lynn Town Centre) has excellent access to parks and community amenities, transit, jobs and the commercial/retail uses along Main Street. Over time, revitalization of this important urban centre will be guided by the community vision as outlined in the Official Community Plan and the Lower Lynn Town Centre Implementation Plan:

“Lower Lynn will be a transit-oriented mixed use community comprised of a wide range of housing types for people in all stages of life, all incomes, with accessible places of work and convenient shopping, amenities and civic uses and services. Over time, Lower Lynn will become an outstanding model of urban living in harmony with the North Shore’s natural environment”.

These Lynn Creek Town Centre Design Guidelines recognize the value of this centre as a gateway

to the District and as a model for sustainable urban living. They strive to create a distinct identity and sense of place through unified design themes that support the community vision. The design guidelines celebrate the connection to, and urban interface with the natural environment, the industrial and port land areas, alternative transportation systems, outdoor recreation and the parks and trails network.



Contextual Aerial Photo



Consultation to inform the Guidelines

The Lynn Creek Town Centre Design Guidelines has been informed by the community and stakeholder input involving a series of engagement events with local residents, business operators, members of the general public, the Advisory Committee on Disability Issues (ACDI), Vancouver Coastal Health, the Advisory Design Panel, as well as development and consulting teams.

People living and working in Lynn Creek value its natural setting of mountains, forest, and the river and the outdoor recreation activities that can be pursued in the area. The industries, both past and present, that have shaped the economy and provide workplaces are seen as important elements of the local character.

As informed by community feedback, the guidelines seek to protect and enhance the strong relationship to the Town Centre's natural surroundings and wide range of outdoor activities, industrial heritage and continuing mix of workplaces, local shopping, and residential uses of varying forms and densities.

The guidelines also seek to encourage an attractive, engaging and walkable pedestrian environment with well-designed public spaces to encourage social interaction

Purpose of the Guidelines

These Design Guidelines provide a design framework for the plazas, open spaces, parks, greenways, streetscapes and buildings within the Lynn Creek Town Centre; and are intended to guide property owners, developers, design consultants, District staff and Council towards the achievement of the community vision for Lynn Creek.

While these design guidelines seek to create a consistent look and feel for the centre; they also enable flexibility, creativity and innovation as the centre develops and grows over time. New development in Lynn Creek Town Centre is encouraged to respond creatively to the context envisioned in the Implementation Plan, while at the same time working towards a cohesive identity and character.

These Design Guidelines are applicable to the Lower Lynn Town Centre as identified in the District's Official Community Plan (2011) and the Lower Lynn Town Centre Implementation Plan (2013). The majority of the design elements apply to the higher density, mixed use core (see map) and the surrounding medium density multi-family residential areas located largely on the east side of Mountain Highway.

Very few design changes are envisioned for the industrial areas on the west side of Mountain Highway so that these areas, which will likely see very gradual change over time, can continue to retain an eclectic mix of older buildings that support established businesses.

These guidelines outline the vision for the public road system including vehicles, pedestrians and bicycles, but has not been engineered and is subject to change.

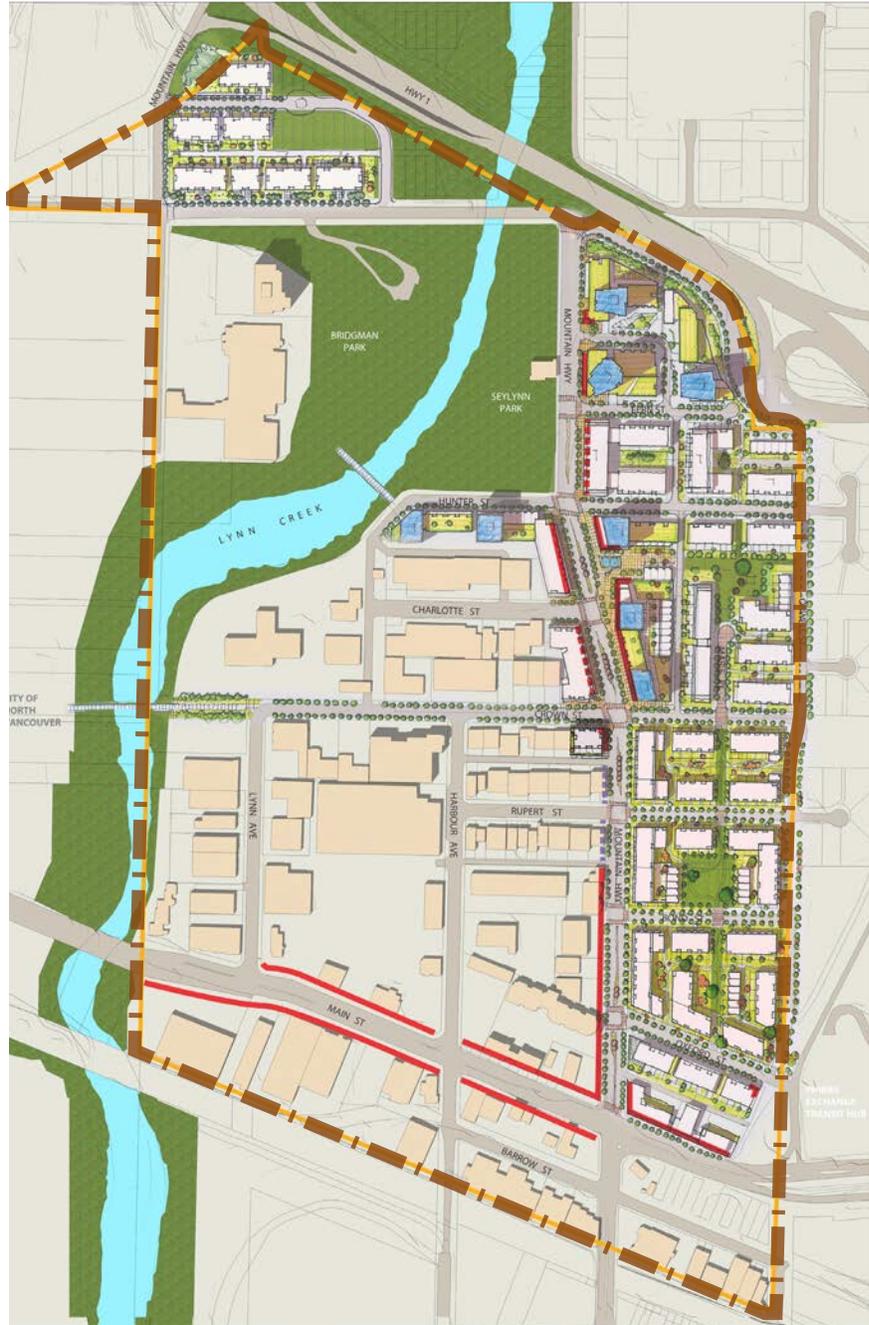


Policy Context

The policy context for the Lynn Creek Town Centre is established in the District's Official Community Plan and the Lower Lynn Town Centre Implementation Plan. Schedule B of the OCP outlines design principles and objectives to guide the form and character of all development throughout the District regardless of location. The Lynn Creek Public Realm Guidelines supplement Schedule B with additional, more detailed design direction for the public realm spaces, streetscapes and built form within the Town Centre.

Other key policies, plans and strategies that are also applicable include:

- Zoning Bylaw,
- Development Services Bylaw and other supplementary municipal standards,
- Lower Lynn Transportation Strategy,
- Lower Lynn/Lynn Creek Flood Management Strategy, and
- Parks and Open Space Strategic Plan
- Accessible Design Guidelines
- Seyllynn & Bridgman Parks Conceptual Masterplan 2015



*Town Centre Concept Plan
(From the LLTC Implementation Plan)*

 Town Centre Boundary
 Commercial / Retail Frontage

How to use this Document

This document is intended to be used in conjunction with the OCP Schedule B, the Lower Lynn Town Centre Implementation Plan, and other applicable municipal policies and regulations. They also supplement the OCP Form and Character design guidelines by providing greater detail to inform the specific identity and character for this Town Centre.

The Lynn Creek Town Centre Design Guidelines are based on

the land use concept plan as outlined in the Lower Lynn Town Centre Implementation Plan.

Importantly, this document is to be used to guide the design of public spaces and to inform the sense of place. The design guidelines enable flexibility to respond to design objectives. Additionally, design direction may be provided during the development application review process.

This guiding document is not intended to be prescriptive nor exhaustive, but rather illustrates the anticipated key design objectives for this centre. Cross sections, plans and details included within these guidelines are intended to inform further detailed design, but are not to be used as an engineering document.



Sample Images of existing context



Key Character Areas

These guidelines are intended to coordinate design of the public realm to achieve a character and sense of place that expresses the area's history and evolution, its natural setting along Lynn Creek, and intentions for its emergence as a Town Centre within the District of North Vancouver.

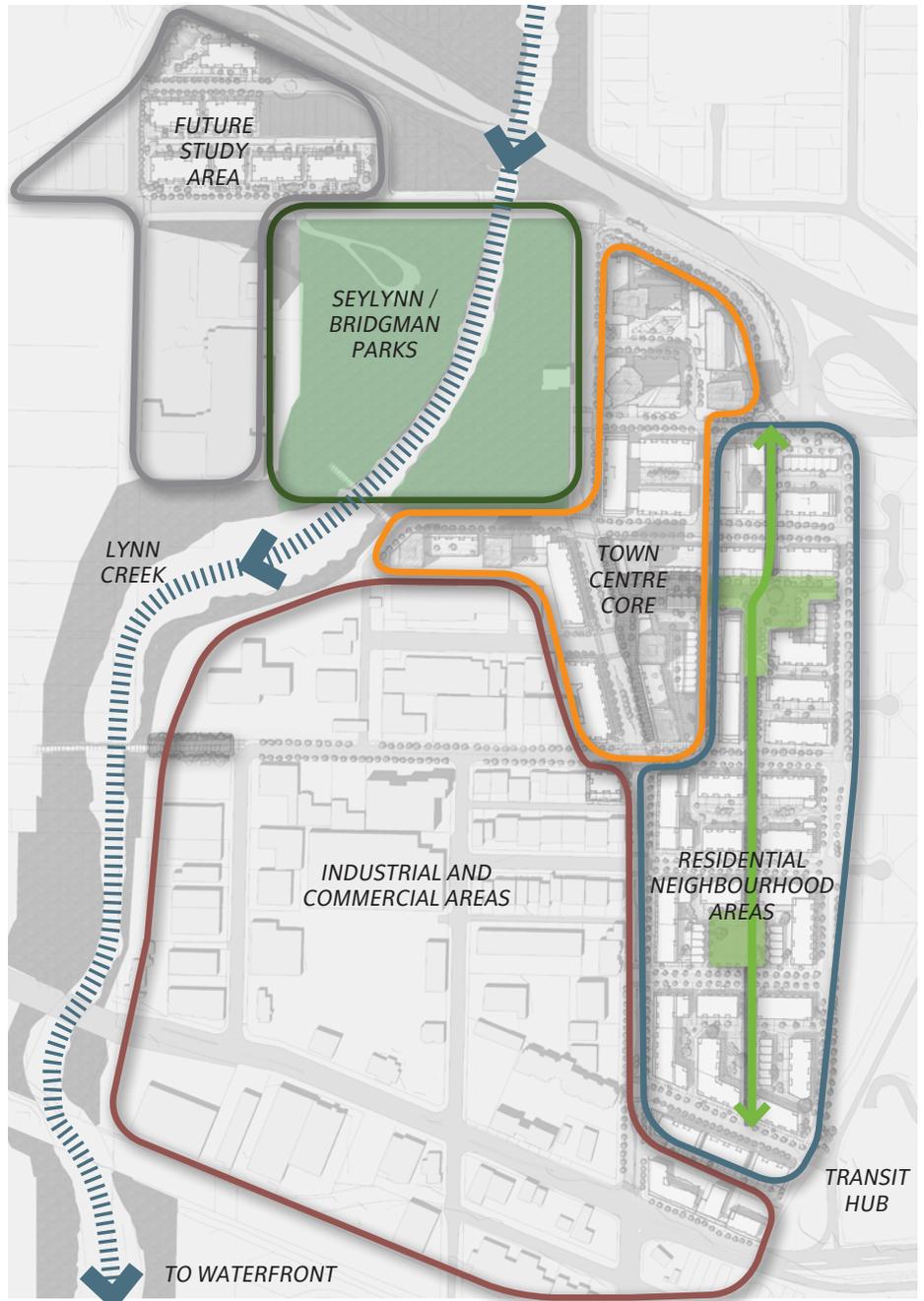
The Lynn Creek Town Centre can be divided into four distinct character areas:

- Town Centre Core
- Residential Neighbourhood
- Industrial and Commercial Areas
- Seyllynn & Bridgman Parks

Together these four areas are complemented by a series of critical public spaces, multi-modal connections and are closely tied to the transit hub at the south east corner of the Town Centre.

Town Centre Core

- The Town Centre core is focused around the intersection of Mountain Highway and Hunter Street.
- This area will be the focus of higher density mixed-use development, including high rise buildings, achieved through redevelopment of larger parcels around Mountain Highway and



Town Centre Concept ↑_N

Hunter Street and fronting Seylynn Park.

- The Town Centre core is envisioned as the heart of the community. This will include key elements for the public realm including a community centre and the central plaza. The focus in this area will be to create a lively, pedestrian oriented shopping area that supports businesses, community needs and establishes an iconic central plaza.
- Streetscapes and building guidelines seek to completely revitalize the public realm to achieve a strong pedestrian environment with wide sidewalks, street trees, weather protection, site furnishings, public art, and interesting, engaging building edges.
- Existing streetscapes will be redeveloped in step with new high density development in order to achieve an urban and high amenity public realm designed to promote pedestrian activity and comfort and to support transit use.
- Open space in the Town Centre core focuses on a combination of Seylynn Park, a green community park with

a variety of recreational and environmental amenities, and a new urban plaza and interconnecting mews, a place edged by buildings with daily and special event programming. The detailed design and programming response in these two complementary spaces should be integrated to offer Lynn Creek a wide and varied mix of public realm amenities.

Residential Neighbourhood

The Residential Neighbourhood Area constitutes the medium to low density, predominantly residential redevelopment area, generally east of Mountain Highway, this area will be designed to support incremental redevelopment around a north-south pedestrian spine on axis with Marie Place, with vehicular access to development provided from adjacent low volume streets.

The Residential Area is anticipated to redevelop through assemblies of several adjacent properties.

To facilitate redevelopment of partial block parcels and avoid awkward transitions where current uses lag in their redevelopment, existing sidewalk and curb locations are retained for many residential streetscapes. These streets typically have

low levels of traffic so there is less need to buffer pedestrians from traffic by an intervening boulevard strip as is specified for the Town Centre core.

The key public realm amenities in the Residential Area are two small neighbourhood parks linked by a pedestrian green spine. This route will offer an alternative north-south connection central to the Residential Area. One park will expand the size of Marie Place Park and the other will be newly created through redevelopment along the spine in the area between Bond and Rupert Streets.

The pedestrian spine will benefit from opportunities for visual access into adjacent semi-private open spaces with the blocks through which it passes. Guidelines ensure that the pedestrian spine is built at the elevation of the adjacent sidewalks as it crosses each block to protect universal access and to maintain open sight-lines, safety, and security.

Light Industrial/Commercial Area

The Industrial and Commercial area is located to the west of Mountain Highway and to the South of Main Street.

These lands are anticipated to undergo a slower process of



renewal over time, applying a design context that integrates with the overall character of Lynn Creek Town Centre while protecting the industrial function.

The streetscapes within the Industrial and Commercial Area are anticipated to have minor changes given the stability of the land uses in this section of the Town Centre. Where redevelopment occurs, it is more likely to be infill on one or more parcels than a full block assembly; sidewalk and underground utility corridors are therefore expected to remain generally in their current locations. This approach aims to encourage retention of existing mature trees and other landscape features that exist on a number of properties in this area, generally on private property.

Commercial and mixed-use redevelopment fronting on Main

Street will be encouraged to respond to the pattern of recent new projects and contribute to the emerging urban character of Main Street, including animated storefronts at grade and weather protection for pedestrians.

The interface with the Phibbs Transit Exchange and frequent transit services along Mountain Highway and Oxford Street will be supported and enhanced by adjacent redevelopment and streetscape improvements.

Seylynn and Bridgman Parks

Seylynn and Bridgman Parks as well as future Spirit Trail connections through the park will continue to play a critical role in the Town Centre, offering active and passive space for the community. This natural asset is closely tied to river, walking trails and further connections

throughout the Lynn Creek system including the Sea to Sky link to the Inter-River community. The proximity of these parks to the Town Centre establishes a natural connection between urban form, open spaces and the Lynn Creek system.



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2 DESIGN PRINCIPLES & INSPIRATION



Concept for Green Spine

Inspiration and Experience

Building on the community vision for the Lynn Creek Town Centre the following key ideas and inspirations for the emerging centre were derived through a multi-stakeholder Town Centre “branding” session. In Brief, future residents will experience the Lynn Creek Town Centre as a place that:

- Connects neighbourhoods to Lynn Creek and other key destinations overcoming existing barriers with pedestrian crossings and bridges.
- Is a contemporary complete community – work places, amenities, shops, key destinations and transit within walking distance from homes.
- Promotes active living –
 - moving people out of cars (reducing need for cars) and embracing active living including walking and cycling.
 - Promotes social interaction and neighbourliness – social gathering spaces and community amenities and facilities become the “outdoor living rooms” for people to meet and get to know their neighbours.
 - Embraces local business – small businesses as key to the local economy + playing an important role in energizing, animating, infusing new ideas & building excitement in this area.
 - Celebrates eclectic, random, funky, organic and somewhat unruly nature of current and future development –
 - different architectural forms, scales of development, lot sizes, types of uses etc.
- Encourages innovation and includes choices for smaller fine grain lot patterns and affordable spaces for families and workers.
- Provides flexibility and choice – flexible spaces that can be adapted to residential and business needs, flexible to provide innovation, opportunity and experimentation.
- Encourages seamless integration- Mountain Highway should connect and not “divide” the neighbourhoods and streets.

Key Images that capture the experience of Lynn Creek Town Centre



Key words that capture the experience of Lynn Creek Town Centre

LYNN CREEK OUTDOOR LIVING + SOCIAL SPACES
 CONNECTION TO NATURE **CONNECTED**
 CRAFT BEER **SMALL LOCAL BUSINESSES**
 ECLECTIC, RANDOM, MESSY, ORGANIC SUSTAINABLE LIVING
SHOPS **WALKING + CYCLING** **CHOICES**
 PUBLIC ART RESTAURANTS COOL, FUNKY, CREATIVE, INNOVATION
TRANSIT **COMMUNITY CENTRE**
SEYLYNN + BRIDGMAN PARKS **BRIDGES**
FARMERS MARKET **TREES** **MOUNTAINS** **TRAILS**

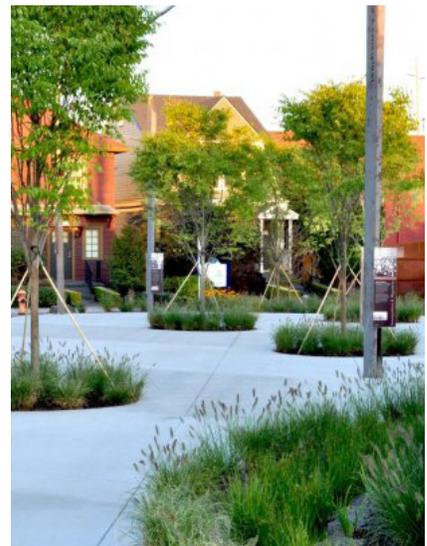


Overall Design Principles

The inspiration for the Town Centre will be expressed through a number of urban design elements to create a cohesive, distinct identity and character for this area.

Redevelopment of the Lynn Creek Town Centre is to be guided by the following overarching planning and design principles:

- Creative, contemporary, functional design;
- Facilitate well-designed, engaging and functional public spaces and streetscapes;
- Apply an appropriate design response to the climate and geography of the North Shore;
- Celebrate connection to Lynn Creek and incorporate best practices in sustainable design;
- Explore creative ways to celebrate and connect to the industrial lands;
- Enhance the public realm for the enjoyment and safety of pedestrians, cyclists, and transit users;
- Connect the Town Centre through priority pedestrian and cycling networks to support a walkable community;
- Well-connected green network emphasizing the linear north-south pedestrian green spine, pedestrian bridge over the creek, Hunter Street and Phibbs Exchange greenways;
- Embedded use of sustainable best practices for stormwater management, energy efficiency and water conservation;
- Support for full tree growth and an increased tree canopy;
- Use of vibrant colour accents to create liveliness and a material palette that complements connection to Lynn Creek, parks, and adjacency to industrial lands;
- Modular stepping both in heights and setbacks from the street to achieve articulated forms and simple geometries;
- Showcase landmark crossroads and gateways through urban design and public art;
- Create opportunities to celebrate public art, music and recreation.



Unifying Design Materials

Sample Colours



A range of unifying design elements are intended to be expressed throughout the Town Centre as a means to establish its identity as a distinct and special place.

The materials and colours should reflect the surrounding natural environment while highlighting the unique industrial and commercial uses situated within the town centre. Wood, steel and concrete will form the basis for the material palette with variations in the types and

uses of steel as a reflection of creative design processes and the variability of the product.

Some design cues may be influenced by the simplicity and materiality of the industrial context eg. use of Corten steel can be used in key public spaces. This material highlights the intersection of nature and industry with its weathered, changing appearance.

Glass, metal, concrete, rough stone, rock and wood may be

used as a highlight material, demarcating softer more pedestrian focused elements such as benches, seating elements and patio spaces.

Colour can be used to highlight focal points or elements of significance within the public realm. Art work may integrate colour and act to draw the attention to significant intersections or junctures.





Green Spine and a Connected Open Space Network

The Lynn Creek Town Centre borders Lynn Creek which includes an extensive system of trails and open spaces. The existing Seylynn and Bridgman parks are valued community assets providing passive and active open space for residents.

Redevelopment of the Town Centre, will enable an improved and expanded green network to enhance pedestrian movements and to create a hierarchy of open spaces.

Critical to this network is the green spine or pedestrian focused linear park which will

run north south the length of the Town Centre.

Along this spine, stormwater management will be evident and integrated into the design. Places to stop, interact and observe will be continuous along the spine.

This green spine will also serve to connect two urban parks: Marie Place Park and a new neighbourhood park to the south. These two parks will offer more urban programmed open spaces, to complement Seylynn and Bridgman parks, establishing a hierarchy that will meet a wide range of needs. Widened,

planted sidewalks and multi-modal connections will knit the neighbourhood together from east to west.

The combination of new park spaces and a linear green spine will serve to fully connect the neighbourhood, provide substantial planting, a network of stormwater management opportunities and a parallel pedestrian network .



Priority Multi-Modal Connections

The Lynn Creek Town Centre is a community that has historically been isolated by major roads, highways, railway and by Lynn Creek. Creating a walkable compact community with opportunities for all modes of movement will help to overcome these barriers while still facilitating the industrial nature and heavy vehicle uses. Multi-modal connections will be integrated throughout the Town Centre and will focus on connecting key destinations including:

- Businesses in the Town Centre core and the industrial lands
- New Community Centre
- Phibbs Exchange Transit Hub
- Seylynn & Bridgman Parks + Lynn Creek trails

Transit

Phibbs Exchange offers close proximity to rapid transit with connections across Metro Vancouver. This asset allows for a more pedestrian-focused, transit-oriented town centre.



Walking

A system of greenways, trails and sidewalks will promote pedestrian movements throughout the Town Centre. The "green spine" will form the primary north south connection. This spine will be supported by east-west sidewalk connections and trails. Wide sidewalks will be provided on key streets to try to provide safe and enjoyable walking conditions.

Cycling

The design guidelines propose a hierarchy of cycling facilities ranging from sharrows and conventional bike lanes to grade separated dedicated bike lanes to accommodate the needs of all users.

Where feasible, busy routes and routes adjacent to heavy traffic should be physically separated to encourage use and support cyclists of all abilities. Intersection design incorporated into priority intersections can provide efficient crossing configurations.





Functional Stormwater Management

The Lynn Creek Town Centre is located at the delta of Lynn Creek forming a low flats where the creek meets the ocean. Rain events are particularly evident here with the swell and crest of the creek and the tidal flux of the ocean. Coarse gravel soils allow for stormwater infiltration into the delta and the management of rainwater offers an opportunity to respect the delta environment while improving the quality of runoff from urban environments.

Redevelopment of the Town Centre presents an opportunity to provide functional stormwater management practices to ensure development is respectful and responsive to its environment.

Taking cues from natural

systems and incorporating green infrastructure and Best Management Practices, stormwater networks will provide a dual function within the Town Centre. They will create an aesthetic legibility of water, of the vulnerability of the natural systems and the connection to the delta. They will also provide a functional way to clean, filter and improve the quality of runoff from the urban environment and recharge the aquifer and relieve pressure from existing drainage infrastructure. Ideal soil conditions will make infiltration a practical solution to be pursued whenever possible.

Rain-gardens may be integrated along urban and residential

streets, filtering stormwater and infiltrating water into the delta soils. Linear trench drains and other conveyance systems will act to move stormwater through the expanded green network and green infrastructure will be utilized whenever possible.

Potential for flooding will be an ever present aspect of the Lynn Creek and the delta location. Innovative flood management solutions must be considered with each new development. Raised floor levels will achieve one component of flood protection, but innovation should be encouraged to identify additional solutions and opportunities.



Healthy Tree Growth and Canopy

A healthy urban tree canopy provides numerous benefits including:

- Evapotranspiration and cooling
- Visual screening and sound attenuation
- Delineation of spaces
- Colour and seasonal variation
- Sense of tranquility and connection to nature
- Improvements to air quality

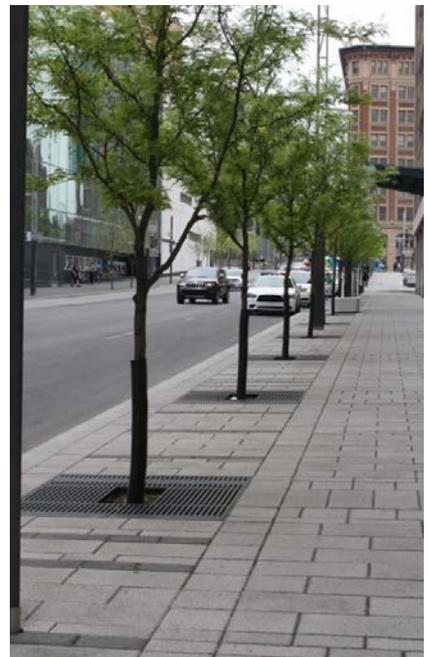
Urban trees and increased canopy will also knit together the Town Centre creating green corridors linking the urban areas to the creek and the park systems.

To ensure that trees are able to grow to full maturity, increasing soil volumes and reducing soil compaction is important. These design guidelines propose a new system of sidewalk installation and linear tree trenches that increase soil volumes and reduces soil compaction to enable trees to grow to full maturity. Increased soil volumes also facilitate stormwater management and rainwater capture.

Tree grates are to be used in high traffic areas to ensure protection of root systems. Tree grates and tree grate extensions will maximize the area created where rain can reach tree roots directly

from the surface. Long and, where possible, continuous, linked tree grates are recommended to maximize water infiltration and oxygenation of tree roots.

Residential areas, where pedestrian traffic is lower, open tree planting with native grasses and understory planting should be used. The tree planting areas should be planted with soft landscape of low shrubs, perennials, and groundcovers rather than tree grates for increased soil volumes and better rainwater access as well as aesthetic benefits of increased green landscaping.





3 PARKS, PLAZAS & OPEN SPACE



General Open Space Guidelines

The proposed open space network for Lynn Creek Town Centre is a series of parks, urban plazas, and natural areas that together provide a range of passive and active recreational opportunities, outdoor experiences, and other amenities connected by trails, multi-use paths, and greenways for the enjoyment of residents, employees, and visitors. Connections between spaces and features for wayfinding and crossing of intervening streets are key design elements of the network.

Design guidelines for these public realm areas are intended to guide redevelopment and redesign to reflect and reinforce the emerging identity and character for the Lynn Creek Town Centre, to strengthen wayfinding, to promote accessibility, and to enhance pedestrian safety.

- ① Park Plaza
- ② Town Centre Plaza
- ③ Interconnecting Mews
- ④ Green Spine
- ⑤ Potential Orwell Pedestrian Zone
- ⑥ Marie Place Park
- ⑦ Neighbourhood Park

- Pedestrian & Cycling Connections
- Future Trail Greenway Connections
- Urban Plazas & Generous Sidewalks
- Pedestrian Green Spine
- Neighbourhood Parks



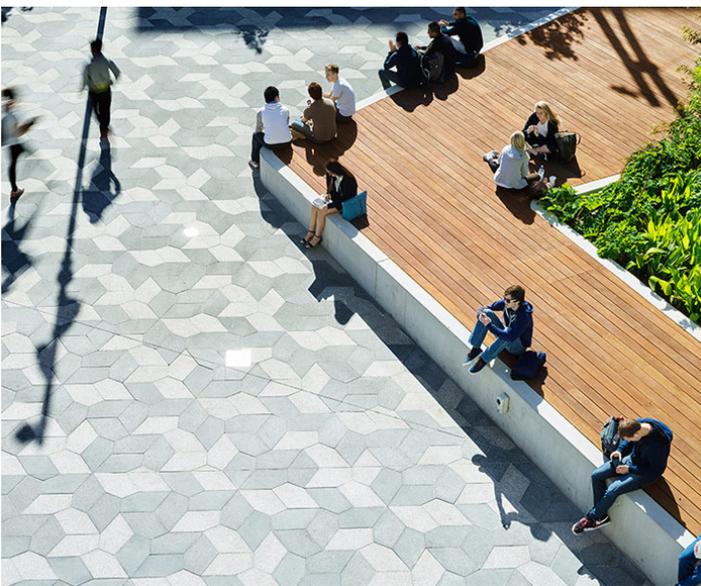
Open Space Network

Exact boundaries of the proposed new neighbourhood park to be determined through property assembly and development application process

Town Centre Plaza and Interconnecting Mews

The Town Centre plaza on the east side of Mountain Highway is intended to be a focus of both daily use for sitting, outdoor seating, and people watching as well as a programmable space that can host community events. It will be designed to accommodate a wide variety of users, and promote accessibility and inclusivity. Design principles for the plaza are as follows:

- Promote direct engagement from retail and restaurant uses at its north and south edges
- Ensure direct at-grade access to the plaza and to retail fronting the plaza to promote accessibility for all users
- Plan circulation routes to allow the areas adjacent to buildings to be used for outdoor seating and displays of merchandise without compromising movement of pedestrians
- Incorporate ample seating with variety in groupings, ways to sit, and choice between sun and shade
- Encourage paving patterns and areas of special paving materials coordinated with the design intent and movement patterns
- Integrate into the design of the plaza visible features that collect stormwater from paved surfaces
- Provide services to support special events and use by food trucks including electrical outlets, water, recycling bins, furnishings to support temporary equipment like speakers or lighting, adaptable and programmable lighting, etc.
- Include public art, both as free-standing installations curated through the District's public art program and as integrated pieces or as part of stormwater features
- Enhance connectivity through the inclusion of interconnecting mid-block pedestrian mews with appropriate landscaping and lighting





Guidelines for the Central Plaza include:

- Lighting standards (see appendix) should be selected from the approved chart of lighting fixtures, but can include unique lighting elements further integrated into the design.
- Paving should integrate with the approved paving pattern for sidewalks along streets. Feature areas with other paving surfaces may be incorporated through the design process in consultation with District staff.
- Stormwater management features should be incorporated into the plaza design including both rain garden areas and recessed trenches with cast iron grates over them to capture water from public areas.
- Numerous seating opportunities should be integrated into the design including seating for groups. Movable seating and tables are encouraged within the plaza.
- Trees should be part of the overall design concept and strategically located to provide shade, and to soften the urban context.
- Public art should be integrated into the public plaza design.



Marie Place Neighbourhood Park

Marie Place Park is currently a neighbourhood pocket park with a children's playground. The Implementation Plan envisions expanding the size of this park and enhancing facilities suited to its role as a locally serving neighbourhood park.

Marie Place park can become an enhanced neighbourhood park that transitions from the hard-surface urban and pedestrian mews to the adjacent residential community. A mix of hard and soft surfaces will extend the durability of the space and accommodate a variety of activities.

New park amenities should be sited in relation to an unobstructed movement route through the park that respects the flow of pedestrians between the Town Centre Plaza and the north-south pedestrian spine that has its north end within Marie Place Park.

The park design should consider opportunities for expanded children's play, gathering and seating areas, and public art. These elements should be integrated within the design rather than stand alone elements.





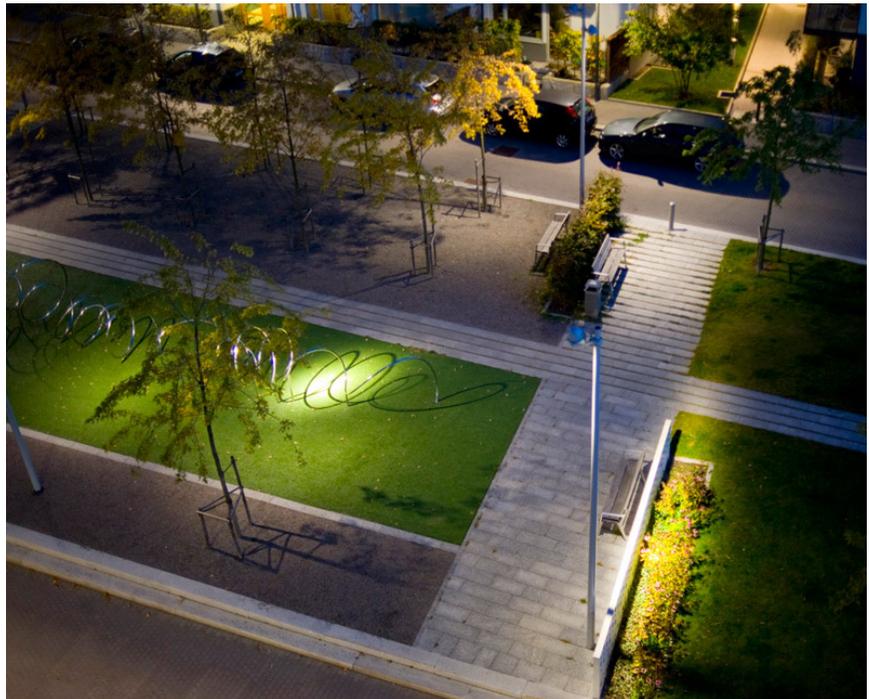
Mid-Block Pedestrian Spine and New Local Neighbourhood Park

The pedestrian green "spine" will run north south from Fern Street to Oxford Street and will be a significant new linear park in the Lynn Creek Town Centre. This aligned pathway will be more than simply a pedestrian trail. It is envisioned as a continuous park providing connectivity with seating, and opportunities for play with stormwater elements integrated along its length.

A new neighbourhood park created along the spine and between Rupert and Bond Streets, will provide a more substantive area for play and act as a node along this linear park.

In order to achieve the desired connectivity, sightlines and openness for this pedestrian spine, the design of each surrounding block of residential development should be guided by the following:

- The entire length of the pedestrian spine should be accessible and delivered at the same grade as connecting sidewalks.
- The spine should result in a direct connection on axis with Marie Place.
- The paving, lighting fixtures, and wayfinding elements of the path should be consistent along the entire length of the spine.



- The spine should be no less than 14m in width, comprised of 10m dedicated green spine and 4m landscaped setback and should also include a minimum 4m continuous unobstructed pedestrian path.
- Surface stormwater features should be incorporated to collect rainwater from paved surfaces on District land and should create a continuous expression of water movement along the spine.
- Spaces for sitting, watching, and relaxing should be positioned along the spine to create areas to stop, rest or gather within each block.
- Where the spine crosses east-west streets, the crossing should be marked by a planting bulge incorporating stormwater raingardens in place of on-street parking.





Seylynn & Bridgman Parks

The Seylynn and Bridgman Parks Conceptual Park Master Plan (April 2015) will guide the revitalization of Seylynn and Bridgman Parks by recommending improvements to the natural environment and park amenities to support existing and future recreation activities. This plan should be referenced for the design of Seylynn and Bridgman Parks. Key design objectives from this plan include:

- Provide innovative design features and elements inspired by the dynamic interface between the spectacular natural landscapes and the vibrant urban environment.
- Increase access and connectivity within, between and to the parks, with consideration for active transportation, universal design, parking, and service access.
- Improve opportunities for recreation, play, art, cultural events, and gathering.
- Improve safety and security.
- Increase opportunities for nature appreciation, interpretation, education and stewardship.
- Provide the opportunity to enjoy Lynn Creek while protecting sensitive

ecosystem values.

- Provide opportunities for dog use that respect the ecological sensitivity.
- Protect and enhance creek, riparian, and forest habitats.

Environmental Management

- Rainwater management features should be installed where necessary to minimise stormwater flows in sensitive areas.

Recreation Amenities

- A new hierarchy of connecting paths including a Spirit Trail connection should be built in the parks, including the upgrading and resurfacing of existing paths.
- Open grass areas should be provided for informal gatherings and unprogrammed uses.

- An event space should be provided in Seylynn Park to accommodate community gatherings, which will include an open plaza with event structure.
- A field suitable for diverse activities, including sports, special events and informal play will be provided in Seylynn Park

Urban Interface

- The transition from natural park to urban centre should be accomplished through tree-lined promenades along Hunter Street and Mountain Highway.
- An entrance plaza to Seylynn Park should act as a gateway from the urban centre.



Semi Private Open Spaces

The three blocks between Crown and Oxford Streets are planned for multi-family housing and will be also structured by the north-south green spine. Access to underground parking in these blocks should be provided directly from adjacent low-traffic local streets (Crown, Rupert, Bond, and Orwell Streets) in order to protect opportunities for both public movement and open space along the north-south spine and for pedestrian-only semi-private open space courtyards for surrounding residents.

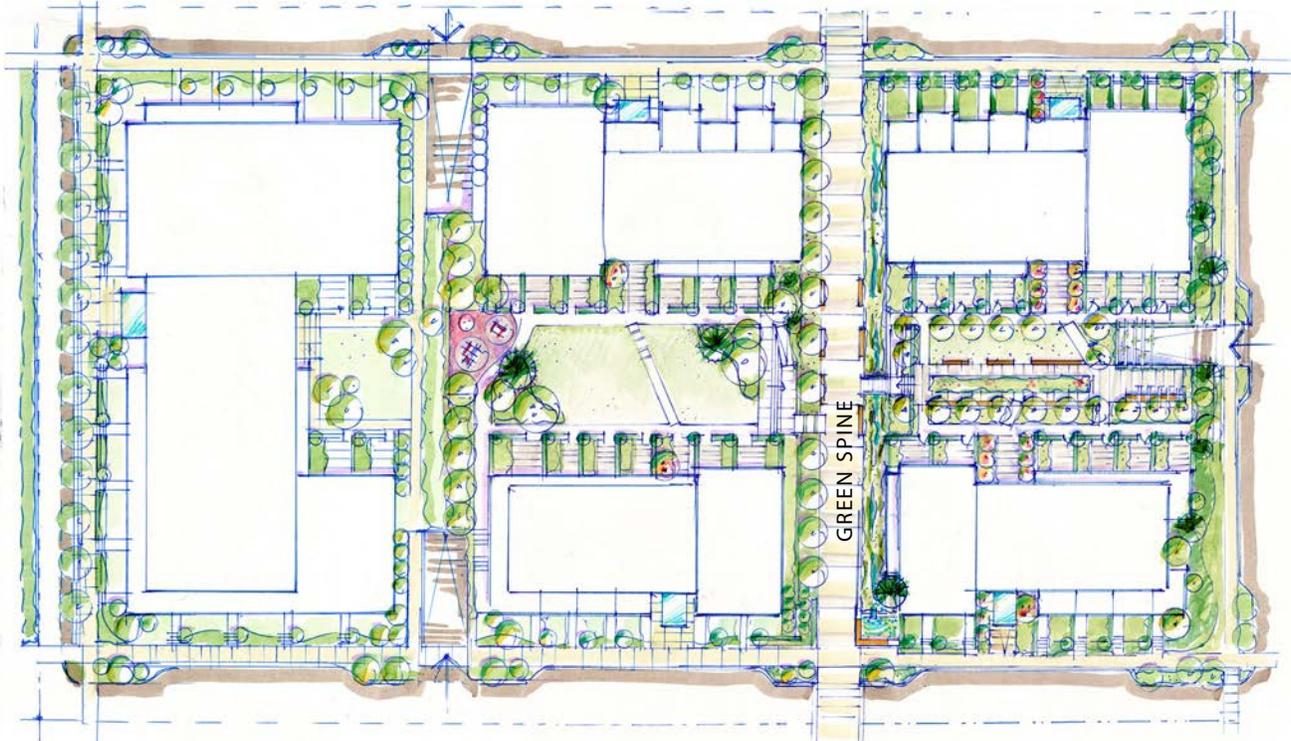
The design of semi private courtyards should integrate the themes and material treatments of the overall public realm. Consistency in material, planting, lighting and form will ensure a cohesive residential area. Subtle visual cues to users of adjacent public paces should indicate that semi private courtyards are not fully intended for public use.

Design elements to achieve this distinction may include: a change in elevation above the adjacent spine with ramps and stairs, low

walls and landscaping.

Semi-private courtyards should be designed for the use and enjoyment of residents with amenities for children's play, social gathering and seating. Urban gardening opportunities should also be included where possible.

For ground oriented units, a minimum 9 square metres per unit should be provided. These may take the form of patios, balconies, or rooftop decks and gardens.



Typical block in Residential Area with pedestrian Green Spine and semi-private courtyards



4 STREETS



Street Network

This section describes the character of the key centre streets including:

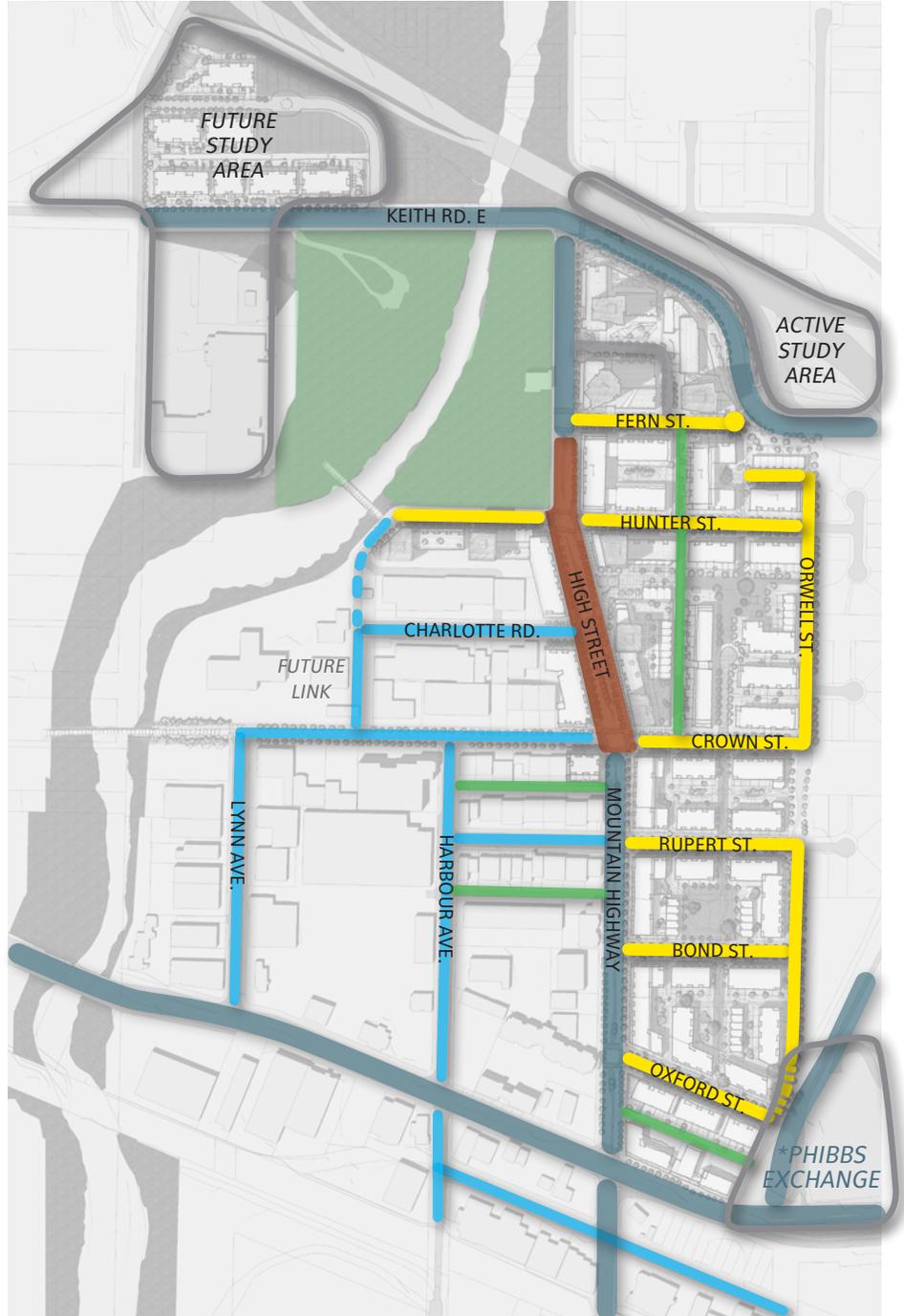
1. Mountain Highway
2. "High Street"
3. Hunter Street
4. Crown Street
5. Orwell Street
6. Residential Streets
7. Industrial Streets
8. Laneways

Each street type offers unique elements relating to traffic volumes, adjacent building use and natural features. Consistent elements including stormwater management features and tree planting will be incorporated throughout. All streets are subject to engineering and design.

Detailed plans for Main St. and Keith Rd. are excluded due to the variability of street type or active redevelopment associated with ongoing design processes.

The locations of new roads and lanes are approximate and may be adjusted during the municipal development application review process depending on timing of development, land assemblies and other factors.

-  » Major Road
-  » High Street (Local retail)
-  » Local Residential Streets
-  » Non Residential Streets
-  » Lane
-  » To be determined through development process



Hierarchy of Streets

* Phibbs Exchange and adjacent connections are under review as part of the Ongoing Phibbs Exchange Design



Cycling Network

Establishing a hierarchy of bike connections through Lynn Creek will facilitate internal circulation, provide a range of commuting routes and promote the use of cycling infrastructure by a variety of different users. Bike lane configurations should be suited to the context, road configuration, availability of land and safety objectives.

Pedestrian and Cycling Route, including greenways are to be off-road shared environments between cyclists and pedestrians with ample space to accommodate mixing of all user types.

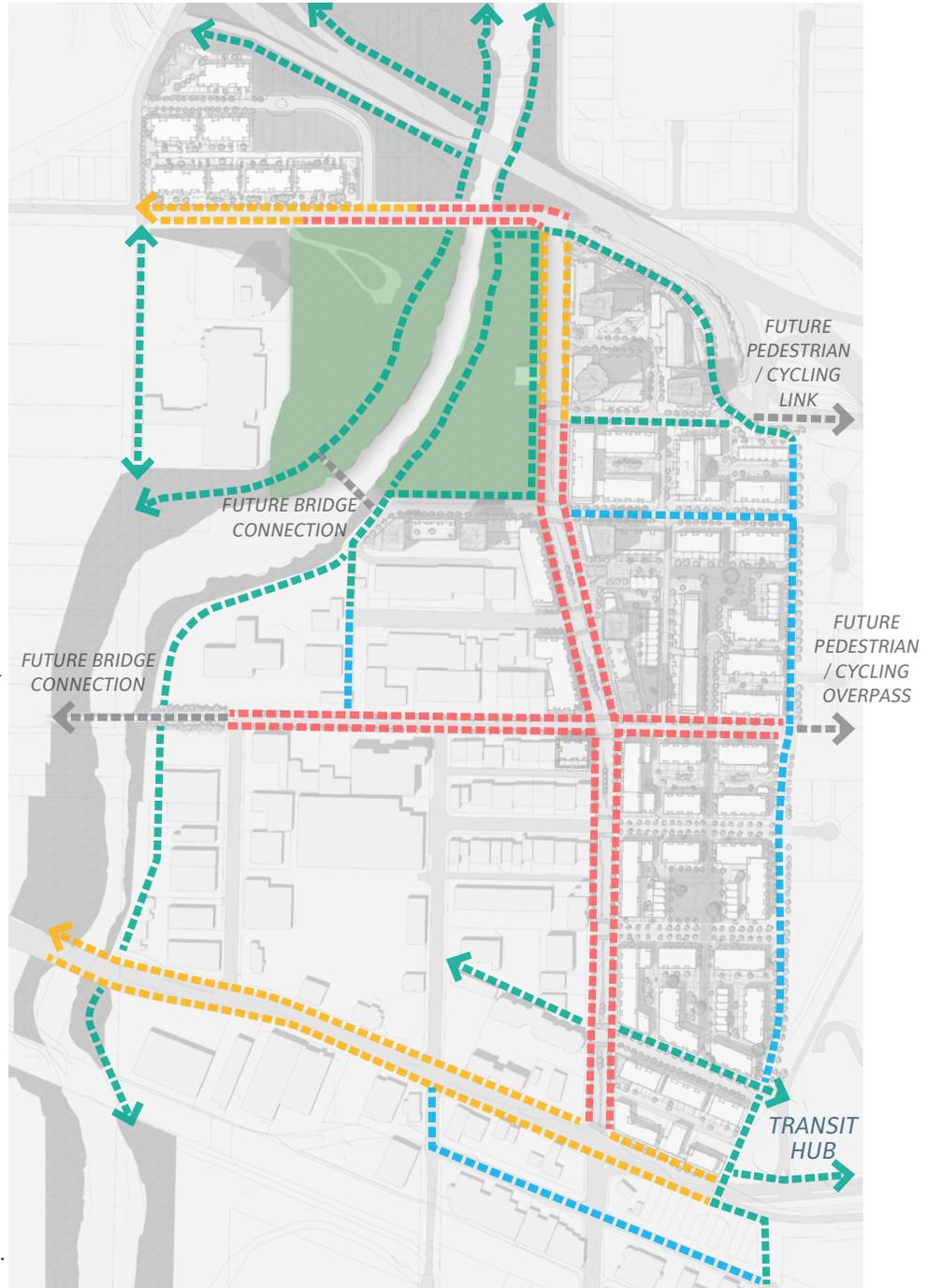
Bike Routes are signed connections or may include sharrow road marking that indicate a shared lane environment for cyclists and automobiles and are located on low-volume roads.

On-Street Bike Lanes offer greater separation of modes with the delineation of spaces with painted lines primarily located on higher volume roads.

Separated Bike Lanes offer safety improvements by physically separating cyclists from automobile traffic. The use of concrete medians and signage can be used to improve safety along higher volume roads and make cycling lanes viable for users of varying ability.

Other unmarked roads are acceptable for bike movements but will not include signage or markings.

- » Pedestrian and Cycling Route
- » Bike Route
- » On-Street Bike Lane
- » Separated Bike Lanes
- » Future Connections



Bike Network

Bike facilities are subject to change through the development process and are anticipated to be achieved over time as opportunities arise.



Mountain Highway - High Street

Major Road Network

The blocks of Mountain Highway between Fern Street and Crown Street are the heart of the Town Centre core. The core will include active street-fronting retail activity on both sides of the block and will form the "High Street" of LCTC intent. The streetscape will be characterized by large street trees on both sides as well as street trees in the median for a portion of the High Street. This will provide a buffer from vehicular traffic and a fuller, more continuous tree canopy.

A wider sidewalk is necessary to accommodate higher volumes of pedestrians accessing the commercial opportunities along the "High Street". Sidewalks should be a minimum of 3.0m (inclusive of tree grate), with a minimum 2.5m setback to building face to provide ample seating and opportunities for movement. This setback should be consistent in material treatment with the sidewalk to create a uniform surface from curb to building face.

To reflect the industrial aesthetic of the adjacent land uses and to provide as much usable pedestrian space as possible, cast metal tree grates should be used along the High Street in place of planted boulevards. This low maintenance solution will facilitate pedestrian access along the length of the High Street.

Tree grate openings should be 1.5m square, though extensions are recommended to offer street

trees access to rainwater. Where several street trees are located in a row that is uninterrupted by other streetscape features, then the tree grates can be connected into a continuous area of tree grate.

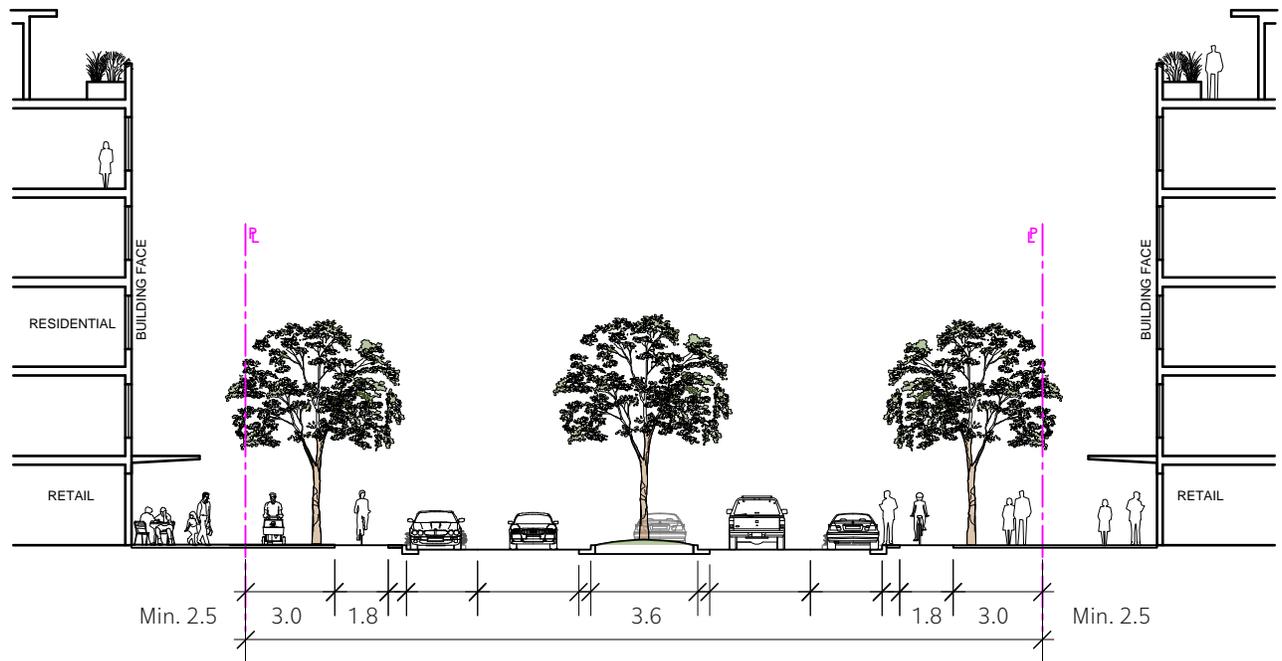
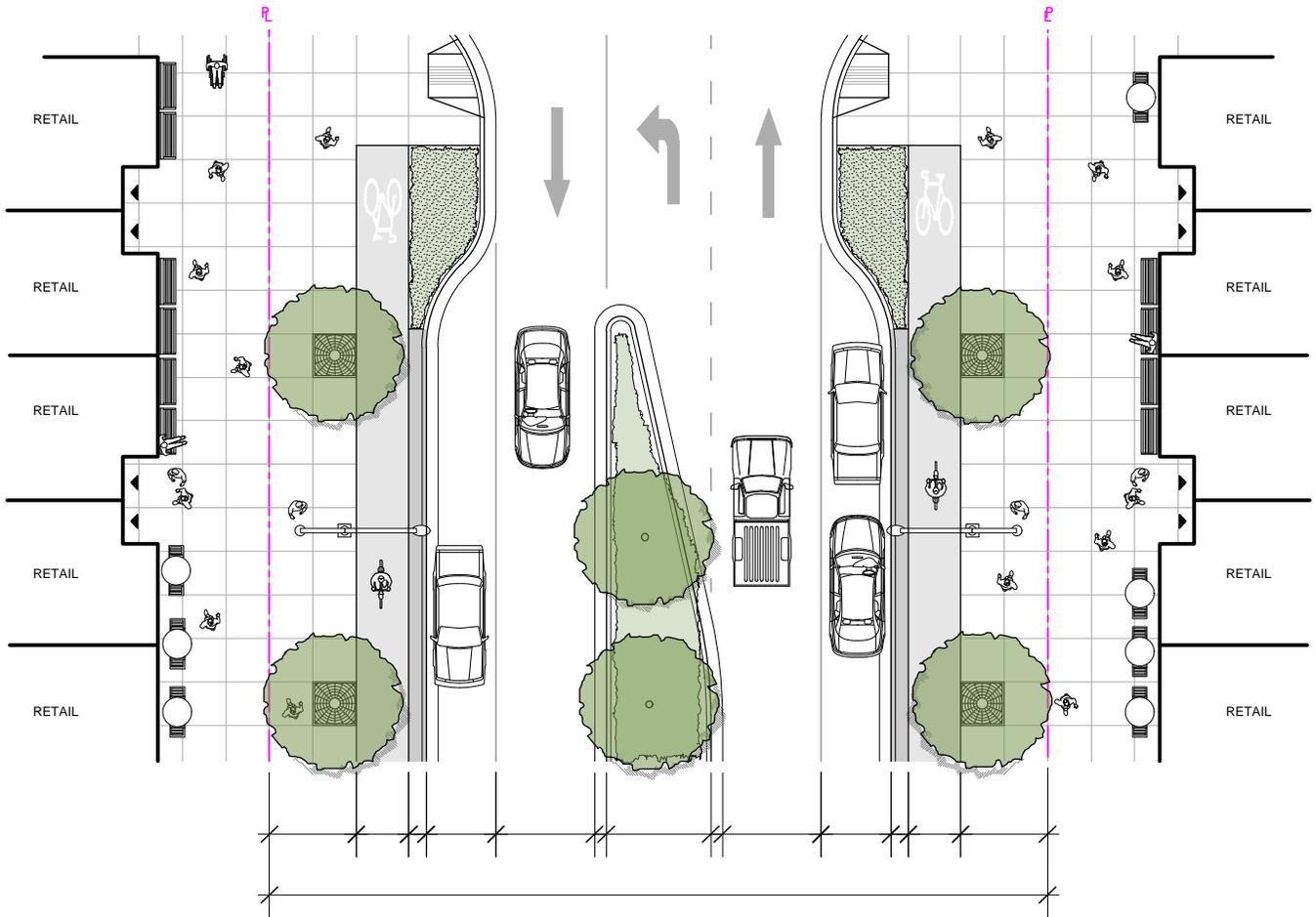
Separated bike lanes should be included the length of Mountain Highway at grade with the sidewalk, buffered from traffic and parked cars by a wide median. This will accommodate the arrival of cyclists to the retail heart of the community. Trees, street furnishing, and lighting will be located between the bike lanes and the sidewalk and act as a buffer between modes.

All storefronts along the High Street should be at grade with the sidewalk to provide universal accessibility, an active street wall and a positive relationship between shops and cafes and the sidewalk environment. Continuous weather protection should be provided over the sidewalk adjacent to the building edge.

To address flood concerns businesses will be required to provide flood-proofing methods outlined in Section 8 - Flood Resilience.

This section of Mountain Highway may have vehicular parking on both sides to ensure access to retail and pick-up / drop-off opportunities. Transit stops will also be provided at key locations to serve the retail heart.





Proposed High Street Section of Mountain Highway

** Trees may be located in centre median where sufficient width is provided for maintenance vehicle access*





Mountain Highway - Crown Street to Main Street

Major Road Network

South of Crown Street, Mountain Highway will be fronted by new medium-density residential development on its east side. The existing industrial / service / commercial uses are expected to remain on the west side for the foreseeable future. All redevelopment on Mountain Highway should be upgraded to reflect the same character and materials of the High Street. Mountain Highway will have wider sidewalks and a full tree canopy to highlight it as a significant North-South connection for vehicles, cyclists and pedestrians. The use of continuous soil trenches will allow for full tree growth contributing to the vision for this to be an iconic street.

A minimum clear 2.0m sidewalk will allow for pedestrian movement. Trees should be integrated into tree grates or boulevard plantings of 1.5m minimum width. A more substantial vegetated setback will be provided between public and private space.

Continuous building frontages on Mountain Highway are encouraged and buildings should properly address the street with units facing mountain highway to provide eyes on the street and a positive relationship with the public realm. In instances where North-South consolidations are not possible, setbacks between buildings should be reduced by

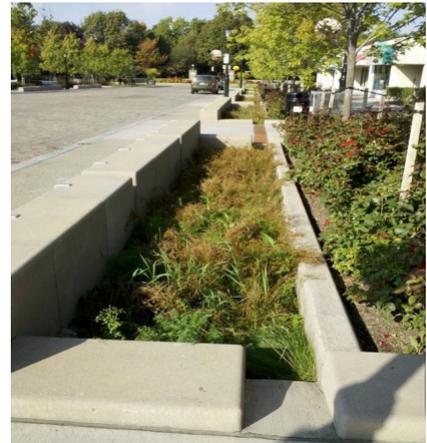
off-setting corner units. This will reduce the visual gaps along the street wall.

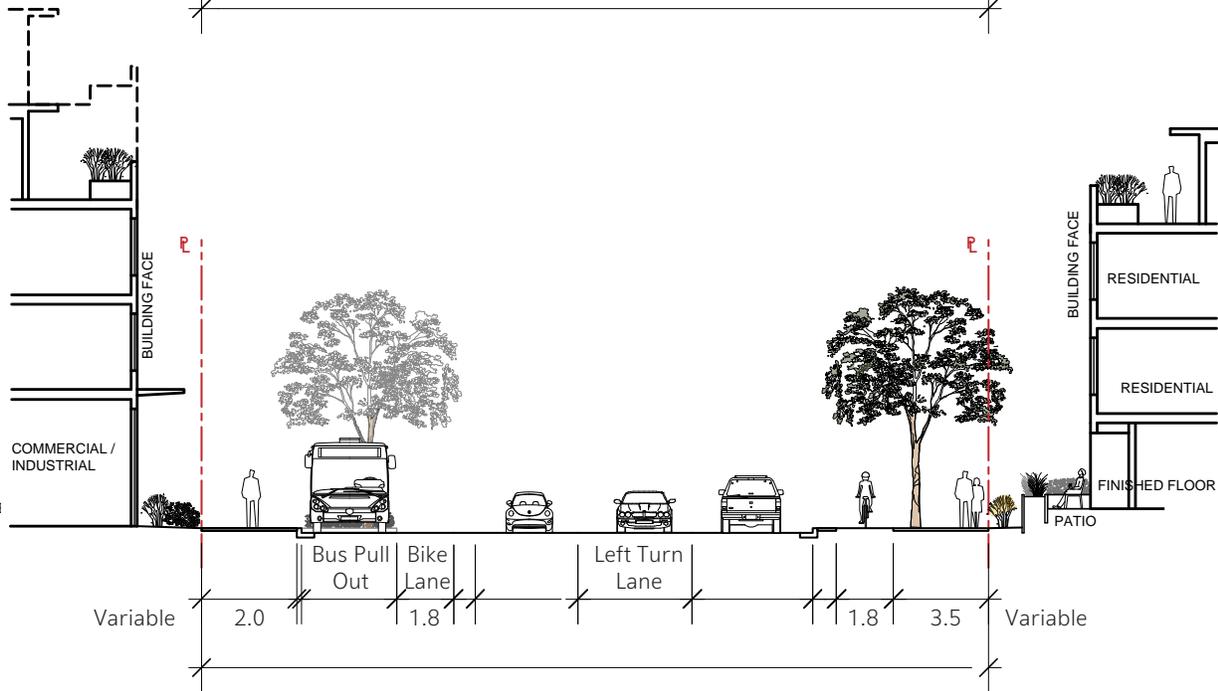
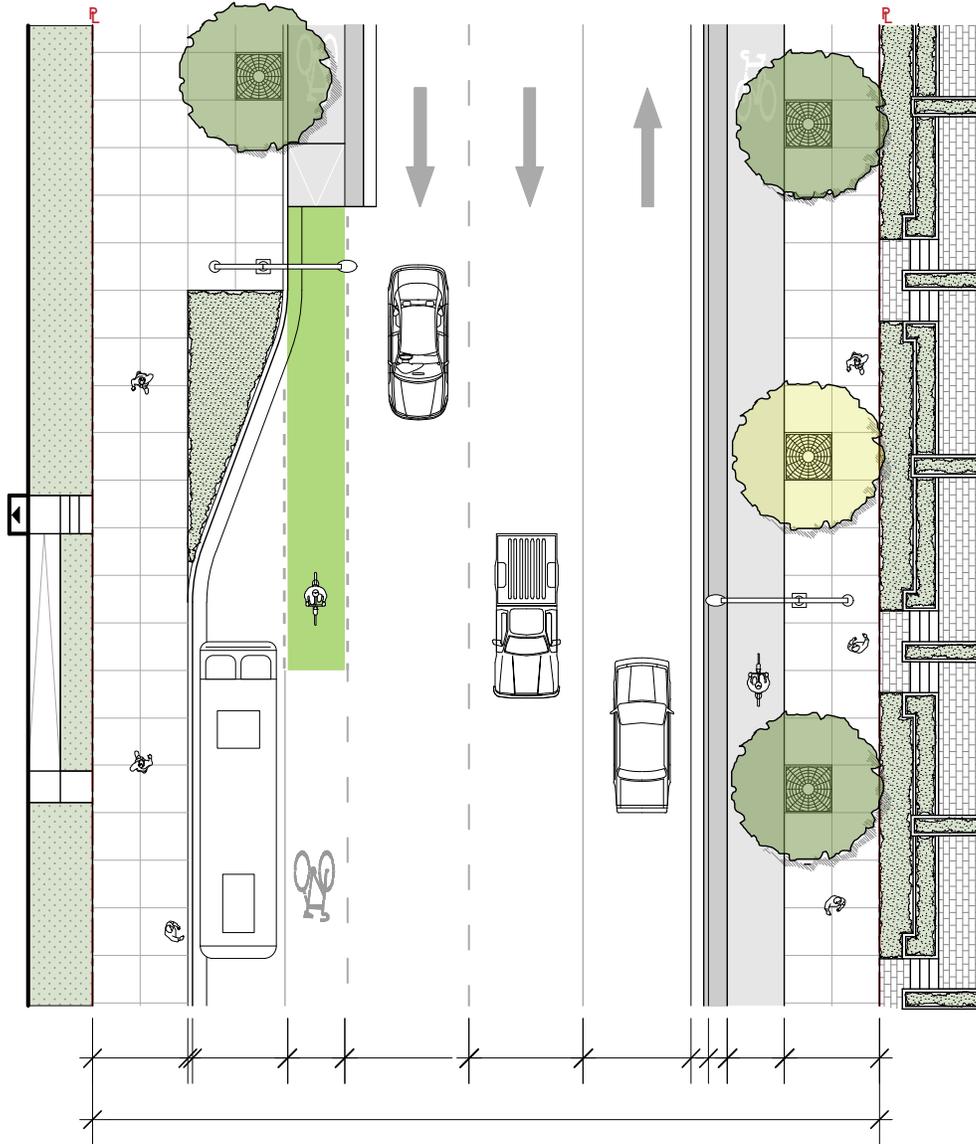
All access to underground parking should be from lanes or flanking streets to avoid vehicles crossing the sidewalks on Mountain Highway.

A continuous full canopy of street trees will be a critical element of Mountain Highway. Trees should be integrated into tree grates where pedestrian traffic is high, but can include boulevard, understory planting where appropriate. Raingardens should be accommodated at all corner bulges and wherever functional opportunities arise and space is available.

Bike lanes will be grade separated and protected from traffic by a wide median. Where space is constrained at bus stops, curb let downs and painted markings will accommodate through bike connections. If space permits, grade separated bike lanes should be accommodated behind bus stops.

Redevelopment of Mountain highway will require a phased strategy to implement the ultimate design.





Proposed Mountain Highway Section South of Crown Street



Crown Street Multi-Use Facility

Crown Street will become an important east-west route for pedestrians and cyclists once the proposed pedestrian / cycle bridge over Lynn Creek and pedestrian overpass across Highway 1 are constructed. Crown Street is a preferred priority bike route. To meet the needs of this important pedestrian and cycling connection the street configurations will provide additional space for grade separated bike lanes in both directions.

East of Mountain Highway a wide median should be provided as a buffer from travel lanes and parking, and street trees, lighting and furnishing will act as a buffer between pedestrians and cyclists. East of Mountain Highway, Crown Street terminates at Orwell Street and will have lower traffic volumes.

Street trees should be integrated along the length of Crown Street providing a full canopy and residential scale to the street. Raingardens should be integrated at all curb bulges and where space is suitable.

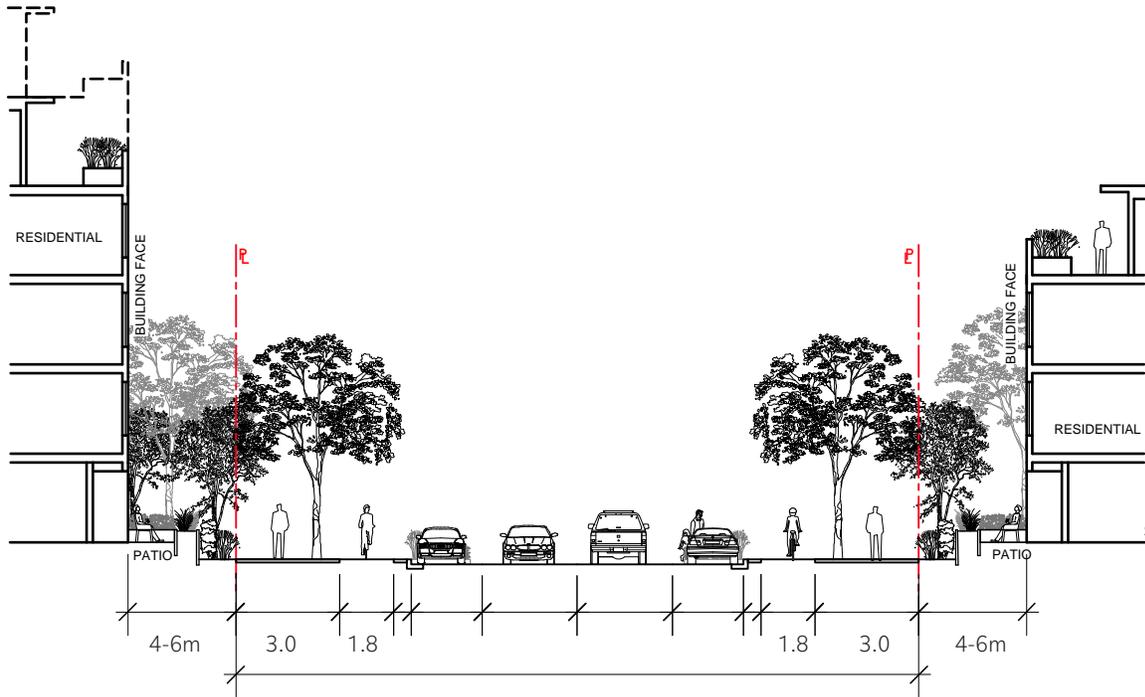
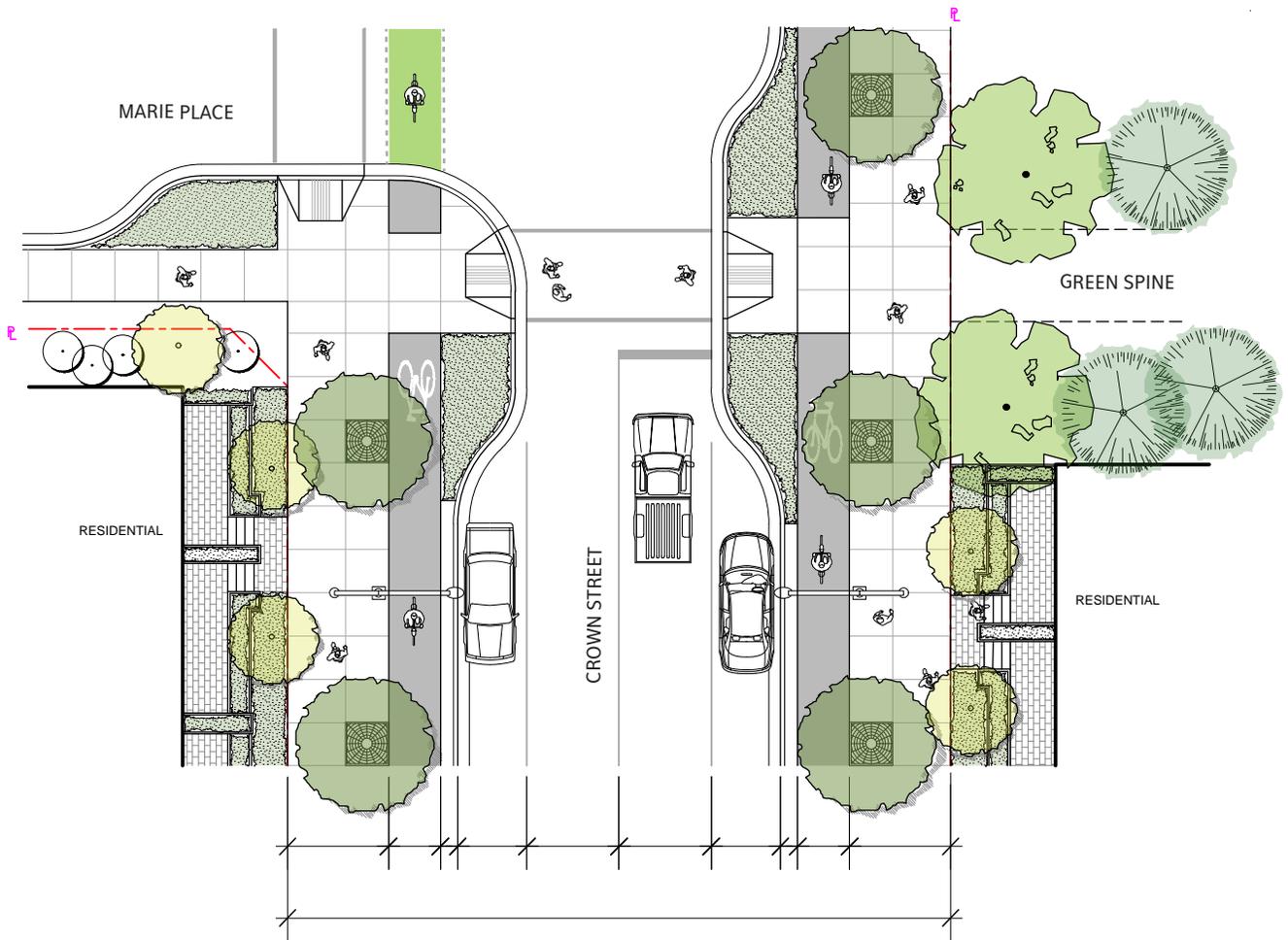
Planted setbacks should be provided between the back of sidewalk and private residential space. Grade changes should be integrated into this planted buffer.

West of Mountain Highway, Crown Street serves traffic traveling to

and from a variety of shopping and workplace destinations. This area is anticipated to develop at a slower rate and a consistent street treatment should be implemented over time. Interim options for better pedestrian and cycling connections should be explored when possible, and may include painted bike lanes, or temporary barriers if necessary. Limiting the number of access points through the redevelopment process should be an important consideration to improve pedestrian and cycling environments.



Possible Interim Solutions West of Mountain Highway



Proposed Crown Street - East of Mountain Highway





Hunter Street

Neighbourhood Street

Hunter Street forms an important east-west connection, connecting residents from the Town Centre core to Seylynn + Bridgman Parks, the proposed new community centre and Lynn Creek. This street will be similar in configuration to other residential streets. However it will transition from tree grates and wider sidewalks at the heart to boulevard understory planting in more residential areas to the east. This transition will move from hardscape to soft and reflect the intensity of pedestrian use moving away from the heart of the community.

To the west of Mountain Highway, Hunter Street will accommodate mixed use development on the south side, and provide parking for Seylynn Park to the north side. This segment of the street will be significantly traffic calmed and serve local access only. Parking along the north edge should serve the needs of park users.

A multi-use trail will run along the south edge of the park from the future Hunter Street pedestrian bridge to Mountain Highway as identified in the Seylynn Bridgman Park Master Plan.

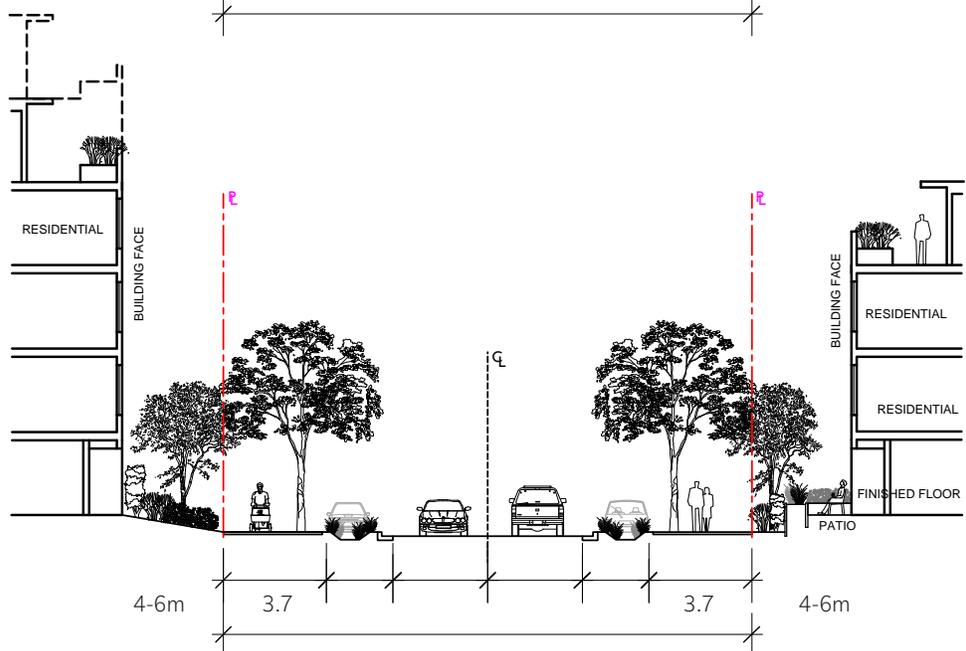
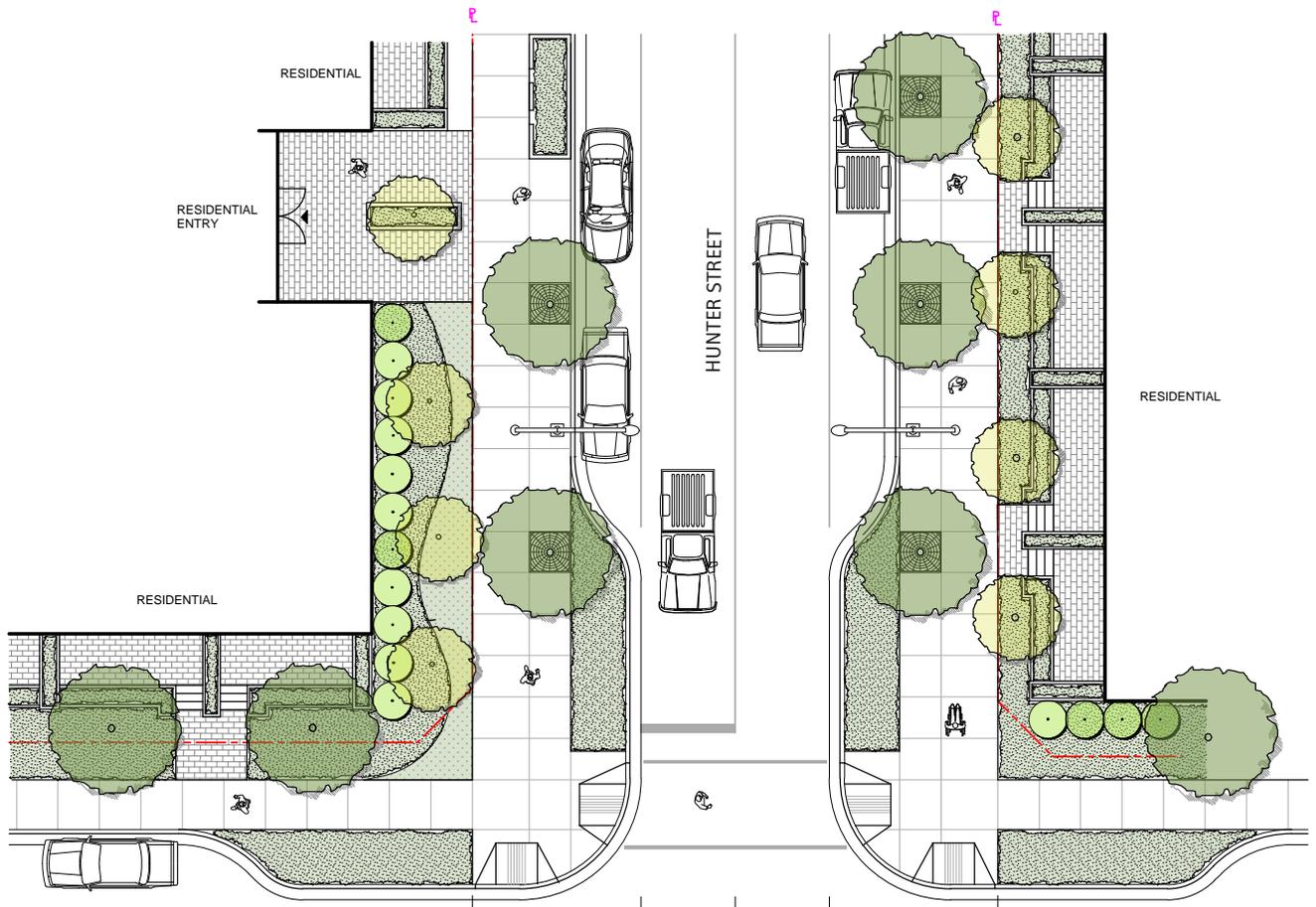
East of Mountain Highway to the service lane, Hunter Street should provide ample pedestrian space, and a transition from retail / commercial towards a more residential function, but

which could also accommodate street festivals and community farmers markets. Street trees will be a consistent element along the length of the street providing a significant canopy and buffer between residents and street traffic.

East of the commercial serving lane Hunter Street will be residential in nature and should include raingardens in corner bulges, or in other areas with sufficient space. Trees can be integrated into boulevard planting to soften the streetscape, or include tree grates in locations with higher volumes of pedestrian traffic or at key crossing points.

The character of this street should be slow and provide a safe condition for cyclists.





Proposed Hunter Street - East of Mountain Highway and the Town Centre Core



Orwell Street Neighbourhood Street

Orwell Street is similar in configuration on its west side to other residential streets. However, to accommodate the existing side yard condition along the east side of Orwell Street, a sidewalk will be established flanked by boulevards and street trees on both sides. This double row of trees will create an allée condition and provide a significant north south pedestrian link and buffer existing residents to the east from new development.

On the west side of Orwell Street, new developments should provide a streetscape treatment similar to that of other neighbourhood local streets. Street trees will be integrated into a planted median to provide a soft landscape treatment. Rain gardens should be integrated into corner bulges or other appropriate locations.

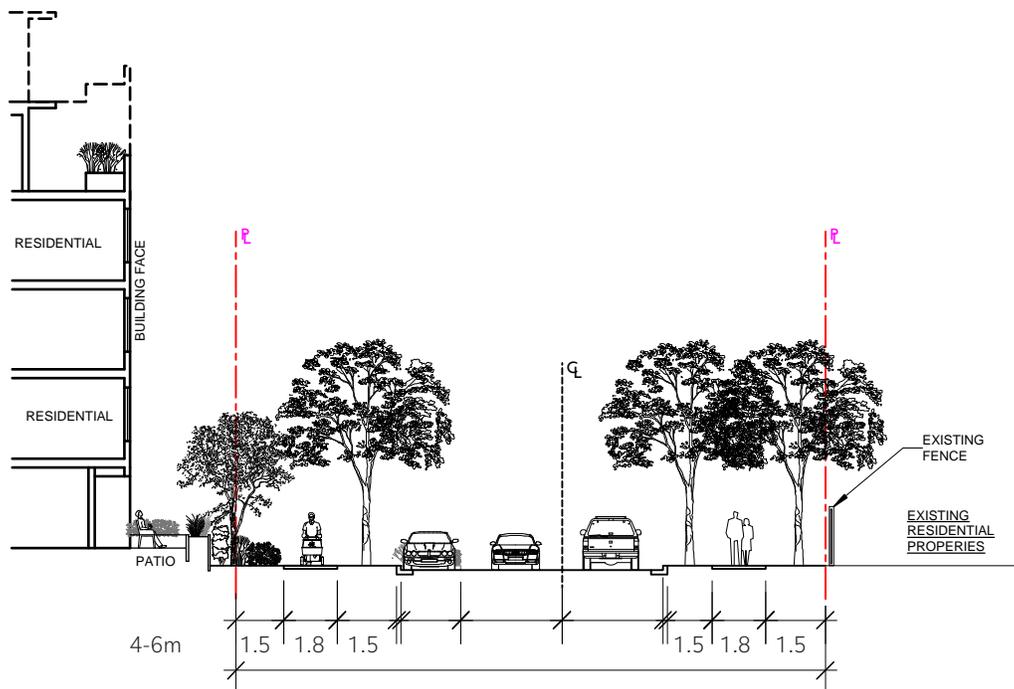
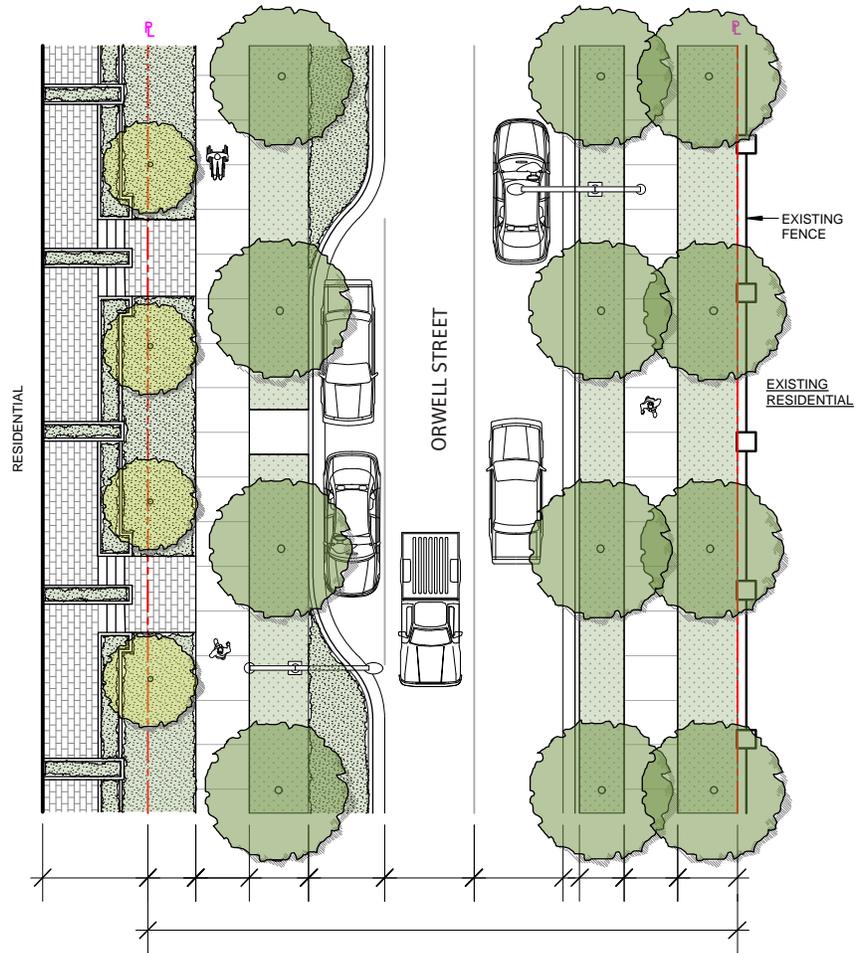
Orwell Street is part of the cycling network for LCTC, planned as an on-street cycling route leading to the multi-use pathway on Oxford Street, to Phibbs Exchange and planned future links to the Ironworker Memorial Second Narrows Bridge and Main Street.

Orwell Street will provide a quiet neighbourhood bikeway that will be suitable for cyclists of all ages and abilities. To ensure traffic is slowed, corner bulges should be considered. An unopen road

right-of-way may be integrated along a segment of Orwell street to encourage slower vehicular travel and a focus on providing a pedestrian and cycling friendly route. This segment could form a pocket plaza connection and reduce through traffic volumes along the length of the street.



Example of Pocket Plaza creating a break in vehicle travel



Proposed Orwell Street





Oxford Street

Neighbourhood Street

Oxford Street is an important transit connection from Mountain Highway to Phibbs Exchange and accommodates more traffic than the other local east-west streets in Lynn Creek Town Centre. This street is also an important pedestrian and cycling connection, linking the Phibbs transit hub to Mountain Highway. As a result, this street will have increased space allotted to pedestrians and cyclists as well as wider travel lanes if necessary to accommodate vehicles and transit.

The preferred design solution is an asymmetrical streetscape. The south side of the street will have on-street parking and street trees in a boulevard strip. With bus traffic, the street trees and setback to a new sidewalk will provide a buffer for pedestrians. The travel lanes will be widened from the current width of 3.0 meters to more safely accommodate frequent transit services.

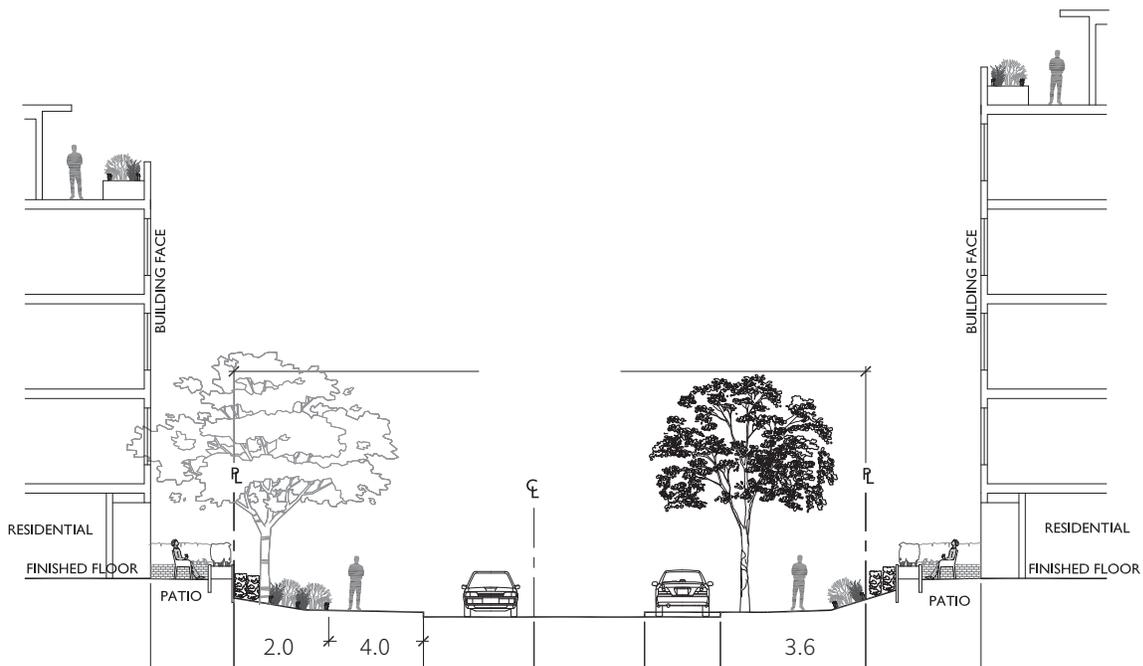
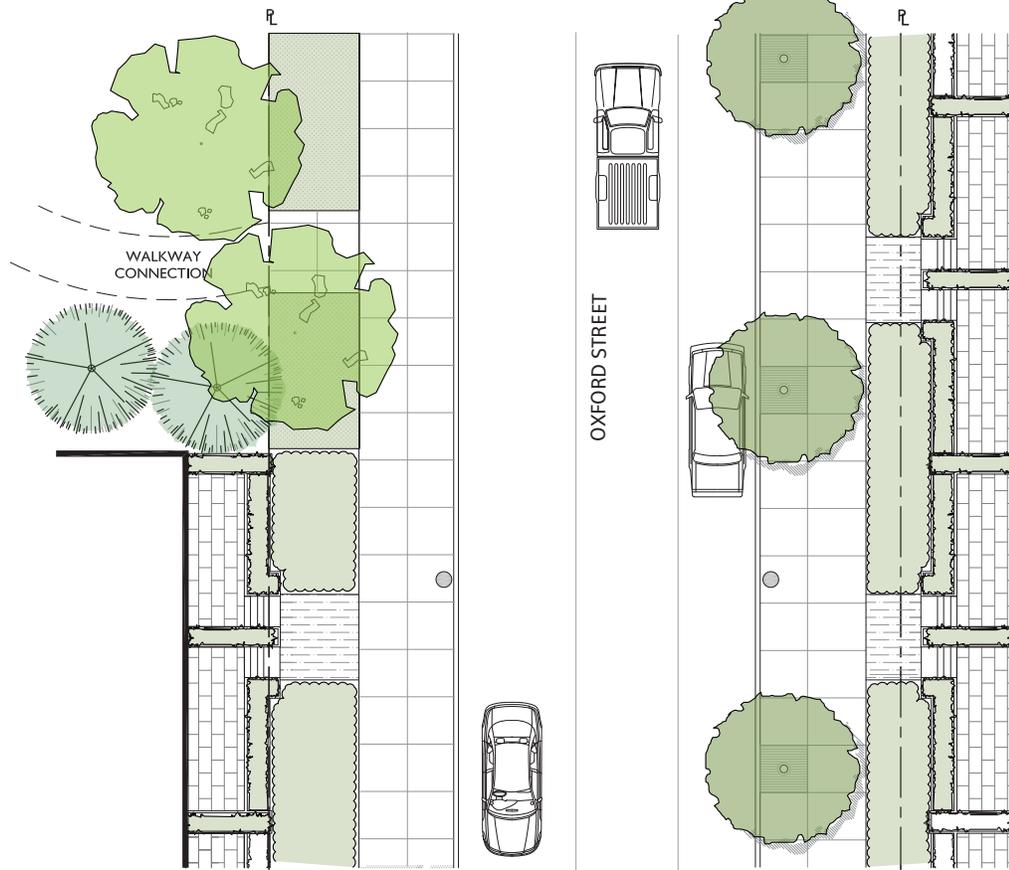
On the north side of Oxford Street, the intent is to establish a 4.0 meter wide multi-use pathway serving pedestrian and cyclist travel in both directions and paved in asphalt or concrete.

This will be an important link from the transit exchange towards the Town Centre core. Setbacks should provide substantial planted space to buffer residential development from traffic.

The intersection of Mountain Highway and Oxford Street will include separated pedestrian and cycling movements to ensure increased safety, and efficient connections from the north and south bike lanes on Mountain Highway.

Consideration should be given to activating the ground-levels at the intersection of Orwell and Oxford streets that might serve transit users or cyclists at the south end of the Orwell cycling route.





Proposed Oxford Street





Typical Residential Local Street

The goal of other quieter residential streets is to provide a well canopied streetscape with inviting sidewalks on both sides of the street. Providing a balance of space for pedestrians, cyclists, parking and travel lanes is important and will meet the needs of residents in the area. To meet sustainability goals, rain-gardens can be integrated at corner bulges and understory planting should be included where possible. This will reduce stormwater runoff from hard surfaces and provide a lush, softer green condition along the length of the street.

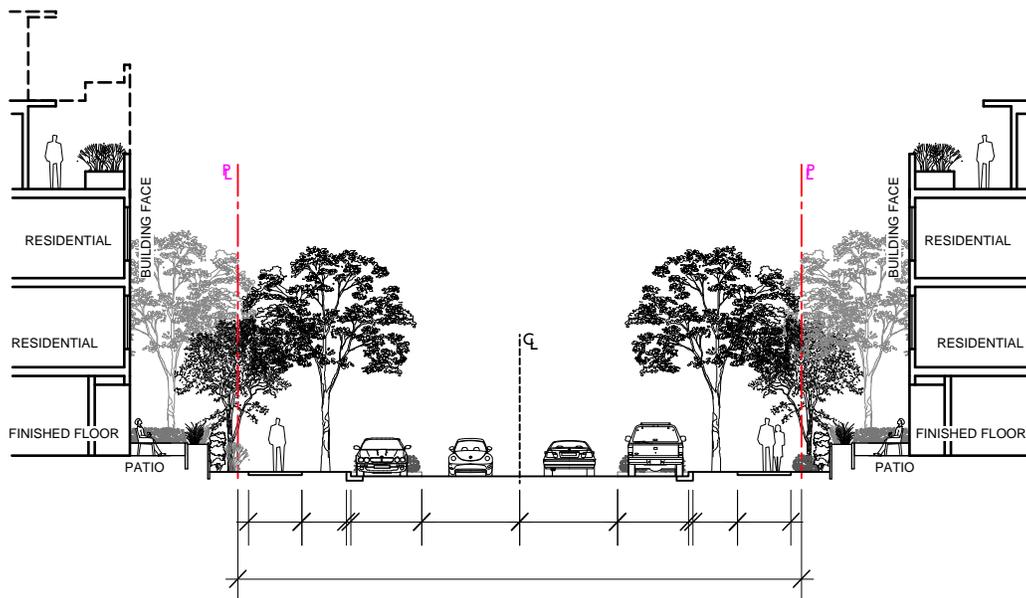
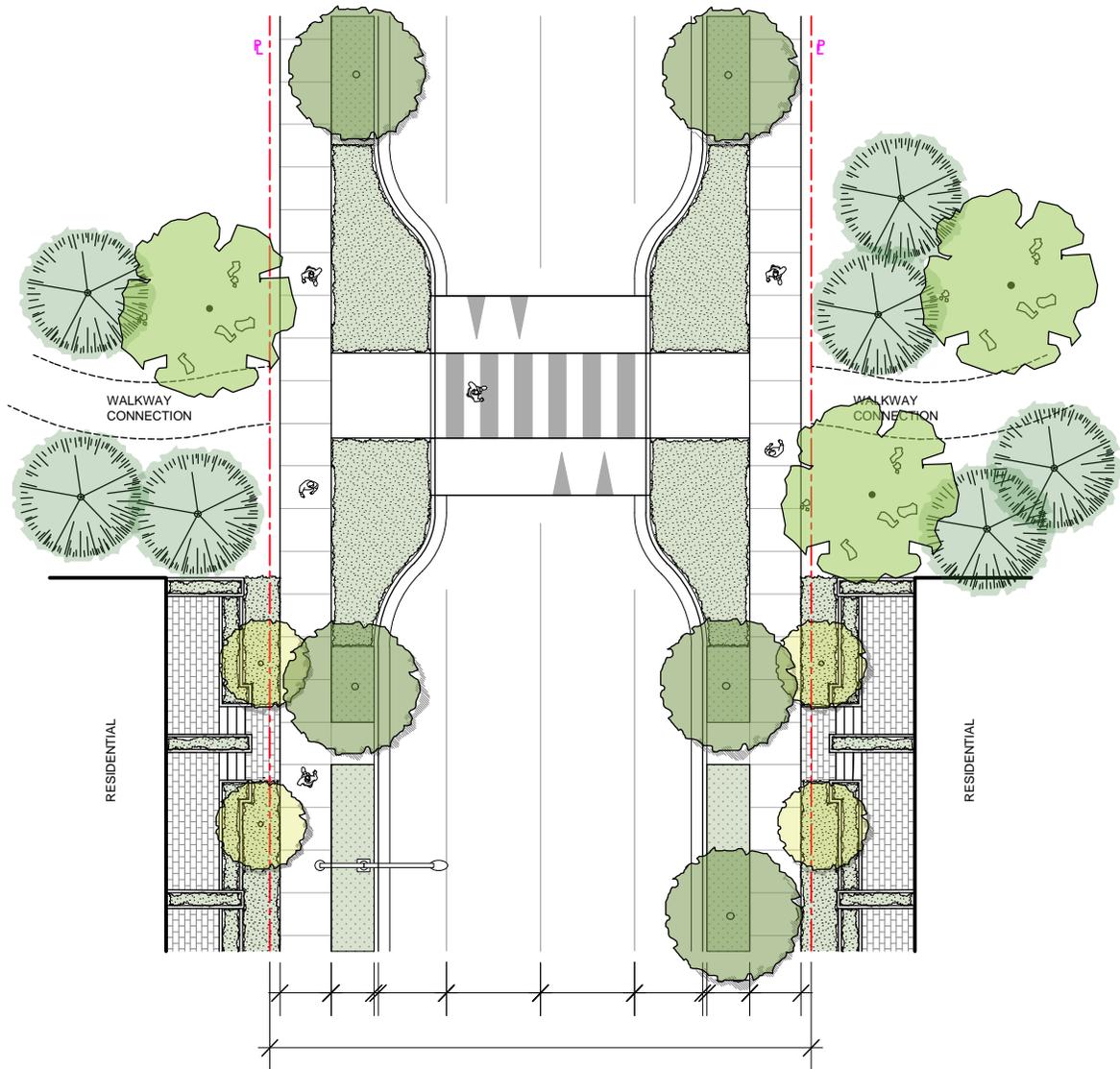
Corner and mid-block crossings should be protected by corner bulges to facilitate safer crossing for pedestrians.

Local streets of this character may include Bond Street, Rupert Street and streets in the future study area.

Private spaces should be separated from the sidewalk by a planted buffer, offering some height and feeling of distance between spaces.

Onstreet parking should be accommodated in pockets to facilitate pick up / drop off and short-term parking. Parking pockets may be needed on both or one side of the street, and should reflect the parking needs of the specific location.







Typical Industrial / Non-Residential Street

Industrial/commercial streets west of Mountain Highway include both low and high volume local and collector streets. Solutions for each type of street should address specific needs and traffic volumes, but should explore creative solutions for providing active street environments that are safe for all modes of travel. Though these streets are expected to redevelop at a slower rate, improvements can be made to provide interim and long-term safety measures.

Through redevelopment, vehicular access points should be reduced, limiting the number of crossing points along the sidewalks. This will create a safer environment for pedestrians and cyclists with fewer conflict points.

Painted lines delineating pedestrian space could be considered where feasible to improve safety.

Pop-up parks which include seating and areas for improved street life is another possible design solution. Existing retail / commercial encouraged to consider innovative improvements along their frontages.

Long-term redevelopment should regularize the street edge and implement permanent sidewalks and street trees.





5 PLANTING



Planting Strategy

The planting strategy for the Lynn Creek Town Centre will be based on a selection of resilient plant species that exemplify seasonal variations in colour. The following plant lists are divided into:

- General Plant List
- General Use and Corner Bulge Planting
- Raingarden Plant List

The progression of plant selection reflects the specificity of planting conditions. Raingarden plants can be used in other conditions, but no other species should be used within a raingarden.

Planting strategies in all locations should focus on providing a mix of colours and seasonal variation. Plants outside of raingardens should be established in large clustered groups of no less than 50. This will create swathes of colour and the ability to feel a progression, particularly in linear conditions. Creating a sequence will be important and significant mass plantings will create a field condition and the sense of immersion for pedestrians. Illumination and up-lighting of plants and trees is encouraged whenever possible to highlight this clustered theme.



Examples of clustered planting and up-lighting



Coneflower



Salal



Hosta various varieties

General Plant List

<i>Rudbeckia hirta</i>	Black-eyed-susan
<i>Calluna vulgaris</i>	Pink Heather
<i>Echinacea purpurea</i> 'Kim's Knee High'	Kim's Knee High Dwarf Purple Coneflower
<i>Mahonia nervosa</i>	Oregon grape
<i>Spiraea douglasii</i>	Hardhack
<i>Ribes sanguineum</i>	Flowering current
<i>Gaultheria Shallon</i>	Salal
<i>Callicarpa profusion</i>	Profusion Beautyberry



Mexican Feather Grass

General Use and Corner Bulge Planting

<i>Sedum</i>	species variable
<i>Lavandula angustifolia</i> 'Munstead'	Munstead Lavender
<i>Genista pilosa</i> 'Vancouver Gold'	Woodwaxen
<i>Daphne cneorum</i>	Rose Dalphne
<i>Erica x darleyensis</i> 'Kramer's Rote'	Kramer's Red Heath
<i>Lavandula angustifolia</i> 'Hidcote'	English Lavender
<i>Helianthemum nummularium</i>	Sun Rose
<i>Escallonia</i> 'Newport Dwarf'	Dwarf Escallonia
<i>Cistus x corbariensis</i>	Rock Rose
<i>Spiraea japonica</i> 'Walburna'	Magic Carpet Spirea
<i>Nassella tenuiss</i> (formerly <i>Stipa</i>)	Mexican Feather Grass
<i>Imperatata cylindrica</i> 'Rubra'	Japanese Blood Grass
<i>Coreopsis verticillata</i> 'Moonbeam'	Coreopsis-Threadleaf



Magic Carpet Spirea



Japanese Blood Grass



Pink Heather



Flowering Currant



Profusion Beautyberry



Raingardens

Raingardens will play both an aesthetic and rainwater management function in the Lynn Creek Town Centre. Raingardens should be integrated along major streets including Mountain Highway, on residential streets in curb bulges and along the length of the green spine.

The plant selection chosen for these raingardens includes species that are hearty, water tolerant and provide variety in form and texture. Bursts of colour through unique grasses and seasonally flowering species will highlight these important sustainable features, and provide interest in the public realm.

Each raingarden should include a mix of species and focus on one theme or colour selection.

The following species have been chosen as they are durable and easily maintained while offering a consistent and unique planting palette.



Western Swordfern



Small Fruited Bullrush



Silver Sage



Blue Fescue



Day Lily



Evergreen Huckleberry

Raingarden Plant List (non submergible)

<i>Helictorichon sempervirens</i>	Blue Oat Grass
<i>Hemerocallis var.</i>	Day Lily
<i>Pennisetum alopecuroides 'Hamelin'</i>	Hamelin Dwarf Fountain Grass
<i>Polystichum munitum</i>	Western Swordfern
<i>Blechnum spicant</i>	Deer Fern
<i>Arctostaphylos uva-ursi</i>	Kinnikinnick
<i>Carex obnupta</i>	Slough Sedge
<i>Carex stipata</i>	Sawbreak Sedge
<i>Iris douglasiana</i>	Douglas Iris
<i>Juncus effusus</i>	Common Rush
<i>Vaccinium ovatum</i>	Evergreen Huckleberry
<i>Iris missouriensis</i>	Western Blue Iris
<i>Scirpus microcarpus</i>	Small Fruited Bullrush
<i>Festuca glauca</i>	Blue Fescue
<i>Salvia argentea</i>	Silver Sage
<i>Rubus calcynoides 'Emerald Carpet'</i>	Emerald Carpet Oriental Raspberry



Deer Fern



Kinnikinnick



Trees

Street trees will be a significant feature of all streetscapes in the Town Centre. Street trees will be installed in the zone along the curb, or as a buffer between sidewalk and bike lanes. Where possible, and in areas of less intensive pedestrian traffic, streetscapes will include boulevard strips for trees and understorey planting.

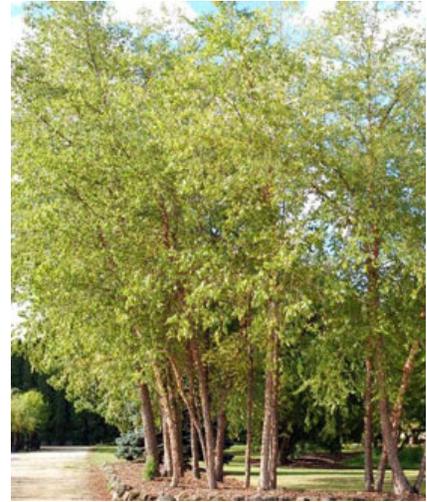
The District of North Vancouver has standards for street tree planting to improve tree health and longevity through the implementation of tree trenches, structural soil under sidewalks, specified growing medium, and other techniques to provide improved access for roots to soil and water. Please refer to the Development Services Bylaw for tree planting standards.

Tree lists have been divided into those species suitable for street trees as well as general trees suitable to park locations. Street trees can also be used in park settings. The selected species should provide variety, seasonal variation and a diversity of colour.

Species for street trees should be selected with reference to existing street trees on the same or adjacent blocks of the street with the intent to integrate new street trees with species already selected and growing on the same streetscape.



Katsura



River Birch



Willow



Black Tupelo



Hawthorn



Swamp Oak

Street Tree Selection

<i>Fraxinus Americana 'Autumn Purple'</i>	White Ash	White Ash
<i>Nyssa sylvatica</i>	Black Tupelo	Black Tupelo
<i>Cercidiphyllum japonicum</i>	Katsura	Katsura
<i>Crataegus</i>	Hawthorn	Hawthorn
<i>Fraxinus pennsylvanica</i>	American / Green Ash	American / Green Ash
<i>Quercus palustris</i>	Green Pillar Pin Oak	Green Pillar Pin Oak
<i>Carpinus betulus fastigiata</i>	Hornbeam	Hornbeam

Park Tree Selection

<i>Salix babylonica</i>	Willow
<i>Quercus bicolor</i>	Swamp White Oak
<i>Acer rubrum 'Autumn Flame'</i>	Red maple 'October Glory'
<i>Acer rubrum 'October glory'</i>	Red maple 'Autumn Flame'
<i>Magnolia soulangeana</i>	Chinese magnolia
<i>Betula nigra</i>	River Birch
<i>Picea omorika</i>	Serbian Spruce



Chinese magnolia



American / Green Ash



Hornbeam



Ensuring Longevity and Full Growth of Street Trees

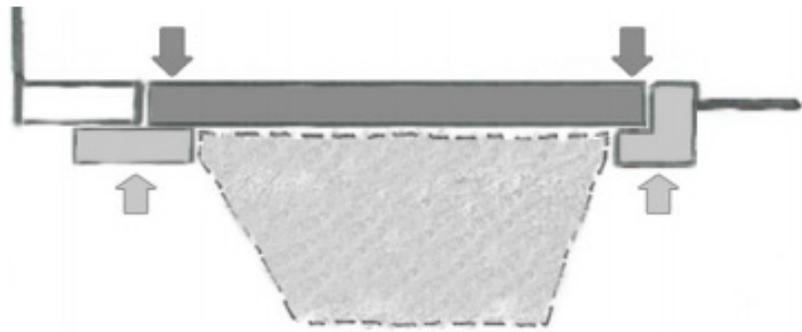
Street tree longevity is directly impacted by four key factors:

1. Provision of adequate soil volumes
2. Ensuring soil remains un-compacted
3. Providing sufficient openings in hard surface
4. Tree spacing that prioritizes long term growth

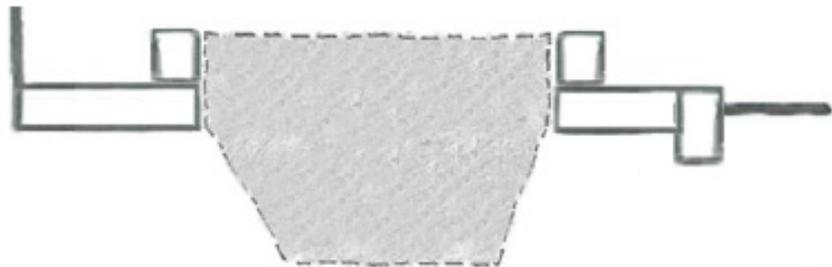
Refer to the Design Services Bylaw for the District standards required for minimum separation of trees. Along priority corridors including Mountain Highway, larger soil volumes are encouraged to be a minimum of 15 cubic meters per tree with a minimum depth of 1.0m.

Soil volumes that remain un-compacted will be a direct result of the construction methods adopted by the District. Along Mountain Highway and other priority corridors within the Lynn Creek Town Centre, increased soil volumes are encouraged and may be integrated into the street design through continuous soil trenches. This provides infiltration opportunities and the opportunity for larger volumes of soil.

To accommodate for reduced compaction, alternate construction methods are



Pre-cast concrete bridge system



Open planter system

encouraged to be utilized along Mountain Highway. Three primary methods are encouraged through the redevelopment process:

- Cast in place pavement bridge system
- Pre-cast pavement bridge system
- Open planter system

Open planter systems are suitable for areas with lower pedestrian traffic that can accommodate

understory planting.

Sidewalk bridge systems allow for easy and efficient maintenance and restoration, while providing a large amount of un-compacted soil. Bridge system construction methods are to be developed in coordination with DNV engineering to meet the needs and requirement of each street type. Creative, cost effective solutions to provide a wide range of benefits for ensuring full tree growth are encouraged.

Tree Spacing

Street tree canopies are a direct result of the soil volumes in which they grow, but also dependant upon the space for which the canopy is provided for full maturity. Often in new developments trees are spaced closely to provide a more dense canopy in the short term.

However, densley-spaced trees with less soil volume per tree will have smaller canopies and a shorter lifespan. Wider spacing which anticipates future growth and longevity of the streetscape will yield larger, fuller canopies and more mature tree growth.

Street tree spacing should be based on the needs of the particular species, placement within the street and the type of street.

Larger trees on major streets including Mountain Highway and Crown Street should be spaced 8-9m apart allowing for a fuller canopy and room to grow. Local streets may integrate smaller street tree species planted 5-6m apart. These could include ornamental varieties.





6 PUBLIC ART AND GATEWAYS



Public Art Opportunities

The District of North Vancouver values public art as an important means to help shape local community identity and character. Public art can have an aesthetic and/or functional role and can be expressed in a variety of different forms such as:

- artistic landmarks (gateway features, signage, community facilities, sculptures, murals)
- functional streetscape elements (street furniture, pavement treatment, tree grates, lighting, entranceways, fencing, playground equipment, and more)
- architectural features (bus shelters, canopy features, shelter elements, roof structures, bridges)
- natural environment elements (pathways, playgrounds, landscaping, wayfinding).

Appropriate artistic “themes” for Lynn Creek, based on staff and community input include:

- industrial and contemporary character: steel, rock, bold colours, geometric shapes, wood
- community connections (Lynn Creek to Lynnmour, CNV and Seymour areas)
- celebrating connections to Lynn Creek (history of annual cycle and activities)
- innovation and creative enterprises
- sustainability (district energy, stormwater management, etc.) and recycling of used materials
- outdoor recreation (mountain biking, hiking, fishing, etc.).

Potential locations for major public art installations in the Lynn Creek Town Centre are:

- the public plaza in the Town

Centre core

- adjacent to the new community centre
- at the entrance to Seyllyn Park
- the southeast corner of Crown Street and Mountain Highway
- colourful banners down along Mountain Highway
- other park areas.

Potential areas for functional art in the Lynn Creek Town Centre area include:

- street furniture, lighting, tree grates, pavement treatment in the Town Centre and especially on the Mountain Highway “High Street” area between Hunter and Crown Streets
- play areas in a redesigned Marie Place Park
- proposed pedestrian and



Gateways

- cyclist bridge over Lynn Creek
- weather protection elements
- signage and wayfinding elements.

Potential areas for art within the natural environment in the LLTC area include:

- trails enhanced with wayfinding features and signage
- interpretive and/or historical signage describing history of place
- artwork associated with the river featuring salmon and local wildlife, especially at bridges
- artistic yet functional stormwater management water features.

Gateways will play an important role in establishing the identity of the Lynn Creek Town Centre. Because of the town centre's unique location bounded by bridges and overpasses at all access points; gateways can establish a sense of arrival, a sequence of focal points and an overall distinct identity.

Critical Gateways include:

- Phibbs Exchange Transit Hub
- Intersection at Orwell and Oxford Streets
- Keith Road Bridge (over Lynn Creek)
- Main Street City / District bounday
- Main Street Highway #1 overpass connection

Pedestrian crossings at:

- Hunter Street crossing Lynn Creek

- Crown Street crossing Lynn Creek
- Crown Street East Highway #1 overpass
- Fern Street Highway #1 overpass connection

These locations may be marked with pedestrian spaces, focal points, public art installations or significant architectural elements.





7 SITE FURNISHINGS & LIGHTING



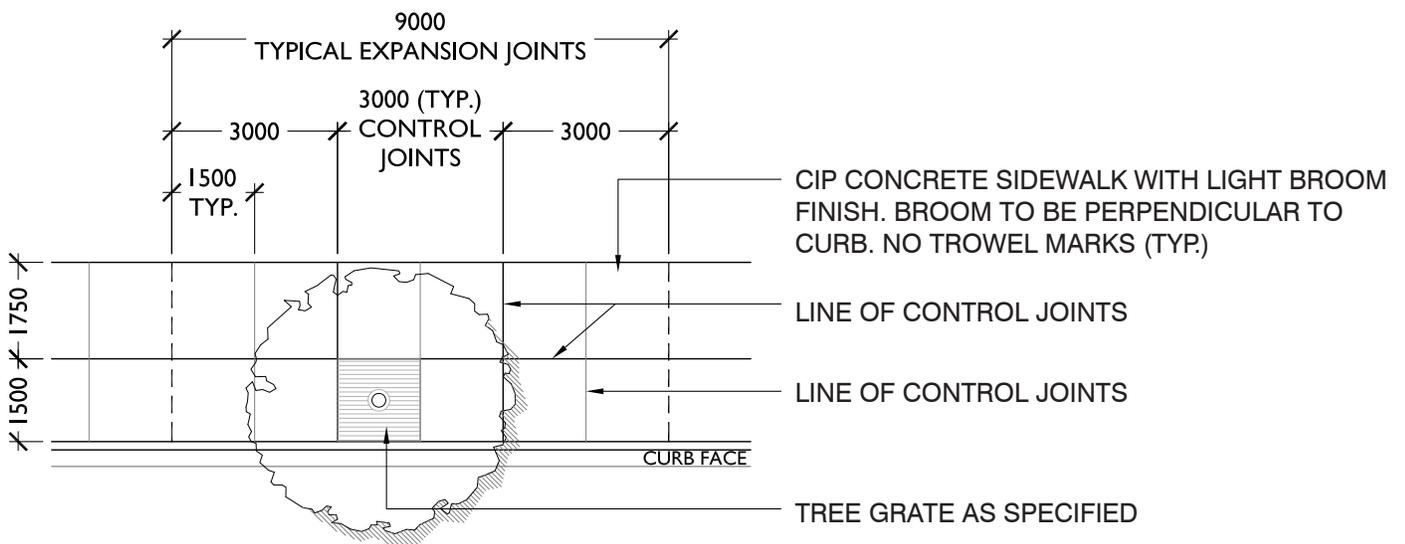
Common Elements

A number of public realm design elements are intended to be used throughout Lynn Creek Town Centre. Together they will support an urban design character across areas although the core will have a greater concentration of public realm amenities due to its uses and density than surrounding residential, service commercial, and industrial areas.

Each streetscape has a typical cross-section illustrated in the Streets Section of these guidelines. In each streetscape, the public realm has vehicular and pedestrian zones. Within the pedestrian zone or sidewalk area, there is a zone for pedestrian movement that is kept free of any furnishings, trees, or other

obstructions and a zone for furnishings where all street furniture, lights, trees, and other streetscape elements are located.

As part of the development process, applicants may propose similar or alternative fixture that may be reviewed and approved by DNV.



Paving Materials

The palette of paving materials is selected for durability and suitability to be installed incrementally over a number of years. Master Municipal Construction Documents and Design Services Bylaw standards apply for sidewalk minimum widths, corners, parking bays, and paving installation.

Paving in the street right-of-way and on adjacent private land where the public has right of passage should be integrated and seamless, to present an appearance of a generous and accessible public realm using the same paving materials and patterning of saw cuts where appropriate. The paving scheme should extend into entries and

publicly accessible plazas and courtyards. Where driveways cross a sidewalk, the concrete paving should be patterned with a finer texture to indicate to pedestrians that vehicles may be expected to cross their path.

In the Town Centre core, the typical paving pattern is intended to mark a 1.5 meter wide strip with a saw cut joints adjacent to the curb, as shown below. Depending on available space in the right-of-way and the volume of pedestrian traffic, the street tree surround material may include an extended tree grate (1.5 meters by 4.5 meters), landscaping, or pavers.



Material Specifications

Streets -- Curbs	Concrete with District standard curb letdowns at intersections
Sidewalks	Concrete: standard concrete sidewalks in all areas except the Town Centre core; saw-cut concrete in Town Centre core areas; saw cut patterns to be designed in relation to standard pattern or modified to respond to design elements in adjacent architecture (entries, columns, pilasters, storefronts etc.)
Pedestrian Paths and Upper Sidewalks along Mountain Highway	Saw-cut concrete or concrete pavers in concrete colour or asphalt in areas outside the Town Centre core
Multi-Use Paths – Off-Street	Asphalt or other appropriate material for use with DNV approval



Lighting

A family of LED lighting fixtures has been selected for the Lynn Creek Town Centre. The size of fixture will be larger along Mountain Highway than on the plazas, parks, pedestrian paths, and multi-use paths elsewhere within the Lynn Creek town centre. The fixtures are contemporary in character with flexibility to adapt size, arm extension length, and accessories like banner brackets to suit both streetscape and greenway/pedestrian path applications. For a consistent identity, the selected paint choice is RAL 7022 - umbra gray. This colour is to be

matched as closely as possible for other painted furnishings such as garbage receptacles and bus shelters.

The lighting standard should be used along streets as they redevelop as well as in park settings. Pedestrian scale lighting should be integrated in key public realm locations including plazas, and along mountain highway, Crown Street, and Hunter Street. Pedestrian scale lighting may also be prioritized along other busier pedestrian and cycling streets potentially including Oxford Street.



PLACE	LUMINAIRE	MOUNTING ARM	POLE AND BASE MOUNT	POLE SIZE	COLOUR
<ul style="list-style-type: none"> Mountain Highway Fern Street 	Lumca CPG0401	CF23	Straight round pole with Nova Pole 'Seymour' base cover	9.1m	RAL 7022
<ul style="list-style-type: none"> East Keith Road 	Cooper OVF	N/A	Davit pole	9.1m	RAL 7022
Mountain Highway pedestrian lighting East Keith Road multi-use pathway	Lumca CPS0401	CF28	Straight round pole with Nova Pole 'Seymour' base cover	4.3m	RAL 7022
Park Pathway	Cree 'the Edge'	N/A	Octagonal post top pole	4.3m	RAL 7022
Plaza	Lumca CP1401	N/A	Straight round pole with Nova Pole 'Seymour' base cover	TBD	RAL 7022

Bicycle Racks

The preferred bike rack is a simple single ring design in matte stainless steel finish. This rack has the advantage of being readily sized to match anticipated demand.

The preferred location for bike racks is within the furnishing zone located between trees and adjacent to parking or bike lanes. In cases where space for bike racks is limited then placement adjacent to building frontages is the second choice location.

The preferred model is the Ring distributed by Landscape Forms:

Bike Rack

Manufacturer: Landscape Forms

Model: Ring

Material / Colour: Titanium colour matte stainless steel finish

Frame made of steel

Bike rack holds 2 bikes and can be arranged in groups





Transit Shelters

Where feasible, transit shelters provided by building canopies or overhangs is preferred, as long as good visibility for transit passengers and bus drivers can be achieved.

Transit shelters should utilize paint colour coordinated with the palette of brushed stainless steel (e.g. bike rack). TransLink’s guidelines for transit-related infrastructure should be referenced for placement of transit shelters to ensure universal access and other design requirements.



Free Standing Transit Shelter

Waste Management

The District’s standard waste receptacle is appropriate for Lynn Creek Town Centre. The need for bear-resistant waste receptacles should be considered and installed where needed.

Public spaces in the Town Centre should provide recycling receptacles in the public realm in strategic locations including, but not limited to, the central plaza and adjacent to the community/recreational facility. These units should be of the same design as the waste receptacles and finished in titanium to coordinate with adjacent standard streetscape furnishings. Implementation of recycling and expected future green initiatives must be done in coordination with District operations.

Waste Receptacle

Manufacturer: Victor Stanley

Model: SDC-36

Material / Colour: Titanium Finish

36 gal (136 L)

Side-Deposit

Side-Door Opening

Recycling Receptacle

Manufacturer: Victor Stanley

Model: RSDC-36

Material / Colour: Titanium Finish

two half moon plastic liners

Side-Deposit

Side-Door Opening



Waste Receptacle



Recycling Receptacle

Seating

For intensively used open spaces and plaza areas, seating should be designed as part of the overall landscape concept to invite a variety of seating opportunities, including in singles and groups, with and without backs, and options that include arms to accommodate accessibility needs. Skate-guards could be considered if required and should be integrated into the design. Generally a concrete base with seating surfaces in wood slats is appropriate.

For applications where a comprehensive landscape plan is not available, such as along existing sidewalks and in parks, single benches with a corten steel frame / base and wood seating and backing for support should be used. The preferred bench is the Ideas L and T benches manufactured by Metalco.

Seating

Manufacturer: Metalco

Model: Ideas L-T Benches

Material / Colour: Corten Steel & Hardwood slats

Multiple Configurations including with or without back





Tree Grates

Street trees are to be located within generous tree grates along high traffic areas including Mountain Highway, Main Street and Crown Street. Other plaza, greenway and park conditions may warrant the use of tree grates.

The preferred standard is the 5' Boston manufactured by Urban Accessories. In all cases the centre expansion should be removed to provide a 2'6" opening to ensure unrestricted tree growth.

In heavy pedestrian use areas multiple tree grates can be combined to create linear elements highlighting the material and the prominence of the street trees in the urban environment.

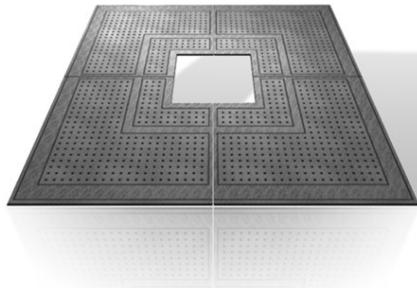
In special locations tree grates may be designed a part of a public art process. Dimensions, material and finish should be consistent, while the design may be varied in unique circumstances.

Tree Grates

Manufacturer: Urban Accessories

Model: 5' Boston

Material / Colour: Ductile Iron Raw finish



Urban Accessories 5' Boston Standard



Examples of Custom Designs

Stormwater Management

Innovative and visible stormwater management features should be integrated throughout the Town Centre; functioning to filter, infiltrate and convey surface runoff whenever possible. Establishing a consistent aesthetic for these systems will include treatments such as trench drains, that can be used in hardscape locations such as sidewalks and plazas.

Key opportunities for surface stormwater features are in the central open space corridor comprised of the Town Centre Plaza, Marie Place Park and the north-south green spine. Implementation of surface stormwater features along these corridors is encouraged.

Within the plaza and other sections in the Town Centre core, these surface features should have a contemporary, urban character. Along the pedestrian spine, stormwater features will be more naturalized in appearance. Where space is constrained, use of trench drain covers should be considered.

Trench Grates

Manufacturer: Urban Accessories

Model: Rainbow various lengths

Material / Colour: Ductile Iron
Raw finish

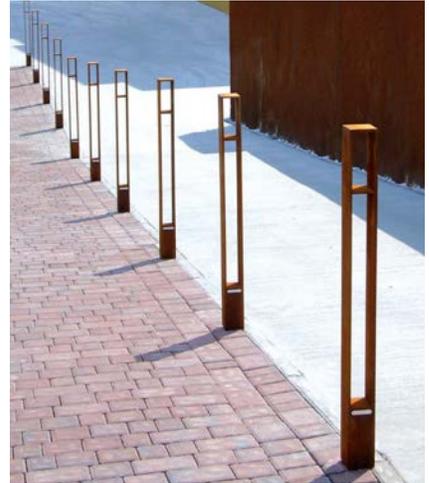




Bollards

Should there be a requirement for bollards within the town centre, Metalco Moka Bollard should be used. These corten steel bollards are simple, robust, but not visually overpowering in a pedestrian space. They should be used sparingly to demarcate necessary pedestrian spaces. A suitable alternative may be necessary where bollards are required to be removable.

Should ground level lighting be required along the green spine or within park locations, the Vision bollard by Metalco is the preferred option. This product is similar in shape and material while providing lighting options.



Bollard

Manufacturer: Metalco

Model: Moka Bollard

Material / Colour: Corten Steel

Lighted Bollard

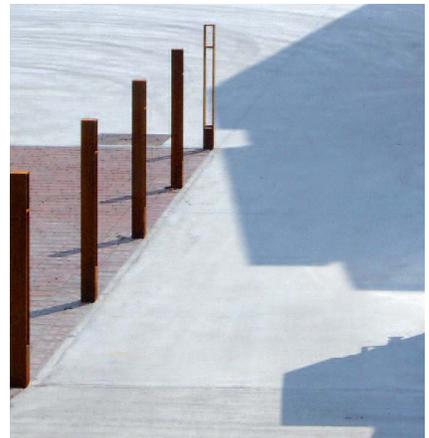
Manufacturer: Metalco

Model: Vision Bollard

Material / Colour: Corten Steel



Vision Bollard



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8 FLOOD RESILIENCE



Flood Protection and Resilience

The Lynn Creek Town Centre is located at the alluvial fan of Lynn Creek and has been identified as a Creek Hazard Development Permit Area. Potential future changes to creek management could result in the need for dike work along the south east side of the creek. However, site development has a key role to play in addressing flood risk and building flood resilient communities.

The Creek Hazard DPA aims to:

- Minimize the risk to people and property from creek hazards
- Ensure development and construction near creeks is done safely
- Reduce the impacts of flooding in developed areas

To achieve these goals in the Lynn Creek Town Centre, minimum flood construction levels (FCL) have been established ensuring that floor levels are raised above street level to mitigate potential flood issues. The FCL established for the Lynn Creek Town Centre is 0.6m measured from the gutterline of the street. This FCL applies to all residential development to ensure that habitable space is adequately protected from possible flooding. Alternate flood construction methods for commercial areas are encouraged as outlined on pg. 79.

Residential Development

Flood construction levels apply across all residential development within the Flood Hazard DPA. Higher density residential development should utilize similar flood resilient construction methods to provide accessible grade change within the building entrances.

Townhouse and other street-fronting residential forms should integrate stairs and ramps to provide grade changes into the landscape component of the residential setback. Utilizing industrial materials and stair access points that create interest along the streetwall are encouraged.



Commercial Development

Commercial space along the High Street requires an alternate approach to flood resiliency. To mitigate universal accessibility issues, storefronts should be accessed at grade from the sidewalk. No exterior stairs should be included within the sidewalk or setback space along commercial frontages.

The minimum commercial setback identified in the OCP Schedule B guidelines may need to be increased in some instances to accommodate the grade change and to maintain adequate space for sidewalks, street furniture, and utilities, while providing meaningful circulation space.

Setbacks should be consistent with the cross sections provided in Section 4: Streets.

Grade change between the curb and building front should not exceed a 3% slope. The remainder of the 0.6m can be accounted for in the following ways:

- Raised internal development
- Flood resilient construction

Raised internal strategies will create a more desirable street condition with commercial display space and internal activity positioned closer to the pedestrian level. This supports a more active commercial frontage and builds a consistent

neighbourhood character. To address flood resiliency stairs or ramps can be integrated inside the building. Large format retail units should utilize ramp systems whenever possible to mitigate accessibility issues.

Smaller commercial units should utilize flood resilient construction methods whenever possible to mitigate accessibility concerns. This strategy includes the use of robust materials such as concrete and steel and would place all damageable materials above the minimum FCL level. See the Flood Hazard DPA for requirements.



Example of flood resilient construction and raised internal development



Sustainable Development

The OCP and Schedule B and other relevant District policy govern green building and sustainable policies in the District.

This section of the Design Guidelines outlines additional sustainability design considerations for development on private property.

- New development should seek to manage and re-use stormwater on site to promote watershed health. Rainwater features should be located in visible locations and integrated with both landscape and architecture.
- Green initiatives such as green walls, rooftop gardens, and the incorporation of sustainability into public art and community amenities are encouraged.
- Purposeful integration of sustainable design elements into the architecture is encouraged where appropriate.
- Opportunities to celebrate and display or reveal sustainable building elements such as external shading systems or other elements are encouraged. Thoughtful design consideration to integrate these elements into the architecture adds a level

of interest and functional honesty to the building design. For example, solar hot water panels located on a south-facing sloped surface such as clerestory or a portion of a roof should be integrated into that surface.

- New development should consider opportunities for urban agriculture through the provision of community garden plots, usable green roof area, and private yard space.



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9 BUILDING FACADE & FORM



Form and Character

This section of guidelines addresses the overall objectives for form and character including design principles; intended character; height and massing, materials; universal design; and sustainability, followed by specific guidelines that pertain to residential and industrial / commercial areas.

Design Principles

New development in the Lynn Creek Town Centre is encouraged to respond creatively to the context envisaged under the LLTC Implementation Plan while at the same time achieve a cohesive identity and character.

The aim of the Form and Character Guidelines is to identify existing and emerging neighbourhood characteristics that are distinct and intrinsic to the Lynn Creek area and to propose design principles that reinforce this character. The character intended for Lynn Creek is derived from a combination of the natural setting, the historical context, existing built patterns (industrial and residential), a trend towards

simple contemporary design, a local culture of outdoor recreation, and sustainable development of energy efficiency and rainwater management.

Unless otherwise noted, the form and character design principles identified below supplement those identified in the OCP (Schedule B) and should be read in parallel.

Height and Massing

In general, building heights and massing objectives in these guidelines aim to promote a mix of sizes and scales of development that is appropriate to the scale of nearby streets, other public spaces, and buildings. Height and massing of new development is intended to respond to both the existing urban context and neighbourhood character as well as that envisioned by these guidelines.

The LLTC Implementation Plan provides guidance on the approximate building height for new development in the Town Centre.

- Buildings should frame public streets and open spaces to

create a sense of enclosure, street vitality, and safety.

- At the scale of the streetscape, new development is encouraged to add interest through well-considered variations in building height, rooflines, and massing that are consistent with the proposed grain of the neighbourhood.

- New buildings should acknowledge and respect the importance of allowing sunlight into important public spaces and private outdoor spaces. Appropriate building setbacks and roofline articulation should be provided to allow light access to the street and broader views of the sky and to reduce the 'canyon' effect for pedestrians at street level. Building siting should take available opportunities to frame views to the mountains.

Urban Street Wall

Podium forms and street walls should maintain an appropriate pedestrian scale.

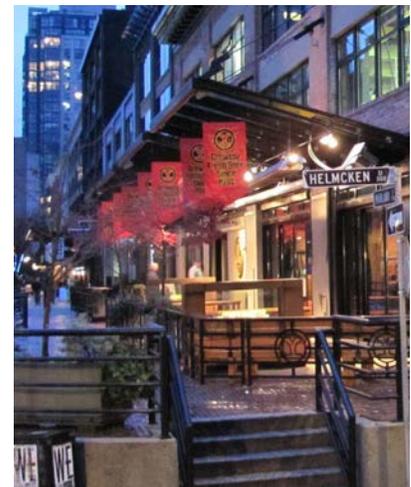
- New developments should have a strong relationship to the street to animate public spaces and enhance the sense of ownership and community spirit. Buildings should be oriented towards public streets, walkways, and amenities.
- Building façades should be modulated at grade level to encourage street activity such as browsing, outdoor cafés, and street entertainment, as well as to enable placement of seating, where appropriate.
- The edges of larger developments should be carefully considered to mediate differences in scale between multi-family

buildings. Refer to OCP Schedule B Guideline A3.2 Scale. The design of new developments should create purposeful and intentional in-between transition zones. The edges of tower developments should mediate differences in scale between neighbouring buildings.

- Expanding on OCP Schedule B *Guideline A1.11: Storefronts*, shops that line larger format retail stores should be scaled to support the functioning of successful businesses and offer a meaningful variety of shopping options. The ceiling height of shopping spaces should be a minimum of 4.3 metres (14 feet).
- Residential-only buildings should have ground-oriented residential units with

individual front doors and porches integrated into the streetscape.

- The urban edges of the Town Core's central urban plaza should be animated by the programming of adjacent interior spaces.
- Building uses that have a public nature or that serve visitors of the plaza should have a direct relationship to the plaza.
- Consideration should be given to design elements such as covered transition spaces, generous openings, large areas of glazing in walls and seating.
- Buildings in the Town Centre Core should provide continuous weather protection to sidewalks along Mountain Highway.



Street wall with weather protection and visual interest



Refer to OCP Schedule B
Guideline A3.12: Weather
Protection.

- Interconnecting pedestrian mews are intended to provide connectivity to and through blocks. These mews will tend to be located adjacent to building faces.
- Building edges that are adjacent to mews and pedestrian paths should be a minimum of 1.5m in width to allow for sufficient space to provide visual and spacial separation of private and public space.
- Windows in walls facing the mews, including end walls, are encouraged, provided that potential overlook from passersby into residential units can be mitigated, e.g. with landscaped screening /

window dressing. Terraced, landscaped, or architectural elements can be used to provide spatial separation from building faces to mews.

- Building and architectural elements adjacent to pedestrian mews should be scaled to relate to pedestrians e.g. tall, blank building faces should be avoided.
- Smaller developments that do not exceed 45m in length may incorporate a streetwall up to the maximum building height. This would need to be considered in the broader context.



Variation and interest in building form and relationship to street

Variation in Building Design Façade Modulation

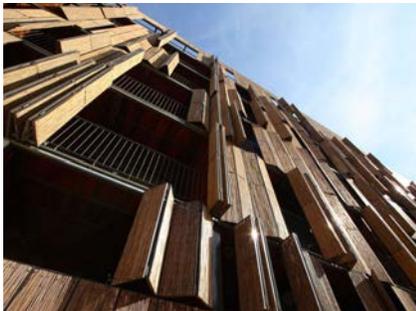
Referencing OCP *Schedule B Guideline A3.1: Variation in Building Design*, neighbouring buildings should be consistent with the emerging neighbourhood character, but differ subtly in their modularity, materiality and façade detail. The expression of neighbouring buildings should not be repetitive, but should relate sufficiently to appear of a family.

The modularity or “grain” expressed in the diagram below, should vary from one development to the next to reinforce a streetscape composition rather than modularity within an individual development in isolation.

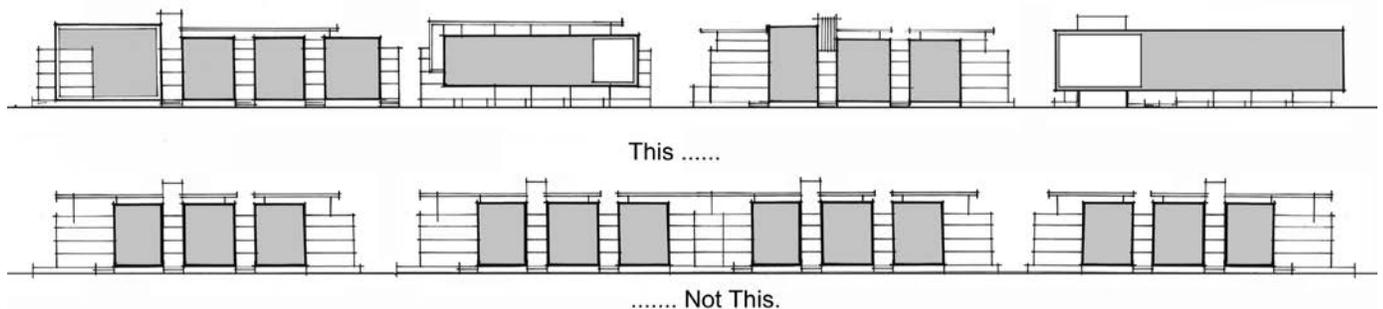
Further to OCP *Schedule B Guideline A1.10: Breaks in streetwall*, which suggests the provision of breaks in the street façade of buildings exceeding 45 metres in length, the height of the dominant eave along the streetwall should also vary along its length (by 0.5m - 1.0m). This can minimize long monotonous lengths of building mass and provide greater clarity to building forms. The module created by these variations should generally be not less than 15m in length and not more than 45m in length. Larger variations in streetwall

height up to the maximum building height, for example a full storey, may be permitted and in some instances encouraged.

Along some blocks within Lynn Creek, existing patterns are characterized by features such as horizontal building expression, large apertures, and varying heights and setbacks of adjacent box-like building forms. It is the legible scale and varying offsetting relationship between the simple forms (“modules”) that should establish the streetscape “grain.”



Examples of unique building forms





Material Selection

The following building materials are recommended to create a distinct look and feel for Lynn Creek Town Centre.

- A range of materials and combinations of materials may be appropriate towards the intended contemporary, functional aesthetic. Among these are: glass, architectural concrete, wood, metal, rough stone, rock dash stucco, and compressed cementitious fibreboard panels.
- Some design cues may be influenced by the simplicity and materiality of the industrial context, such as the occasional use of bold accent colours or the use of corten steel.
- The creative use of wood in soffits, entranceways and other areas is encouraged. When used as an exterior cladding material, wood should be appropriately protected from the weather.
- Building exteriors should be designed to express relationships between building form, function, and materiality. Imitative materials or the imitative application of materials to represent building elements should be avoided. The application of finish materials should complement the overall composition of the development and facilitate the legibility of the building forms.
- Façade materials should be durable and wear well to maintain a quality lasting appearance into the future incorporating the effects of weather.
- The use of ornate and traditional design elements and trim commonly associated with single family homes should be avoided.
- Façade composition should be simple and free of visual clutter by limiting the number of materials on a façade and maintaining a simple arrangement and composition of elements.



Residential Areas

A key objective for the residential area is to encourage a mix of residential built forms including mid and low rise apartments with some ground-oriented housing such as townhouses and rowhouses. In most cases, the streets in the area are relatively short and have low traffic volumes so that vehicles will access underground parking from adjacent roads, leaving the central part of blocks available for public and semi-private uses like pedestrian walkways, children's play areas, a new neighbourhood park, and other social spaces.

The intent for general form and massing of residential buildings in Lynn Creek Town Centre is to take on the same modularity characteristics as streetwalls associated within the Town Centre core (described on pg. 87), but with a more residential focus. Contemporary expression, the use

of durable and varied materials, the grain, module, and the breadth of building face on the streetscape one intended to be consistent throughout the Town Centre, while also encouraging individual building identity.

a. Buildings should exhibit a horizontal expression with variations in building heights and setbacks from the street, with rectilinear forms and flat, floating or low sloping roofs.

b. Provide front doors and steps to the street from individual street-fronting townhouses and apartments, where possible, with a change in elevation to support privacy as well as flood construction requirements.

Redevelopment is encouraged to occur on minimum lot assemblies of approximately 1500 square metres (approximately 4-5 residential parcels).





Industrial and Commercial Areas

Industrial and commercial areas in the Lynn Creek Town Centre are not anticipated to see much renewal and change to 2030. For new development that may occur, a key objective is to encourage new buildings to contribute in a simple manner to the intended contemporary character of basic forms with varying heights and setbacks.

- A broad range of design expression within the intended character is available. Opportunities for innovative departure from conventional commercial design responses is particularly encouraged to strengthen the Town Centre's identity on sites that are highly-visible, such as the north corners of Main Street and Mountain Highway, which will convey the notion of a gateway to the Lynn Creek Town Centre.
- Main Street will remain as a destination retail area, but with more refined design elements that contribute to the above character.
- The design of new buildings in industrial and commercial areas should express simple box-like patterns or buildings of varying heights and setbacks that establish a rhythm with a human scale. This module adds interest both along the streetscape and in the lanes. New development should avoid long unvaried stretches of frontages.
- Where appropriate, consider the inclusion of design elements that offer pedestrian interest and engagement and that accommodate safe pedestrian guidance and movement. This may include covered well-lit walkways, small treed areas

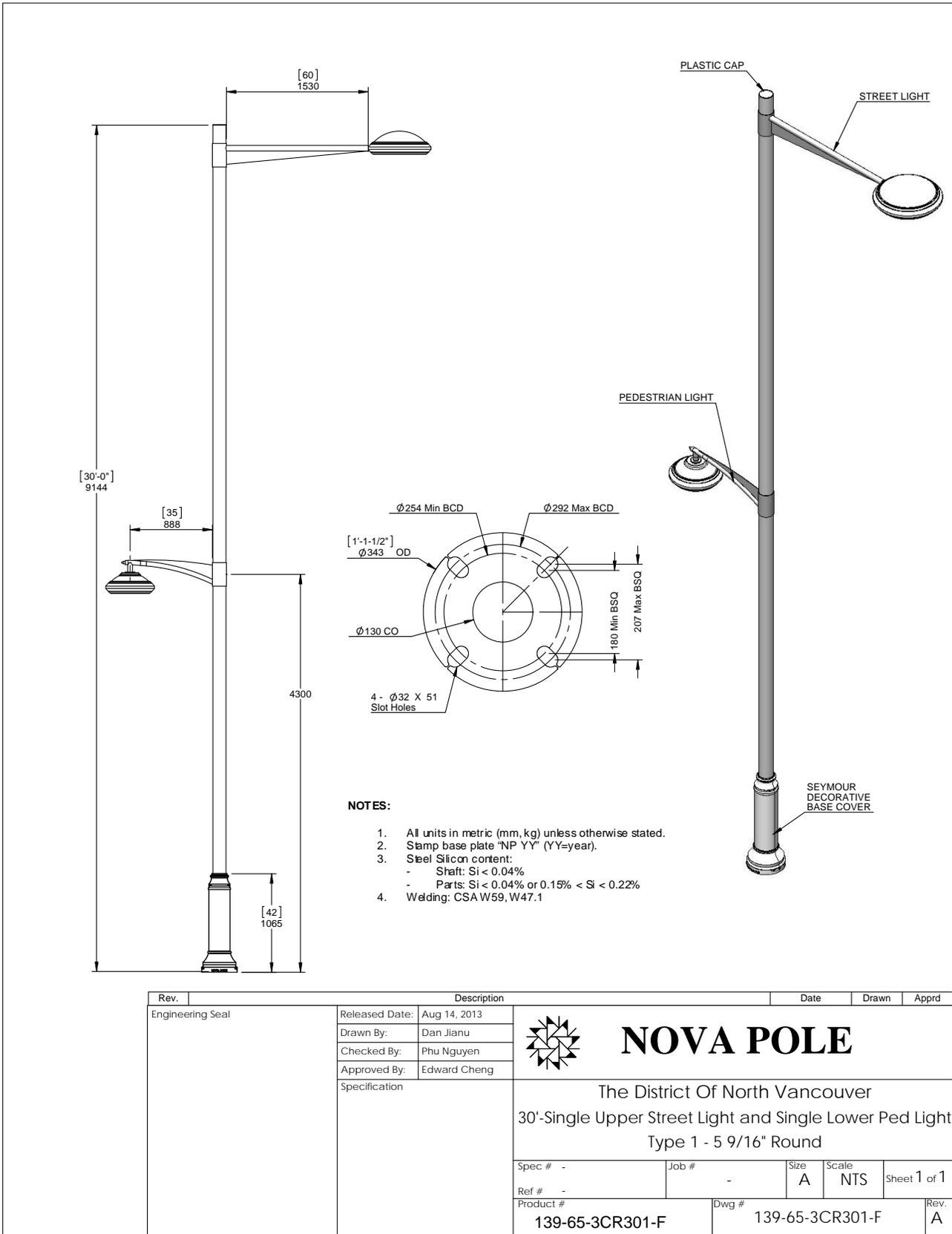


for seating, large openings in building fronts, clear signage, and other visual or physical amenities.

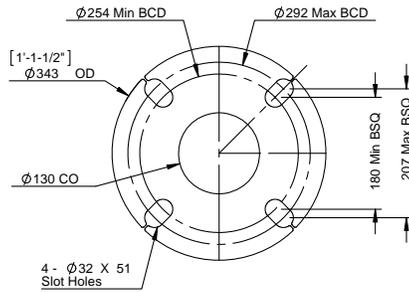
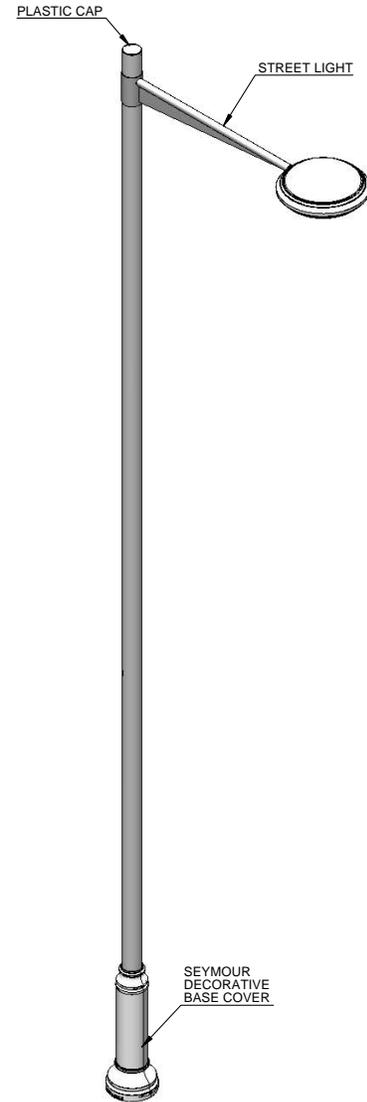
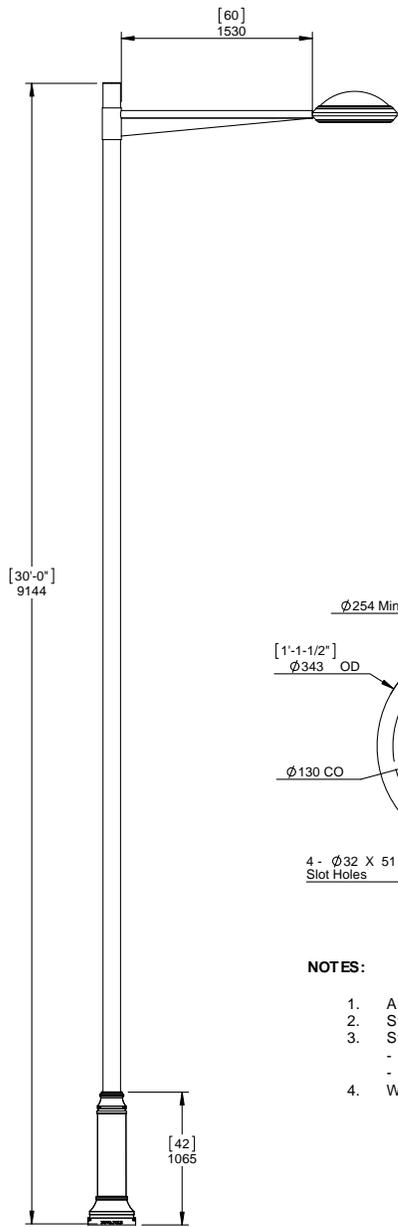
- Residential use permitted above street level in the Commercial and Industrial Area on the north corners of the intersection of Main Street and Mountain Highway. New development incorporating residential uses at this location should take measures to mitigate vehicle noise from Main Street.



10 APPENDIX



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NOTES:

1. All units in metric (mm, kg) unless otherwise stated.
2. Stamp base plate "NP YY" (YY=year).
3. Steel Silicon content:
 - Shaft: Si < 0.04%
 - Parts: Si < 0.04% or 0.15% < Si < 0.22%
4. Welding: CSA W59, W47.1

Rev.	Description	Date	Drawn	Appr
Engineering Seal	Released Date: Aug 14, 2013			
	Drawn By: Dan Jianu			
	Checked By: Phu Nguyen			
	Approved By: Edward Cheng			
	Specification			
		 <h1 style="margin: 0;">NOVA POLE</h1>		
		<p>The District Of North Vancouver 30'-Single Upper Street Light Type 6 - 5 9/16" Round</p>		
Spec # -	Job # -	Size A	Scale NTS	Sheet 1 of 1
Ref # -	Product # 139-65-3AR301-F	Dwg # 139-65-3AR301-F	Rev. A	

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NOTES:

1. All units in metric (mm, kg) unless otherwise stated.
2. Stamp base plate "NP YY" (YY=year).
3. Steel Silicon content:
 - Shaft: Si < 0.04%
 - Parts: Si < 0.04% or 0.15% < Si < 0.22%
4. Welding: CSA W59, W47.1

Rev.	Description	Date	Drawn	Apprd
Engineering Seal	Released Date: Aug 14, 2013			
	Drawn By: Dan Jianu			
	Checked By: Phu Nguyen			
	Approved By: Edward Cheng			
	Specification			



NOVA POLE

The District Of North Vancouver
14'-Single Ped Light
Type 4 - 5 9/16" Round

Spec # -	Job # -	Size A	Scale NTS	Sheet 1 of 1
Ref # -	Product # 139-65-3BR141-F	Dwg # 139-65-3BR141-F	Rev. A	

File Name: 139-65-3BR141-F Saved: 8/20/2013

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IDEAS - L



Alfredo Tasca

Ideas L-T benches

187

Disponibile nei seguenti materiali e dimensioni:
Available in the following materials and sizes:
Disponibile dans les dimensions et matériaux suivants:
Disponibile en los siguientes materiales y tamaños:
Disponível nos seguintes materiais e tamanhos:
Erhältlich in den folgenden Materialien und Größen:



Acciaio Corten / Corten Steel
Acier Corten / Acero Corten
Aço Corten / Corten-Stahl



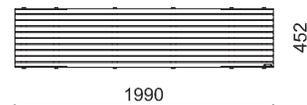
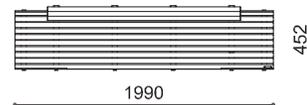
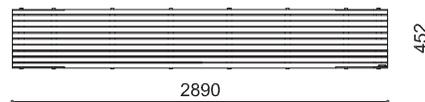
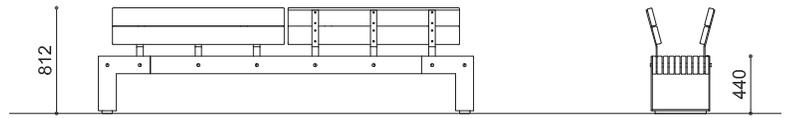
Acciaio Zincato e Verniciato / Galvanized and Powder Coated Steel
Acier Galvanisé et Thermolaqué / Acero Galvanizado y Barnizado
Aço Galvanizado e Pintado / Stahl, Verzinkt und Pulverbeschichtet



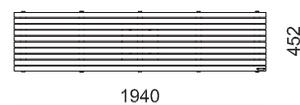
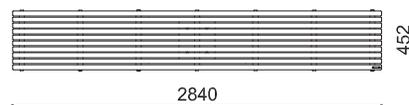
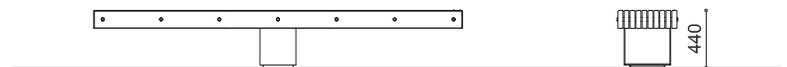
Legno / Wood
Bois / Madera
Madeira / Holz

Vedi disegni a pag.: / See drawings on page:
Voir plans page: / Vea los dibujos de la pág.:
Veja os desenhos em p.: / Siehe Zeichnungen auf Seite:
697

Fissaggio: / Fixings: / Fixation: / Fijación: / Fixação: / Befestigung:
Predisposizione per tasselli / Pre-drilled for anchors
Réservation chevilles / Predisposición para tacos
Preparação para buchas / Mit Bohrungen für Verschraubung



IDEAS - T





Sjit

Moka bollards

453

Disponibile nei seguenti materiali e dimensioni:
Available in the following materials and sizes:
Disponibile dans les dimensions et matériaux suivants:
Disponible en los siguientes materiales y tamaños:
Disponível nos seguintes materiais e tamanhos:
Erhältlich in den folgenden Materialien und Größen:



Acciaio Zincato e Verniciato / Galvanized and Powder Coated Steel
Acier Galvanisé et Thermolaqué / Acero Galvanizado y Barnizado
Aço Galvanizado e Pintado / Stahl, Verzinkt und Pulverbeschichtet



Acciaio Corten / Corten Steel
Acier Corten / Acero Corten
Aço Corten / Corten-Stahl



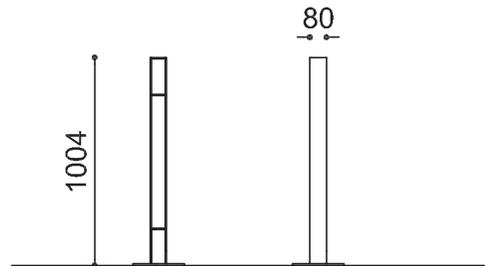
Acciaio Inox / Stainless Steel
Acier Inox / Acero Inox
Aço Inox / Edelstahl

250 (80X80)mm
H 1004 mm

Disegni / Drawings / Plans / Dibujos / Desenhos / Zeichnungen/ p.: 709

Fissaggio: / Fixings: / Fixation: / Fijación: / Fixação: / Befestigung:
Predisposizione per tasselli - Cementazione
Pre-drilled for anchors - Below ground installation
Réserve chevilles - Scellement
Predisposición para tacos - Cimentación
Preparação para buchas - Cimentação
Mit Bohrungen für Verschraubung - Zementierung

MOKA



250

Sjit

Vision

463

bollards

Disponibile nei seguenti materiali e dimensioni:
Available in the following materials and sizes:
Disponible dans les dimensions et matériaux suivants:
Disponible en los siguientes materiales y tamaños:
Disponível nos seguintes materiais e tamanhos:
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Acciaio Zincato e Verniciato / Galvanized and Powder Coated Steel
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Acciaio Corten / Corten Steel
Acier Corten / Acero Corten
Aço Corten / Corten-Stahl



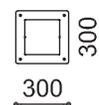
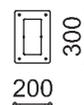
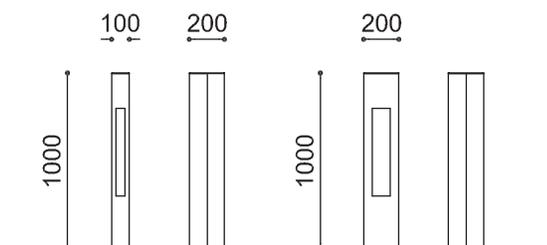
Acciaio Inox / Stainless Steel
Acier Inox / Acero Inox
Aço Inox / Edelstahl

300 X 300 (200X200) X H 1000 mm
200 X 300 (100X200) X H 1000 mm

Disegni / Drawings / Plans / Dibujos / Desenhos / Zeichnungen/ p.: 710

Fissaggio: / Fixings: / Fixation: / Fijación: / Fixação: / Befestigung:
Predisposizione per tasselli - Cementazione
Pre-drilled for anchors - Below ground installation
Réservation chevilles - Scellement
Predisposición para tacos - Cimentación
Preparação para buchas - Cimentação
Mit Bohrungen für Verschraubung - Zementierung

VISION





SPECIFICATIONS

- Material will be high quality 100% recycled grey iron; ASTM A48 class 35b or better; hardness 170-223 brinnell (unless specified otherwise; see below).

Material:

- Grey iron ASTM A48 (standard)
- Aluminum, ASTM B26
- Ductile iron, (required for all load ratings higher than pedestrian ASTM A536 class 65-45-12.
- Nickel bronze (ASTM B30)
- Bronze (ASTM B26)

- Finish will be natural patina of raw iron (unless specified otherwise; see below).

Finish:

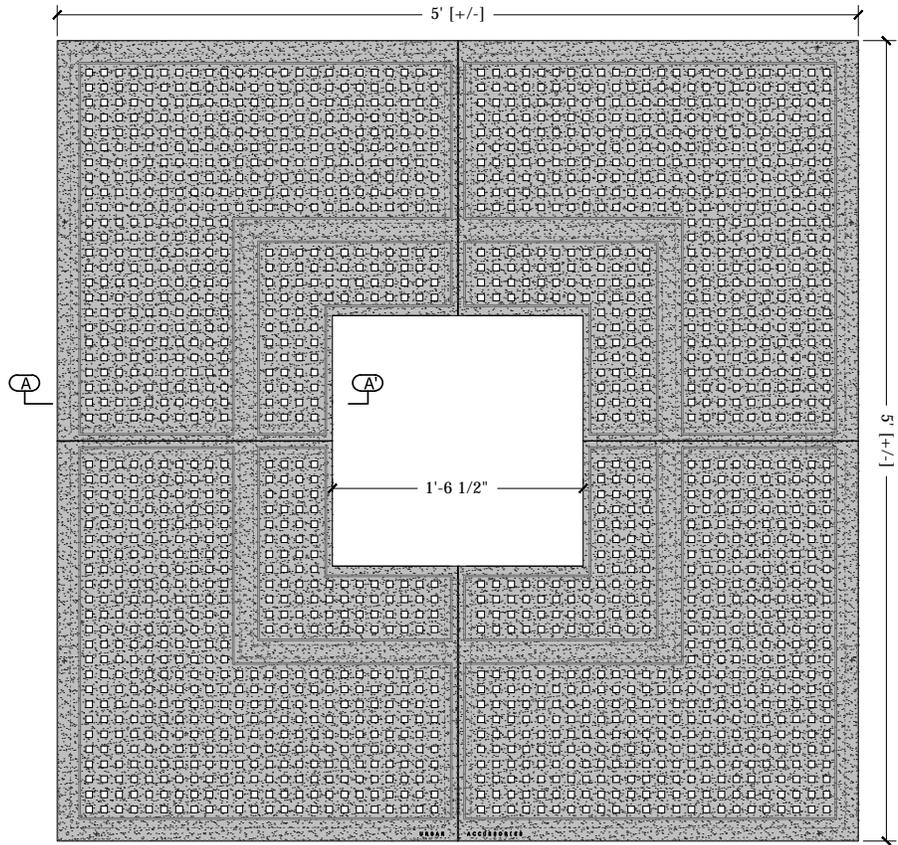
- Raw (standard)
 - Rust conditioner
 - Polyester Powder Coat*
 - Liquid Coat (wet paint)*
- Color:
- *Please specify standard UA color or mfr. name and color code.
 - Brush (bronze/nickel/aluminum only)
 - Polish (bronze/nickel/aluminum only)
 - Galvanized (grey iron and ductile iron only)
 - Other:

- Dimensions are nominal.

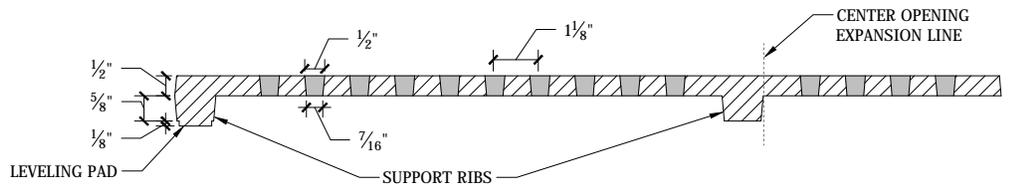
Notes

- 1) Cast in four pieces.
- 2) Grate is 1 1/4" thick at edge.
- 3) Center opening expansion at 2'-6".
- 4) No openings greater than 1/2", in conformance with ADA Accessibility Guidelines.
- 5) Grate weighs 476 lbs.

Comments:



Plan



Section A-A'

Tree Grate

5' Sq. Boston

Page 1 of 1 Date: 5/22/12



465 E. FIFTEENTH ST. sales@urbanaccessories.com
 TACOMA, WA 98421 www.urbanaccessories.com
 (877) 487-0488

SPECIFICATIONS

- Material will be high quality 100% recycled grey iron; ASTM A48 class 35b or better; hardness 170-223 brinnell (unless specified otherwise; see below).

Material:

- Grey iron ASTM A48 (standard)
- Aluminum, ASTM B26
- Ductile iron, (required for all load ratings higher than pedestrian) ASTM A536 class 65-45-12.
- Nickel bronze (ASTM B30)
- Bronze (ASTM B26)

- Finish will be natural patina of raw iron (unless specified otherwise; see below).

Finish:

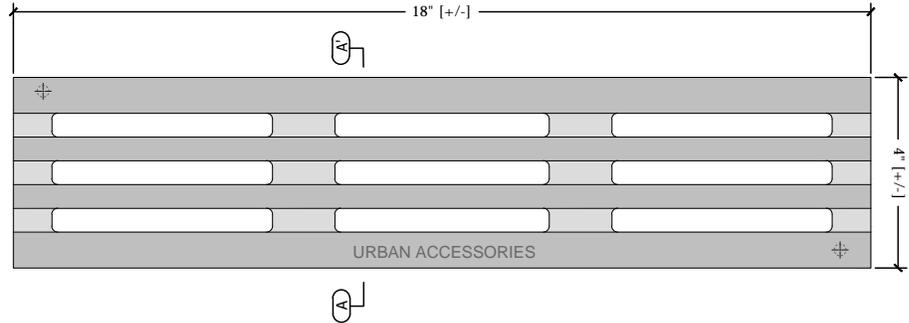
- Raw (standard)
 - Rust conditioner
 - Polyester Powder Coat*
 - Liquid Coat (wet paint)*
- Color:
- *Please specify standard UA color or mfr. name and color code.
 - Brush (bronze/nickel/aluminum only)
 - Polish (bronze/nickel/aluminum only)
 - Galvanized (grey iron and ductile iron only)
 - Other:

- Dimensions are nominal.

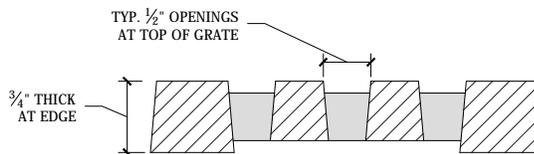
Notes

- 1) Grate is $\frac{3}{4}$ " thick at edge.
- 2) No openings greater than $\frac{1}{2}$ ", in conformance with ADA Accessibility Guidelines.
- 3) By default, the last grate in a trench drain will be sheared to meet a drain length specified at the time of order. Alternately, the trench drain length may be adjusted to the nearest whole grate (tolerances in the grate size WILL compound across the overall run). Please provide instructions in the comments section below.
- 4) 20.5 sq. in. open area per grate.
- 5) Grate weighs 10 lbs.

Comments:



Plan



Section A-A'

Trench Grate

4" x 18" Rainbow

Page 1 of 1 Date: 5/30/12



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SUMMARY OF FEEDBACK RECEIVED FROM THE LYNN CREEK PUBLIC REALM GUIDELINES OPEN HOUSE

Overall

- Number of people attending December 3 Open House: approximately 45
- Number of completed feedback forms received:
 - 16 forms submitted at open house
 - 24 on-line responses
 - **40 feedback forms received in total**

In addition, the District received feedback from the Advisory Committee on Disability Issues (ACDI) and HUB (cycling) group have submitted comments. These comments are attached, but are not reflected in the survey analysis %.

Analysis of the response to the seven questions on the feedback form is summarized below:

1. Inspiration and Overall Design Principles

Do you support the inspiration and overall design principles for the Lynn Creek Town Centre?

Strongly or somewhat agree:	69%
Somewhat or strongly disagree:	28%
Not sure:	3%

Summary of comments received:

- Good job with the design.
- Need a range of conveniences like grocery stores, liquor stores and restaurants
- Need a community garden, fresh local food. Is there a community centre or neighbourhood house?
- Marie Place/Orwell better served by high-rise development than low-rise apartment to provide same number of apartments but increase green space
- Important to protect Lynn Creek habitat
- Ensure that waste receptacles have cigarette butt containers, especially at bus shelters
- Expect increased use of Lynn creek corridor, more pets, more litter. Consider fencing to protect riparian area
- Ensure that users of mobility aids have access to and from public and private areas
- Concern that jobs to residents ratio will worsen in the area since no expansion of industrial area is proposed
- Need dog walking areas
- Mid-rise is best development form for the area

Public realm guideline response:

- The guidelines prioritize access for persons of all abilities with separated or buffered pedestrian routes; smooth walking surfaces and fully accessible entry routes to shops, facilities and other community amenities
- The town centre core will support a range of commercial uses, including a grocery store, as well as civic uses
- The District is currently developing new draft zoning to implement the Lynn Creek Employment Lands Strategy to facilitate business investment and job growth in the Lynn Creek Town Centre

2. Parks and Open Space Network

Does the network of parks, open spaces, and connections support the recreation, civic, and mobility needs of the community?

Strongly or somewhat agree:	86%
Somewhat or strongly disagree:	14%
Not sure:	0%

Summary of comments received:

- Must appeal to all developmental age groups
- Parks should be children friendly
- Spaces to sit and meet, water features to mask traffic noise
- Need improved trail access to and from Inter-River
- Educational/interpretive signage needed re sensitivity of Creek
- Tennis courts, soccer needed
- Pedestrian crossing of Lynn Creek needed
- Linear play elements along spine

Public realm guideline response:

The Lynn Creek Public Realm Guidelines propose a variety of programmed and unprogrammed parks and open space that can be used to meet the needs of people of all ages and abilities.

3. Active Lifestyle

Will the network of pedestrian and cycling connections serve the needs of all users?

Strongly or somewhat agree:	81%
Somewhat or strongly disagree:	19%
Not sure:	0%

Summary of comments received:

- Keep natural connections to the creek
- Good to see protected/separated cycling facilities
- Green spine will be a good walking route
- Need better connections north and to 2nd Narrows
- Need efficient ways to get around the bottleneck and improve traffic flow for this area

Public realm guideline response:

- The public realm design guidelines create a network of sidewalks, trails and greenways and cycling facilities to encourage active living. The 'green spine' serves as a primary north to south pedestrian route.

4. Streetscapes

Does the configuration of the “high street” support the establishment of an active, engaging, and walkable retail heart for this community?

Strongly or somewhat agree: 76%
Somewhat or strongly disagree: 18%
Not sure: 6%

Summary of comments received:

- Storefronts should be inward facing instead of facing the street
- Better use of land if sidewalks widened instead of medians
- Need bus shelters at each stop
- Deciduous trees, benches, water fountain
- Traffic calming
- Easy access to parking needed
- Extend High Street farther toward Main Street

Public realm guideline response:

- The “High Street” will have storefronts facing the street to enliven and create interest within the pedestrian realm
- Generous sidewalks, street trees and street furniture will add to the pedestrian experience

5. Street Furnishings and Planting

Does the proposed street furniture and planting selection complement the vision for this town centre?

Strongly or somewhat agree: 72%
Somewhat or strongly disagree: 14%
Not sure: 14%

Summary of comments received:

- Bike racks, preferably under cover
- Drought tolerance and native species
- Annual maintenance costs
- Concern that water features could be dangerous for kids and will require regular maintenance
- Trees to replace those lost with the new Keith bridge

Public realm guideline response:

- Safety concerns will be taken into account during detailed design of stormwater management and other water features
- The public realm guidelines include a variety of hardy, low maintenance native plants species for use in landscaping

6. Public Art

Does the location of public art at key gateways, plazas, and other significant locations support the vision for this town centre?

Strongly or somewhat agree: 76%
Somewhat or strongly disagree: 17%

Not sure: 7%

Summary of comments received:

- Locate in public gathering spaces, where well lit to reduce vandalism
- Functional public art
- Art with environmental messages encouraging care and consideration of Creek
- Focal points in parks
- Public art money better spent on amenities
- Aboriginal theme

Public realm guideline response:

- The public art strategy recommends both aesthetic and functional public art at key locations, building on local themes and helping to shape the identity and character for the town centre.

Overall Feedback

In general, do you believe the proposed Public Realm Design Guidelines will support this vision?

Strongly or somewhat support: 71%

Somewhat or strongly disagree: 19%

Not sure: 10%

Summary of comments received:

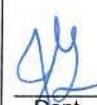
- Noted that not having to rely on cars is beneficial
- Need to fix traffic problems was noted
- Comment that affordable housing needs to be addressed
- Bike network is good but need safe connections to Second Narrows and Lynn Valley
- Improvements are needed to Phibbs Exchange and Park & Ride
- Speed limits should be lowered where bike lanes meet with auto routes
- Questioned if enough employment will be created for new residents
- Need increased frequency of bus service
- Like to see Friday night markets, artisans, music, beer gardens
- More public use of waterfront land along Lynn Creek

Public realm guideline response:

- Traffic infrastructure initiatives including Highway 1 interchange improvements and Phibbs Exchange are inter-jurisdictional projects. The District is working with Translink and other agencies to advance Highway 1 improvements to this area and to inform the re-design of Phibbs Exchange to better serve community needs.
- Work is ongoing to extend the DNV network of cycling infrastructure including safer links to Second Narrows and other connections beyond the town centre boundaries.
- The public realm guidelines propose few changes to the industrial lands allowing these areas to maintain their gritty and eclectic character. Implementation of the Lynn Creek Town Centre Employment Land Strategy will help create more flexible zoning to encourage business investment and job growth
- The central plaza will be designed to accommodate community events such as markets and festivals

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AGENDA INFORMATION	
<input checked="" type="checkbox"/> Regular Meeting	Date: <u>January 18, 2016</u>
<input type="checkbox"/> Committee of the Whole	Date: _____


 Dept.
 Manager


 GM/
 Director


 CAO

The District of North Vancouver REPORT TO COUNCIL

January 4, 2016

File: 08.3060.20/085.12

AUTHOR: Linda Brick, Deputy Municipal Clerk

SUBJECT: Bylaws 8138 and 8139: 756-778 Forsman Avenue

RECOMMENDATION:

THAT the "The District of North Vancouver Rezoning Bylaw 1334, (Bylaw 8138)" is given SECOND and THIRD Readings.

THAT the "Housing Agreement Bylaw 8139, 2015 (756 & 778 Forsman Ave.)" is given SECOND and THIRD Readings.

BACKGROUND:

Bylaws 8138 and 8139 received First Reading on November 2, 2015. A Public Hearing for Bylaw 8138 was held and closed on November 17, 2015.

The bylaws are now ready to be considered for Second and Third Readings by Council.

OPTIONS:

1. Give the bylaws Second and Third Readings; or,
2. Give no further Readings to the bylaws and abandon the bylaws at First Reading.

Respectfully submitted,



Linda Brick
Deputy Municipal Clerk

Attachments:

- The District of North Vancouver Rezoning Bylaw 1334, (Bylaw 8138)
- Housing Agreement Bylaw 8139, 2015 (756 & 778 Forsman Ave.)
- Public Hearing Minutes – November 17, 2015
- Staff Report – dated October 16, 2015

REVIEWED WITH:		
<input type="checkbox"/> Sustainable Community Dev.	_____	
<input checked="" type="checkbox"/> Development Services		
<input type="checkbox"/> Utilities	_____	
<input type="checkbox"/> Engineering Operations	_____	
<input type="checkbox"/> Parks	_____	
<input type="checkbox"/> Environment	_____	
<input type="checkbox"/> Facilities	_____	
<input type="checkbox"/> Human Resources	_____	
<input type="checkbox"/> Clerk's Office	_____	
<input type="checkbox"/> Communications	_____	
<input type="checkbox"/> Finance	_____	
<input type="checkbox"/> Fire Services	_____	
<input type="checkbox"/> ITS	_____	
<input type="checkbox"/> Solicitor	_____	
<input type="checkbox"/> GIS	_____	
<input type="checkbox"/> Real Estate	_____	
External Agencies:		
<input type="checkbox"/> Library Board	_____	
<input type="checkbox"/> NS Health	_____	
<input type="checkbox"/> RCMP	_____	
<input type="checkbox"/> NVRC	_____	
<input type="checkbox"/> Museum & Arch.	_____	
<input type="checkbox"/> Other:	_____	

The Corporation of the District of North Vancouver

Bylaw 8138

A bylaw to amend the District of North Vancouver Zoning Bylaw 3210, 1965

The Council for The Corporation of the District of North Vancouver enacts as follows:

1. Citation

This bylaw may be cited as “The District of North Vancouver Rezoning Bylaw 1334, (Bylaw 8138)”.

2. Amendments

2.1 The District of North Vancouver Zoning Bylaw 3210, 1965 is amended as follows:

(a) Section 301 (2) by inserting the following zoning designation:

“Comprehensive Development Zone 91 CD91”

(b) Part 4B Comprehensive Development Zone Regulations by inserting the following, inclusive of Schedule B:

“4B91 Comprehensive Development Zone 91 CD 91

The CD 91 zone is applied to:

756 Forsman Avenue, Lot C of Lot 6 Block A District Lot 613 Plan 20979,
PID: 005-225-957

778 Forsman Avenue, Lot A Block A District Lot 613 Group 1 New Westminster District
Plan BCP39525, PID: 027-780-228

4B 91-1 Intent

The purpose of the CD 91 Zone is to establish specific land use and development regulations for a 9 unit townhouse project.

4B 91- 2 Permitted Uses:

The following *principal* uses shall be permitted in the CD 91 Zone:

(a) Uses Permitted Without Conditions:

- (i) Residential building, multiple-family townhouse

(b) Conditional Uses:

Not Applicable

4B 91-3 Conditions of Use

Not Applicable

4B 91-4 Accessory Use

(a) *Accessory uses* are permitted and may include but are not necessarily limited to:

- (i) Home occupations in accordance with the regulations in Section 405 of the Zoning Bylaw, 1965

4B 91-5 Density

- (a) The maximum permitted density in the CD91 Zone is limited to a floor space ratio (FSR) of 0.45, inclusive of any density bonus for energy performance;
- (b) For the purposes of calculating floor space ratio, the area within the parking garage, storage (to a maximum of 100 sq ft per unit), balconies and landscape trellis are excluded.

4B 91-6 Amenities

(a) Despite subsection 4B91-5, density in the CD91 Zone is increased to a maximum floor space ratio of 0.7 FSR, inclusive of any density bonus for energy performance, if the owner:

1. Enters into a Housing Agreement prohibiting any restrictions preventing the owners in the project from renting their units; and
2. Contributes \$41,576.25 to the municipality to be used for any or all of the following amenities (with allocation to be determined by the municipality in its sole discretion): public art; park, trail, environmental, pedestrian or other public realm, infrastructure improvements; municipal, recreation or social

service facility or service / facility improvements; and/or the affordable housing fund.

4B 91-7 Maximum Principal Building Size:

Not applicable

4B 91-8 Setbacks:

- a) Buildings shall be set back from property lines to the closest building face as established by development permit and in accordance with the following regulations:

Setback	Buildings (Min Setback)
Front (west property line)	4.57m (15 ft) to the building face
Rear (east property line)	6.10m (20 ft) to the building face
Side (north)	1.83m (6 ft) to the building face
Side (south)	3.05m (10 ft) to the building face

- b) Projections at the ground level are permissible as follows:

Setback	Maximum Setback Reduction
Front Yard	1.83 m (6.0 ft)

- c) Balconies and trellises are excluded from any setback requirements.

4B 91-9 Building Orientation:

Not applicable

4B 91-10 Building Depth and Width:

Not applicable

4B 91-11 Coverage:

- (a) Building Coverage shall not exceed 45%.
- (b) Site Coverage shall not exceed 70%.

4B 91-12 Height:

The maximum permitted height for each building, inclusive of a 15% bonus for sloping roofs, is 12.1m (40 ft);

4B 91-13 Acoustic Requirements:

In the case of residential purposes, a development permit application shall require evidence in the form of a report and recommendations prepared by persons trained in acoustics and current techniques of noise measurements, demonstrating that the noise levels in those portions of the dwelling listed below shall not exceed the noise levels expressed in decibels set opposite such portions of the dwelling units:

Portion of Dwelling Unit	Noise Level (Decibels)
Bedrooms	35
Living and Dining rooms	40
Kitchen, Bathrooms and Hallways	45

4B 91-14 Flood Construction Requirements:

No basement, or habitable floor space, other than garage and storage space, shall be constructed below the established flood construction levels as identified in a flood hazard report prepared by a qualified registered professional engineer.

4B 91-15 Landscaping:

- (a) All land areas not occupied by buildings, structures, parking spaces, loading spaces, driveways, manoeuvring aisles and sidewalks shall be landscaped or finished in accordance with an approved landscape plan; and
- (b) All electrical kiosks and garbage and recycling container pads not located underground or within a building shall be screened with landscaping.

4B 91-16 Subdivision Requirements:

Not applicable

4B 91-17 Additional Accessory Structure Regulations:

Not applicable.

4B 91-18 Parking and Loading Regulations:

- (a) Parking spaces shall be provided on the basis of 2 spaces/unit plus 2 visitor spaces;
- (b) Not more than 7 spaces may be small car spaces;

- (c) Not more than 5 parking spaces may be in a tandem arrangement;
- (d) All parking spaces shall meet the minimum length and width standards established in Part 10 of the District of North Vancouver Zoning Bylaw.”
- (C) The Zoning Map is amended in the case of the lands illustrated on the attached map (Schedule A) by rezoning the land from the Residential Single Family 7200 Zone (RS3) to Comprehensive Development Zone 91 (CD 91).

READ a first time November 2, 2015

PUBLIC HEARING held November 17, 2015

READ a second time

READ a third time

Certified a true copy of “Rezoning Bylaw 1334 (Bylaw 8138)” as at Third Reading

Municipal Clerk

APPROVED by the Ministry of Transportation and Infrastructure on

ADOPTED

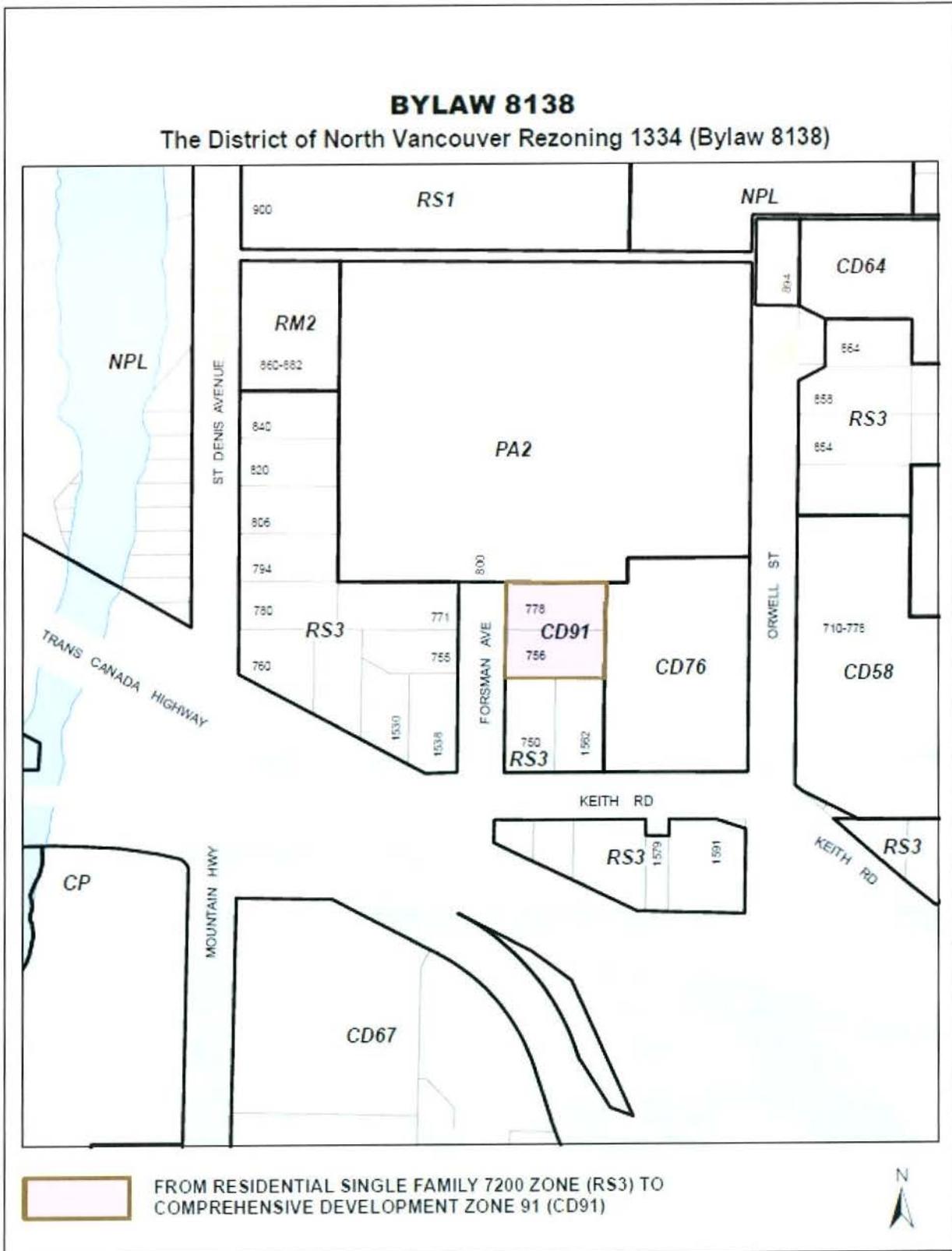
Mayor

Municipal Clerk

Certified a true copy

Municipal Clerk

Schedule A to Bylaw 8138



The Corporation of the District of North Vancouver

Bylaw 8139

A bylaw to enter into a Housing Agreement (756 & 778 Forsman Ave.)

The Council for The Corporation of the District of North Vancouver enacts as follows:

1. Citation

This bylaw may be cited as "Housing Agreement Bylaw 8139, 2015 (756 & 778 Forsman Ave.)".

2. Authorization to Enter into Agreement

2.1 The Council hereby authorizes a housing agreement between The Corporation of the District of North Vancouver and 1009198 B.C. Ltd., Inc.No. BC1009198 substantially in the form attached to this Bylaw as Schedule "A" with respect to the following lands:

- a) Lot C of Lot 6 Block A District Lot 613 Plan 20979, PID: 005-225-957
- b) Lot A Block A District Lot 613 Group 1 New Westminster District Plan BCP39525, PID: 027-780-228

3. Execution of Documents

The Mayor and Municipal Clerk are authorized to execute any documents required to give effect to the Housing Agreement.

READ a first time November 2, 2015

READ a second time

READ a third time

ADOPTED

Mayor

Municipal Clerk

Certified a true copy

Municipal Clerk

Schedule A to Bylaw 8139

SECTION 219 COVENANT – HOUSING AGREEMENT

This agreement dated for reference the ____ day of _____, 20__ is

BETWEEN:

1009198 B.C. Ltd., Inc.No. BC1009198

(the “Owner”)

AND:

THE CORPORATION OF THE DISTRICT OF NORTH VANCOUVER, a municipality incorporated under the *Local Government Act*, R.S.B.C. 1996, c.323 and having its office at 355 West Queens Road, North Vancouver, BC V7N 4N5

(the “District”)

WHEREAS:

1. The Owner is the registered owner of the Lands (as hereinafter defined);
2. The Owner wishes to obtain development permissions with respect to the Lands and wishes to create a condominium development which will contain residential strata units on the Lands;
3. Section 905 of the *Local Government Act* authorises the District, by bylaw, to enter into a housing agreement to provide for the prevention of rental restrictions on housing, and provides for the contents of the agreement; and
4. Section 219 of the *Land Title Act* (British Columbia) permits the registration in favour of the District of a covenant of a negative or positive nature relating to the use of land or a building thereon, or providing that land is to be built on in accordance with the covenant, or providing that land is not to be built on except in accordance with the covenant, or providing that land is not to be subdivided except in accordance with the covenant;

NOW THEREFORE in consideration of the mutual promises contained in it, and in consideration of the payment of \$1.00 by the District to the Owner (the receipt and sufficiency of which are hereby acknowledged by the Owner), the parties covenant and agree with each other as follows, as a housing agreement under Section 905 of the *Local Government Act*, as a contract and a deed under seal between the parties, and as a covenant under Section 219 of the *Land Title Act*, and the Owner hereby further covenants and agrees that neither the Lands nor any building constructed thereon shall be used or built on except in accordance with this Agreement::

1. **DEFINITIONS**

1.01 Definitions

In this agreement:

- (a) “*Development Permit*” means development permit No. _____ issued by the District;
- (b) “*Lands*” means land described in Item 2 of the *Land Title Act* Form C to which this agreement is attached;
- (c) “*Proposed Development*” means the proposed development containing not more than 9 Units to be constructed on the Lands in accordance with the Development Permit;
- (d) “*Unit*” means a residential dwelling strata unit in the Proposed Development; and
- (e) “*Unit Owner*” means the registered owner of a Dwelling Unit in the Proposed Development.

2. **TERM**

This Agreement will commence upon adoption by District Council of Bylaw 8138 and remain in effect until terminated by the District as set out in this Agreement.

3. **RENTAL ACCOMODATION**

3.01 Rental Disclosure Statement

No Unit in the Proposed Development may be occupied unless the Owner has:

- (a) before the first Unit is offered for sale, or conveyed to a purchaser without being offered for sale, filed with the Superintendent of Real Estate a Rental Disclosure Statement designating all of the Units as rental strata lots and imposing a ninety-nine (99) year rental period in relation to all of the Units pursuant to the *Strata Property Act* (or any successor or replacement legislation); and
- (b) given a copy of the Rental Disclosure Statement to each prospective purchaser of any Unit before the prospective purchaser enters into an agreement to purchase in respect of the Unit.

3.02 Rental Accommodation

The Units constructed on the Lands from time to time may always be used to provide rental accommodation as the Owner or a Unit Owner may choose from time to time.

3.03 Binding on Strata Corporation

This agreement shall be binding upon all strata corporations created by the subdivision of the Lands or any part thereof (including the Units) pursuant to the *Strata Property Act*, and upon all Unit Owners. .

3.04 Strata Bylaw Invalid

Any strata corporation bylaw which prevents, restricts or abridges the right to use any of the Units as rental accommodations shall have no force or effect.

3.05 No Bylaw

The strata corporation shall not pass any bylaws preventing, restricting or abridging the use of the Lands, the Proposed Development or the Units contained therein from time to time as rental accommodation.

3.06 Vote

No Unit Owner, nor any tenant or mortgagee thereof, shall vote for any strata corporation bylaw purporting to prevent, restrict or abridge the use of the Lands, the Proposed Development and the units contained therein from time to time as rental accommodation.

3.07 Notice

The Owner will provide notice of this Agreement to any person or persons intending to purchase a Unit prior to any such person entering into an agreement of purchase and sale, agreement for sale, or option or similar right to purchase as part of the Disclosure Statement for any part of the Proposed Development prepared by the Owner pursuant to the *Real Estate Development Marketing Act*.

4. DEFAULT AND REMEDIES

4.01 Notice of Default

The District may, acting reasonably, give to the Owner written notice to cure a default under this Agreement within thirty (30) days of delivery of the notice. The notice must specify the nature of the default. The Owner must act with diligence to correct the default within the time specified.

4.02 Costs

The Owner will pay to the District upon demand all the District's costs of exercising its rights or remedies under this Agreement, on a full indemnity basis.

4.03 Damages an Inadequate Remedy

The Owner acknowledges and agrees that in the case of a breach of this Agreement which is not fully remediable by the mere payment of money and promptly so remedied,

the harm sustained by the District and to the public interest will be irreparable and not susceptible of adequate monetary compensation.

4.04 Equitable Remedies

Each party to this Agreement, in addition to its rights under this Agreement or at law, will be entitled to all equitable remedies including specific performance, injunction and declaratory relief, or any of them, to enforce its rights under this Agreement.

4.05 No Penalty or Forfeiture

The Owner acknowledges and agrees that it is entering into this Agreement to benefit the public interest in providing rental accommodation, and that the District's rights and remedies under this Agreement are necessary to ensure that this purpose is carried out, and the District's rights and remedies under this Agreement are fair and reasonable and ought not to be construed as a penalty or forfeiture.

4.06 Cumulative Remedies

No reference to nor exercise of any specific right or remedy under this Agreement or at law or at equity by any party will prejudice, limit or preclude that party from exercising any other right or remedy. No right or remedy will be exclusive or dependent upon any other right to remedy, but any party, from time to time, may exercise any one or more of such rights or remedies independently, successively, or in combination. The Owner acknowledges that specific performance, injunctive relief (mandatory or otherwise) or other equitable relief may be the only adequate remedy for a default by the Owner under this Agreement.

5. LIABILITY

5.01 Indemnity

Except if arising directly from the negligence of the District or its employees, agents or contractors, the Owner will indemnify and save harmless each of the District and its board members, officers, directors, employees, agents, and elected or appointed officials,, and their heirs, executors, administrators, personal representatives, successors and assigns, from and against all claims, demands, actions, loss, damage, costs and liabilities that all or any of them will or may be liable for or suffer or incur or be put to any act or omission by the Owner or its officers, directors, employees, agents, contractors, or other persons for whom the Owner is at law responsible, or by reason of or arising out of the Owner's ownership, operation, management or financing of the Proposed Development or any part thereof.

5.02 Release

The Owner hereby releases and forever discharges the District, its elected officials, board members, officers, directors, employees and agents, and its and their heirs, executors, administrators, personal representatives, successors and assigns from and against all

claims, demands, damages, actions or causes of action by reason of or arising out of advice or direction respecting the ownership, operation or management of the Proposed Development or any part thereof which has been or hereafter may be given to the Owner by all or any of them.

5.03 Survival

The covenants of the Owner set out in Sections 5.01 and 5.02 will survive termination of this Agreement and continue to apply to any breach of the Agreement or claim arising under this Agreement during the ownership by the Owner of the Lands or any Unit therein, as applicable.

6. GENERAL PROVISIONS

6.01 District's Power Unaffected

Nothing in this Agreement:

- (a) affects or limits any discretion, rights, powers, duties or obligations of the District under any enactment or at common law, including in relation to the use or subdivision of land;
- (b) affects or limits any enactment relating to the use of the Lands or any condition contained in any approval including any development permit concerning the development of the Lands; or
- (c) relieves the Owner from complying with any enactment, including the District's bylaws in relation to the use of the Lands.

6.02 Agreement for Benefit of District Only

The Owner and District agree that:

- (a) this Agreement is entered into only for the benefit of the District;
- (b) this Agreement is not intended to protect the interests of the Owner, any Unit Owner, any Occupant or any future owner, occupier or user of any part of the Proposed Development, including any Unit, or the interests of any third party, and the District has no obligation to anyone to enforce the terms of this Agreement; and
- (c) The District may at any time terminate this Agreement, in whole or in part, and execute a release and discharge of this Agreement in respect of the Proposed Development or any Unit therein, without liability to anyone for doing so.

6.03 Agreement Runs With the Lands

This Agreement burdens and runs with the Lands and any part into which any of them may be subdivided or consolidated, by strata plan or otherwise. All of the covenants and agreements contained in this Agreement are made by the Owner for itself, its successors and assigns, and all persons who acquire an interest in the Lands or in any Unit after the date of this Agreement.

6.04 Release

The covenants and agreements on the part of the Owner and any Unit Owner and herein set forth in this Agreement have been made by the Owner and any Unit Owner as contractual obligations as well as being made pursuant to Section 905 of the *Local Government Act* (British Columbia) and as such will be binding on the Owner and any Unit Owner, except that neither the Owner nor any Unit Owner shall be liable for any default in the performance or observance of this Agreement occurring after such party ceases to own the Lands or a Unit as the case may be.

6.05 Priority of This Agreement

The Owner will, at its expense, do or cause to be done all acts reasonably necessary to ensure this Agreement is registered against the title to each Unit in the Proposed Development, including any amendments to this Agreement as may be required by the Land Title Office or the District to effect such registration.

6.06 Agreement to Have Effect as Deed

The District and the Owner each intend by execution and delivery of this Agreement to create both a contract and a deed under seal.

6.07 Waiver

An alleged waiver by a party of any breach by another party of its obligations under this Agreement will be effective only if it is an express waiver of the breach in writing. No waiver of a breach of this Agreement is deemed or construed to be a consent or waiver of any other breach of this Agreement.

6.08 Time

Time is of the essence in this Agreement. If any party waives this requirement, that party may reinstate it by delivering notice to another party.

6.09 Validity of Provisions

If a Court of competent jurisdiction finds that any part of this Agreement is invalid, illegal, or unenforceable, that part is to be considered to have been severed from the rest of this Agreement and the rest of this Agreement remains in force unaffected by that holding or by the severance of that part.

6.10 Extent of Obligations and Costs

Every obligation of a party which is set out in this Agreement will extend throughout the Term and, to the extent that any obligation ought to have been observed or performed prior to or upon the expiry or earlier termination of the Term, such obligation will survive the expiry or earlier termination of the Term until it has been observed or performed.

6.11 Notices

All notices, demands, or requests of any kind, which a party may be required or permitted to serve on another in connection with this Agreement, must be in writing and may be served on the other parties by registered mail, by facsimile transmission, or by personal service, to the following address for each party:

If to the District:

District Municipal Hall
355 West Queens Road
North Vancouver, BC V7N 4N5

Attention: Planning Department
Facsimile: (604) 984-9683

If to the Owner:

Attention:
Facsimile: (604)

If to the Unit Owner:

The address of the registered owner which appears on title to the Unit at the time of notice.

Service of any such notice, demand, or request will be deemed complete, if made by registered mail, 72 hours after the date and hour of mailing, except where there is a postal service disruption during such period, in which case service will be deemed to be complete only upon actual delivery of the notice, demand or request; if made by facsimile transmission, on the first business day after the date when the facsimile transmission was transmitted; and if made by personal service, upon personal service being effected. Any party, from time to time, by notice in writing served upon the other parties, may designate a different address or different or additional persons to which all notices, demands, or requests are to be addressed.

6.12 Further Assurances

Upon request by the District, the Owner will promptly do such acts and execute such documents as may be reasonably necessary, in the opinion of the District, to give effect to this Agreement.

6.13 Enuring Effect

This Agreement will enure to the benefit of and be binding upon each of the parties and their successors and permitted assigns.

7. **INTERPRETATION**

7.01 References

Gender specific terms include both genders and include corporations. Words in the singular include the plural, and words in the plural include the singular.

7.02 Construction

The division of this Agreement into sections and the use of headings are for convenience of reference only and are not intended to govern, limit or aid in the construction of any provision. In all cases, the language in this Agreement is to be construed simply according to its fair meaning, and not strictly for or against either party.

7.03 No Limitation

The word “including” when following any general statement or term is not to be construed to limit the general statement or term to the specific items which immediately follow the general statement or term similar items whether or not words such as “without limitation” or “but not limited to” are used, but rather the general statement or term is to be construed to refer to all other items that could reasonably fall within the broadest possible scope of the general statement or term.

7.04 Terms Mandatory

The words “must” and “will” and “shall” are to be construed as imperative.

7.05 Statutes

Any reference in this Agreement to any statute or bylaw includes any subsequent amendment, re-enactment, or replacement of that statute or bylaw.

7.06 Entire Agreement

- (d) This is the entire agreement between the District and the Owner concerning its subject, and there are no warranties, representations, conditions or collateral agreements relating to this Agreement, except as included in this Agreement.
- (e) This Agreement may be amended only by a document executed by the parties to this Agreement and by bylaw, such amendment to be effective only upon adoption by District Council of a bylaw to amend Bylaw 8139.

7.07 Governing Law

This Agreement is to be governed by and construed and enforced in accordance with the laws of British Columbia.

As evidence of their agreement to be bound by the terms of this instrument, the parties hereto have executed the *Land Title Act* Form C that is attached hereto and forms part of this Agreement.

CONSENT AND PRIORITY AGREEMENT

GIVEN THAT:

1. _____ (the "Owner") is the Registered Owner of the Land described in Item 2 of Page 1 of the Form C (the "Land");
2. The Owner granted _____ (the "Prior Chargeholder") a Mortgage and Assignment of Rents registered against title to the Land in the Lower Mainland Land Title Office (the "LTO") under Nos. _____, as extended by _____ and _____, as extended by _____ (together, the "Prior Charge");
3. The Owner granted to THE CORPORATION OF THE DISTRICT OF NORTH VANCOUVER (the "District") a Covenant attached to this Agreement and registered against title to the Land in the LTO immediately before registration of this Agreement (the "Subsequent Charge"); and
4. Section 207 of the *Land Title Act* permits the Prior Chargeholder to grant priority over a charge to the District as Subsequent Chargeholder.

THEREFORE this Agreement is evidence that in consideration of \$1.00 and other good and valuable consideration received by the Prior Chargeholder from the District (the receipt and sufficiency of which the Prior Chargeholder acknowledges):

1. The Prior Chargeholder consents to the granting and registration of the Subsequent Charge and the Prior Chargeholder agrees that the Subsequent Charge shall be binding upon their interest in and to the Land.
2. The Prior Chargeholder grants to the District, as a Subsequent Chargeholder, priority for the Subsequent Charge over the Prior Chargeholder's right, title and interest in and to the Land, and the Prior Chargeholder postpones the Prior Charge and all of their right, title and interest thereunder to the Subsequent Charge as if the Subsequent Charge had been executed, delivered and registered prior to the execution, delivery and registration of the Prior Charge.

As evidence of its agreement to be bound by the terms of this instrument, the Prior Chargeholder has executed the Land Title Office Form C to which this Agreement is attached and which forms part of this Agreement.

**DISTRICT OF NORTH VANCOUVER
PUBLIC HEARING**

REPORT of the Public Hearing held in the Council Chambers of the Municipal Hall, 355 West Queens Road, North Vancouver, B.C. on Tuesday, November 17, 2015 commencing at 7:00 p.m.

Present: Acting Mayor D. MacKay-Dunn
Councillor R. Bassam
Councillor M. Bond
Councillor J. Hanson
Councillor R. Hicks

Absent: Mayor R. Walton
Councillor L. Muri

Staff: Mr. D. Milburn, Deputy General Manager, Planning & Permits
Mr. J. Gordon, Manager – Administrative Services
Ms. C. Peters, Planner
Ms. S. Dale, Confidential Council Clerk

The District of North Vancouver Rezoning Bylaw 1334 (Bylaw 8138)

Purpose of Bylaw:

Bylaw 8138 proposes to amend the District's Zoning Bylaw to create a new Comprehensive Development Zone 91 (CD91) and rezone the subject site from Residential Single Family 7200 Zone (RS3) to Comprehensive Development Zone 91 (CD91) to allow the development of a nine unit townhouse project.

1. OPENING BY THE MAYOR

Acting Mayor MacKay-Dunn welcomed everyone and advised that the purpose of the Public Hearing was to receive input from the community and staff on the proposed bylaw as outlined in the Notice of Public Hearing.

In Acting Mayor MacKay-Dunn's preamble he addressed the following:

- All persons who believe that their interest in property is affected by the proposed bylaw will be afforded a reasonable opportunity to be heard and to present written submissions;
- Use the established speakers list. At the end of the speakers list, the Chair may call on speakers from the audience;
- You will have 5 minutes to address Council for a first time. Begin your remarks to Council by stating your name and address;
- After everyone who wishes to speak has spoken once, speakers will then be allowed one additional five minute presentation;
- Any additional presentations will only be allowed at the discretion of the Chair; and,

- All members of the audience are asked to refrain from applause or other expressions of emotion. Council wishes to hear everyone's views in an open and impartial forum.

The Municipal Clerk stated that:

- Council is here to listen to the public, not to debate the merits of the bylaw;
- The Clerk has a binder containing documents and submissions related to this bylaw which Council has received and which you are welcome to review;
- Everyone at the Hearing will be provided an opportunity to speak. If necessary, we will continue the Hearing on a second night; and,
- At the conclusion of the public input Council may request further information from staff which may or may not require an extension of the hearing, or Council may close the hearing after which Council should not receive further new information from the public.

2. INTRODUCTION OF BYLAW BY CLERK

Mr. James Gordon, Manager – Administrative Services, introduced the proposed bylaw stating that Bylaw 8138 proposes to amend the District's Zoning Bylaw to create a new Comprehensive Development Zone 91 (CD91) and rezone the subject site from Residential Single Family 7200 Zone (RS3) to Comprehensive Development Zone 91 (CD91) to allow the development of a nine unit townhouse project.

3. PRESENTATION BY STAFF

Ms. Casey Peters, Planner, provided an overview of the proposal elaborating on the Clerk's introduction.

Ms. Peters advised that:

- The site consists of two single family lots on the east side of Forsman Avenue;
- Adjacent properties consist of single family lots to the west and south, Lynnmour Elementary School to the north, townhouses (under construction) to the east and the Trans-Canada Highway to the South;
- The Official Community Plan designates the surrounding single family properties as Residential Level 3: Attached Residential;
- The density of the proposal is 0.7 FSR with exclusions for parking, storage and balconies;
- The project consists of nine townhouses in three buildings;
- Two duplexes are sited adjacent to the driveway entrance and a five unit building are sited on the east side of the site;
- The townhouses are three storeys each with their own at grade parking garage;
- All units have three bedrooms on the upper level and range in size from 1,644 sq. ft. to 1,816 sq. ft., excluding the garages;
- The proposal includes two parking spaces per unit and the applicant has added two visitor spaces to the project;
- The District's Community Amenity Contribution Policy requires an amenity contribution of \$41,575;
- It is anticipated that the Community Amenity Contribution from this development will include contributions toward public art; park, trail, environmental, pedestrian or other

- public realm, infrastructure improvements; municipal recreation or social service facility or service/facility improvements; and/or the affordable housing fund;
- Compliance with the Green Building Strategy is mandatory with the project targeting an energy performance rating of Energuide 80 and will achieve a building performance equivalent to Built Green “Gold” 2011;
- The developer will be required to provide a Construction Traffic Management plan as a condition of a Development Permit;
- The Construction Traffic Management plan must minimize construction impacts on pedestrian and vehicle movement and the plan is required to be approved by the District prior to issuance of a building permit; and,
- Particular attention will be paid to the impact on the adjacent Lynnmour School regarding pedestrian movement and student drop-off and pick-up.

4. PRESENTATION BY APPLICANT

Mr. Kanaris Demetre Lazos, Royal Columbia Development Corp:

- Provided an overview of the proposed site plan and building design;
- Noted that two visitor spaces have been added and a covenant is required to secure these two spaces as visitor stalls;
- Commented that the proposed development will provide affordable housing;
- Opined that the proposed townhouses will enhance the character of the neighbourhood;
- Advised that the project will be built in accordance with the flood guidelines;
- Noted that Lynnmour School will be fenced to protect the children from construction;
- Stated that the townhouses to the east currently under construction will be complete before this project begins; and,
- Advised that the Community Amenity Contribution package will include road improvements.

Mr. Gen Yada, Gen Yada Architect:

- Commented that the proposed development will provide affordable housing;
- Provided an overview of the building design;
- Noted that the proposed development is in fitting with the character of the neighbourhood;
- Noted that all units have three bedrooms on the upper level and range in size from 1,644 sq. ft. to 1,816 sq. ft.; and,
- Commented on the spacious internal courtyard.

Mr. Steve Wong, Landscape Architect:

- Highlighted that the landscape design objectives include:
 - Sustainable landscape that reflects the character of the North Shore;
 - Refined urban streetscape along Forsman frontage including lighting and public art;
 - Meets development requirements of the flood hazard assessment report;
 - Enhanced storm water management; and,
 - Resident amenities include benches, tables, arbours and decorative pavers.

5. REPRESENTATIONS FROM THE PUBLIC

Nil

5. QUESTIONS FROM COUNCIL

In response to a question from Council regarding parking, staff advised that vehicle access to the site is off Forsman Avenue between the two front buildings and the proposal includes twenty parking stalls. The duplex units have two spaces per unit in a side by side arrangement. The five unit building proposes a one car garage with a second car space in a tandem arrangement in front of the garage. Two additional visitor spaces are proposed on either end of the five unit building. The proposal includes seven small car spaces (35%) and 13 standard car spaces (65%). The visitor spaces are both standard car spaces and each unit has one standard car space.

Council expressed concerns with tandem parking.

Staff advised that on-street parking adjacent to the proposed development is restricted during school hours as well as overnight from December to March to allow for snow removal.

In response to a question from Council regarding the Bicycle Master Plan, staff advised that a dedicated bike path along East Keith Road is proposed which would allow children to ride their bikes to Lynnmour School.

7. COUNCIL RESOLUTION

MOVED by Councillor BASSAM

SECONDED by Councillor HANSON

THAT the November 17, 2015 Public Hearing be closed;

AND THAT "The District of North Vancouver Rezoning Bylaw 1334 (Bylaw 8138)" be returned to Council for further consideration.

CARRIED
(7:29 p.m.)

CERTIFIED CORRECT:



Confidential Council Clerk

AGENDA INFORMATION	
<input checked="" type="checkbox"/> Regular Meeting	Date: <u>Nov 2, 2015</u>
<input type="checkbox"/> Workshop (open to public)	Date: _____


 Dept.
 Manager


 GM/
 Director


 CAO

The District of North Vancouver REPORT TO COUNCIL

October 16, 2015
 File: 08.3060.20/085.12

AUTHOR: Casey Peters, Community Planner

SUBJECT: BYLAWS 8138 AND 8139: REZONING AND HOUSING AGREEMENT FOR A 9 UNIT TOWNHOUSE PROJECT: 756-778 FORSMAN AVENUE

RECOMMENDATIONS: It is recommended that:

1. Bylaw 8138, which rezones the subject site from Residential Single Family 7200 Zone (RS3) to Comprehensive Development 91 (CD91) to enable the development of a 9 unit residential townhouse project, be given First Reading;
2. Bylaw 8139, which authorizes a Housing Agreement to prevent future rental restrictions, be given First Reading; and
3. Bylaw 8138 be referred to a Public Hearing.

REASON FOR REPORT:

The proposed project requires Council's consideration of:

- Bylaw 8138 to rezone the subject properties; and
- Bylaw 8139 to authorize entry into a Housing Agreement to ensure that owners are not prevented from renting their units.

SUMMARY:

The applicant proposes to redevelop 2 single family lots located at 756-778 Forsman Avenue for a 9 unit townhouse project which requires rezoning and issuance of a development permit. The Rezoning Bylaw and Housing Agreement Bylaw are recommended for Introduction and the Rezoning Bylaw is recommended for referral to a Public Hearing.



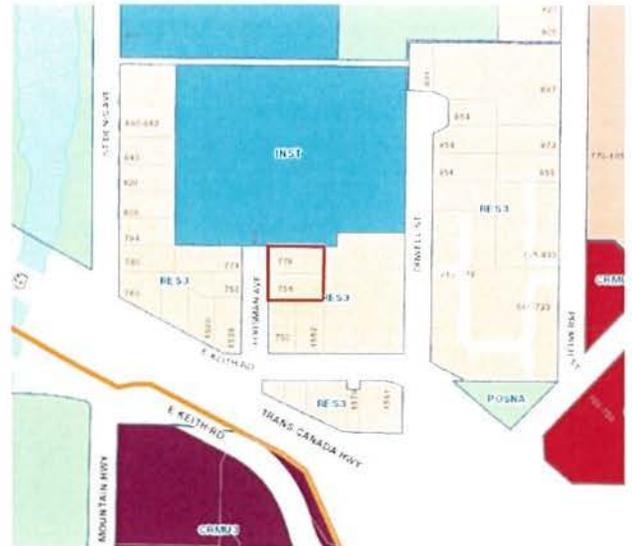
BACKGROUND:

Official Community Plan

The subject properties are designated as *Residential Level 3: Attached Residential* in the District Official Community Plan (OCP), which envisions ground oriented multifamily housing up to approximately 0.8 FSR.

The proposed townhouse units are all 3 bedroom units, which will be attractive to families, and as such, the proposal responds to Goal #2 of the OCP to “encourage and enable a diverse mix of housing types...to accommodate the lifestyles and needs of people at all stages of life.” It also addresses the intent of the housing diversity policies in Section 7.1 of the OCP by providing units suitable for families (Policy 7.1.4).

The Lynnmour Inter-River Local Plan Reference Policy document designated this site as “Low Density Multi-Family Housing” up to 0.7 FSR. A plan goal of the Lynnmour Inter-River Local Plan Reference Policy document was “to support the primarily family orientation of the residential area, while ensuring any new development contributes directly to the overall improvement of the community”.



The density of the proposal is 0.7 FSR with exclusions for parking, storage and balconies and therefore compliant with the Official Community Plan and the Lynnmour Inter-River Local Plan Reference Policy document. The Lynnmour Inter-River design guidelines support the proposed exemptions for storage areas and garages.

Zoning:

The subject properties are zoned Residential Single Family 7200 Zone (RS3) and therefore rezoning is required to permit this multi-family project. Bylaw 8138 proposes the establishment of a new Comprehensive Development Zone 91 (CD91) tailored specifically to this project.

Development Permit

The subject lots are designated as Development Permit Areas for the following purposes:

- Form and Character of Multi-Family Development (Ground-Oriented Housing);
- Energy and Water Conservation and Greenhouse Gas Emission Reductions; and
- Protection from Natural Hazards (Creek Hazard).

SUBJECT: BYLAWS 8138 AND 8139: REZONING AND HOUSING AGREEMENT FOR A 9 UNIT TOWNHOUSE PROJECT: 756-778 FORSMAN AVENUE

October 16, 2015

Page 3

A detailed development permit report, outlining the projects' compliance with the applicable DPA guidelines will be provided for Council's consideration at the Development Permit stage should the rezoning advance.

Strata Rental Protection Policy

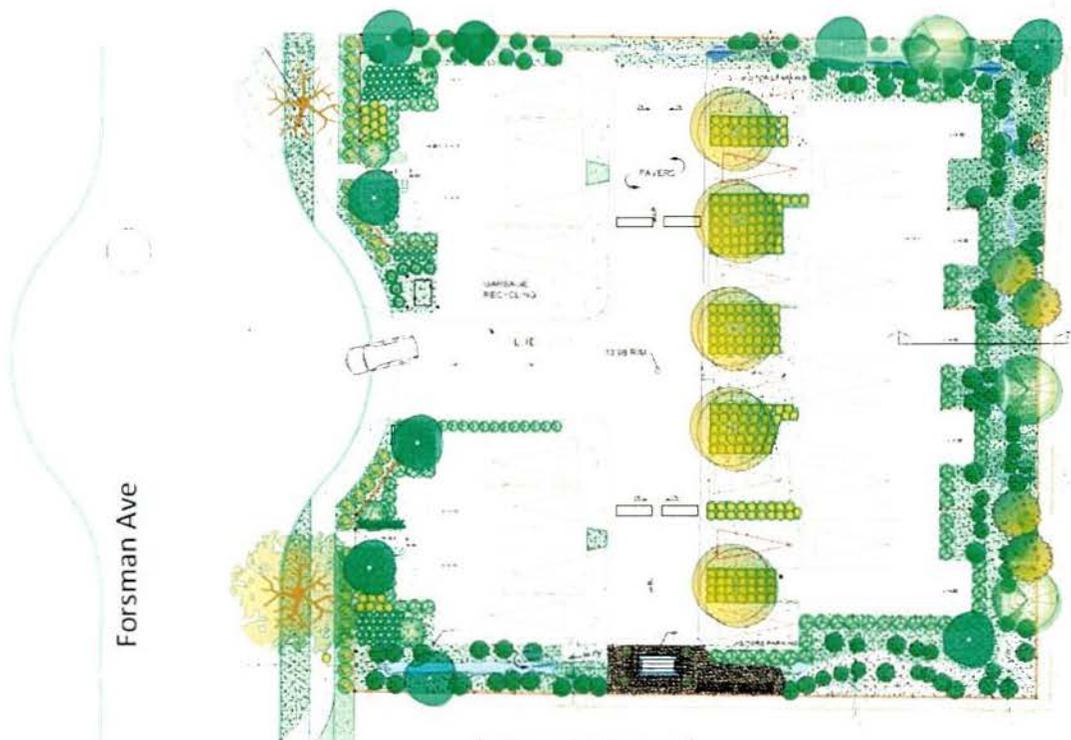
Corporate Policy 8-3300-2 "Strata Rental Protection Policy" applies to this project as the rezoning application would permit development of more than five units. The policy requires a Housing Agreement to ensure that future strata bylaws do not prevent owners from renting their units and Bylaw 8139 is provided to implement that Policy.



ANALYSIS

The Site and Surrounding Area:

The site consists of 2 single family lots on the east side of Forsman Avenue. Adjacent properties consist of single family lots (zoned RS3) to the west and south, Lynnmour Elementary School to the north, townhouses (under construction) to the east and the Trans-Canada Highway to the South. The OCP designates the surrounding single family properties as Residential Level 3: Attached Residential.



Site Plan

Project Description:

Site Plan/Building Description:

The project consists of nine townhouses in three buildings. Two duplexes are sited adjacent to the driveway entrance and a five unit building on the east side of the site as illustrated on the Site Plan.

The townhouses are three storeys each with their own at grade parking garage. The garages are accessed off one driveway from Forsman Avenue. All the units have 3 bedrooms on the upper level and range in size from 153m² (1,644 sq ft) to 169m² (1,816 sq ft), excluding the garages. The individual buildings are approximately 12.3m (40.5 ft) in height.



Parking

Vehicle access to the site is off Forsman Avenue between the two front buildings and the proposal includes 20 parking stalls. The duplex units have two spaces per unit in a side by side arrangement. The 5 unit building proposes a one car garage with a second car space in a tandem arrangement in front of the garage. Two additional visitor spaces are proposed on

either end of the 5 unit building.

The proposal includes 7 small car spaces (35%) and 13 standard car spaces (65%). The visitor spaces are both standard car spaces and each unit has one standard car space.

Landscaping

The landscaping is included at the perimeter of the site and along on the interior drive aisles. A swale is proposed around the exterior of the site to address storm water management on the site. Benches and an arbour are proposed on the south side of the project to create a space for the residents to sit.



BENCHES

The proposed landscaping will provide an improvement to Forsman Avenue and the entrance to the Lynnmour Elementary school. In addition, there may be opportunities to further improve the entrance to the school with the use of the Community Amenity Contribution for this project.

Engineering

The off-site engineering works include the creation of half of a cul de sac in front of the lot. The site on the west side of Forsman will finish the cul de sac when it redevelops. The proposal will also install a side walk, new street lighting, and improvements to the storm network.

The site is located within a Development Permit area for Creek Hazard and the applicant has submitted a report from Northwest Hydraulic Consultants that states that the proposal will allow for unimpeded flow of floodwaters and all habitable areas are set above the required flood construction level.

Acoustic Regulations

Bylaw 8138 includes the District's residential acoustic regulations for maximum noise levels in the bedrooms, living areas and other areas of the units. The applicant will be required to submit a report from a qualified noise consultant confirming the building design will enable these standards to be met.

Reduced copies of site, architectural and landscaping plans are included as Attachment A for Council's reference.

IMPLEMENTATION:

Implementation of this project will require consideration of a rezoning bylaw, Bylaw 8138, and a Housing Agreement Bylaw, Bylaw 8139, as well as issuance of a development permit and registration of legal agreements.

Bylaw 8138 (Attachment B) rezones the subject properties from Single Family Residential 7200 Zone (RS3) to a new Comprehensive Development 91 Zone (CD91) which:

- establishes the multi-family residential use;
- allows home occupations as an accessory use;
- establishes a base density FSR (Floor Space Ratio) of 0.45;
- establishes a density bonus to an FSR of 0.7 subject to payment of a \$41,576.25 CAC and entering into a housing agreement to restrict future strata rental restrictions;
- establishes setback, height, building coverage and site coverage regulations;
- incorporates acoustic requirements; and
- establishes parking regulations specific to this project.

Bylaw 8139, (Attachment C) authorizes the District to enter into a Housing Agreement to ensure that the proposed units remain available as rental units.

A contribution of \$24,146.15 will be required to the dyke infrastructure fund for future maintenance of the flood works installed in the Inter-River area. This contribution will be collected prior to adoption of Bylaw 8139.

In addition, the following legal agreements will be required prior to zoning bylaw adoption to secure:

- a housing agreement to ensure that owners are not prevented from renting their units;
- a right of way to secure pedestrian access over the proposed sidewalk;
- a parking covenant to secure two parking spaces as visitor stalls;
- a green building covenant;
- a stormwater management covenant;
- a covenant to ensure that the project is built in accordance with the flood report;
- an engineering servicing agreement; and
- a consolidation plan with road dedication.

COMMUNITY AMENITY CONTRIBUTION:

The District's Community Amenity Contribution (CAC) Policy requires an amenity contribution for projects including an increase in residential density. In this case, a CAC of \$41,575 has been calculated and this amount is included in the proposed CD91 Zone. It is anticipated that the CACs from this development will include contributions toward public art; park, trail, environmental, pedestrian or other public realm, infrastructure improvements; municipal, recreation or social service facility or service / facility improvements; and/or the affordable housing fund.

GREEN BUILDING MEASURES:

Compliance with the Green Building Strategy is mandatory given the need for rezoning and the project is targeting an energy performance rating of Energuide 80 and will achieve a building performance equivalent to Built Green "Gold" 2011.

CONCURRENCE:

Staff

The project has been reviewed by staff from Environment, Permits, Parks, Engineering, Policy Planning, Urban Design, Transportation Planning, the Fire Department and the Arts Office.

Advisory Design Panel

The application was considered by the Advisory Design Panel on January 17, 2013 and the panel recommended approval of the project subject to a review of enhanced weather protection; of the drive court for use of vehicles and children's play; the selected material palette; and the opportunity for additional glazing on the north and south elevations.

The applicant has noted that additional weather protection measures have been added over the entrances to the units. Speed bumps are being added within the driveways on site to slow cars and provide a visual cue for drivers that children may be at play. The applicant has provided clarification of the brick selection to improve harmony of the selected material palette. Windows have been added to the building elevations that face the school and that the north end of the buildings also have balconies.

PUBLIC INPUT:

Public Information Meeting

The applicant held a facilitated Public Information Meeting on April 30, 2013. The meeting was attended by 12 residents.

There was a concern expressed at the meeting regarding the challenges of street parking in the area. At the time of the meeting no visitor stalls were proposed on site which was consistent with the Lynnmour Design Guidelines. The proposal includes two spaces per unit and the applicant has added two visitor spaces to the project. The spaces will be marked as visitor spaces and a covenant is required to secure these two spaces as visitor stalls.

The neighbours also noted that indoor parking can be used for storage resulting in use of street spaces by residents. The five unit building proposes a one car garage with a second car space in a tandem arrangement in front of the garage. This design results in several outdoor spaces which will be used for parking rather than storage.

Additional questions were asked regarding the future of the lots adjacent to the subject property, the size of the proposed units, the anticipated sales prices for the units, and the District's approval process.

CONSTRUCTION MANAGEMENT PLAN:

In order to address the goal to reduce development's impact on pedestrian and vehicular movements, the developer will be required to provide a construction traffic management plan as a condition of a Development Permit. The Construction Management plan must minimize construction impacts on pedestrian and vehicle movement. The plan is required to be approved by the District prior to issuance of a building permit.

In particular, the 'construction traffic management' must:

1. Coordinate with other construction projects in the area;
2. Provide safe passage for pedestrians, cyclists, and vehicle traffic;
3. Outline roadway efficiencies (i.e. location of traffic management signs and flaggers);
4. Provide a point of contact for all calls and concerns;
5. Provide a sequence and schedule of construction activities;
6. Ascertain a location for truck marshalling and trade vehicle parking which is acceptable to the District and minimizes impacts to neighbourhoods; and
7. Include a communication plan to notify surrounding school and residents.

Particular attention will be paid to the impact on the adjacent Lynnmour School regarding pedestrian movement and student drop-off and pick-up.

CONCLUSION:

This project is consistent with the directions established in the OCP and the Lynnmour Inter-River Local Plan. It addresses OCP housing policies related to the provision of a range of housing options, in this case, family housing in a townhouse format.

The project is now ready for Council's consideration.

Options:

The following options are available Council's consideration:

- 1) Introduce Bylaws 8138 and 8139 and refer Bylaw 8138 to a Public Hearing (staff recommendation); or
- 2) Defeat Bylaw 8138 and 8139 at First Reading.



Casey Peters
Community Planner

- A – Reduced project plans
- B – Bylaw 8138
- C – Bylaw 8139

**SUBJECT: BYLAWS 8138 AND 8139: REZONING AND HOUSING AGREEMENT FOR
A 9 UNIT TOWNHOUSE PROJECT: 756-778 FORSMAN AVENUE**

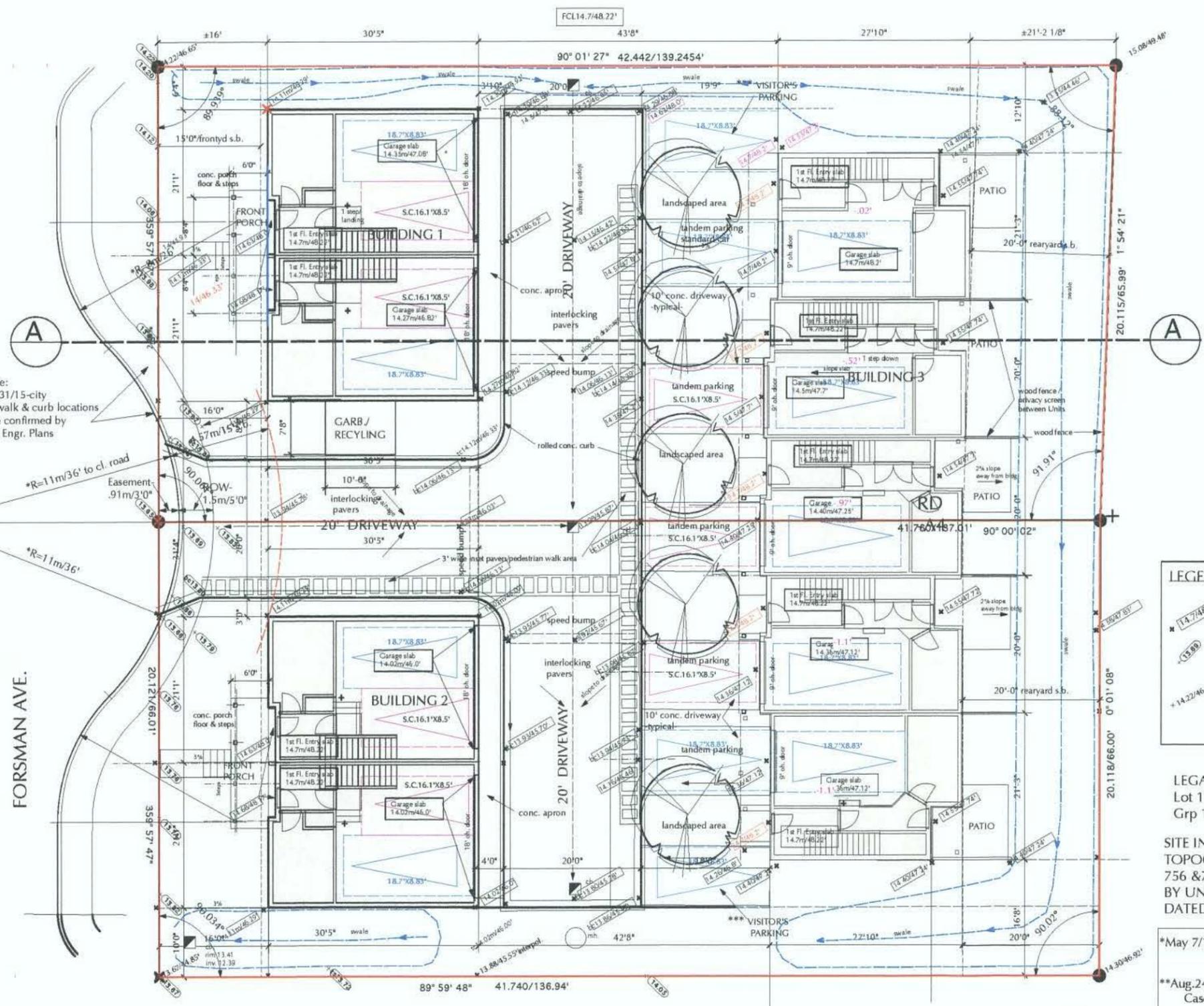
October 16, 2015

Page 9

REVIEWED WITH:		
<input type="checkbox"/> Sustainable Community Dev. _____	<input type="checkbox"/> Clerk's Office _____	External Agencies:
<input type="checkbox"/> Development Services _____	<input type="checkbox"/> Communications _____	<input type="checkbox"/> Library Board _____
<input type="checkbox"/> Utilities _____	<input type="checkbox"/> Finance _____	<input type="checkbox"/> NS Health _____
<input type="checkbox"/> Engineering Operations _____	<input type="checkbox"/> Fire Services _____	<input type="checkbox"/> RCMP _____
<input type="checkbox"/> Parks & Environment _____	<input type="checkbox"/> ITS _____	<input type="checkbox"/> Recreation Com. _____
<input type="checkbox"/> Economic Development _____	<input type="checkbox"/> Solicitor _____	<input type="checkbox"/> Museum & Arch. _____
<input type="checkbox"/> Human resources _____	<input type="checkbox"/> GIS _____	<input type="checkbox"/> Other: _____



<p>REDRAW DP-</p> <p>ARCHITECT: GEN YADA ARCHITECT 7707 ONTARIO ST., VANCOUVER, B.C. V5X 3C6 tel: 604 321-8997 email: gyada@shaw.ca</p> <p>LANDSCAPE ARCHITECT: SW LANDSCAPE ARCHITECT 919 MELBOURNE AVE, NORTH VANCOUVER, B.C. V7R 1N8 tel: 778 834-8959 email: steve.wong@gmail.com</p> <p>CIVIL ENGINEER: WEBSTER ENGINEERING LTD 3745 DELBROOK AVENUE, NORTH VANCOUVER, B.C. V7N 3Z4 tel: 604 983-0458</p>	<p>*REDRAW /DP DRAWINGS LIST * these Redraw Plans are based on original DP applications Plans prepared by Robert Kleyn Architect Plans(Nov./12) & as Approved ADP</p> <p>RD/A0 COVER PAGE/DRAWING LIST RD/L1 LANDSCAPE PLAN RD/L2 LANDSCAPE SECTION & DETAILS</p> <p>RD/A1 SITE PLAN</p> <p>RD/A3 ARCHITECTURAL PLANS RD/A4 ELEVATIONS- FORSMAN AVE. RD/A5 ELEVATIONS- INTERIOR COURTYARD/DRIVEWAY- EAST RD/A6 ELEVATIONS- INTERIOR COURTYARD/DRIVEWAY- WEST RD/A7 ELEVATIONS- REARYARD (@ EAST) RD/A8 ELEVATIONS-END RD/A9 SITE SECTION</p> <p>Civil Drawings- Redone/Webster Engineering Ltd LG-1 LOT GRADING & ON-LOT ROADWORKS -refer to Webster Plans for Civil Drawing list -Appendix -Cover page with *RK Architect Drawing List *original DP application Plans by Robert Kleyn Architect, June/10</p>
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*Note:
Aug.31/15-city
sidewalk & curb locations
to be confirmed by
Civil Engr. Plans

PROJECT DATA*
*-as noted on original RK Dev. Permit Application -
Plan -Drawing A0, dated Nov. 2012

Legal Address- Lot 10, Blk 810, DL562,
Grp 1, NWD Plan 6011
Civic Address 756 & 778 Forsman Avenue

Zoning -Existing-RS3/Single family residential
-Proposed- Multi-family, ground oriented (0.7 FAR max.)

Statistics
-Lot area (Gross) 18155ft²
Easement- 568ft²
Lot Area(Net) 17587ft²(0.404 A)
Coverage 5394ft²/18084= .30

Residential Units
-Proposed 9units=22.5upA(22.37) (< 24upA max.)
FAR
-Proposed 12085ft²(parking & storage excluded)
-FAR gross 12085/18084= 0.67(<0.7max)
-FAR net 12085/17587= 0.69(<0.7max)

-storage exemption 9units@100ft²
-parking exemption 9units@500ft²
Parking
-Required 2 * 9= 18
-Provided 18 covered & surface
-standard spaces 11> (11/18= 0.61< 0.65)
-small spaces 7> (7/18= 0.39> 0.35)

Setbacks reqd/prop.
-Front(Forsman Ave.) 15'/ 15'
-Rear 20'/ 20'
-Side-North(school) 10'/ 10'
-South 10'/ 10'

Height-Front(Forsman Ave.) ?
Flood Control Level
-48.22'= 14.7m
-see KWL Report for recommendations

LEGEND

- *proposed finished spot grade(mft)
- civil finished grade (mft)
-refer to Civil Engr. Drawings
- existing spot grade (mft)
-as per Survey by Underhill Geomatics Ltd.

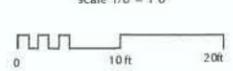
LEGAL DESCRIPTION:
Lot 10, Blk 810, DL562,
Grp 1, NWD Plan 6011

SITE INFORMATION FROM TOPOGRAPHIC SURVEY OF 756 & 778 FORSMAN AVE. BY UNDERHILL GEOMATICS LTD. DATED NOV. 12, 2012

*May 7/14 -revised redraw RK Plans prop. lines as per Cad/.dwg Underhill survey

**Aug.24/15 -add 2 Visitor Parking as per Casey/DNV Aug. 24 meeting

- REDRAW KEY SITE PLAN -
scale 1/8"= 1'0"



Gen Yada Architect
7707 Ontario Street
Vancouver, B.C. V5X 3C6
Tel. or Fax.(604) 321-8997
Email: gyada@shaw.ca

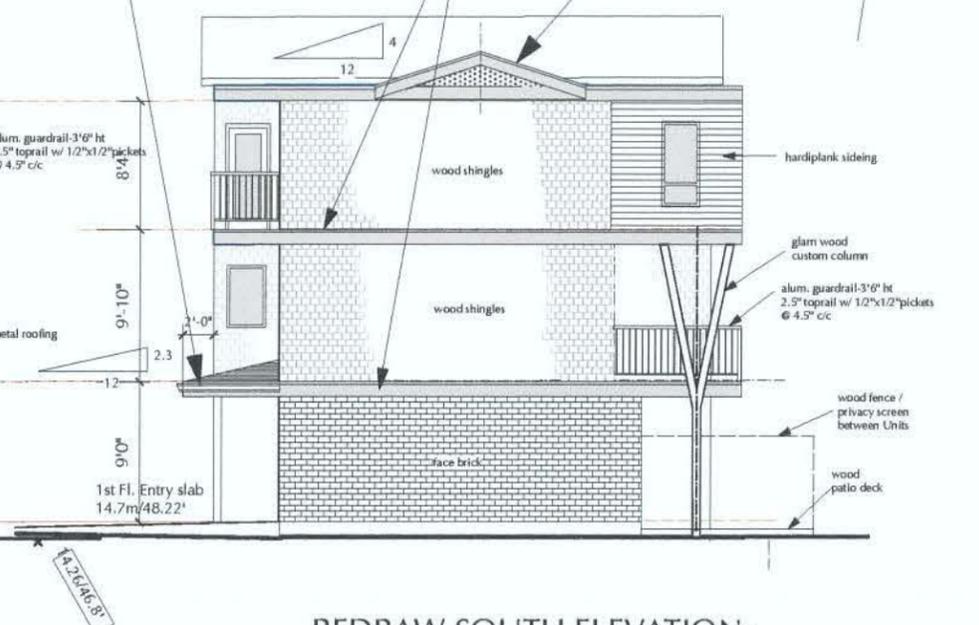
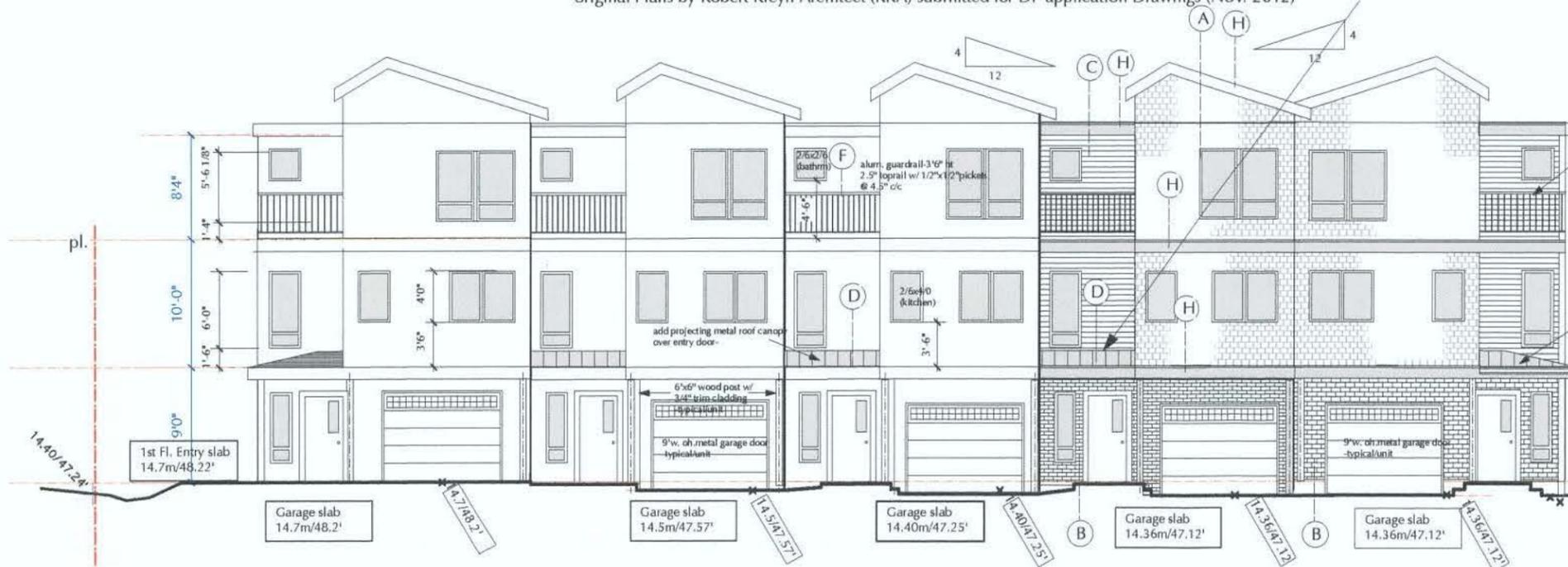
**REDRAW /DP SITE PLAN -
756 & 778 FORSMAN AVE.**

**RD
-A1**



MATERIALS + COLOURS			
LINE	SYMBOL	DESCRIPTION	REMARKS
A	[Orange Box]	CLAY BRICK	2ND FLOOR WALL
B	[Red Box]	SPARK PAINT CONCRETE FOUNDATION	1ST FLOOR WALL
C	[Yellow Box]	HARD PLANK SIDING	2ND FLOOR WALL
D	[Grey Box]	METAL SIDING	1ST FLOOR WALL
F	[Light Green Box]	METAL PAINT & TRIM	TRIM, DOORS
G	[Light Green Box]	CEILING ROOF & TRUSS	ROOF
H	[Light Green Box]	WOOD TRIM	TRIM

ORIGINAL *RKA DRAWING-EXCERPT
 *-for Reference- Approved Design Panel
 -original Plans by Robert Kleyn Architect (RKA) submitted for DP application Drawings (Nov. 2012)



REDRAW EAST/REARYARD ELEVATION--BUILDING 3-

REDRAW SOUTH ELEVATION--BUILDING 3-

-REDRAW DP-
 Scale 3/16"=1'-0"

BUILDING 3/ELEVATION- DRIVEWAYSIDE/WEST

Gen Yada Architect
 7707 Ontario Street
 Vancouver, B.C. V5X 3C6
 Tel. or Fax. (604) 321-8997
 Email: gyada@shaw.ca

REDRAW DP-FORSMAN GARDENS
756 & 778 FORSMAN AVE.

RD
-A5



MATERIALS + COLOURS		
A	CLAY BRICK	CREAM
B	CONCRETE FOUNDATION	RED
C	HARD-PLANK DECK	WARM OAK
D	METAL SIDING	GREY GREEN
F	METAL PAIL & ROSETTE	GREY SLICK
G	WOOD PLANT SCREEN	RED BRICK
H	WOOD TRIM	WARM OAK

EAST/INTERIOR DRIVEWAYSIDE ELEVATION

ORIGINAL *RKA DRAWING-EXCERPT

*-for Reference- Approved Design Panel
 -original Plans by Robert Kleyn Architect (RKA) submitted for DP application Drawings (Nov. 2012)



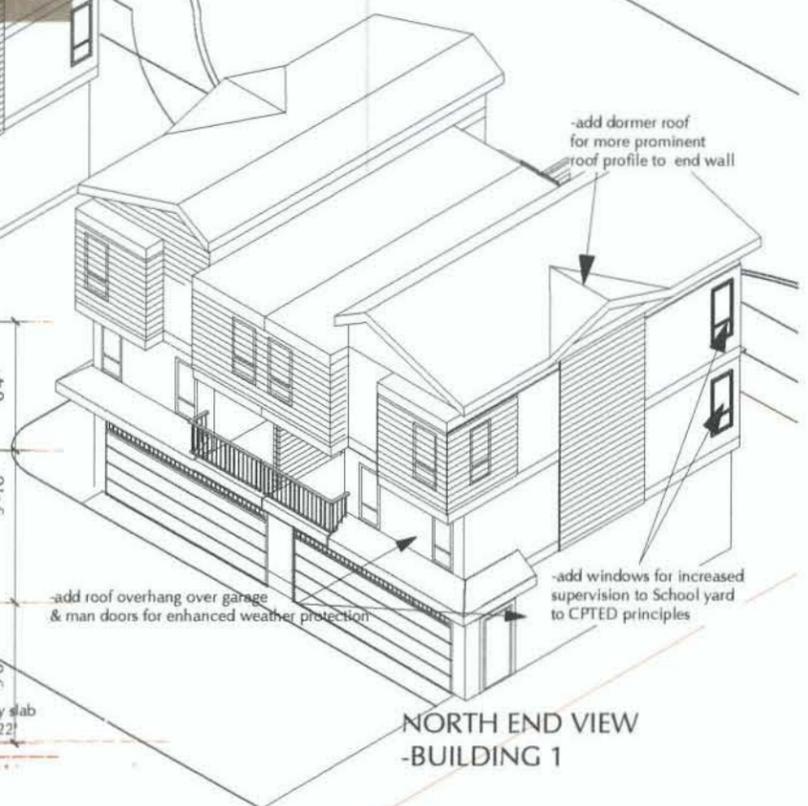
REDRAW BUILDING 2



REDRAW BUILDING 1

EAST/INTERIOR DRIVEWAYSIDE ELEVATION

-REDRAW DP -



NORTH END VIEW -BUILDING 1

BUILDING 1&2(DUPLEX) ELEVATION- STREET/WEST-

REDRAW DP -FORSMAN GARDENS
 756 & 778 FORSMAN AVE.

Gen Yada Architect
 7707 Ontario Street
 Vancouver, B.C. V5X 3C6
 Tel. or Fax. (604) 321-8997
 Email: gyada@shaw.ca

RD
 -A6

Sept.8/15- Updated to Casey/DNV
 Aug.24/15 meeting items
 June 12/14



MATERIALS + COLOURS			
LETTER	MATERIAL	COLOUR	FINISH
A	CEILING PANELS	GREY-TRUSS	STAIN
B	BRICK ON CONCRETE FOUNDATION	RED	TRADITIONAL
C	HARD-PLANK SIDING	WARM OAK	REF. FINISHED
D	METAL TRIM	GREY TRUSS	PAINT
F	METAL PANELS	GREY TRUSS	MATCH TO D.P.
G	CEILING TRUSS & TRUSS	GREY TRUSS	STAIN
H	WOOD TRIM	WARM OAK	STAIN

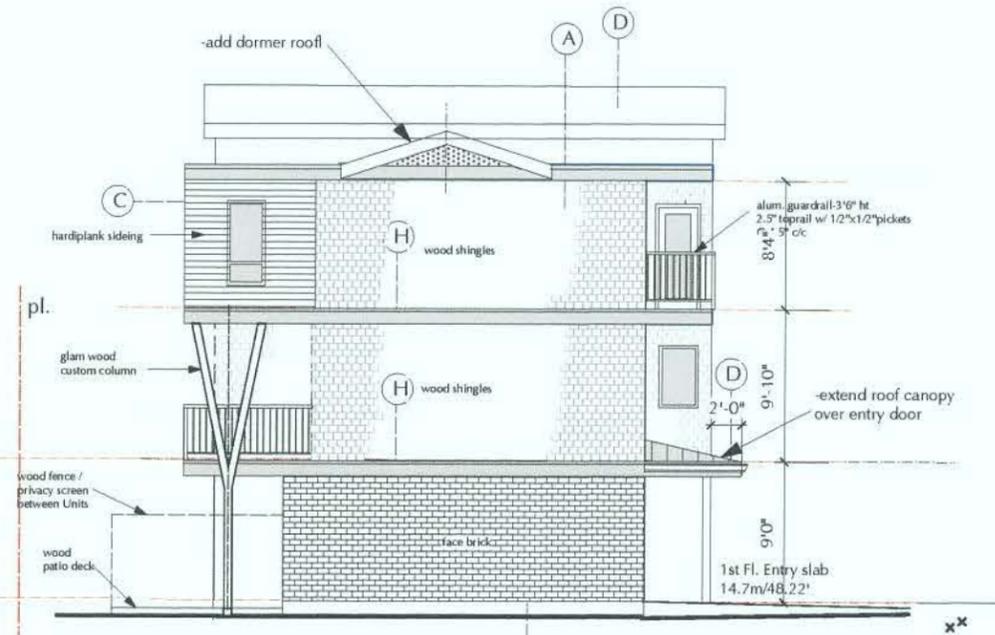
**EAST
WEST ELEVATION**
ORIGINAL *RKA DRAWING-EXCERPT
 *-for Reference- Approved Design Panel
 -original Plans by Robert Kleyn Architect (RKA) submitted for DP application Drawings (Nov. 2012)

revise/increase window size to suit view & daylight for Living rm



REDRAW EAST/REARYARD ELEVATION-BUILDING 3-

-REDRAW DP -
 Scale 3/16"=1'-0"



REDRAW NORTH ELEVATION- BUILDING 3

BUILDING 3/ELEVATION- REARYARD SIDE/EAST

REDRAW DP -FORSMAN GARDENS
 756 & 778 FORSMAN AVE.

RD
 -A7

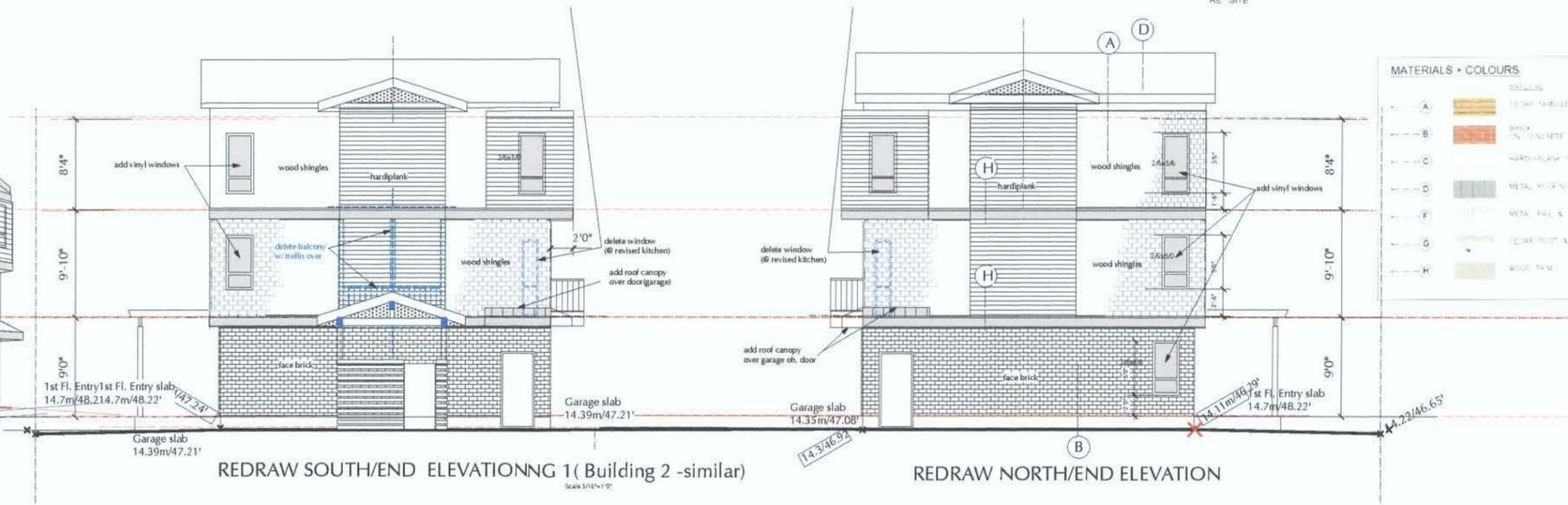
Sept. 8/15- Updated to Casey/DNV
 Aug. 24/15 meeting items
 Apr. 30/14

Gen Yada Architect
 7707 Ontario Street
 Vancouver, B.C. V5X 3C6
 Tel. or Fax. (604) 321-8997
 Email: gyada@shaw.ca



2 WE ORIGINAL *RKA DRAWING-EXCERPT
 3/16" = 1'-0" -for Reference- Approved Design Panel
 RE DUPI -original Plans by Robert Kleyn Architect (RKA) submitted for DP application Drawings (Nov. 2012)

1 WEST ELEV
 3/16" = 1'-0"
 RE SITE



-REDRAW DP -

Gen Yada Architect
 7707 Ontario Street
 Vancouver, B.C. V5X 3C6
 Tel. or Fax.(604) 321-8997
 Email: gyada@shaw.ca

END ELEVATIONS-BUILDING 1(BUILDING 3-similar)

REDRAW DP - FORSMAN GARDENS
 756 & 778 FORSMAN AVE.

RD
 -A8



VINE MAPLE



PICNIC TABLE



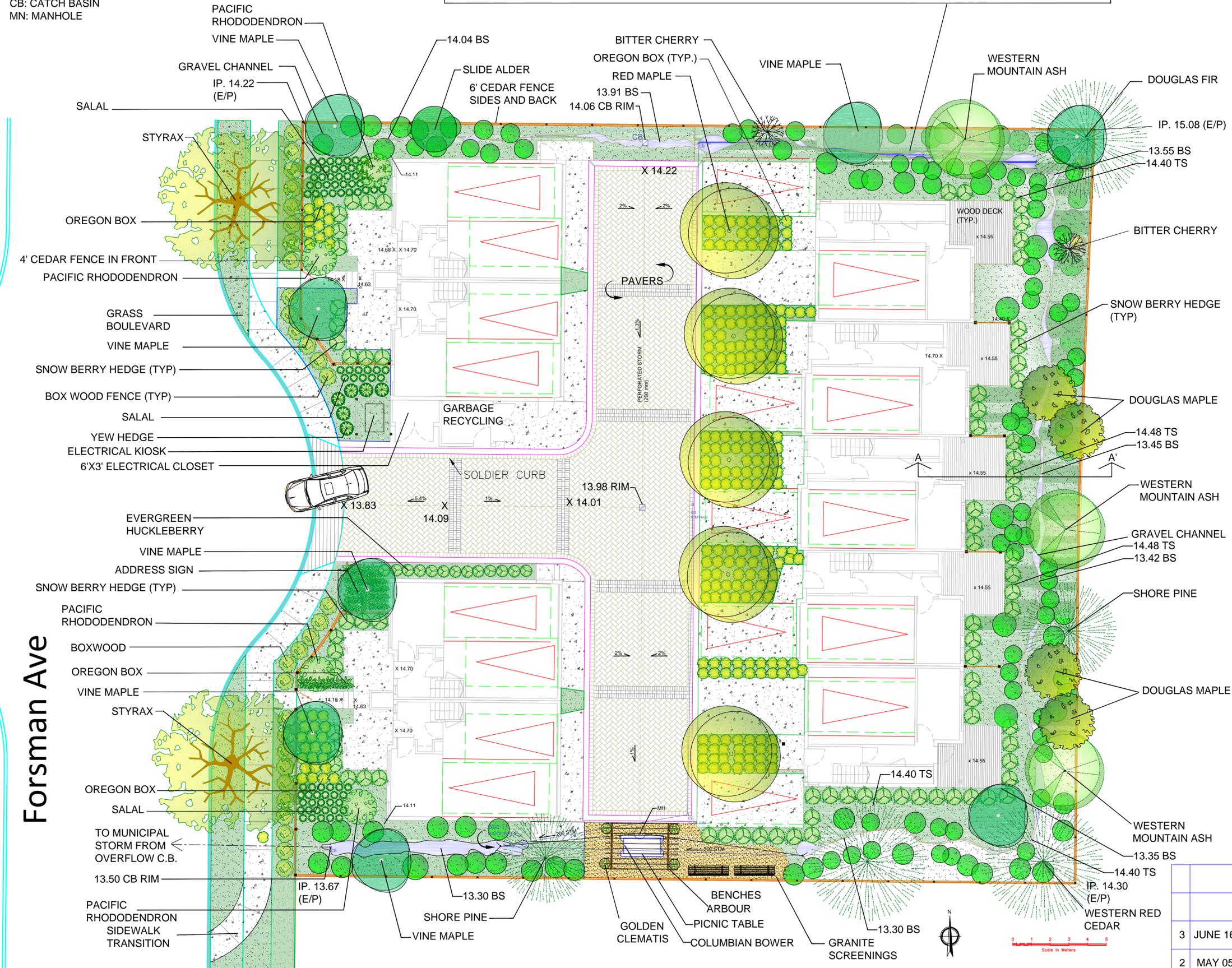
BENCHES



ARBOUR DETAIL

NOTES:
 BS: BOTTOM OF SWALE
 TS: TOP OF SWALE
 E/P: EXISTING/PROPOSED GRADE
 CB: CATCH BASIN
 MN: MANHOLE

REMAINDER PLANTS
 • BOTTOM OF SWALE - RUSH, SEGOE, NATIVE GRASS (TYP)
 • SIDE OF SWALE - FERN, IRIS, SALAL, DOGWOOD, HARDJACK (TYP)
 • TOP OF SWALE - OCEANSPRAY, OREGON GRAPE, PACIFIC NINEBARK, ROSE, SALMON BERRY, HUCKLEBERRY (TYP)



COMMON RUSH



SLOUGH SEDGE



SWORD FERN



RED COLUMBINE



RED HUCKLEBERRY



SALMONBERRY

Forsman Ave

Forsman Ave Townhouses

756 + 778 Forsman Ave

L1: LANDSCAPE PLAN

Royal Columbia Development Corporation
 1108, W. 8TH AVE.,
 VANCOUVER, BC, Canada, V6H 3Z5

SW Landscape Architect
 919 Melbourne Avenue
 North Vancouver, B.C. V7R 1N8

ISSUED / REVISIONS		
3	JUNE 16 2014	SUBMIT TO DNV
2	MAY 05 2014	REVIEW
1	NOV 14 2013	SUBMISSION

LANDSCAPE STATEMENT OF INTENT

The intent of the landscape design is to have a landscape treatment that is sustainable and reflects the natural heritage of the Lynnmoor area in North Vancouver. Once through the front gate the majority of plants are native species. In the front yard, one is greeted by the arching, dappled shade of the vine maple, and the lush evergreen groundcover of native Oregon box and salal. Along the perimeter is a delectable rain garden that catches water from the site, with biofiltration by sedges, rushes, ferns and other moist loving species. Under the shade of firs, cedars and vine maples is the natural understory of native rhododendron, huckleberry, oceanspray and roses. This unique landscape treatment is environmentally sensitive and will also be a delight to the senses.

MATERIALS LIST

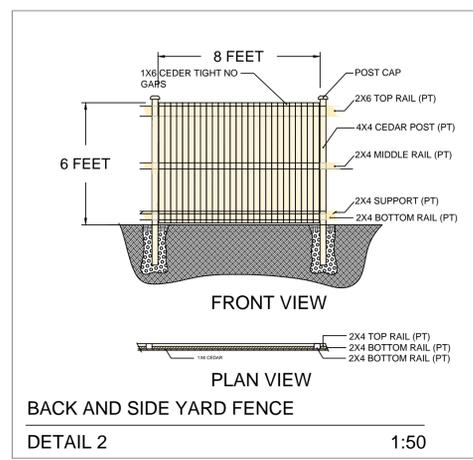
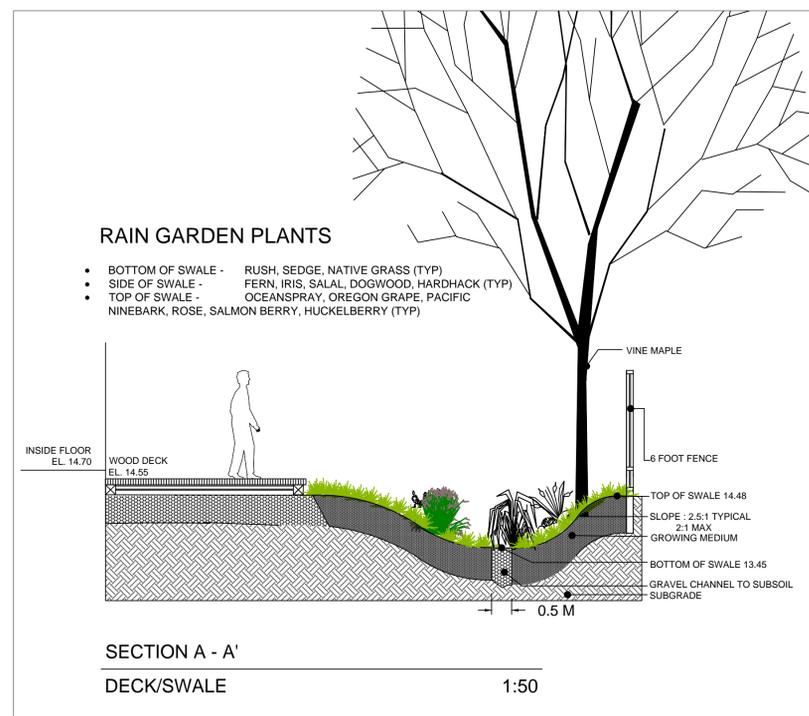
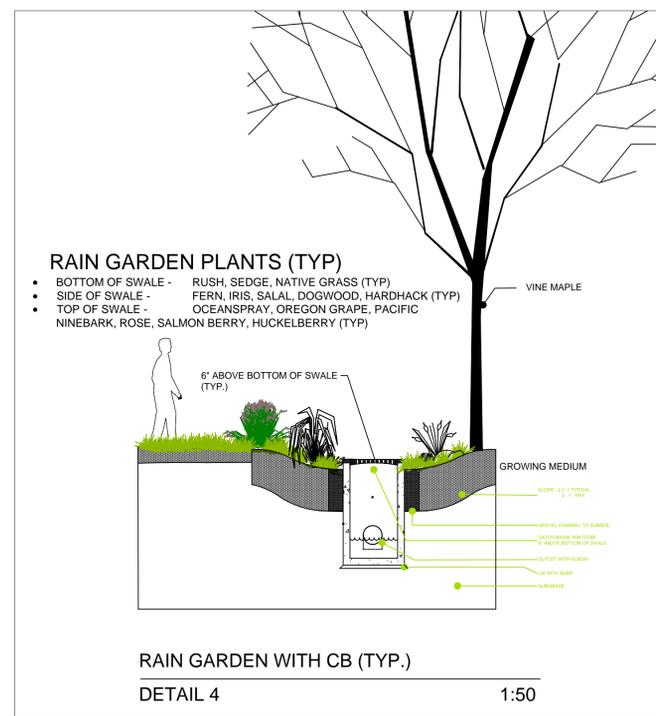
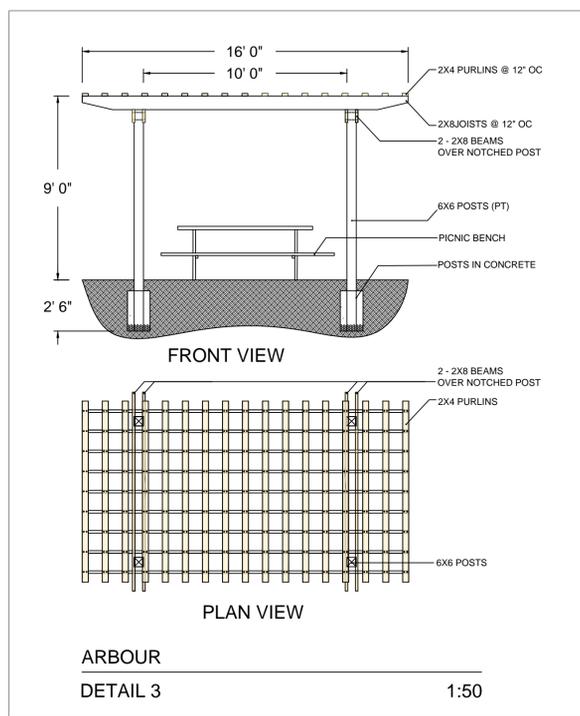
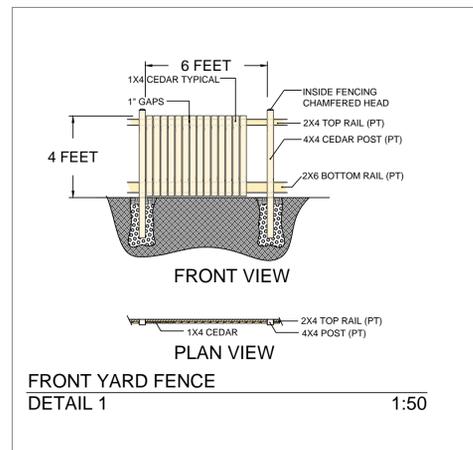
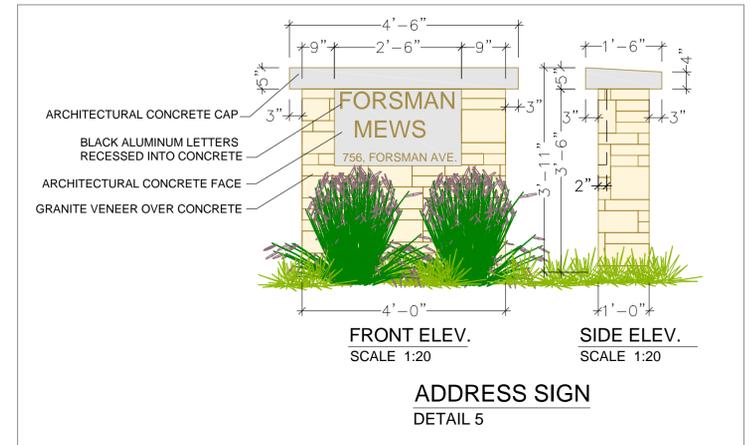
BENCHES: Victor Stanley F-50, 6 feet length, lpe wood slats in natural finish, black powder coated ductile metal frame
PICNIC TABLE: Victor Stanley ST-6, Homestead Collection, 6 foot length, lpe wood slats in natural finish, black powder coated ductile metal frame
WOOD FENCE AND GATES: 1 x4, 1 X6 cedar boards finished with Sikkens semi-transparent cedar stain
ARBOUR: All wood members to be PT, natural finish
WOOD DECKS: 5/4 inch radius PT deck boards, natural finish
PAVERS: Abbotsford Pavers, Standard Series, 4.5 by 9.0 by 2.4 inches, natural colour, herringbone pattern, with charcoal soldier coarse on perimeters and accent banding
GRAVEL CHANNELS AT BOTTOM OF SWALES: graded from 4 inches to 1.25 inch diameter river rock, approximately 2 foot wide channel
GRANITE SCREENINGS UNDER ARBOUR: 3/8 inch granite screenings over a compacted 4 inches compacted base
CONCRETE: Sidewalk, driveway aprons, front porches finished in a medium broom finish
PLANT MATERIAL: All plant material are to meet current BCLNTA standards and installed according to current BCLNTA standards
GROWING MEDIUM: to BCLNTA standards and prepared off site; typical depths are 12-18 inches in shrub beds

NOTES

- The District of North Vancouver is responsible for the on-going maintenance of street trees on off-site areas. Please ensure that the developer is aware that on-going maintenance of shrubs/ groundcovers on the off-site areas (i.e. boulevards) is the responsibility of the future property owner.
- The project landscape contractor, the project landscape architect and a District of North Vancouver Parks (DNV parks) representative must be present at the project pre-construction meeting. **If this is not possible, the developer must ensure that all three groups meet before any landscape construction work takes place onsite.**
- All plants/trees used in this project must first be inspected by a representative of the District of North Vancouver parks department (DNV Parks) before installation. **The District of North Vancouver has the right to refuse any or if of the selected plant material if it does not meet current BCLNTA guidelines.**

PLANT LIST					
	Botanical Name	Common Name	Size	Spacing	No.
TREES	1 Acer circinatum	Vine maple	2.5 m B&B	as shown	
	2 Acer glabrum	Douglas maple	2.5m B&B	as shown	
	3 Acer rubrum 'Red Sunset'	Red Sunset maple	50 cm cal, B&B	as shown	
	4 Alnus viridis	Slide alder	#15 pot	as shown	
	5 Cornus nuttallii	Pacific dogwood	2.5m B&B	as shown	
	6 Pinus contorta contorta	Shore pine	2.5m B&B	as shown	
	7 Prunus emarginata	Bitter cherry	#15 pot	as shown	
	8 Pseudotsuga menziesii	Douglas fir	2.5m B&B	as shown	
	9 Sorbus scopulina canadensis	Westen mountain ash	#15 pot	as shown	
	10 Thuja plicata	Western red cedar	2.5m B&B	as shown	
SHRUBS, GROUND COVERS AND VINES	11 Buxus microphylla 'Green Beauty'	Green Beauty Boxwood	#2 pot	.6m oc	
	12 Clematis occidentalis	Columbia bower	#1 pot	as shown	
	13 Clematis tangutica	Golden clematis	#1 pot	as shown	
	14 Cornus sericea	Red twig dogwood	#3 pot	1.2 m oc	
	15 Gaultheria shallon	Salal	#1 pot	.5 m oc	
	16 Holodiscus discolor	Oceanspray	#5 pot	1.5 m oc	
	17 Mahonia nervosa	Low Oregon grape	#2 pot	.6 m oc	
	18 Mahonia aquifolium	Oregon grape	#2 pot	.7 m oc	
	19 Paxistima myrsinites	Oregon box	#2 pot	.6 m oc	
	20 Physocarpus capitatus	Pacific ninebark	#5 pot	1.5 m oc	
	21 Rhododendron macrophyllum	Pacific rhododendron	2.0 m B&B	2.5 m oc	
	22 Rosa gymnocarpa	Bald hip rose	#2 pot	1.2 m oc	
	23 Rubus spectabilis	Salmonberry	#3 pot	1.2 m oc	
	24 Spiraea douglasii	Hardhack	#2 pot	1.2 m oc	
	25 Symphoricarpos albus	Snowberry	#2 pot	.5 m oc	
	26 Vaccinium ovatum	Evergreen huckleberry	#2 pot	1 m oc	
	27 Vaccinium parvifolium	Red huckleberry	#2 pot	1 m oc	
	HERBACIOUS PERENNIALS	28 Aquilegia formosa	Red Columbine	#1 pot	.45 m oc
29 Asarum caudatum		Wild ginger	10 cm pot	.45 m oc	
30 Aster subspicatus		Douglas aster	10 cm pot	.45 m oc	
31 Blechnum spicant		Deer fern	#1 pot	.45 m oc	
32 Carex obtusa		Slough sedge	10 cm pot	.45 m oc	
33 Carex stipata		Sawbeak sedge	10 cm pot	.45 m oc	
34 Deschampsia cespitosa		Tufted hair grass	10 cm pot	.45 m oc	
35 Hemerocallis spp.		Daylily	#1 pot	.45 m oc	
36 Iris douglasiana		Douglas iris	#1 pot	.45 m oc	
37 Juncus effusus		Common rush	10 cm pot	.45 m oc	
38 Polystichum munitum		Western swordfern	#1 pot	.6 m oc	

Note: All plants and installation shall be in accordance with the latest B.C. Landscape Standard.



No.	Date	Description
3	JUNE 16 2014	SUBMIT TO DNV
2	MAY 05 2014	REVIEW
1	NOV 14 2013	SUBMISSION
ISSUED / REVISIONS		

The Corporation of the District of North Vancouver

Bylaw 8138

A bylaw to amend the District of North Vancouver Zoning Bylaw 3210, 1965

The Council for The Corporation of the District of North Vancouver enacts as follows:

1. Citation

This bylaw may be cited as “The District of North Vancouver Rezoning Bylaw 1334, (Bylaw 8138)”.

2. Amendments

2.1 The District of North Vancouver Zoning Bylaw 3210, 1965 is amended as follows:

(a) Section 301 (2) by inserting the following zoning designation:

“Comprehensive Development Zone 91 CD91”

(b) Part 4B Comprehensive Development Zone Regulations by inserting the following, inclusive of Schedule B:

“4B91 Comprehensive Development Zone 91 CD 91

The CD 91 zone is applied to:

756 Forsman Avenue, Lot C of Lot 6 Block A District Lot 613 Plan 20979,
PID: 005-225-957

778 Forsman Avenue, Lot A Block A District Lot 613 Group 1 New Westminster District
Plan BCP39525, PID: 027-780-228

4B 91-1 Intent

The purpose of the CD 91 Zone is to establish specific land use and development regulations for a 9 unit townhouse project.

4B 91- 2 Permitted Uses:

The following *principal* uses shall be permitted in the CD 91 Zone:

(a) Uses Permitted Without Conditions:

- (i) Residential building, multiple-family townhouse

(b) Conditional Uses:

Not Applicable

4B 91-3 Conditions of Use

Not Applicable

4B 91-4 Accessory Use

(a) *Accessory uses* are permitted and may include but are not necessarily limited to:

- (i) Home occupations in accordance with the regulations in Section 405 of the Zoning Bylaw, 1965

4B 91-5 Density

- (a) The maximum permitted density in the CD91 Zone is limited to a floor space ratio (FSR) of 0.45, inclusive of any density bonus for energy performance;
- (b) For the purposes of calculating floor space ratio, the area within the parking garage, storage (to a maximum of 100 sq ft per unit), balconies and landscape trellis are excluded.

4B 91-6 Amenities

(a) Despite subsection 4B91-5, density in the CD91 Zone is increased to a maximum floor space ratio of 0.7 FSR, inclusive of any density bonus for energy performance, if the owner:

1. Enters into a Housing Agreement prohibiting any restrictions preventing the owners in the project from renting their units; and
2. Contributes \$41,576.25 to the municipality to be used for any or all of the following amenities (with allocation to be determined by the municipality in its sole discretion): public art; park, trail, environmental, pedestrian or other public realm, infrastructure improvements; municipal, recreation or social

service facility or service / facility improvements; and/or the affordable housing fund.

4B 91-7 Maximum Principal Building Size:

Not applicable

4B 91-8 Setbacks:

- a) Buildings shall be set back from property lines to the closest building face as established by development permit and in accordance with the following regulations:

Setback	Buildings (Min Setback)
Front (west property line)	4.57m (15 ft) to the building face
Rear (east property line)	6.10m (20 ft) to the building face
Side (north)	1.83m (6 ft) to the building face
Side (south)	3.05m (10 ft) to the building face

- b) Projections at the ground level are permissible as follows:

Setback	Maximum Setback Reduction
Front Yard	1.83 m (6.0 ft)

- c) Balconies and trellises are excluded from any setback requirements.

4B 91-9 Building Orientation:

Not applicable

4B 91-10 Building Depth and Width:

Not applicable

4B 91-11 Coverage:

- (a) Building Coverage shall not exceed 45%.

- (b) Site Coverage shall not exceed 70%.

4B 91-12 Height:

The maximum permitted height for each building, inclusive of a 15% bonus for sloping roofs, is 12.1m (40 ft);

4B 91-13 Acoustic Requirements:

In the case of residential purposes, a development permit application shall require evidence in the form of a report and recommendations prepared by persons trained in acoustics and current techniques of noise measurements, demonstrating that the noise levels in those portions of the dwelling listed below shall not exceed the noise levels expressed in decibels set opposite such portions of the dwelling units:

Portion of Dwelling Unit	Noise Level (Decibels)
Bedrooms	35
Living and Dining rooms	40
Kitchen, Bathrooms and Hallways	45

4B 91-14 Flood Construction Requirements:

No basement, or habitable floor space, other than garage and storage space, shall be constructed below the established flood construction levels as identified in a floor hazard report prepared by a qualified registered professional engineer.

4B 91-15 Landscaping:

- (a) All land areas not occupied by buildings, structures, parking spaces, loading spaces, driveways, manoeuvring aisles and sidewalks shall be landscaped or finished in accordance with an approved landscape plan; and
- (b) All electrical kiosks and garbage and recycling container pads not located underground or within a building shall be screened with landscaping.

4B 91-16 Subdivision Requirements:

Not applicable

4B 91-17 Additional Accessory Structure Regulations:

Not applicable.

4B 91-18 Parking and Loading Regulations:

- (a) Parking spaces shall be provided on the basis of 2 spaces/unit plus 2 visitor spaces;
- (b) Not more than 7 spaces may be small car spaces;

- (c) Not more than 5 parking spaces may be in a tandem arrangement;
- (d) All parking spaces shall meet the minimum length and width standards established in Part 10 of the District of North Vancouver Zoning Bylaw.”
- (C) The Zoning Map is amended in the case of the lands illustrated on the attached map (Schedule A) by rezoning the land from the Residential Single Family 7200 Zone (RS3) to Comprehensive Development Zone 91 (CD 91).

READ a first time

PUBLIC HEARING held

READ a second time

READ a third time

Certified a true copy of “Rezoning Bylaw 1334 (Bylaw 8138)” as at Third Reading

Municipal Clerk

APPROVED by the Ministry of Transportation and Infrastructure on

ADOPTED

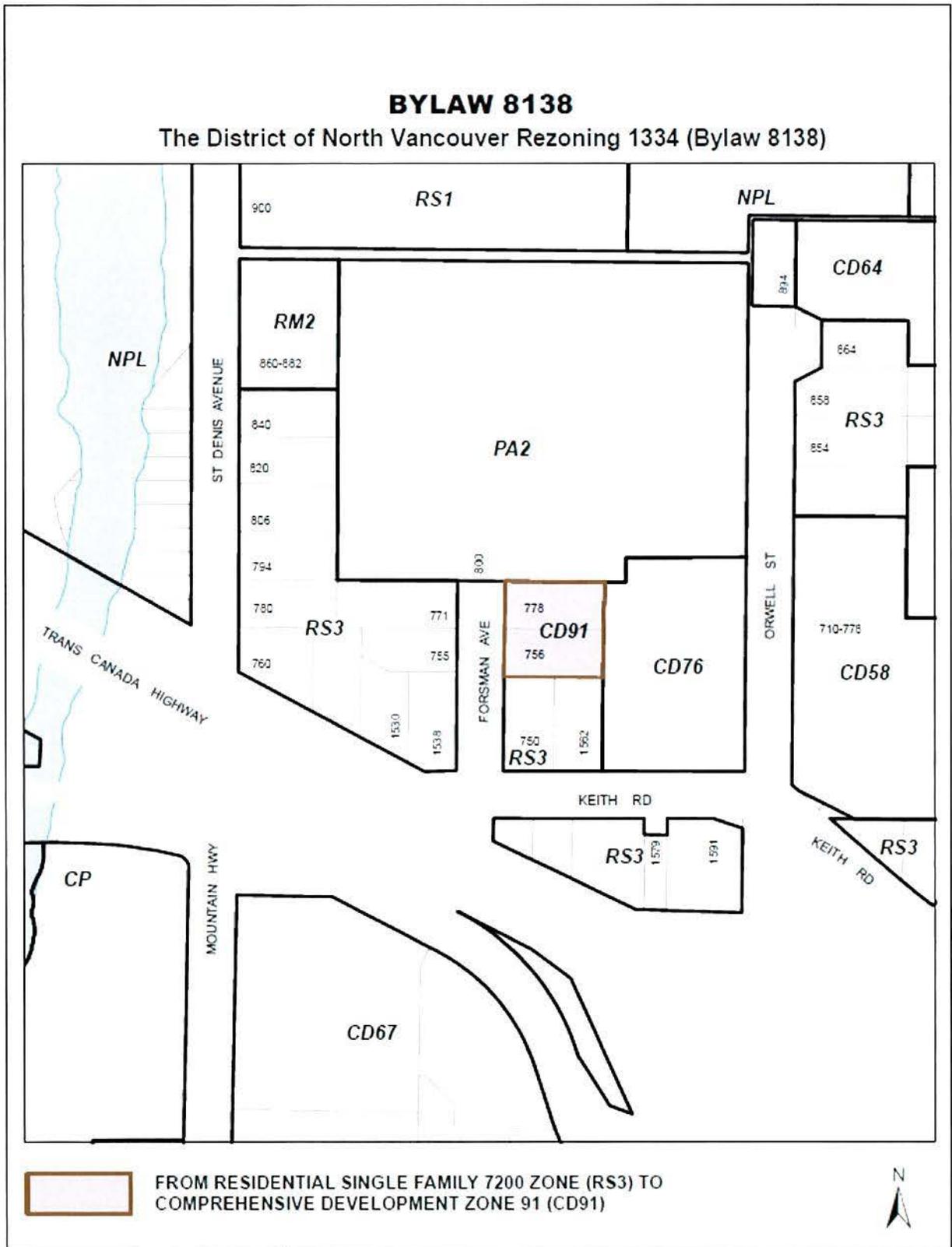
Mayor

Municipal Clerk

Certified a true copy

Municipal Clerk

Schedule A to Bylaw 8138



The Corporation of the District of North Vancouver

Bylaw 8139

A bylaw to enter into a Housing Agreement (756 & 778 Forsman Ave.)

The Council for The Corporation of the District of North Vancouver enacts as follows:

1. Citation

This bylaw may be cited as "Housing Agreement Bylaw 8139, 2015 (756 & 778 Forsman Ave.)".

2. Authorization to Enter into Agreement

2.1 The Council hereby authorizes a housing agreement between The Corporation of the District of North Vancouver and 1009198 B.C. Ltd., Inc.No. BC1009198 substantially in the form attached to this Bylaw as Schedule "A" with respect to the following lands:

- a) Lot C of Lot 6 Block A District Lot 613 Plan 20979, PID: 005-225-957
- b) Lot A Block A District Lot 613 Group 1 New Westminster District Plan BCP39525, PID: 027-780-228

3. Execution of Documents

The Mayor and Municipal Clerk are authorized to execute any documents required to give effect to the Housing Agreement.

READ a first time

READ a second time

READ a third time

ADOPTED

Mayor

Municipal Clerk

Certified a true copy

Municipal Clerk

Schedule A to Bylaw 8139

SECTION 219 COVENANT – HOUSING AGREEMENT

This agreement dated for reference the ____ day of _____, 20____ is

BETWEEN:

1009198 B.C. Ltd., Inc.No. BC1009198

(the “Owner”)

AND:

THE CORPORATION OF THE DISTRICT OF NORTH VANCOUVER, a municipality incorporated under the *Local Government Act*, R.S.B.C. 1996, c.323 and having its office at 355 West Queens Road, North Vancouver, BC V7N 4N5

(the “District”)

WHEREAS:

1. The Owner is the registered owner of the Lands (as hereinafter defined);
2. The Owner wishes to obtain development permissions with respect to the Lands and wishes to create a condominium development which will contain residential strata units on the Lands;
3. Section 905 of the *Local Government Act* authorises the District, by bylaw, to enter into a housing agreement to provide for the prevention of rental restrictions on housing, and provides for the contents of the agreement; and
4. Section 219 of the *Land Title Act* (British Columbia) permits the registration in favour of the District of a covenant of a negative or positive nature relating to the use of land or a building thereon, or providing that land is to be built on in accordance with the covenant, or providing that land is not to be built on except in accordance with the covenant, or providing that land is not to be subdivided except in accordance with the covenant;

NOW THEREFORE in consideration of the mutual promises contained in it, and in consideration of the payment of \$1.00 by the District to the Owner (the receipt and sufficiency of which are hereby acknowledged by the Owner), the parties covenant and agree with each other as follows, as a housing agreement under Section 905 of the *Local Government Act*, as a contract and a deed under seal between the parties, and as a covenant under Section 219 of the *Land Title Act*, and the Owner hereby further covenants and agrees that neither the Lands nor any building constructed thereon shall be used or built on except in accordance with this Agreement::

1. **DEFINITIONS**

1.01 Definitions

In this agreement:

- (a) “*Development Permit*” means development permit No. _____ issued by the District;
- (b) “*Lands*” means land described in Item 2 of the *Land Title Act* Form C to which this agreement is attached;
- (c) “*Proposed Development*” means the proposed development containing not more than 9 Units to be constructed on the Lands in accordance with the Development Permit;
- (d) “*Unit*” means a residential dwelling strata unit in the Proposed Development; and
- (e) “*Unit Owner*” means the registered owner of a Dwelling Unit in the Proposed Development.

2. **TERM**

This Agreement will commence upon adoption by District Council of Bylaw 8028 and remain in effect until terminated by the District as set out in this Agreement.

3. **RENTAL ACCOMODATION**

3.01 Rental Disclosure Statement

No Unit in the Proposed Development may be occupied unless the Owner has:

- (a) before the first Unit is offered for sale, or conveyed to a purchaser without being offered for sale, filed with the Superintendent of Real Estate a Rental Disclosure Statement designating all of the Units as rental strata lots and imposing a ninety-nine (99) year rental period in relation to all of the Units pursuant to the *Strata Property Act* (or any successor or replacement legislation); and
- (b) given a copy of the Rental Disclosure Statement to each prospective purchaser of any Unit before the prospective purchaser enters into an agreement to purchase in respect of the Unit.

3.02 Rental Accommodation

The Units constructed on the Lands from time to time may always be used to provide rental accommodation as the Owner or a Unit Owner may choose from time to time.

3.03 Binding on Strata Corporation

This agreement shall be binding upon all strata corporations created by the subdivision of the Lands or any part thereof (including the Units) pursuant to the *Strata Property Act*, and upon all Unit Owners. .

3.04 Strata Bylaw Invalid

Any strata corporation bylaw which prevents, restricts or abridges the right to use any of the Units as rental accommodations shall have no force or effect.

3.05 No Bylaw

The strata corporation shall not pass any bylaws preventing, restricting or abridging the use of the Lands, the Proposed Development or the Units contained therein from time to time as rental accommodation.

3.06 Vote

No Unit Owner, nor any tenant or mortgagee thereof, shall vote for any strata corporation bylaw purporting to prevent, restrict or abridge the use of the Lands, the Proposed Development and the units contained therein from time to time as rental accommodation.

3.07 Notice

The Owner will provide notice of this Agreement to any person or persons intending to purchase a Unit prior to any such person entering into an agreement of purchase and sale, agreement for sale, or option or similar right to purchase as part of the Disclosure Statement for any part of the Proposed Development prepared by the Owner pursuant to the *Real Estate Development Marketing Act*.

4. DEFAULT AND REMEDIES

4.01 Notice of Default

The District may, acting reasonably, give to the Owner written notice to cure a default under this Agreement within thirty (30) days of delivery of the notice. The notice must specify the nature of the default. The Owner must act with diligence to correct the default within the time specified.

4.02 Costs

The Owner will pay to the District upon demand all the District's costs of exercising its rights or remedies under this Agreement, on a full indemnity basis.

4.03 Damages an Inadequate Remedy

The Owner acknowledges and agrees that in the case of a breach of this Agreement which is not fully remediable by the mere payment of money and promptly so remedied,

the harm sustained by the District and to the public interest will be irreparable and not susceptible of adequate monetary compensation.

4.04 Equitable Remedies

Each party to this Agreement, in addition to its rights under this Agreement or at law, will be entitled to all equitable remedies including specific performance, injunction and declaratory relief, or any of them, to enforce its rights under this Agreement.

4.05 No Penalty or Forfeiture

The Owner acknowledges and agrees that it is entering into this Agreement to benefit the public interest in providing rental accommodation, and that the District's rights and remedies under this Agreement are necessary to ensure that this purpose is carried out, and the District's rights and remedies under this Agreement are fair and reasonable and ought not to be construed as a penalty or forfeiture.

4.06 Cumulative Remedies

No reference to nor exercise of any specific right or remedy under this Agreement or at law or at equity by any party will prejudice, limit or preclude that party from exercising any other right or remedy. No right or remedy will be exclusive or dependent upon any other right to remedy, but any party, from time to time, may exercise any one or more of such rights or remedies independently, successively, or in combination. The Owner acknowledges that specific performance, injunctive relief (mandatory or otherwise) or other equitable relief may be the only adequate remedy for a default by the Owner under this Agreement.

5. LIABILITY

5.01 Indemnity

Except if arising directly from the negligence of the District or its employees, agents or contractors, the Owner will indemnify and save harmless each of the District and its board members, officers, directors, employees, agents, and elected or appointed officials,, and their heirs, executors, administrators, personal representatives, successors and assigns, from and against all claims, demands, actions, loss, damage, costs and liabilities that all or any of them will or may be liable for or suffer or incur or be put to any act or omission by the Owner or its officers, directors, employees, agents, contractors, or other persons for whom the Owner is at law responsible, or by reason of or arising out of the Owner's ownership, operation, management or financing of the Proposed Development or any part thereof.

5.02 Release

The Owner hereby releases and forever discharges the District, its elected officials, board members, officers, directors, employees and agents, and its and their heirs, executors, administrators, personal representatives, successors and assigns from and against all

claims, demands, damages, actions or causes of action by reason of or arising out of advice or direction respecting the ownership, operation or management of the Proposed Development or any part thereof which has been or hereafter may be given to the Owner by all or any of them.

5.03 Survival

The covenants of the Owner set out in Sections 5.01 and 5.02 will survive termination of this Agreement and continue to apply to any breach of the Agreement or claim arising under this Agreement during the ownership by the Owner of the Lands or any Unit therein, as applicable.

6. GENERAL PROVISIONS

6.01 District's Power Unaffected

Nothing in this Agreement:

- (a) affects or limits any discretion, rights, powers, duties or obligations of the District under any enactment or at common law, including in relation to the use or subdivision of land;
- (b) affects or limits any enactment relating to the use of the Lands or any condition contained in any approval including any development permit concerning the development of the Lands; or
- (c) relieves the Owner from complying with any enactment, including the District's bylaws in relation to the use of the Lands.

6.02 Agreement for Benefit of District Only

The Owner and District agree that:

- (a) this Agreement is entered into only for the benefit of the District;
- (b) this Agreement is not intended to protect the interests of the Owner, any Unit Owner, any Occupant or any future owner, occupier or user of any part of the Proposed Development, including any Unit, or the interests of any third party, and the District has no obligation to anyone to enforce the terms of this Agreement; and
- (c) The District may at any time terminate this Agreement, in whole or in part, and execute a release and discharge of this Agreement in respect of the Proposed Development or any Unit therein, without liability to anyone for doing so.

6.03 Agreement Runs With the Lands

This Agreement burdens and runs with the Lands and any part into which any of them may be subdivided or consolidated, by strata plan or otherwise. All of the covenants and agreements contained in this Agreement are made by the Owner for itself, its successors and assigns, and all persons who acquire an interest in the Lands or in any Unit after the date of this Agreement.

6.04 Release

The covenants and agreements on the part of the Owner and any Unit Owner and herein set forth in this Agreement have been made by the Owner and any Unit Owner as contractual obligations as well as being made pursuant to Section 905 of the *Local Government Act* (British Columbia) and as such will be binding on the Owner and any Unit Owner, except that neither the Owner nor any Unit Owner shall be liable for any default in the performance or observance of this Agreement occurring after such party ceases to own the Lands or a Unit as the case may be.

6.05 Priority of This Agreement

The Owner will, at its expense, do or cause to be done all acts reasonably necessary to ensure this Agreement is registered against the title to each Unit in the Proposed Development, including any amendments to this Agreement as may be required by the Land Title Office or the District to effect such registration.

6.06 Agreement to Have Effect as Deed

The District and the Owner each intend by execution and delivery of this Agreement to create both a contract and a deed under seal.

6.07 Waiver

An alleged waiver by a party of any breach by another party of its obligations under this Agreement will be effective only if it is an express waiver of the breach in writing. No waiver of a breach of this Agreement is deemed or construed to be a consent or waiver of any other breach of this Agreement.

6.08 Time

Time is of the essence in this Agreement. If any party waives this requirement, that party may reinstate it by delivering notice to another party.

6.09 Validity of Provisions

If a Court of competent jurisdiction finds that any part of this Agreement is invalid, illegal, or unenforceable, that part is to be considered to have been severed from the rest of this Agreement and the rest of this Agreement remains in force unaffected by that holding or by the severance of that part.

6.10 Extent of Obligations and Costs

Every obligation of a party which is set out in this Agreement will extend throughout the Term and, to the extent that any obligation ought to have been observed or performed prior to or upon the expiry or earlier termination of the Term, such obligation will survive the expiry or earlier termination of the Term until it has been observed or performed.

6.11 Notices

All notices, demands, or requests of any kind, which a party may be required or permitted to serve on another in connection with this Agreement, must be in writing and may be served on the other parties by registered mail, by facsimile transmission, or by personal service, to the following address for each party:

If to the District:

District Municipal Hall
355 West Queens Road
North Vancouver, BC V7N 4N5

Attention: Planning Department
Facsimile: (604) 984-9683

If to the Owner:

Attention:
Facsimile: (604)

If to the Unit Owner:

The address of the registered owner which appears on title to the Unit at the time of notice.

Service of any such notice, demand, or request will be deemed complete, if made by registered mail, 72 hours after the date and hour of mailing, except where there is a postal service disruption during such period, in which case service will be deemed to be complete only upon actual delivery of the notice, demand or request; if made by facsimile transmission, on the first business day after the date when the facsimile transmission was transmitted; and if made by personal service, upon personal service being effected. Any party, from time to time, by notice in writing served upon the other parties, may designate a different address or different or additional persons to which all notices, demands, or requests are to be addressed.

6.12 Further Assurances

Upon request by the District, the Owner will promptly do such acts and execute such documents as may be reasonably necessary, in the opinion of the District, to give effect to this Agreement.

6.13 Enuring Effect

This Agreement will enure to the benefit of and be binding upon each of the parties and their successors and permitted assigns.

7. INTERPRETATION

7.01 References

Gender specific terms include both genders and include corporations. Words in the singular include the plural, and words in the plural include the singular.

7.02 Construction

The division of this Agreement into sections and the use of headings are for convenience of reference only and are not intended to govern, limit or aid in the construction of any provision. In all cases, the language in this Agreement is to be construed simply according to its fair meaning, and not strictly for or against either party.

7.03 No Limitation

The word “including” when following any general statement or term is not to be construed to limit the general statement or term to the specific items which immediately follow the general statement or term similar items whether or not words such as “without limitation” or “but not limited to” are used, but rather the general statement or term is to be construed to refer to all other items that could reasonably fall within the broadest possible scope of the general statement or term.

7.04 Terms Mandatory

The words “must” and “will” and “shall” are to be construed as imperative.

7.05 Statutes

Any reference in this Agreement to any statute or bylaw includes any subsequent amendment, re-enactment, or replacement of that statute or bylaw.

7.06 Entire Agreement

- (d) This is the entire agreement between the District and the Owner concerning its subject, and there are no warranties, representations, conditions or collateral agreements relating to this Agreement, except as included in this Agreement.
- (e) This Agreement may be amended only by a document executed by the parties to this Agreement and by bylaw, such amendment to be effective only upon adoption by District Council of a bylaw to amend Bylaw 8139.

7.07 Governing Law

This Agreement is to be governed by and construed and enforced in accordance with the laws of British Columbia.

As evidence of their agreement to be bound by the terms of this instrument, the parties hereto have executed the *Land Title Act* Form C that is attached hereto and forms part of this Agreement.

CONSENT AND PRIORITY AGREEMENT

GIVEN THAT:

1. _____ (the "Owner") is the Registered Owner of the Land described in Item 2 of Page 1 of the Form C (the "Land");
2. The Owner granted _____ (the "Prior Chargeholder") a Mortgage and Assignment of Rents registered against title to the Land in the Lower Mainland Land Title Office (the "LTO") under Nos. _____, as extended by _____ and _____, as extended by _____ (together, the "Prior Charge");
3. The Owner granted to THE CORPORATION OF THE DISTRICT OF NORTH VANCOUVER (the "District") a Covenant attached to this Agreement and registered against title to the Land in the LTO immediately before registration of this Agreement (the "Subsequent Charge"); and
4. Section 207 of the *Land Title Act* permits the Prior Chargeholder to grant priority over a charge to the District as Subsequent Chargeholder.

THEREFORE this Agreement is evidence that in consideration of \$1.00 and other good and valuable consideration received by the Prior Chargeholder from the District (the receipt and sufficiency of which the Prior Chargeholder acknowledges):

1. The Prior Chargeholder consents to the granting and registration of the Subsequent Charge and the Prior Chargeholder agrees that the Subsequent Charge shall be binding upon their interest in and to the Land.
2. The Prior Chargeholder grants to the District, as a Subsequent Chargeholder, priority for the Subsequent Charge over the Prior Chargeholder's right, title and interest in and to the Land, and the Prior Chargeholder postpones the Prior Charge and all of their right, title and interest thereunder to the Subsequent Charge as if the Subsequent Charge had been executed, delivered and registered prior to the execution, delivery and registration of the Prior Charge.

As evidence of its agreement to be bound by the terms of this instrument, the Prior Chargeholder has executed the Land Title Office Form C to which this Agreement is attached and which forms part of this Agreement.

AGENDA INFORMATION	
<input checked="" type="checkbox"/> Regular Meeting	Date: <u>Jan 18, 2016</u>
<input type="checkbox"/> Committee of the Whole	Date: _____


 Dept.
 Manager


 GM/
 Director


 CAO

9.3

The District of North Vancouver REPORT TO COUNCIL

January 4, 2016
 File: 08.3060.20/033.15

AUTHOR: Erik Wilhelm, Community Planner

SUBJECT: Bylaws 8149 and 8150: 14 Unit Townhouse Development at 115 and 123 W. Queens Rd.

RECOMMENDATION:

THAT "The District of North Vancouver Rezoning Bylaw 1337, (Bylaw 8149)" is given SECOND and THIRD Readings.

THAT "Housing Agreement Bylaw 8150, 2015 (115 and 123 West Queens Road)" is given SECOND and THIRD Readings.

BACKGROUND:

Bylaws 8149 and 8150 received First Reading on November 9, 2015. A Public Hearing for Bylaw 8149 was held and closed on December 1, 2015. No Public Hearing is necessary for Bylaw 1850.

At the Public Hearing, Council questioned the amount of on-street parking used for long periods of the day along West Queens Road. Council directed staff to review implementation of potential parking restrictions to limit the parking along this block of West Queens Road to a maximum of two hours ending at 10 pm.

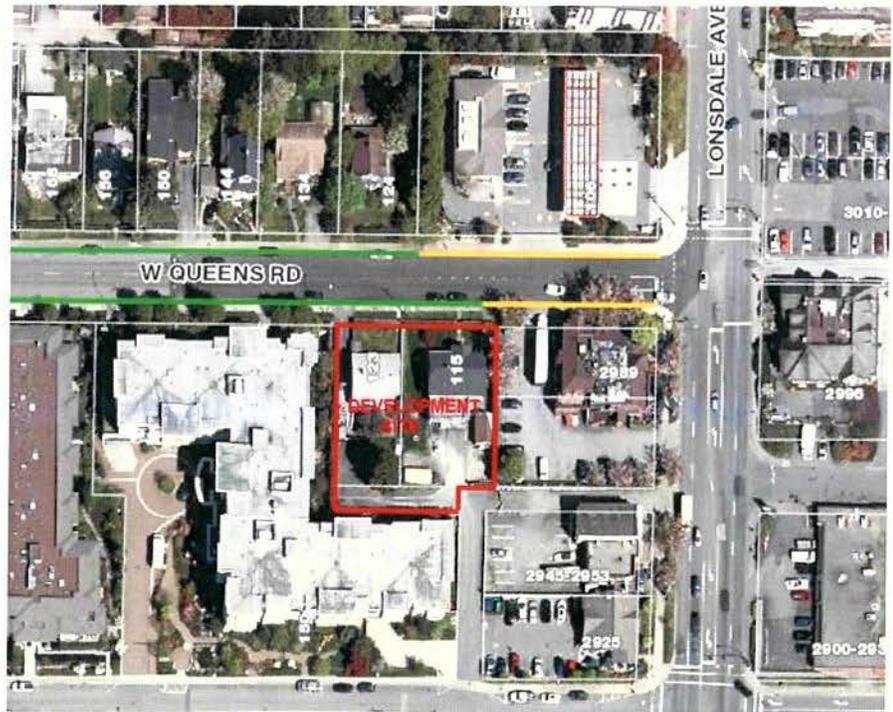
ANALYSIS:

An opportunity exists to provide time duration parking in order to provide on-street parking for nearby retail/service uses and for local residents and visitors in this portion of West Queens Road.

There are existing 'no parking zones' along West Queens Road as shown in yellow on the following map. These 'no parking zones' are to ensure efficient traffic movements near the intersection of West Queens Road and Lonsdale Avenue.

Existing 'no parking zones' (seen in yellow) will remain to ensure intersection manoeuvrability at this corner. The Transportation Section will review the existing parking demand along West Queens Road with intention to implement short term parking limits within this block.

Local residents and affected businesses will be informed of any proposed parking changes and offered an opportunity for input and comment. Any changes to parking regulation signage needed to implement time limited parking will be provided by the developer through the development servicing process.



OPTIONS:

The bylaws are now ready to be considered for Second and Third Readings by Council.

1. Give the bylaws Second and Third Readings; or,
2. Give no further Readings to the bylaws and abandon the bylaws at First Reading.

Respectfully submitted,

Erik Wilhelm,
Community Planner

Attachments:

- The District of North Vancouver Rezoning Bylaw 1337, (Bylaw 8149)
- Housing Agreement Bylaw 8150, 2015 (115 and 123 West Queens Road)
- Public Hearing Minutes – December 1, 2015
- Staff Report – dated October 29, 2015

REVIEWED WITH:		
<input type="checkbox"/> Sustainable Community Dev. _____	<input checked="" type="checkbox"/> Clerk's Office <i>IB</i> _____	External Agencies:
<input type="checkbox"/> Development Services _____	<input type="checkbox"/> Communications _____	<input type="checkbox"/> Library Board _____
<input type="checkbox"/> Utilities _____	<input type="checkbox"/> Finance _____	<input type="checkbox"/> NS Health _____
<input type="checkbox"/> Engineering Operations _____	<input type="checkbox"/> Fire Services _____	<input type="checkbox"/> RCMP _____
<input type="checkbox"/> Parks _____	<input type="checkbox"/> ITS _____	<input type="checkbox"/> NVRC _____
<input type="checkbox"/> Environment _____	<input type="checkbox"/> Solicitor _____	<input type="checkbox"/> Museum & Arch. _____
<input type="checkbox"/> Facilities _____	<input type="checkbox"/> GIS _____	<input type="checkbox"/> Other: _____
<input type="checkbox"/> Human Resources _____	<input type="checkbox"/> Real Estate _____	

The Corporation of the District of North Vancouver

Bylaw 8149

A bylaw to amend the District of North Vancouver Zoning Bylaw 3210, 1965

The Council for The Corporation of the District of North Vancouver enacts as follows:

1. Citation

This bylaw may be cited as "The District of North Vancouver Rezoning Bylaw 1337, (Bylaw 8149)".

2. Amendments

The District of North Vancouver Zoning Bylaw 3210, 1965 is amended as follows:

2.1. Section 301(2) by inserting the following zoning designation in numeric sequence:

“Comprehensive Development Zone CD89”

2.2 Part 4B by inserting the following:

“4B89 Comprehensive Development Zone 89 (CD89)”

4B89-1 Intent:

The purpose of the CD89 zone is to permit development of a 14 unit townhouse development at 115 and 123 West Queens Road.

4B89-2 Uses:

The following *principal uses* shall be permitted in the Comprehensive Development 89 Zone:

(a) Uses Permitted without Conditions:

Residential building, multiple-family townhouse

For the purposes of this CD89 Zone, “Residential building, multiple-family townhouse” means a building having not more than three residential storeys and consisting of two or more dwelling units with individual, exterior access to grade and whereby entrance areas associated with units which are located in the parkade level do not count as a residential storey.

(b) Conditional Uses

Not applicable

4B89-3 Conditions of Use:

Not applicable.

4B89-4 Accessory Use:

(a) *Accessory uses* are permitted and are limited to:

- (i) Home occupations in accordance with the regulations in Section 405 of the Zoning Bylaw, 1965.

4B89-5 Density:

- (a) The maximum permitted density in the CD89 Zone is limited to a floor space ratio (FSR) of 0.45 and a maximum number of 4 units, inclusive of any density bonus for energy performance; and
- (b) For the purposes of calculating floor space ratio, the area of parking garages, electrical/mechanical rooms, garbage and recycling collection areas, and bicycle storage areas, is exempted.

4B89-6 Amenities:

Despite subsection 4B89-5, density in the CD89 Zone is increased to a maximum floor space ratio (FSR) of 1.6 FSR and a maximum number of 14 units, inclusive of any density bonus for energy performance, if the owner:

1. Enters into a Housing Agreement requiring a rental disclosure statement to be filed and prohibiting any strata bylaw or regulation establishing rental restrictions; and
2. Contributes \$91,835 to the municipality to be used for any or all of the following amenities (with allocation to be determined by the municipality in its sole discretion): public art; park, trail, environmental or other public realm improvements; municipal or recreation service or facility improvements; and/or the affordable housing fund.

4B89-7 Maximum Principal Building Size:

Not applicable.

4B89-8 Setbacks:

(a) Basement Level:

Location	Buildings and Structures Minimum setback
Front Yard (West Queens Road)	0 m (0 ft)
Lane	0 m (0 ft)
Rear Yard	2.44 m (8 ft)
West Side Yard	0 m (0 ft)
East Side Yard	0 m (0 ft)

(b) Ground Level:

Location	Buildings and Structures Minimum setback
Front Yard (West Queens Road)	3.56 m (11.67 ft)
Lane	0 m (0 ft)
Rear Yard	2.44 m (8 ft)
West Side Yard	3.75 m (12.3 ft)
East Side Yard	2.59 m (8.5 ft)

(c) Projections above ground level are permissible as follows:

Setback	Maximum Setback Reduction
Building Projections - West Side Yard	0.43 m (1.41 ft)
Building Projections - East Side Yard	0.43 m (1.41 ft)
Balconies – Front Yard	1.52 m (5 ft)

(d) Minimum separation (excluding doorway entrances) at the ground level required between the buildings within the CD89 zone shall be at least 9.14 m. (30 ft).

4B89-9 Building Orientation:

Not applicable.

4B89-10 Building Depth and Width:

Not applicable.

4B89-11 Coverage:

- a) Building Coverage shall not exceed 50%
- b) Site Coverage shall not exceed 82%.

4B89-12 Height:

- a) The maximum building height is 13.7 meters (45 ft).
- b) In the case of a roof, the slope of which is 6 in 12 or greater, the maximum permitted height may be increased by 15%.

4B89-13 Acoustic Requirements:

In the case of residential purposes, a development permit application shall require evidence in the form of a report and recommendations prepared by persons trained in acoustics and current techniques of noise measurements, demonstrating that the noise levels in those portions of the dwelling listed below shall not exceed the noise levels expressed in decibels set opposite such portions of the dwelling units:

Portion of Dwelling Unit	Noise Level (Decibels)
Bedrooms	35
Living and Dining rooms	40
Kitchen, Bathrooms and Hallways	45

4B89-14 Landscaping:

- a) All land areas not occupied by buildings, structures, parking spaces, loading spaces, driveways, manoeuvring aisles and sidewalks shall be landscaped or finished in accordance with an approved landscape plan; and
- b) All electrical kiosks and garbage and recycling container pads not located underground or within a building shall be screened with landscaping or fencing in accordance with an approved landscape plan.

4B89-15 Subdivision Requirements

Within the CD89 zone, the Minimum Lot Area must be at least 1,425 sq. m. (15,338 sq. ft.).

4B89-16 Additional Accessory Structure Regulations

Not applicable.

4B89-17 Parking and Loading Regulations:

- (a) Parking shall be provided at a ratio of 2 parking spaces per unit inclusive of designated visitor and parking for persons with disabilities;
- (b) All regular, small car and handicapped parking spaces shall meet the minimum width and length requirements established in Part 10 of this Bylaw;
- (c) Small car parking spaces shall not exceed 36% of the required parking spaces

(d) Vehicular drive aisles shall meet the minimum width requirements established in Part 10 of this Bylaw

(e) A minimum of 19 Class 2 bicycle parking spaces shall be provided

(f) A bike rack within the shared courtyard that can accommodate a minimum of 5 bicycles shall be provided.”

2.3 The Zoning Map is amended in the case of the lands in Schedule A, by rezoning the land outlined and noted as “site” from Residential Single Family Residential 6000 Zone (RS-4) to Comprehensive Development 89 Zone (CD89).

READ a first time November 9th, 2015

PUBLIC HEARING held December 1st, 2015

READ a second time

READ a third time

Certified a true copy of “Bylaw 8149” as at Third Reading

Municipal Clerk

APPROVED by the Ministry of Transportation and Infrastructure on

ADOPTED

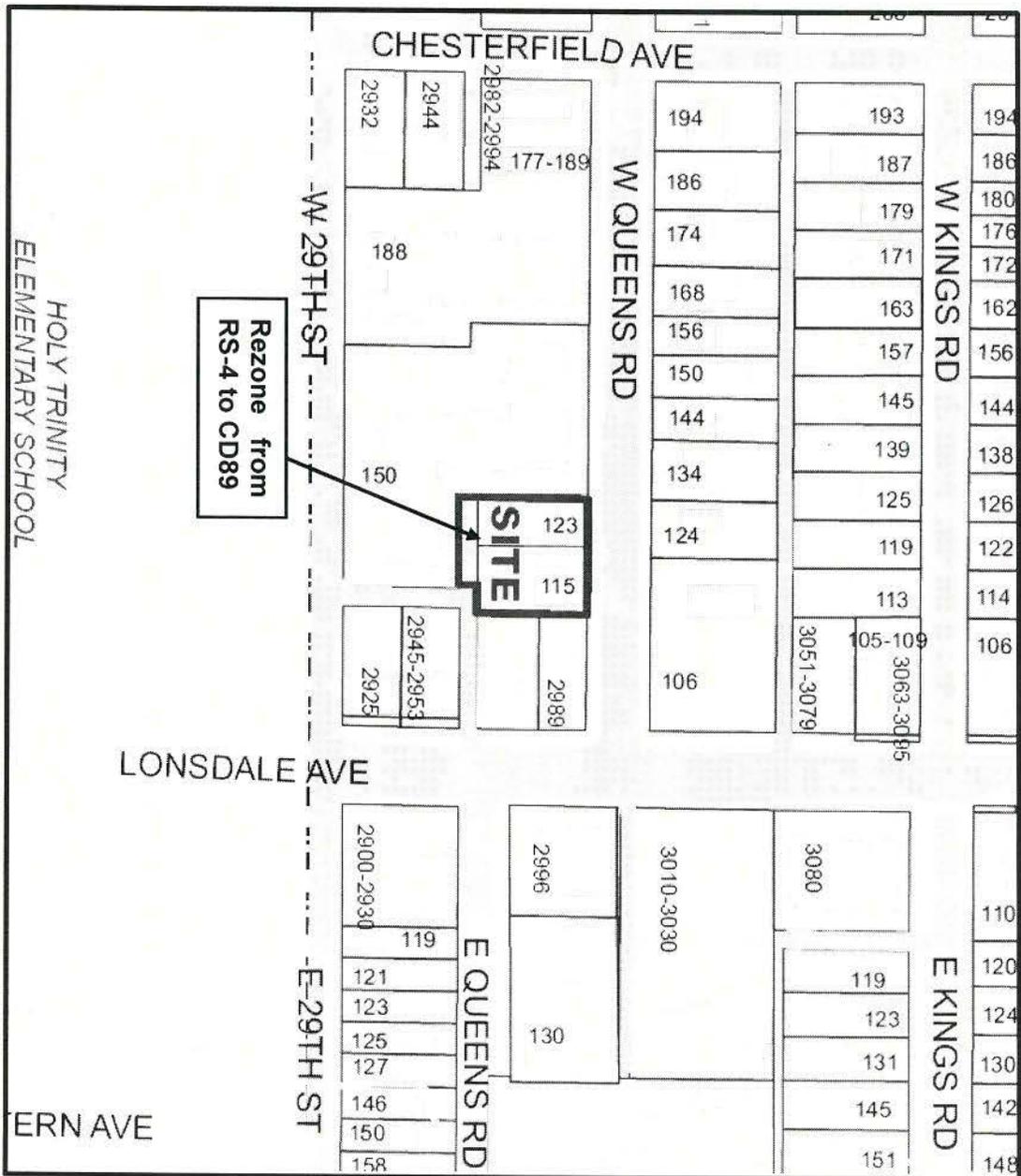
Mayor

Municipal Clerk

Certified a true copy

Municipal Clerk

Schedule A to Bylaw 8149



The Corporation of the District of North Vancouver

Bylaw 8150

A bylaw to enter into a Housing Agreement (115 and 123 West Queens Road)

The Council for The Corporation of the District of North Vancouver enacts as follows:

1. Citation

This bylaw may be cited as "Housing Agreement Bylaw 8150, 2015 (115 and 123 West Queens Road)".

2. Authorization to Enter into Agreement

2.1 The Council hereby authorizes a housing agreement between The Corporation of the District of North Vancouver and Noort Holdings Ltd., Inc. No. BC0092361 substantially in the form attached to this Bylaw as Schedule "A" with respect to the following lands:

- a) Amended Lot 3 (See 290725L) of Lot B, Blocks 4 to 13, District Lot 2026, Plan 3544C (PID: 012-786-683).
- b) Amended Lot 4 (See 290724L) of Lot B, Blocks 4 to 13, District Lot 2026, Plan 3544C (PID: 002-708-035).

3. Execution of Documents

The Mayor and Municipal Clerk are authorized to execute any documents required to give effect to the Housing Agreement.

READ a first time November 9th, 2015

READ a second time

READ a third time

ADOPTED

Mayor

Municipal Clerk

Certified a true copy

Municipal Clerk

Schedule A to Bylaw 8150

SECTION 219 COVENANT – HOUSING AGREEMENT

This agreement dated for reference the ____ day of _____, 20__ is

BETWEEN:

Noort Holdings Ltd., Inc. No. BC0092361 of _____

(the “Owner”)

AND:

THE CORPORATION OF THE DISTRICT OF NORTH VANCOUVER, a municipality incorporated under the *Local Government Act*, R.S.B.C. 1996, c.323 and having its office at 355 West Queens Road, North Vancouver, BC V7N 4N5

(the “District”)

WHEREAS:

1. The Owner is the registered owner of the Lands (as hereinafter defined);
2. The Owner wishes to obtain development permissions with respect to the Lands and wishes to create a development which will contain residential strata units on the Lands;
3. Section 905 of the *Local Government Act* authorises the District, by bylaw, to enter into a housing agreement to provide for the prevention of rental restrictions on housing, and provides for the contents of the agreement; and
4. Section 219 of the *Land Title Act* (British Columbia) permits the registration in favour of the District of a covenant of a negative or positive nature relating to the use of land or a building thereon, or providing that land is to be built on in accordance with the covenant, or providing that land is not to be built on except in accordance with the covenant, or providing that land is not to be subdivided except in accordance with the covenant;

NOW THEREFORE in consideration of the mutual promises contained in it, and in consideration of the payment of \$1.00 by the District to the Owner (the receipt and sufficiency of which are hereby acknowledged by the Owner), the parties covenant and agree with each other as follows, as a housing agreement under Section 905 of the *Local Government Act*, as a contract and a deed under seal between the parties, and as a covenant under Section 219 of the *Land Title Act*, and the Owner hereby further covenants and agrees that neither the Lands nor any building constructed thereon shall be used or built on except in accordance with this Agreement::

Article 1 DEFINITIONS

1.01 Definitions

In this agreement:

- (a) “*Development Permit*” means development permit No. _____ issued by the District;
- (b) “*Lands*” means land described in Item 2 of the *Land Title Act* Form C to which this agreement is attached;
- (c) “*Proposed Development*” means the proposed development containing not more than 14 Units to be constructed on the Lands in accordance with the Development Permit;
- (d) “*Unit*” means a residential dwelling strata unit in the Proposed Development; and
- (e) “*Unit Owner*” means the registered owner of a Dwelling Unit in the Proposed Development.

Article 2TERM

This Agreement will commence upon adoption by District Council of Bylaw 8149 and remain in effect until terminated by the District as set out in this Agreement.

Article 3RENTAL ACCOMODATION

3.01 Rental Disclosure Statement

No Unit in the Proposed Development may be occupied unless the Owner has:

- (f) before the first Unit is offered for sale, or conveyed to a purchaser without being offered for sale, filed with the Superintendent of Real Estate a Rental Disclosure Statement designating all of the Units as rental strata lots and imposing a ninety-nine (99) year rental period in relation to all of the Units pursuant to the *Strata Property Act* (or any successor or replacement legislation); and
- (g) given a copy of the Rental Disclosure Statement to each prospective purchaser of any Unit before the prospective purchaser enters into an agreement to purchase in respect of the Unit.

3.02 Rental Accommodation

The Units constructed on the Lands from time to time may always be used to provide rental accommodation as the Owner or a Unit Owner may choose from time to time.

3.03 Binding on Strata Corporation

This agreement shall be binding upon all strata corporations created by the subdivision of the Lands or any part thereof (including the Units) pursuant to the *Strata Property Act*, and upon all Unit Owners. .

3.04 Strata Bylaw Invalid

Any strata Corporation bylaw which prevents, restricts or abridges the right to use any of the Units as rental accommodations shall have no force or effect.

3.05 No Bylaw

The strata corporation shall not pass any bylaws preventing, restricting or abridging the use of the Lands, the Proposed Development or the Units contained therein from time to time as rental accommodation.

3.06 Vote

No Unit Owner, nor any tenant or mortgagee thereof, shall vote for any strata corporation bylaw purporting to prevent, restrict or abridge the use of the Lands, the Proposed Development and the units contained therein from time to time as rental accommodation.

3.07 Notice

The Owner will provide notice of this Agreement to any person or persons intending to purchase a Unit prior to any such person entering into an agreement of purchase and sale, agreement for sale, or option or similar right to purchase as part of the Disclosure Statement for any part of the Proposed Development prepared by the Owner pursuant to the *Real Estate Development Marketing Act*.

Article 4 DEFAULT AND REMEDIES

4.01 Notice of Default

The District may, acting reasonably, give to the Owner written notice to cure a default under this Agreement within thirty (30) days of delivery of the notice. The notice must specify the nature of the default. The Owner must act with diligence to correct the default within the time specified.

4.02 Costs

The Owner will pay to the District upon demand all the District's costs of exercising its rights or remedies under this Agreement, on a full indemnity basis.

4.03 Damages an Inadequate Remedy

The Owner acknowledges and agrees that in the case of a breach of this Agreement which is not fully remediable by the mere payment of money and promptly so remedied, the harm sustained by the District and to the public interest will be irreparable and not susceptible of adequate monetary compensation.

4.04 Equitable Remedies

Each party to this Agreement, in addition to its rights under this Agreement or at law, will be entitled to all equitable remedies including specific performance, injunction and declaratory relief, or any of them, to enforce its rights under this Agreement.

4.05 No Penalty or Forfeiture

The Owner acknowledges and agrees that it is entering into this Agreement to benefit the public interest in providing rental accommodation, and that the District's rights and remedies under this Agreement are necessary to ensure that this purpose is carried out,

and the District's rights and remedies under this Agreement are fair and reasonable and ought not to be construed as a penalty or forfeiture.

4.06 Cumulative Remedies

No reference to nor exercise of any specific right or remedy under this Agreement or at law or at equity by any party will prejudice, limit or preclude that party from exercising any other right or remedy. No right or remedy will be exclusive or dependent upon any other right to remedy, but any party, from time to time, may exercise any one or more of such rights or remedies independently, successively, or in combination. The Owner acknowledges that specific performance, injunctive relief (mandatory or otherwise) or other equitable relief may be the only adequate remedy for a default by the Owner under this Agreement.

Article 5 LIABILITY

5.01 Indemnity

Except if arising directly from the negligence of the District or its employees, agents or contractors, the Owner will indemnify and save harmless each of the District and its board members, officers, directors, employees, agents, and elected or appointed officials,, and their heirs, executors, administrators, personal representatives, successors and assigns, from and against all claims, demands, actions, loss, damage, costs and liabilities that all or any of them will or may be liable for or suffer or incur or be put to any act or omission by the Owner or its officers, directors, employees, agents, contractors, or other persons for whom the Owner is at law responsible, or by reason of or arising out of the Owner's ownership, operation, management or financing of the Proposed Development or any part thereof.

5.02 Release

The Owner hereby releases and forever discharges the District, its elected officials, board members, officers, directors, employees and agents, and its and their heirs, executors, administrators, personal representatives, successors and assigns from and against all claims, demands, damages, actions or causes of action by reason of or arising out of advice or direction respecting the ownership, operation or management of the Proposed Development or any part thereof which has been or hereafter may be given to the Owner by all or any of them.

5.03 Survival

The covenants of the Owner set out in Sections 5.01 and 5.02 will survive termination of this Agreement and continue to apply to any breach of the Agreement or claim arising under this Agreement during the ownership by the Owner of the Lands or any Unit therein, as applicable.

Article 6 GENERAL PROVISIONS

6.01 District's Power Unaffected

Nothing in this Agreement:

- (a) affects or limits any discretion, rights, powers, duties or obligations of the District under any enactment or at common law, including in relation to the use or subdivision of land;
- (b) affects or limits any enactment relating to the use of the Lands or any condition contained in any approval including any development permit concerning the development of the Lands; or
- (c) relieves the Owner from complying with any enactment, including the District's bylaws in relation to the use of the Lands.

6.02 Agreement for Benefit of District Only

The Owner and District agree that:

- (a) this Agreement is entered into only for the benefit of the District;
- (b) this Agreement is not intended to protect the interests of the Owner, any Unit Owner, any Occupant or any future owner, occupier or user of any part of the Proposed Development, including any Unit, or the interests of any third party, and the District has no obligation to anyone to enforce the terms of this Agreement; and
- (c) The District may at any time terminate this Agreement, in whole or in part, and execute a release and discharge of this Agreement in respect of the Proposed Development or any Unit therein, without liability to anyone for doing so.

6.03 Agreement Runs With the Lands

This Agreement burdens and runs with the Lands and any part into which any of them may be subdivided or consolidated, by strata plan or otherwise. All of the covenants and agreements contained in this Agreement are made by the Owner for itself, its successors and assigns, and all persons who acquire an interest in the Lands or in any Unit after the date of this Agreement.

6.04 Release

The covenants and agreements on the part of the Owner and any Unit Owner and herein set forth in this Agreement have been made by the Owner and any Unit Owner as contractual obligations as well as being made pursuant to Section 905 of the *Local Government Act* (British Columbia) and as such will be binding on the Owner and any Unit Owner, except that neither the Owner nor any Unit Owner shall be liable for any default in the performance or observance of this Agreement occurring after such party ceases to own the Lands or a Unit as the case may be.

6.05 Priority of This Agreement

The Owner will, at its expense, do or cause to be done all acts reasonably necessary to ensure this Agreement is registered against the title to each Unit in the Proposed Development, including any amendments to this Agreement as may be required by the Land Title Office or the District to affect such registration.

6.06 Agreement to Have Effect as Deed

The District and the Owner each intend by execution and delivery of this Agreement to create both a contract and a deed under seal.

6.07 Waiver

An alleged waiver by a party of any breach by another party of its obligations under this Agreement will be effective only if it is an express waiver of the breach in writing. No waiver of a breach of this Agreement is deemed or construed to be a consent or waiver of any other breach of this Agreement.

6.08 Time

Time is of the essence in this Agreement. If any party waives this requirement, that party may reinstate it by delivering notice to another party.

6.09 Validity of Provisions

If a Court of competent jurisdiction finds that any part of this Agreement is invalid, illegal, or unenforceable, that part is to be considered to have been severed from the rest of this Agreement and the rest of this Agreement remains in force unaffected by that holding or by the severance of that part.

6.10 Extent of Obligations and Costs

Every obligation of a party which is set out in this Agreement will extend throughout the Term and, to the extent that any obligation ought to have been observed or performed prior to or upon the expiry or earlier termination of the Term, such obligation will survive the expiry or earlier termination of the Term until it has been observed or performed.

6.11 Notices

All notices, demands, or requests of any kind, which a party may be required or permitted to serve on another in connection with this Agreement, must be in writing and may be served on the other parties by registered mail, by facsimile transmission, or by personal service, to the following address for each party:

If to the District:

District Municipal Hall
355 West Queens Road
North Vancouver, BC V7N 4N5

Attention: Planning Department
Facsimile: (604) 984-9683

If to the Owner:

Attention:
Facsimile: (604)

If to the Unit Owner:

The address of the registered owner which appears on title to the Unit at the time of notice.

Service of any such notice, demand, or request will be deemed complete, if made by registered mail, 72 hours after the date and hour of mailing, except where there is a postal service disruption during such period, in which case service will be deemed to be complete only upon actual delivery of the notice, demand or request; if made by facsimile transmission, on the first business day after the date when the facsimile transmission was transmitted; and if made by personal service, upon personal service being effected. Any party, from time to time, by notice in writing served upon the other parties, may designate a different address or different or additional persons to which all notices, demands, or requests are to be addressed.

6.12 Further Assurances

Upon request by the District, the Owner will promptly do such acts and execute such documents as may be reasonably necessary, in the opinion of the District, to give effect to this Agreement.

6.13 Enuring Effect

This Agreement will enure to the benefit of and be binding upon each of the parties and their successors and permitted assigns.

Article 7 INTERPRETATION

7.01 References

Gender specific terms include both genders and include corporations. Words in the singular include the plural, and words in the plural include the singular.

7.02 Construction

The division of this Agreement into sections and the use of headings are for convenience of reference only and are not intended to govern, limit or aid in the construction of any provision. In all cases, the language in this Agreement is to be construed simply according to its fair meaning, and not strictly for or against either party.

7.03 No Limitation

The word “including” when following any general statement or term is not to be construed to limit the general statement or term to the specific items which immediately follow the general statement or term similar items whether or not words such as “without limitation” or “but not limited to” are used, but rather the general statement or term is to be construed to refer to all other items that could reasonably fall within the broadest possible scope of the general statement or term.

7.04 Terms Mandatory

The words “must” and “will” and “shall” are to be construed as imperative.

7.05 Statutes

Any reference in this Agreement to any statute or bylaw includes any subsequent amendment, re-enactment, or replacement of that statute or bylaw.

7.06 Entire Agreement

- (a) This is the entire agreement between the District and the Owner concerning its subject, and there are no warranties, representations, conditions or collateral agreements relating to this Agreement, except as included in this Agreement.
- (b) This Agreement may be amended only by a document executed by the parties to this Agreement and by bylaw, such amendment to be effective only upon adoption by District Council of a bylaw to amend Bylaw 8150.

7.07 Governing Law

This Agreement is to be governed by and construed and enforced in accordance with the laws of British Columbia.

As evidence of their agreement to be bound by the terms of this instrument, the parties hereto have executed the *Land Title Act* Form C that is attached hereto and forms part of this Agreement.

CONSENT AND PRIORITY AGREEMENT

GIVEN THAT:

1. _____ (the "Owner") is the Registered Owner of the Land described in Item 2 of Page 1 of the Form C (the "Land");
2. The Owner granted _____ (the "Prior Chargeholder") a Mortgage and Assignment of Rents registered against title to the Land in the Lower Mainland Land Title Office (the "LTO") under Nos. _____, as extended by _____ and _____, as extended by _____ (together, the "Prior Charge");
3. The Owner granted to THE CORPORATION OF THE DISTRICT OF NORTH VANCOUVER (the "District") a Covenant attached to this Agreement and registered against title to the Land in the LTO immediately before registration of this Agreement (the "Subsequent Charge"); and
4. Section 207 of the *Land Title Act* permits the Prior Chargeholder to grant priority over a charge to the District as Subsequent Chargeholder.

THEREFORE this Agreement is evidence that in consideration of \$1.00 and other good and valuable consideration received by the Prior Chargeholder from the District (the receipt and sufficiency of which the Prior Chargeholder acknowledges):

1. The Prior Chargeholder consents to the granting and registration of the Subsequent Charge and the Prior Chargeholder agrees that the Subsequent Charge shall be binding upon their interest in and to the Land.
2. The Prior Chargeholder grants to the District, as a Subsequent Chargeholder, priority for the Subsequent Charge over the Prior Chargeholder's right, title and interest in and to the Land, and the Prior Chargeholder postpones the Prior Charge and all of their right, title and interest thereunder to the Subsequent Charge as if the Subsequent Charge had been executed, delivered and registered prior to the execution, delivery and registration of the Prior Charge.

As evidence of its agreement to be bound by the terms of this instrument, the Prior Chargeholder has executed the Land Title Office Form C to which this Agreement is attached and which forms part of this Agreement.

**DISTRICT OF NORTH VANCOUVER
PUBLIC HEARING**

REPORT of the Public Hearing held in the Council Chambers of the Municipal Hall, 355 West Queens Road, North Vancouver, B.C. on Tuesday, December 1, 2015 commencing at 7:05 p.m.

Present: Mayor R. Walton (7:17 pm)
Councillor R. Bassam
Councillor M. Bond
Councillor J. Hanson
Councillor R. Hicks

Absent: Councillor D. MacKay-Dunn
Councillor L. Muri

Staff: Mr. D. Milburn, Deputy General Manager – Planning & Permits
Mr. J. Gordon, Manager – Administrative Services
Ms. J. Paton, Manager – Development Planning
Ms. L. Brick, Deputy Municipal Clerk
Mr. E. Wilhelm, Planner
Ms. C. Archer, Confidential Council Clerk

Also in

Attendance: Mr. B. Wallace, Consultant, BWW Consulting
Mr. M. Wick, Development Manager, Noort Developments Ltd.
Mr. T. Yamamoto, Principal, Yamamoto Architecture Inc.

MOVED by Councillor HANSON

SECONDED by Councillor BOND

THAT Councillor Bassam be appointed the Presiding Member.

CARRIED

Absent for Vote: Mayor WALTON

The District of North Vancouver Rezoning Bylaw 1337 (Bylaw 8149)

Purpose of Bylaw:

Bylaw 8149 proposes to amend the District's Zoning Bylaw by rezoning the subject site from Residential Single-Family Residential 6000 Zone (RS4) to Comprehensive Development 89 Zone (CD89) to allow the development of a fourteen unit townhouse project.

1. OPENING BY THE CHAIR

Councillor Bassam welcomed everyone and advised that the purpose of the Public Hearing was to receive input from the community and staff on the proposed bylaw as outlined in the Notice of Public Hearing.

In the Chair's preamble he addressed the following:

- All persons who believe that their interest in property is affected by the proposed bylaw will be afforded a reasonable opportunity to be heard and to present written submissions;
- Use the established speakers list. At the end of the speakers list, the Chair may call on speakers from the audience;
- Speakers will have five minutes to address Council for a first time and are asked to begin remarks to Council by stating their names and addresses;
- After everyone who wishes to speak has spoken once, speakers will then be allowed one additional five minute presentation;
- Any additional presentations will only be allowed at the discretion of the Chair;
- All members of the audience are asked to refrain from applause or other expressions of emotion. Council wishes to hear everyone's views in an open and impartial forum;
- Council is here to listen to the public, not to debate the merits of the bylaw;
- The Clerk has a binder containing documents and submissions related to this bylaw which Council has received and which attendees at the Hearing are welcome to review;
- Everyone at the Hearing will be provided an opportunity to speak. If necessary, the Hearing will be continued on a second night;
- At the conclusion of the public input Council may request further information from staff which may or may not require an extension of the Hearing, or Council may close the Hearing after which Council cannot receive further new information from the public; and,
- The Public Hearing is streamed live over the internet and recorded in accordance with the Freedom of Information and Protection of Privacy Act.

2. INTRODUCTION OF BYLAW BY CLERK

Ms. Linda Brick, Deputy Municipal Clerk, introduced the proposed bylaw stating that Bylaw 8149 proposes to amend the District's Zoning Bylaw by rezoning the subject site from Residential Single-Family Residential 6000 Zone (RS4) to Comprehensive Development 89 Zone (CD89) to allow the development of a fourteen unit townhouse project at 115 and 123 West Queens Road.

3. PRESENTATION BY STAFF

Mr. Erik Wilhelm, Planner, advised that the site is composed of two single family lots and a portion of the lane to the rear on the south side of West Queens Road, just west of the Queens Cross Pub. The Churchill House Retirement Residence is located west and southwest of the site.

The original Rezoning Bylaw proposed was for an eighteen unit townhouse, reduced to seventeen units following a Public Hearing. The Bylaw was defeated at Second Reading, with Council noting the following items of concern:

- Number of units;
- Laneway traffic circulation; and,
- The inclusion of tandem parking stalls.

To address these concerns, the revised proposal is for fourteen units, with a new laneway traffic circulation plan and the elimination of all tandem parking stalls.

The proposal is for two primary buildings separated by an interior courtyard. Access to the units is via West Queens Road for the six units in the north building and via the courtyard for the eight units in the south building.

Mr. Wilhelm reviewed the revised laneway circulation, highlighting that:

- The existing barrier in the laneway between the development and Lonsdale Avenue will be removed;
- Traffic exiting the underground parking will be left-turn only and one-way towards Lonsdale Avenue;
- Traffic entering the north-south lane towards the building will be one-way northbound starting after the Starbucks parking lot; and,
- The proposed laneway circulation is to address traffic congestion during peak morning hours.

Mr. Wilhelm provided a project overview, noting that the north building fronting West Queens Road is a three-storey, six unit townhouse with traditional rooflines, staggered in three sections sloping downwards with the slope of the road. It was noted that cherry trees located at the Queens Cross Pub will not be affected by the development and that street trees will be added at the front of the building. The revised proposal includes 28 vehicle parking spaces, including three visitor stalls, 14 secure bicycle stalls for residents and a five-unit bicycle rack in the courtyard for visitors.

The Community Amentity Contribution for the project has been calculated at \$91,835. The project also provides other community benefits, including new widened sidewalks and street lighting.

Mayor Walton arrived at this point in the proceedings.

4. PRESENTATION BY THE APPLICANT

Mr. Taizo Yamamoto, Principal, Yamamoto Architecture, advised that the design has been changed to reduce the number of units on the north side by changing from upper and lower stacked units to single, three level units, eliminating the need for secondary stairs and second entrances above each ground floor entrance. This has also allowed for an increase in storage area for each unit below grade. The garbage area has been moved closer to the lane to simplify pickup.

Mr. Brian Wallace, BWW Consulting, advised that he is the traffic consultant on the project. Mr. Wallace advised that the use of street parking during construction could be minimized by making use of the parking lot under the Queens Cross Pub during the day, when it is underutilized, and the development's underground parking lot once constructed.

Mr. Wallace advised that he has concluded that the effect of this project on the neighbourhood traffic would be negligible after construction is completed. The traffic circulation plan is in response to the comments and discussion around traffic and congestion raised at the Public Hearing for the previous proposal. In response to a question from Council, Mr. Wallace advised that the lane exiting onto Lonsdale Avenue would be signed right turn only.

In response to a question from Council, staff advised there has been no discussion about restricting overnight parking on West Queens Road.

Mr. Merlin Wick, Noort Developments Ltd., explained that the increased number of parking spaces per unit and the elimination of tandem spaces was in response to comments from Council and that the increase in floor space per unit was in response to comments made at the previous Public Hearing that many North Vancouver residents selling single family homes are looking for larger townhouses.

5. REPRESENTATIONS FROM THE PUBLIC

5.1. Mr. Corrie Kost, 2800 Block Colwood Drive: COMMENTING

- Commented on the increased size of the units over the original application; and,
- Noted that laneway traffic would be slightly exacerbated by the development.

5.2. Mr. Scott Schneider, 124 West Queens Road: COMMENTING

- Commented on street parking near the proposed development; and,
- Suggested introducing parking restrictions during the day.

6. QUESTIONS FROM COUNCIL

Nil

7. COUNCIL RESOLUTION

MOVED by Councillor BASSAM

SECONDED by Councillor BOND

THAT the December 1, 2015 Public Hearing be closed;

AND THAT "The District of North Vancouver Rezoning Bylaw 1337 (Bylaw 8149)" be returned to Council for further consideration.

CARRIED
(7:40 p.m.)

CERTIFIED CORRECT:



Confidential Council Clerk

AGENDA INFORMATION	
<input checked="" type="checkbox"/> Regular Meeting	Date: <u>Nov 9, 2015</u>
<input type="checkbox"/> Workshop (open to public)	Date: _____



**The District of North Vancouver
REPORT TO COUNCIL**

October 29, 2015
File: 08.3060-20/033.15

AUTHOR: Erik Wilhelm, Planner

SUBJECT: BYLAW 8149 (REZONING BYLAW 1337) &
BYLAW 8150 (HOUSING AGREEMENT BYLAW):
14 UNIT TOWNHOUSE DEVELOPMENT AT 115 & 123 WEST QUEENS RD.

RECOMMENDATION: It is recommended that:

1. Bylaw 8149, which amends the Zoning Bylaw to rezone the properties at 115 and 123 West Queens Road from Single-Family Residential 6000 zone (RS4) to Comprehensive Development Zone 89 (CD89) to permit an 14 unit townhouse project, be given First Reading;
2. Bylaw 8149 be referred to a Public Hearing; and
3. Bylaw 8150, which authorizes a Housing Agreement to prevent future rental restrictions, be given First Reading.

REASON FOR REPORT:

The proposed townhouse project requires Council's consideration of Bylaw 8149 to rezone the subject properties. Bylaw 8150 implements the District's Strata Rental Protection Policy for the proposed project to ensure the units retain the option to be rented. The development proposal has been reduced in density and number of units since previously being considered by Council. In addition, access from the rear lane and underground parking have been reconfigured to address concerns regarding potential lane conflicts and to remove tandem parking. With these changes the applicant has submitted a new rezoning application and staff feel the revised application merits Council consideration of first reading.



SUMMARY:

Noort Developments Ltd. seeks rezoning of two single family properties to allow a 14 unit townhouse project at 115 and 123 West Queens Road. The proposal complies with the Official Community Plan (OCP) designation and the bylaws supporting the project are recommended for introduction and referral to Public Hearing.

BACKGROUND:

In the spring of 2014, Council considered Bylaw 8113 which would have facilitated rezoning of the development site to allow an 18 unit stacked townhouse development. A Public Hearing was held on April 21, 2015. Following input at the Public Hearing, the applicant reduced the number of units to 17. Council defeated Bylaw 8113 and Bylaw 8114 (Housing Agreement Bylaw) at second reading on May 4, 2015 with concerns of lane traffic circulation, number of units and number of tandem stalls.

Following Council input at defeat of the bylaws, the applicant has revised the development to address Council concerns. Specifically, the Floor Space Ratio (FSR) has been reduced from 1.68 to 1.6, the number of units has been reduced from 17 to 14 units, underground parking has been reconfigured to improve circulation and remove all tandem parking stalls. A revised traffic circulation pattern in the lanes rear of the development site is proposed.

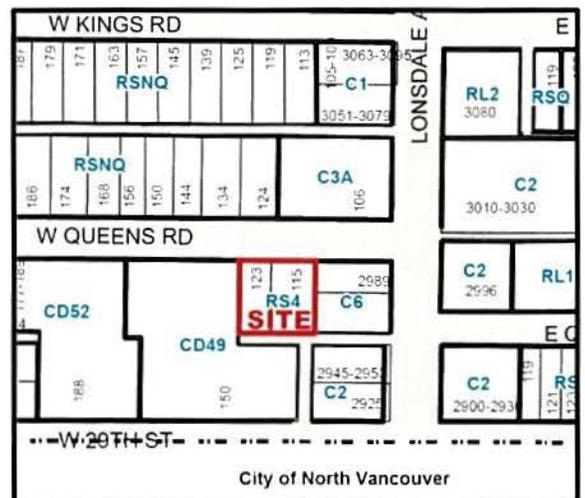
The new development application is substantially altered and addresses Council's concerns raised with the previous application; accordingly, the application requires consideration by Council.

ANALYSIS:

Site and Surrounding Area:

The development site is currently two detached residential properties on the south side of West Queens Road just west of Lonsdale Avenue. The Queens Cross Pub is located directly east of the site and 'Churchill Retirement Residences' is located south and west of the site. A gas station is located northeast of the site and single family homes are located to the north and northwest of the site. The site is currently zoned Single-Family Residential 6000 (RS4).

The development site is within the Queensdale "Village Centre" – one of six designated village centres throughout the District. Generally, the OCP designations near the core of village centres support increased multi-family and mixed use densities in order to take advantage of transit options, access to services and shops and general walkability. The



objective for each village centre is to build on their own unique characteristics and to create distinct urban village environments.

As is the case for the Queensdale Village Centre, transit service on Lonsdale and West Queens Road corridors and nearby access to goods and services within a compact commercial area further reduces the need for additional vehicle trips for local residents.

EXISTING POLICY:

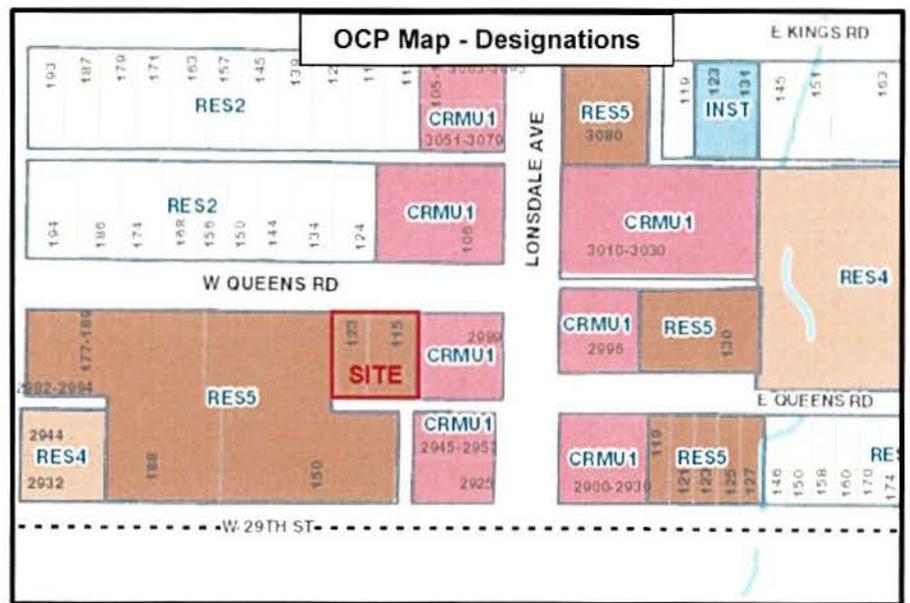
Official Community Plan (OCP)

The site is designated Residential Level 5: "Low Density Apartment" (RES5) in the OCP and medium density multi-family in the North Lonsdale-Delbrook Plan reference policy document.

Areas designated for low density apartment within the OCP are intended predominantly for multifamily housing in centres and corridors at a density of up to approximately 1.75 FSR. Development in this designation will typically be expressed in low-rise apartments, but may include some townhouses.

Properties to the west have the same RES5 OCP designation as the development site while those along Lonsdale Avenue are designated Commercial Residential Mixed Use Level One (CRMU1) which allows mixed-use development to a maximum density of up to approximately 1.75 FSR.

The proposed FSR for the development has been reduced from approximately 1.68 to 1.6 and is consistent with the OCP designation which allows for a density of up to approximately 1.75 FSR.



Zoning

The site is currently zoned Single-Family Residential 6000 (RS4). To accommodate the proposed townhouse development the site requires rezoning; accordingly, Bylaw 8149 (Attachment B) proposes to create a new Comprehensive Development Zone 89 (CD89).

Strata Rental Protection Policy

Corporate Policy 8-3300-2 "Strata Rental Protection Policy" applies to this project as the rezoning application would permit development of more than five residential units. The policy requires a Housing Agreement to ensure that future strata bylaws do not prevent owners from renting their units. Bylaw 8150 (Attachment C) authorizes a Housing Agreement to implement this policy.

Development Permit Areas (DPA)

The development site is designated as Development Permit Areas for the following purposes:

- Form and Character of Multi-Family Development (Multi-Family Housing); and
- Energy and Water Conservation and Greenhouse Gas Emission Reductions.

A detailed development permit report, outlining the project's compliance with the applicable DPA guidelines will be provided for Council's consideration at the Development Permit stage should the rezoning proceed.

PROJECT DESCRIPTION:

The development includes 14 townhouse units in two separate buildings arranged around a central courtyard. Six units will have direct front door access to West Queens Road while the remaining 8 units will be accessed from the courtyard. The adjacent site plan indicates the general siting of the proposed buildings.



The application requires purchase of a portion of municipal lane located south of the development site and the lane closure bylaw has been given 3 readings by Council. The secured underground parking area will be accessed from the rear of the site via the existing lane.

A mix of two bedroom, three bedroom and three bedroom with dens units is proposed within the development. The drawing below illustrates the proposal as viewed from West Queens Road and the appearance of the development from the street. The architectural drawing package is attached as Attachment A.

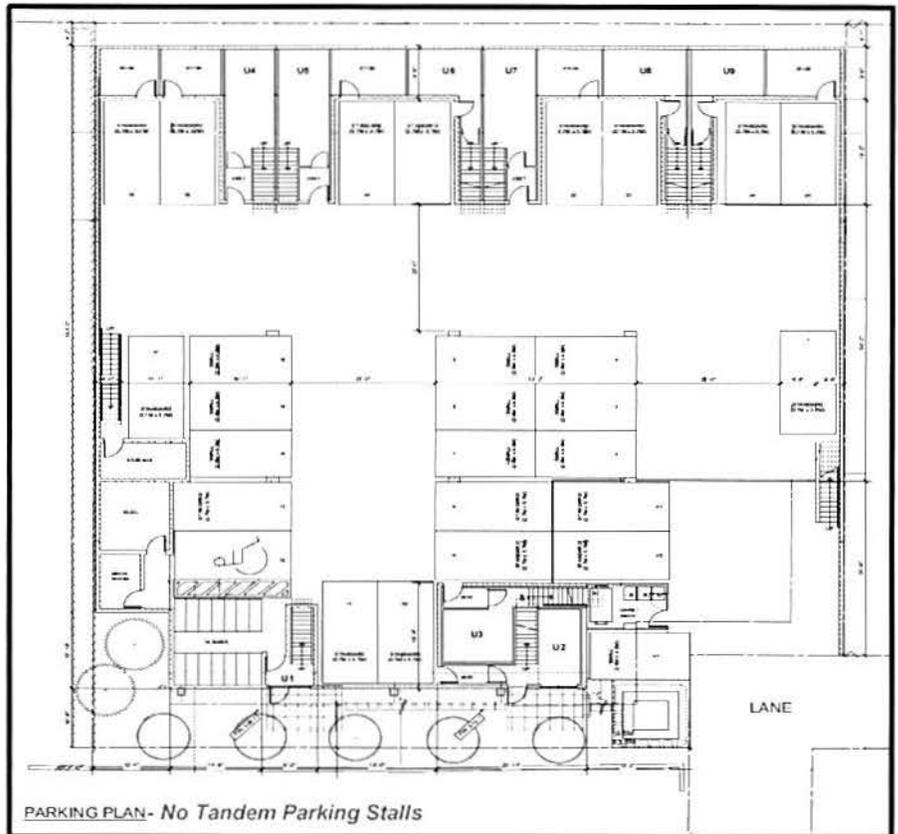
Rendering- Looking Southwest on W. Queens Rd.



Twenty-eight parking stalls are provided for the development in an underground parking garage; 3 of the stalls are designated visitor spaces. Parking proposed for the development meets Zoning Bylaw minimum requirements of two stalls per unit.

The development will provide:

- 19 bicycle stalls;
- 28 parking stalls (25 for residents & 3 for visitors);
- No tandem parking stalls;
- 10 small car parking stalls (36% of total parking);
- 1 universally accessible parking stall.



As seen below, the development includes a sloped-roof design that provides some variety in design compared to the flat roof design seen in the existing Churchill Residences building to the west and to reflect the sloped roof designs seen in the single family homes northwest of the site and the Queens Cross Pub to the east. The revised design includes six front door entrances onto West Queens Road.



NORTH ELEVATION (W. QUEENS RD.)

Accessible Units

In response to the District's "Accessible Design Policy for Multi-Family Housing", at the development permit stage, the developer will be required to submit an "Accessible Multi-Family Housing Design Checklist" which identifies how the development attains the requirements of the policy. Two units (units 4 and 9) will provide "basic accessible design elements" and have built in features which allow easy retrofit installation of "enhanced accessible design elements" which provides a higher level accessibility.

REZONING BYLAW 8149

To implement this project, Bylaw 8149 (Attachment B) creates a new Comprehensive Development Zone 89 (CD89). This new zone:

- establishes ground-oriented multiple family residential use as a permitted use;
- limits the number of units to 14;
- limits the FSR to a maximum of 1.60;
- establishes a maximum building height of 45 ft with a small height bonus to accommodate the sloped roofs;
- sets site coverage at a maximum of 82%
- sets building coverage at a maximum of 50%;
- establishes acoustic performance requirements;
- requires a parking ratio of 2 stalls per unit;
- establishes minimum drive aisle widths in accordance with Part 10 of the Zoning Bylaw; and
- establishes the following building setbacks
 - Front Setback: 3.56 metres
 - West Side Yard Setback: 3.75 metres
 - East Side Yard Setback: 2.59 metres
 - Rear Yard Setback 3.35 metres
 - Setback from Lane: 0 metres

ACOUSTIC REGULATIONS

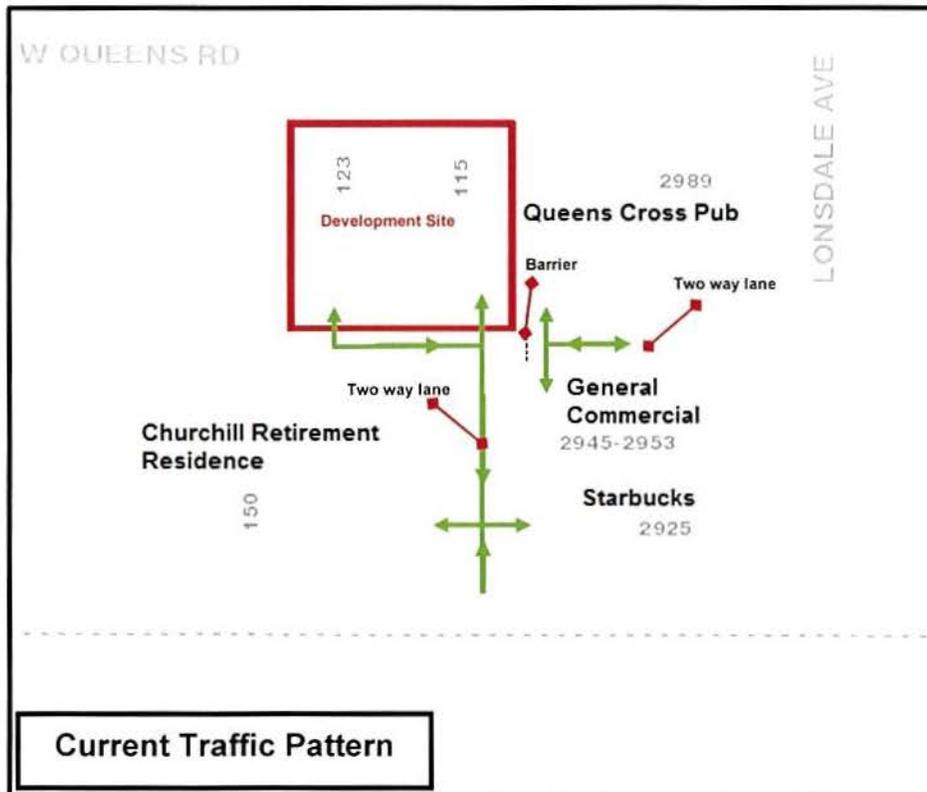
The CD89 zone includes the District's residential acoustic regulations for maximum noise levels in bedrooms, living areas and other areas of the residential units where units front an arterial roadway. The application will be required to meet these requirements.

COMMUNITY AMENITY CONTRIBUTIONS

As the subject property requires rezoning, a community amenity contribution (CAC) has been calculated in the amount of \$91,835.00 in accordance with District policy. The CD89 specifies this amount in order to achieve the maximum density of 1.6 FSR in the zone and outlines what the CAC can be applied toward including public art, park, trail, environmental or other public realm improvements, municipal or recreation service or facility improvements and/or affordable housing.

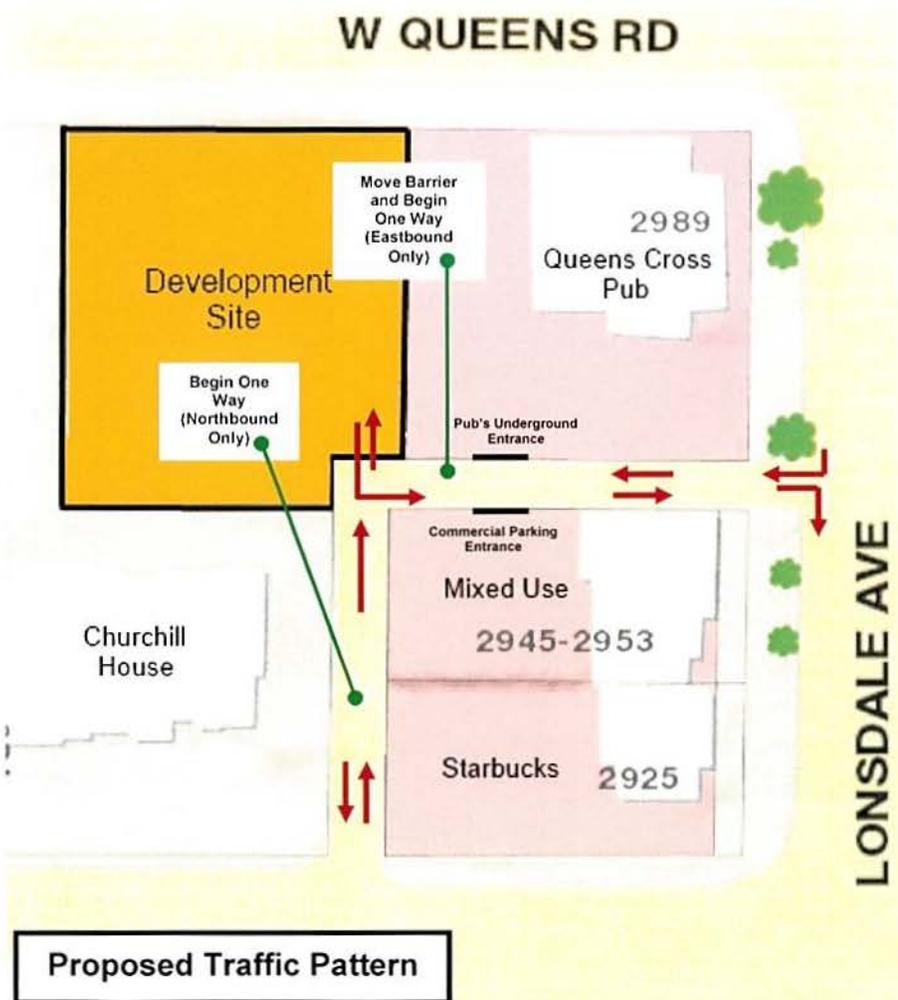
VEHICLE/LANE ACCESS:

The map below indicates the existing traffic movements in the local area. The existing “two-way” north/south lane allows traffic to move in both directions. There is an existing barrier within the “two-way” east/west lane just south of the Queens Cross Pub that stops movements westward from Lonsdale Avenue. This barrier allows entrance into the Queen Cross Pub underground parking and the parking lot for the commercial site at 2945-2953 Lonsdale Avenue.



During consideration of the previous bylaw Council expressed concerns with proposed lane movements and access to the site with particular concern regarding the “Starbucks Corner” at the intersection of the lane and West 29th Street. To address this concern the applicant’s transportation engineer studied the access and lane and has proposed changes to alleviate potential traffic conflicts within the lane area and “Starbucks Corner”.

The map on the next page indicates the proposed traffic movements within the N/S and E/W lanes at the rear the proposed development. The aim of the proposed traffic configuration is to reduce movements/conflicts at the Starbucks Corner in the peak am hours (i.e. morning coffee) and to spread the traffic generated by the development onto two lanes.



In simple terms, the development's vehicular traffic leaves the site through the E/W lane and must return to the development via the N/S lane which reduces the overall traffic at the Starbucks Corner (when compared to a two way laneway traffic scenario).

The developer's traffic engineer has submitted a traffic analysis report which supports the proposed traffic circulation within the lane areas. Furthermore, the developer will be required to provide a post development analysis (after 6 months of the development being fully occupied) in order to review the traffic movements in the area and to make recommendations for further changes if required. The Traffic Engineering Department has reviewed the traffic analysis report and is supportive of the proposed traffic circulation and requirement for a post development analysis.

CONSTRUCTION TRAFFIC MANAGEMENT PLAN:

In order to reduce development's impact on pedestrian and vehicular movements, the developer will be required to provide a 'Construction Traffic Management Plan' as a condition of a Development Permit. The Construction Traffic Management Plan must minimize construction impacts on pedestrian movement and vehicular traffic along West Queens Road and within the lanes behind the development site. The plan is required to be approved by the District prior to issuance of a Building Permit.

In particular, the 'Construction Traffic Management Plan' must:

1. Limit sidewalk closures to those necessary for sidewalk upgrades along West Queens Road and include measures to reduce any impacts to traffic and pedestrians;
2. Outline roadway efficiencies (i.e. location of traffic management signs and flaggers);
3. Provide a point of contact for all calls and concerns;
4. Provide a sequence and schedule of construction activities;
5. Identify methods of sharing construction schedule with other developments in the area;
6. Define locations for truck marshalling and trade vehicle parking which are acceptable to the District and minimize impacts to neighbourhoods; and
7. Include a communication plan to notify surrounding businesses and residents.

There are no other pending rezoning or development permit projects within the Queensdale Village Centre that are slated for consideration by Council.

Advisory Design Panel:

The initial 18 unit proposal was presented to the Advisory Design Panel on December 11, 2014. The Panel recommended approval subject to staff resolution of material use and location, finishes and simplification of design. The applicant revised the proposal to address ADP recommendations.

The revised 14 unit development has a more simplified design, improved courtyard landscaping and provided for more front door entrances onto West Queens Road. The current application maintains a similar architectural form and finish and therefore was not referred to the ADP.

CONCURRENCE:

Staff:

The project has been reviewed by Building, Parks, Engineering and Transportation, Urban Design Planning, Real Estate and Properties, and Fire Prevention staff. Staff has made recommendations, throughout the development process, to improve the development and to address Council concerns.

Public Input:

As part of the initial 18 unit development proposal, the applicant held a facilitated Public Information Meeting on November 27, 2014 at 2641 Chesterfield Avenue (St. Andrew's and St. Stephen's Presbyterian Church). The applicant provided notices to owners and occupants within 75 metres (246 ft.) of the development site. The meeting was attended by five people and an excerpt from the facilitator's summary is attached (Attachment D). At the Public Hearing held on April 21, 2015, three residents raised concerns and questions related to proposed unit count, stacked townhouse configuration, lane circulation and number of tandem parking stalls within the previous 18 unit proposal.

Municipal notification of the current proposal was sent to owners and occupiers of properties within 75 metres (246 ft.) of the site which outlined the 14 unit proposal and described the proposed traffic alterations in the laneway area.

The business owner of the Queens Cross Pub questioned how the revised laneway circulation would affect access to the pub. Given the proposed laneway circulation will not affect the existing

lane access to and from the pub's underground entrance; there were no concerns from the business owner of the Queens Cross Pub.

CONCLUSION:

This rezoning proposal for 14 townhouses is in conformity with the Official Community Plan and applicable development permit guidelines. Bylaws 8149 and 8150 are ready for Council consideration.

OPTIONS:

The following options are available for Council's consideration:

1. Introduce Bylaws 8149 and 8150 and refer Bylaw 8149 to a Public Hearing (staff recommendation); or
2. Defeat Bylaws 8149 and 8150 at First Reading.



Erik Wilhelm
Development Planner

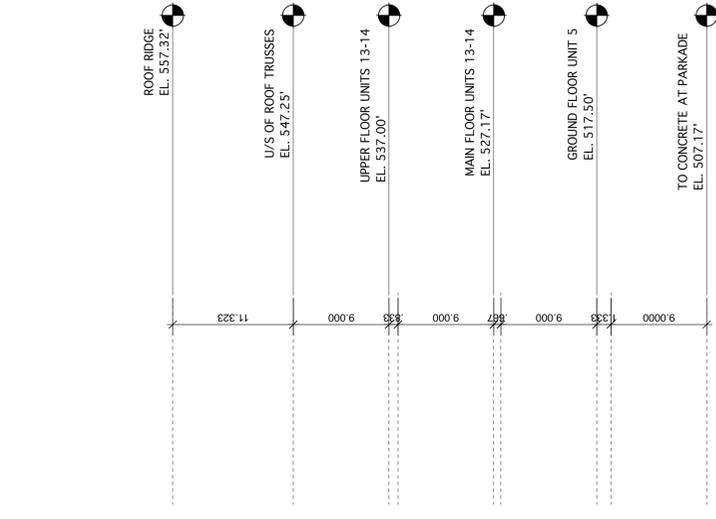
Attachments:

- A - Architectural Drawing Package;
- B - Bylaw 8149 "The District of North Vancouver Rezoning Bylaw 1337 (Bylaw 8149)";
- C - Bylaw 8150 "Housing Agreement Bylaw 8150, 2015 (115 and 123 West Queens Road)"; and
- D - Excerpt of Facilitator's Summary of Nov. 27, 2014 Public Information Meeting.

REVIEWED WITH:		
<input type="checkbox"/> Sustainable Community Dev. _____	<input type="checkbox"/> Clerk's Office _____	External Agencies:
<input type="checkbox"/> Development Services _____	<input type="checkbox"/> Communications _____	<input type="checkbox"/> Library Board _____
<input type="checkbox"/> Utilities _____	<input type="checkbox"/> Finance _____	<input type="checkbox"/> NS Health _____
<input type="checkbox"/> Engineering Operations _____	<input type="checkbox"/> Fire Services _____	<input type="checkbox"/> RCMP _____
<input type="checkbox"/> Parks & Environment _____	<input type="checkbox"/> ITS _____	<input type="checkbox"/> Recreation Com. _____
<input type="checkbox"/> Economic Development _____	<input type="checkbox"/> Solicitor _____	<input type="checkbox"/> Museum & Arch. _____
<input type="checkbox"/> Human resources _____	<input type="checkbox"/> GIS _____	<input type="checkbox"/> Other: _____

ELEVATION KEY:

- 1 ASPHALT ROOF SHINGLES - (CHARCOAL)
- 2 DOUBLE GLAZED VINYL FRAMED WINDOW W/ TRIM - (ALMOND)
- 3 5" ALUMINUM GUTTER ON 2x8 FASCIA - (WHITE)
- 4 GUARD RAIL WITH TEMPERED GLASS - (BLACK)
- 5 5" WOOD SHAKE - PAINTED (BM: EVENING DOVE)
- 6 HARDIE-PANEL - PAINTED (BM: EDGE COB GRAY)
- 7 4" HARDIE-PLANK SIDING - PAINTED (BM: SANDY HOOK GRAY)
- 8 CULTURED STONE BASE - (COUNTRY LEDGESTONE)
- 9 WOOD WIN./DR. TRIM, POST, HORIZ. BAND, BRACKET - PAINTED (BM: EDGE COB GRAY)
- 10 METAL CLAD ENTRY DOOR 1 - PAINTED (BM: HUNTINGTON BEIGE)
- 11 METAL CLAD ENTRY DOOR 2 - PAINTED (BM: BLACK PANTHER)



NORTH ELEVATION (W. QUEENS RD.)

NO.	DATE	REVISIONS
1	FEB. 5, 2014	ISSUED FOR REZONING
2	APR. 4, 2014	RZ REVISION
3	APR. 10, 2014	RZ REVISION
4	MAY 16, 2014	RZ REVISION
5	NOV. 6, 2014	ADP SUBMISSION
6	DEC. 3, 2014	ADP RESUBMISSION
7	JAN. 28, 2015	ADP DETAILED SUBMISSION
8	APR. 27, 2015	POST PH REVISIONS
9	JUN. 15, 2015	POST COUNCIL REVISIONS
10	JULY 28, 2015	GENERAL REVISIONS

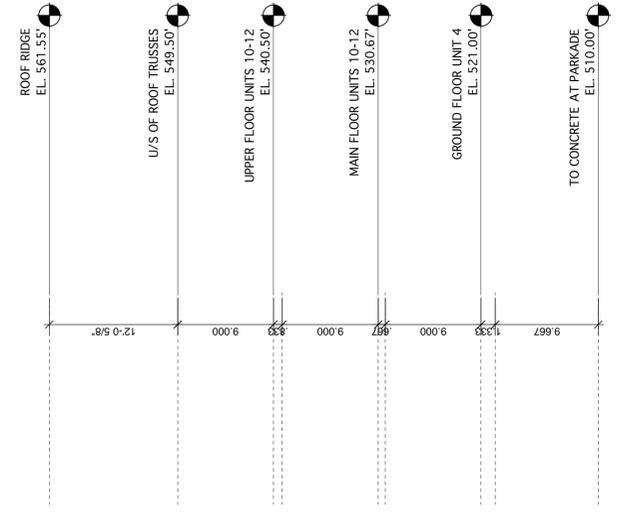
PROJECT
18 UNIT TOWNHOUSE DEVELOPMENT

115, 123 WEST QUEENS ROAD
NORTH VANCOUVER, B. C.

**Yamamoto
Architecture Inc.**

2386 oak street, vancouver, british columbia
V6H 4J1 tel: 604-731-1127 fax: 604-731-1327
DRAWING TITLE
ELEVATIONS

SCALE	1/8" = 1'-0"	SHEET NO.	A2.1
DATE	OCT. 7, 2013	DRAWN	KP
CHECKED		PROJ. NO.	1321



REAR BUILDING - SOUTH ELEVATION

SCALE: 1/8"=1'-0"

ELEVATION KEY:

- 1 ASPHALT ROOF SHINGLES - (CHARCOAL)
- 2 DOUBLE GLAZED VINYL FRAMED WINDOW W/ TRIM - (ALMOND)
- 3 5" ALUMINUM GUTTER ON 2x8 FASCIA - (WHITE)
- 4 GUARD RAIL WITH TEMPERED GLASS - (BLACK)
- 5 5" WOOD SHAKE - PAINTED (BM: EVENING DOVE)
- 6 HARDIE-PANEL - PAINTED (BM: EDGE COB GRAY)
- 7 4" HARDIE-PLANK SIDING - PAINTED (BM: SANDY HOOK GRAY)
- 8 CULTURED STONE BASE - (COUNTRY LEDGESTONE)
- 9 WOOD WIN./DR. TRIM, POST, HORIZ. BAND, BRACKET - PAINTED (BM: EDGE COB GRAY)
- 10 METAL CLAD ENTRY DOOR 1 - PAINTED (BM: HUNTINGTON BEIGE)
- 11 METAL CLAD ENTRY DOOR 2 - PAINTED (BM: BLACK PANTHER)



FRONT BUILDING - SOUTH ELEVATION (COURTYARD)
SCALE: 1/8"=1'-0"



REAR BUILDING - NORTH ELEVATION (COURTYARD)
SCALE: 1/8"=1'-0"

10	JULY 28, 2015	GENERAL REVISIONS
9	JUN 15, 2015	POST COUNCIL REVISIONS
8	APR. 27, 2015	POST PH REVISIONS
7	JAN. 28, 2015	ADP DETAILED SUBMISSION
6	DEC. 3, 2014	ADP RESUBMISSION
5	NOV. 6, 2014	ADP SUBMISSION
4	MAY 16, 2014	RZ REVISION
3	APR. 10, 2014	RZ REVISION
2	APR. 4, 2014	RZ REVISION
1	FEB. 5, 2014	ISSUED FOR REZONING
NO.	DATE	REVISIONS

CONSULTANT

PROJECT
18 UNIT TOWNHOUSE DEVELOPMENT

115, 123 WEST QUEENS ROAD
NORTH VANCOUVER, B. C.

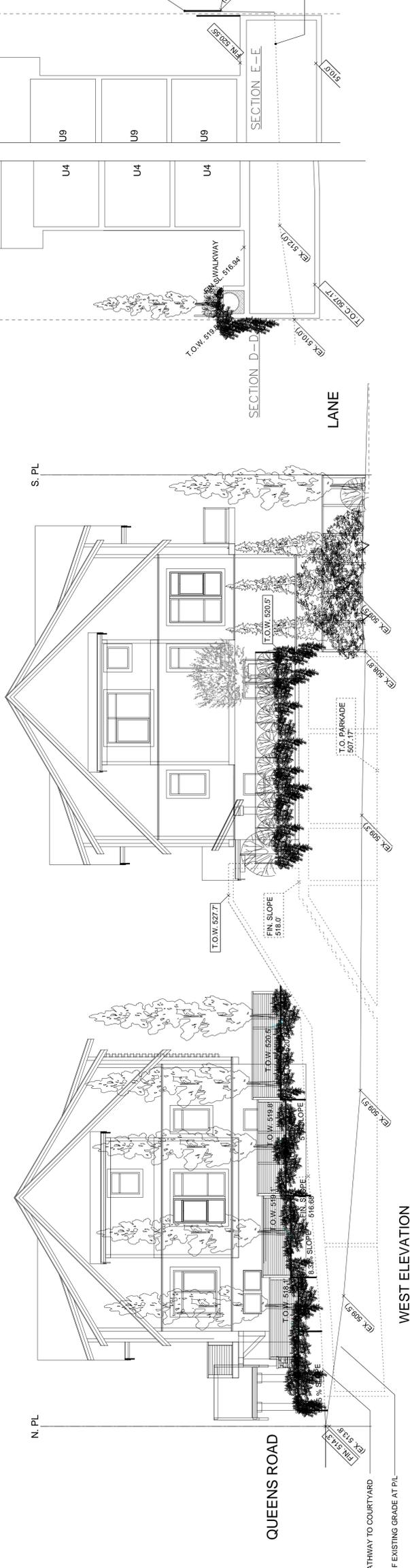
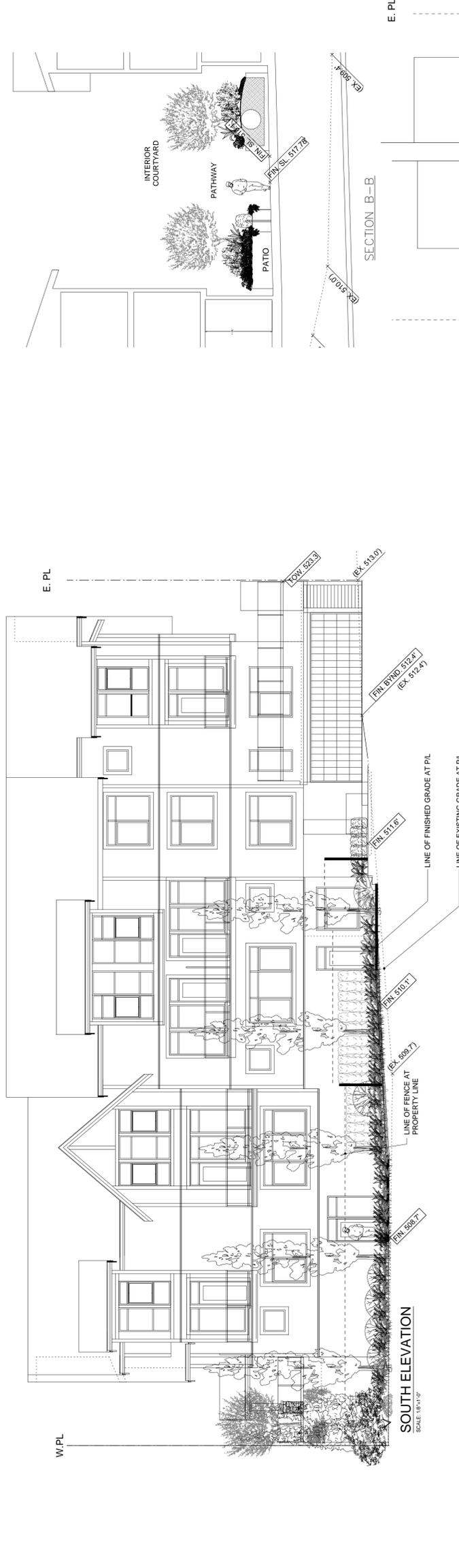
Yamamoto
Architecture Inc.

2386 oak street, vancouver, british columbia
V6H 4J1 tel: 604-731-1127 fax: 604-731-1327

DRAWING TITLE
INTERIOR COURTYARD ELEVATION /
SECTION AT WALKWAY

SCALE	1/8" = 1'-0"	SHEET NO.	A2.3
DATE	OCT. 7, 2013	DRAWN	KP
CHECKED		PROJ. NO.	1321

SEAL:



NO.	DATE	REVISION DESCRIPTION	DR.
11	15.JUL.24	NEW SITE PLAN	DD
10	15.JUL.10	NEW SITE PLAN	DD
9	15.JAN.20	REVISION	DD
8	15.JAN.16	AS PER CLIENT REQUEST	DD
7	14.DEC.03	ADD FENCE DETAILS	DD
6	14.DEC.03	NEW SITE PLAN, REV. DETAILS	MM
5	14.NOV.06	EX TREE NOTES ADDED	MM
4	14.NOV.05	REVISE NEW SITE PLAN	RC
3	14.OCT.06	NEW SITE PLAN - NUN. COMMENTS	MM
2	14.MAY.14	NEW SITE PLAN	DD
1	14.FEB.04	NEW SITE PLAN	DD

CLIENT:
NOORT HOMES WITH YAMAMOTO ARCHITECTURE INC.

PROJECT:
**18 UNIT TOWNHOUSES
 115 & 123 W. QUEENS
 NORTH VANCOUVER, B.C.**

DRAWING TITLE:
LANDSCAPE ELEVATIONS

DATE: 14.01.07
 SCALE: 1/8" = 1'-0"
 DRAWN: MM
 DESIGN: MM
 CHK'D: PCM

L4

OF 10
 PMG PROJECT NUMBER: 13-180

DNV # 08-3050-20/027.14
 13180-13.ZIP

The Corporation of the District of North Vancouver**Bylaw 8149**

A bylaw to amend the District of North Vancouver Zoning Bylaw 3210, 1965

The Council for The Corporation of the District of North Vancouver enacts as follows:

1. Citation

This bylaw may be cited as "The District of North Vancouver Rezoning Bylaw 1337, (Bylaw 8149)".

2. Amendments

The District of North Vancouver Zoning Bylaw 3210, 1965 is amended as follows:

2.1. Section 301(2) by inserting the following zoning designation in numeric sequence:

"Comprehensive Development Zone CD89"

2.2 Part 4B by inserting the following:

"4B89 Comprehensive Development Zone 89 (CD89)"

4B89-1 Intent:

The purpose of the CD89 zone is to permit development of a 14 unit townhouse development at 115 and 123 West Queens Road.

4B89-2 Uses:

The following *principal uses* shall be permitted in the Comprehensive Development 89 Zone:

(a) Uses Permitted without Conditions:

Residential building, multiple-family townhouse

For the purposes of this CD89 Zone, "Residential building, multiple-family townhouse" means a building having not more than three residential storeys and consisting of two or more dwelling units with individual, exterior access to grade and whereby entrance areas associated with units which are located in the parkade level do not count as a residential storey.

(b) Conditional Uses

Not applicable

4B89-3 Conditions of Use:

Not applicable.

4B89-4 Accessory Use:

(a) *Accessory uses* are permitted and are limited to:

(i) Home occupations in accordance with the regulations in Section 405 of the Zoning Bylaw, 1965.

4B89-5 Density:

- (a) The maximum permitted density in the CD89 Zone is limited to a floor space ratio (FSR) of 0.45 and a maximum number of 4 units, inclusive of any density bonus for energy performance; and
- (b) For the purposes of calculating floor space ratio, the area of parking garages, electrical/mechanical rooms, garbage and recycling collection areas, and bicycle storage areas, is exempted.

4B89-6 Amenities:

Despite subsection 4B89-5, density in the CD89 Zone is increased to a maximum floor space ratio (FSR) of 1.6 FSR and a maximum number of 14 units, inclusive of any density bonus for energy performance, if the owner:

1. Enters into a Housing Agreement requiring a rental disclosure statement to be filed and prohibiting any strata bylaw or regulation establishing rental restrictions; and
2. Contributes \$91,835 to the municipality to be used for any or all of the following amenities (with allocation to be determined by the municipality in its sole discretion): public art; park, trail, environmental or other public realm improvements; municipal or recreation service or facility improvements; and/or the affordable housing fund.

4B89-7 Maximum Principal Building Size:

Not applicable.

4B89-8 Setbacks:

(a) Basement Level:

Location	Buildings and Structures Minimum setback
Front Yard (West Queens Road)	0 m (0 ft)
Lane	0 m (0 ft)
Rear Yard	2.44 m (8 ft)
West Side Yard	0 m (0 ft)
East Side Yard	0 m (0 ft)

(b) Ground Level:

Location	Buildings and Structures Minimum setback
Front Yard (West Queens Road)	3.56 m (11.67 ft)
Lane	0 m (0 ft)
Rear Yard	2.44 m (8 ft)
West Side Yard	3.75 m (12.3 ft)
East Side Yard	2.59 m (8.5 ft)

(c) Projections above ground level are permissible as follows:

Setback	Maximum Setback Reduction
Building Projections - West Side Yard	0.43 m (1.41 ft)
Building Projections - East Side Yard	0.43 m (1.41 ft)
Balconies – Front Yard	1.52 m (5 ft)

(d) Minimum separation (excluding doorway entrances) at the ground level required between the buildings within the CD89 zone shall be at least 9.14 m. (30 ft).

4B89-9 Building Orientation:

Not applicable.

4B89-10 Building Depth and Width:

Not applicable.

4B89-11 Coverage:

- a) Building Coverage shall not exceed 50%
- b) Site Coverage shall not exceed 82%.

4B89-12 Height:

- a) The maximum building height is 13.7 meters (45 ft).
- b) In the case of a roof, the slope of which is 6 in 12 or greater, the maximum permitted height may be increased by 15%.

4B89-13 Acoustic Requirements:

In the case of residential purposes, a development permit application shall require evidence in the form of a report and recommendations prepared by persons trained in acoustics and current techniques of noise measurements, demonstrating that the noise levels in those portions of the dwelling listed below shall not exceed the noise levels expressed in decibels set opposite such portions of the dwelling units:

Portion of Dwelling Unit	Noise Level (Decibels)
Bedrooms	35
Living and Dining rooms	40
Kitchen, Bathrooms and Hallways	45

4B89-14 Landscaping:

- a) All land areas not occupied by buildings, structures, parking spaces, loading spaces, driveways, manoeuvring aisles and sidewalks shall be landscaped or finished in accordance with an approved landscape plan; and
- b) All electrical kiosks and garbage and recycling container pads not located underground or within a building shall be screened with landscaping or fencing in accordance with an approved landscape plan.

4B89-15 Subdivision Requirements

Within the CD89 zone, the Minimum Lot Area must be at least 1,425 sq. m. (15,338 sq. ft.).

4B89-16 Additional Accessory Structure Regulations

Not applicable.

4B89-17 Parking and Loading Regulations:

- (a) Parking shall be provided at a ratio of 2 parking spaces per unit inclusive of designated visitor and parking for persons with disabilities;
- (b) All regular, small car and handicapped parking spaces shall meet the minimum width and length requirements established in Part 10 of this Bylaw;
- (c) Small car parking spaces shall not exceed 36% of the required parking spaces

(d) Vehicular drive aisles shall meet the minimum width requirements established in Part 10 of this Bylaw

(e) A minimum of 19 Class 2 bicycle parking spaces shall be provided

(f) A bike rack within the shared courtyard that can accommodate a minimum of 5 bicycles shall be provided.”

2.3 The Zoning Map is amended in the case of the lands in Schedule A, by rezoning the land outlined and noted as “site” from Residential Single Family Residential 6000 Zone (RS-4) to Comprehensive Development 89 Zone (CD89).

READ a first time

PUBLIC HEARING held

READ a second time

READ a third time

Certified a true copy of “Bylaw 8149” as at Third Reading

Municipal Clerk

APPROVED by the Ministry of Transportation and Infrastructure on

ADOPTED

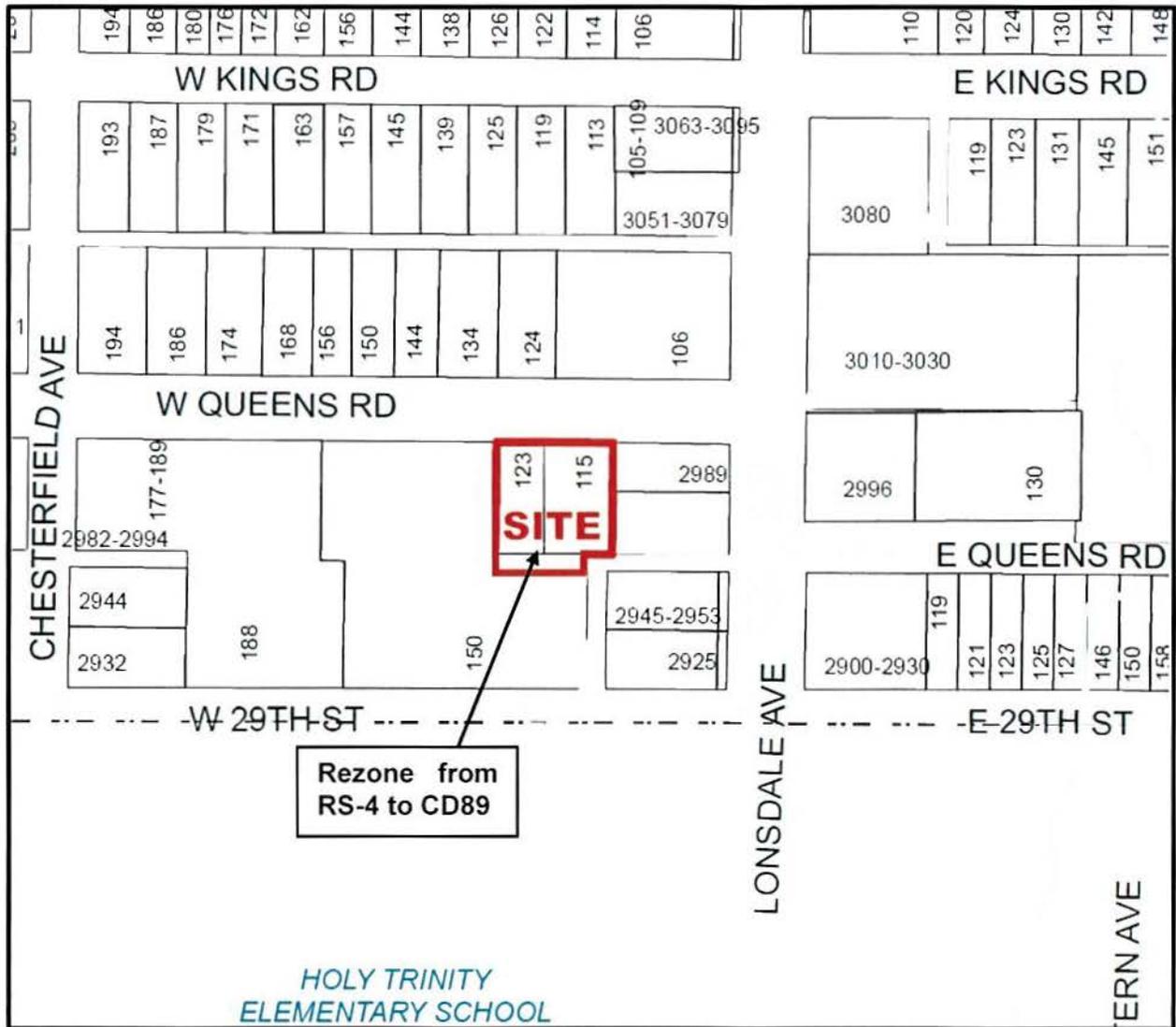
Mayor

Municipal Clerk

Certified a true copy

Municipal Clerk

Schedule A to Bylaw 8149



The Corporation of the District of North Vancouver

Bylaw 8150

A bylaw to enter into a Housing Agreement (115 and 123 West Queens Road)

The Council for The Corporation of the District of North Vancouver enacts as follows:

1. Citation

This bylaw may be cited as "Housing Agreement Bylaw 8150, 2015 (115 and 123 West Queens Road)".

2. Authorization to Enter into Agreement

2.1 The Council hereby authorizes a housing agreement between The Corporation of the District of North Vancouver and Noort Holdings Ltd., Inc. No. BC0092361 substantially in the form attached to this Bylaw as Schedule "A" with respect to the following lands:

- a) Amended Lot 3 (See 290725L) of Lot B, Blocks 4 to 13, District Lot 2026, Plan 3544C (PID: 012-786-683).
- b) Amended Lot 4 (See 290724L) of Lot B, Blocks 4 to 13, District Lot 2026, Plan 3544C (PID: 002-708-035).

3. Execution of Documents

The Mayor and Municipal Clerk are authorized to execute any documents required to give effect to the Housing Agreement.

READ a first time

READ a second time

READ a third time

ADOPTED

Mayor

Municipal Clerk

Certified a true copy

Municipal Clerk

Schedule A to Bylaw 8150

SECTION 219 COVENANT – HOUSING AGREEMENT

This agreement dated for reference the ____ day of _____, 20__ is

BETWEEN:

Noort Holdings Ltd., Inc. No. BC0092361 of _____

(the “Owner”)

AND:

THE CORPORATION OF THE DISTRICT OF NORTH VANCOUVER, a municipality incorporated under the *Local Government Act*, R.S.B.C. 1996, c.323 and having its office at 355 West Queens Road, North Vancouver, BC V7N 4N5

(the “District”)

WHEREAS:

1. The Owner is the registered owner of the Lands (as hereinafter defined);
2. The Owner wishes to obtain development permissions with respect to the Lands and wishes to create a development which will contain residential strata units on the Lands;
3. Section 905 of the *Local Government Act* authorises the District, by bylaw, to enter into a housing agreement to provide for the prevention of rental restrictions on housing, and provides for the contents of the agreement; and
4. Section 219 of the *Land Title Act* (British Columbia) permits the registration in favour of the District of a covenant of a negative or positive nature relating to the use of land or a building thereon, or providing that land is to be built on in accordance with the covenant, or providing that land is not to be built on except in accordance with the covenant, or providing that land is not to be subdivided except in accordance with the covenant;

NOW THEREFORE in consideration of the mutual promises contained in it, and in consideration of the payment of \$1.00 by the District to the Owner (the receipt and sufficiency of which are hereby acknowledged by the Owner), the parties covenant and agree with each other as follows, as a housing agreement under Section 905 of the *Local Government Act*, as a contract and a deed under seal between the parties, and as a covenant under Section 219 of the *Land Title Act*, and the Owner hereby further covenants and agrees that neither the Lands nor any building constructed thereon shall be used or built on except in accordance with this Agreement::

Article 1 DEFINITIONS

1.01 Definitions

In this agreement:

- (a) “*Development Permit*” means development permit No. _____ issued by the District;
- (b) “*Lands*” means land described in Item 2 of the *Land Title Act* Form C to which this agreement is attached;
- (c) “*Proposed Development*” means the proposed development containing not more than 14 Units to be constructed on the Lands in accordance with the Development Permit;
- (d) “*Unit*” means a residential dwelling strata unit in the Proposed Development; and
- (e) “*Unit Owner*” means the registered owner of a Dwelling Unit in the Proposed Development.

Article 2 TERM

This Agreement will commence upon adoption by District Council of Bylaw 8114 and remain in effect until terminated by the District as set out in this Agreement.

Article 3 RENTAL ACCOMODATION

3.01 Rental Disclosure Statement

No Unit in the Proposed Development may be occupied unless the Owner has:

- (f) before the first Unit is offered for sale, or conveyed to a purchaser without being offered for sale, filed with the Superintendent of Real Estate a Rental Disclosure Statement designating all of the Units as rental strata lots and imposing a ninety-nine (99) year rental period in relation to all of the Units pursuant to the *Strata Property Act* (or any successor or replacement legislation); and
- (g) given a copy of the Rental Disclosure Statement to each prospective purchaser of any Unit before the prospective purchaser enters into an agreement to purchase in respect of the Unit.

3.02 Rental Accommodation

The Units constructed on the Lands from time to time may always be used to provide rental accommodation as the Owner or a Unit Owner may choose from time to time.

3.03 Binding on Strata Corporation

This agreement shall be binding upon all strata corporations created by the subdivision of the Lands or any part thereof (including the Units) pursuant to the *Strata Property Act*, and upon all Unit Owners. .

3.04 Strata Bylaw Invalid

Any strata Corporation bylaw which prevents, restricts or abridges the right to use any of the Units as rental accommodations shall have no force or effect.

3.05 No Bylaw

The strata corporation shall not pass any bylaws preventing, restricting or abridging the use of the Lands, the Proposed Development or the Units contained therein from time to time as rental accommodation.

3.06 Vote

No Unit Owner, nor any tenant or mortgagee thereof, shall vote for any strata corporation bylaw purporting to prevent, restrict or abridge the use of the Lands, the Proposed Development and the units contained therein from time to time as rental accommodation.

3.07 Notice

The Owner will provide notice of this Agreement to any person or persons intending to purchase a Unit prior to any such person entering into an agreement of purchase and sale, agreement for sale, or option or similar right to purchase as part of the Disclosure Statement for any part of the Proposed Development prepared by the Owner pursuant to the *Real Estate Development Marketing Act*.

Article 4 DEFAULT AND REMEDIES

4.01 Notice of Default

The District may, acting reasonably, give to the Owner written notice to cure a default under this Agreement within thirty (30) days of delivery of the notice. The notice must specify the nature of the default. The Owner must act with diligence to correct the default within the time specified.

4.02 Costs

The Owner will pay to the District upon demand all the District's costs of exercising its rights or remedies under this Agreement, on a full indemnity basis.

4.03 Damages an Inadequate Remedy

The Owner acknowledges and agrees that in the case of a breach of this Agreement which is not fully remediable by the mere payment of money and promptly so remedied, the harm sustained by the District and to the public interest will be irreparable and not susceptible of adequate monetary compensation.

4.04 Equitable Remedies

Each party to this Agreement, in addition to its rights under this Agreement or at law, will be entitled to all equitable remedies including specific performance, injunction and declaratory relief, or any of them, to enforce its rights under this Agreement.

4.05 No Penalty or Forfeiture

The Owner acknowledges and agrees that it is entering into this Agreement to benefit the public interest in providing rental accommodation, and that the District's rights and remedies under this Agreement are necessary to ensure that this purpose is carried out,

and the District's rights and remedies under this Agreement are fair and reasonable and ought not to be construed as a penalty or forfeiture.

4.06 Cumulative Remedies

No reference to nor exercise of any specific right or remedy under this Agreement or at law or at equity by any party will prejudice, limit or preclude that party from exercising any other right or remedy. No right or remedy will be exclusive or dependent upon any other right to remedy, but any party, from time to time, may exercise any one or more of such rights or remedies independently, successively, or in combination. The Owner acknowledges that specific performance, injunctive relief (mandatory or otherwise) or other equitable relief may be the only adequate remedy for a default by the Owner under this Agreement.

Article 5 LIABILITY

5.01 Indemnity

Except if arising directly from the negligence of the District or its employees, agents or contractors, the Owner will indemnify and save harmless each of the District and its board members, officers, directors, employees, agents, and elected or appointed officials,, and their heirs, executors, administrators, personal representatives, successors and assigns, from and against all claims, demands, actions, loss, damage, costs and liabilities that all or any of them will or may be liable for or suffer or incur or be put to any act or omission by the Owner or its officers, directors, employees, agents, contractors, or other persons for whom the Owner is at law responsible, or by reason of or arising out of the Owner's ownership, operation, management or financing of the Proposed Development or any part thereof.

5.02 Release

The Owner hereby releases and forever discharges the District, its elected officials, board members, officers, directors, employees and agents, and its and their heirs, executors, administrators, personal representatives, successors and assigns from and against all claims, demands, damages, actions or causes of action by reason of or arising out of advice or direction respecting the ownership, operation or management of the Proposed Development or any part thereof which has been or hereafter may be given to the Owner by all or any of them.

5.03 Survival

The covenants of the Owner set out in Sections 5.01 and 5.02 will survive termination of this Agreement and continue to apply to any breach of the Agreement or claim arising under this Agreement during the ownership by the Owner of the Lands or any Unit therein, as applicable.

Article 6 GENERAL PROVISIONS

6.01 District's Power Unaffected

Nothing in this Agreement:

- (a) affects or limits any discretion, rights, powers, duties or obligations of the District under any enactment or at common law, including in relation to the use or subdivision of land;
- (b) affects or limits any enactment relating to the use of the Lands or any condition contained in any approval including any development permit concerning the development of the Lands; or
- (c) relieves the Owner from complying with any enactment, including the District's bylaws in relation to the use of the Lands.

6.02 Agreement for Benefit of District Only

The Owner and District agree that:

- (a) this Agreement is entered into only for the benefit of the District;
- (b) this Agreement is not intended to protect the interests of the Owner, any Unit Owner, any Occupant or any future owner, occupier or user of any part of the Proposed Development, including any Unit, or the interests of any third party, and the District has no obligation to anyone to enforce the terms of this Agreement; and
- (c) The District may at any time terminate this Agreement, in whole or in part, and execute a release and discharge of this Agreement in respect of the Proposed Development or any Unit therein, without liability to anyone for doing so.

6.03 Agreement Runs With the Lands

This Agreement burdens and runs with the Lands and any part into which any of them may be subdivided or consolidated, by strata plan or otherwise. All of the covenants and agreements contained in this Agreement are made by the Owner for itself, its successors and assigns, and all persons who acquire an interest in the Lands or in any Unit after the date of this Agreement.

6.04 Release

The covenants and agreements on the part of the Owner and any Unit Owner and herein set forth in this Agreement have been made by the Owner and any Unit Owner as contractual obligations as well as being made pursuant to Section 905 of the *Local Government Act* (British Columbia) and as such will be binding on the Owner and any Unit Owner, except that neither the Owner nor any Unit Owner shall be liable for any default in the performance or observance of this Agreement occurring after such party ceases to own the Lands or a Unit as the case may be.

6.05 Priority of This Agreement

The Owner will, at its expense, do or cause to be done all acts reasonably necessary to ensure this Agreement is registered against the title to each Unit in the Proposed Development, including any amendments to this Agreement as may be required by the Land Title Office or the District to affect such registration.

6.06 Agreement to Have Effect as Deed

The District and the Owner each intend by execution and delivery of this Agreement to create both a contract and a deed under seal.

6.07 Waiver

An alleged waiver by a party of any breach by another party of its obligations under this Agreement will be effective only if it is an express waiver of the breach in writing. No waiver of a breach of this Agreement is deemed or construed to be a consent or waiver of any other breach of this Agreement.

6.08 Time

Time is of the essence in this Agreement. If any party waives this requirement, that party may reinstate it by delivering notice to another party.

6.09 Validity of Provisions

If a Court of competent jurisdiction finds that any part of this Agreement is invalid, illegal, or unenforceable, that part is to be considered to have been severed from the rest of this Agreement and the rest of this Agreement remains in force unaffected by that holding or by the severance of that part.

6.10 Extent of Obligations and Costs

Every obligation of a party which is set out in this Agreement will extend throughout the Term and, to the extent that any obligation ought to have been observed or performed prior to or upon the expiry or earlier termination of the Term, such obligation will survive the expiry or earlier termination of the Term until it has been observed or performed.

6.11 Notices

All notices, demands, or requests of any kind, which a party may be required or permitted to serve on another in connection with this Agreement, must be in writing and may be served on the other parties by registered mail, by facsimile transmission, or by personal service, to the following address for each party:

If to the District:

District Municipal Hall
355 West Queens Road
North Vancouver, BC V7N 4N5

Attention: Planning Department
Facsimile: (604) 984-9683

If to the Owner:

Attention:
Facsimile: (604)

If to the Unit Owner:

The address of the registered owner which appears on title to the Unit at the time of notice.

Service of any such notice, demand, or request will be deemed complete, if made by registered mail, 72 hours after the date and hour of mailing, except where there is a postal service disruption during such period, in which case service will be deemed to be complete only upon actual delivery of the notice, demand or request; if made by facsimile transmission, on the first business day after the date when the facsimile transmission was transmitted; and if made by personal service, upon personal service being effected. Any party, from time to time, by notice in writing served upon the other parties, may designate a different address or different or additional persons to which all notices, demands, or requests are to be addressed.

6.12 Further Assurances

Upon request by the District, the Owner will promptly do such acts and execute such documents as may be reasonably necessary, in the opinion of the District, to give effect to this Agreement.

6.13 Enuring Effect

This Agreement will enure to the benefit of and be binding upon each of the parties and their successors and permitted assigns.

Article 7 INTERPRETATION

7.01 References

Gender specific terms include both genders and include corporations. Words in the singular include the plural, and words in the plural include the singular.

7.02 Construction

The division of this Agreement into sections and the use of headings are for convenience of reference only and are not intended to govern, limit or aid in the construction of any provision. In all cases, the language in this Agreement is to be construed simply according to its fair meaning, and not strictly for or against either party.

7.03 No Limitation

The word “including” when following any general statement or term is not to be construed to limit the general statement or term to the specific items which immediately follow the general statement or term similar items whether or not words such as “without limitation” or “but not limited to” are used, but rather the general statement or term is to be construed to refer to all other items that could reasonably fall within the broadest possible scope of the general statement or term.

7.04 Terms Mandatory

The words “must” and “will” and “shall” are to be construed as imperative.

7.05 Statutes

Any reference in this Agreement to any statute or bylaw includes any subsequent amendment, re-enactment, or replacement of that statute or bylaw.

7.06 Entire Agreement

- (a) This is the entire agreement between the District and the Owner concerning its subject, and there are no warranties, representations, conditions or collateral agreements relating to this Agreement, except as included in this Agreement.
- (b) This Agreement may be amended only by a document executed by the parties to this Agreement and by bylaw, such amendment to be effective only upon adoption by District Council of a bylaw to amend Bylaw 8097.

7.07 Governing Law

This Agreement is to be governed by and construed and enforced in accordance with the laws of British Columbia.

As evidence of their agreement to be bound by the terms of this instrument, the parties hereto have executed the *Land Title Act* Form C that is attached hereto and forms part of this Agreement.

CONSENT AND PRIORITY AGREEMENT

GIVEN THAT:

1. _____ (the "Owner") is the Registered Owner of the Land described in Item 2 of Page 1 of the Form C (the "Land");
2. The Owner granted _____ (the "Prior Chargeholder") a Mortgage and Assignment of Rents registered against title to the Land in the Lower Mainland Land Title Office (the "LTO") under Nos. _____, as extended by _____ and _____, as extended by _____ (together, the "Prior Charge");
3. The Owner granted to THE CORPORATION OF THE DISTRICT OF NORTH VANCOUVER (the "District") a Covenant attached to this Agreement and registered against title to the Land in the LTO immediately before registration of this Agreement (the "Subsequent Charge"); and
4. Section 207 of the *Land Title Act* permits the Prior Chargeholder to grant priority over a charge to the District as Subsequent Chargeholder.

THEREFORE this Agreement is evidence that in consideration of \$1.00 and other good and valuable consideration received by the Prior Chargeholder from the District (the receipt and sufficiency of which the Prior Chargeholder acknowledges):

1. The Prior Chargeholder consents to the granting and registration of the Subsequent Charge and the Prior Chargeholder agrees that the Subsequent Charge shall be binding upon their interest in and to the Land.
2. The Prior Chargeholder grants to the District, as a Subsequent Chargeholder, priority for the Subsequent Charge over the Prior Chargeholder's right, title and interest in and to the Land, and the Prior Chargeholder postpones the Prior Charge and all of their right, title and interest thereunder to the Subsequent Charge as if the Subsequent Charge had been executed, delivered and registered prior to the execution, delivery and registration of the Prior Charge.

As evidence of its agreement to be bound by the terms of this instrument, the Prior Chargeholder has executed the Land Title Office Form C to which this Agreement is attached and which forms part of this Agreement.

Summary of Public Information Meeting Held Nov. 27, 2014

The project team present at the meeting included:

- Merlin Wick, Noort Holdings Ltd.
- Taizo Yamamoto, Yamamoto Architecture Ltd.
- Lilian Arishenkoff, Community Planner, District of North Vancouver
- Brian Wallace, BWW Consulting, Traffic Consultant
- Jay Hiscox, Main Street Architecture, Meeting Facilitator

A total of 5 people signed in to the meeting and 5 comment forms were returned.

Based on the feedback received, all of the attendees had their questions answered and none appeared to have left with outstanding questions. Several visitors were supportive of the scheme as presented, and several comments were noted that felt the project would be a welcome redevelopment for this frontage of West Queens Road.

Questions fielded during the meeting included the following:

- Concern for additional traffic that could be generated along West Queens Road, especially as relates to short term visitor parking to the project, and existing overflow parking from the pub.
- Concern regarding the number of visitor parking stalls provided.
- Concern for additional dogs' impact on private green lawns across West Queens Road
- Concern for project height and setbacks relative to existing single family housing along West Queens Road.
- Concerns regarding the attendance of the meeting, questions about the most suitable time to hold such a public information meeting.
- Concern regarding speed of traffic along West Queens Road
- Specific questions regarding parking signage and permitted parking on the street frontage.
- Questions regarding project timelines, expected construction completion.
- Questions regarding building materials, noise impact of street wall of 3 storeys.
- Questions regarding materials, colours, roof shapes

2.4 Community Inquiries

Noort Holdings Ltd. did not receive any community inquiries via emails and phone calls regarding the project.

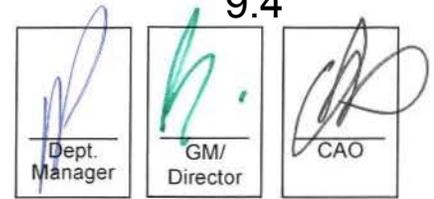
2.5 Response to Public feedback

The following changes are presently contemplated as a result of the Public Information meeting:

- Noort would consider changing 2 resident parking spaces to visitor (if recommended by DNV staff)
- Secondary projections redesigned to a shed roof to reduce street noise along W. Queens Rd.
- Large trees proposed along the boulevard to reduce street noise along W. Queens Rd.
- Noort Homes Ltd. has also agreed to promote the use of an alternative mode of transportation by offering transit passes to the residents. This will help minimize vehicle activity in the area.

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AGENDA INFORMATION	
<input checked="" type="checkbox"/> Regular Meeting	Date: <u>JAN 18 2016</u>
<input type="checkbox"/> Committee of the Whole	Date: _____



The District of North Vancouver REPORT TO COUNCIL

January 6, 2015
File: 08.3060.20/030.15

AUTHOR: Natasha Letchford

SUBJECT: Bylaws 8159, 8160 and 8162: OCP Amendment, rezoning, and housing agreement for a 24 unit apartment project - 1103, 1109, 1123 Ridgewood Dr. and 3293 Edgemont Blvd.

RECOMMENDATION:

THAT the “District of North Vancouver Official Community Plan Bylaw 7900, 2011, Amendment Bylaw 8159, 2015 (Amendment 16)” to amend the Official Community Plan (OCP) from Residential Level 2 (RES2) to Residential Level 4 (RES4) be given FIRST reading;

AND THAT the “District of North Vancouver Rezoning Bylaw 3210 (Bylaw 8160)” to rezone the subject site from Single Family Residential Edgemont (RSE) to Comprehensive Development Zone 92 (CD92) be given FIRST reading;

AND THAT the “District of North Vancouver Housing Agreement Bylaw 8162” to authorize a Housing Agreement to prevent future rental restrictions on the subject property, be given FIRST READING;

AND THAT pursuant to Section 879 of the Local Government Act, additional consultation is not required beyond that already undertaken with respect to Bylaw 8159;

AND THAT in accordance with Section 882 of the Local Government Act, Council has considered Bylaw 8159 in conjunction with its Financial Plan and applicable Waste Management Plans.

AND THAT Bylaw 8159 and Bylaw 8160 be referred to a Public Hearing.



REASON FOR REPORT

The proposed project requires Council's consideration of:

- Bylaw 8159 to amend the Official Community Plan (OCP) for the subject properties;
- Bylaw 8160 to rezone the subject properties;
- Bylaw 8162 to ensure all future owners are eligible to rent their units.

SUMMARY

The applicant proposes to redevelop four residential lots located at 1103, 1109, 1123 Ridgewood Dr. and 3293 Edgemont Blvd. to create a 24 unit townhouse project. Implementation of the project requires an OCP amendment (Bylaw 8159), a rezoning (Bylaw 8160), and a Housing Agreement Bylaw (8162). The OCP amendment and rezoning is consistent with the recently approved Edgemont Village Centre: Plan and Design Guidelines. A development permit will be forwarded to Council if the OCP amendment and rezoning are approved.

BACKGROUND:

Official Community Plan

The subject properties are designated Residential Level 2: Detached Residential (RES2) in the Official Community Plan which allows for an FSR of up to 0.55.

The Edgemont Village Centre: Plan and Design Guidelines identifies these four lots as a future townhouse site, with an FSR of up to 1.20. The Edgemont Village: Plan and Design Guidelines envisions OCP amendments for the townhouse, multiplex, and duplex sites in the residential periphery of the village. The OCP amendment would change the designation of the lots



Map 4: Land Use - Residential Periphery

to Residential Level 4: Transition Multifamily (RES4), with an FSR of up to 1.20, consistent with the Edgemont Village Centre: Plan and Design Guidelines.

SUBJECT: Bylaws 8159, 8160 and 8162: OCP Amendment, rezoning, and housing agreement for a 24 unit apartment project - 1103, 1109, 1123 Ridgewood Dr. and 3293 Edgemont Blvd.

January 6, 2016

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The RES4 designation allows for a mix of townhouse and apartment development within or in close proximity to centres and corridors. Additionally, the OCP amendment will add the site to the following Development Permit Areas:

- Form and Character of Commercial, Industrial, and Multifamily Development
- Energy and Water Conservation and Greenhouse Gas Emission Reductions

The proposed units are a mix of two and three bedroom floor plans, which will be attractive to both families and downsizers, responding to Goal #2 of the OCP to “encourage and enable a diverse mix of housing types....to accommodate the lifestyles and needs of people at all stages of life.”

The proposed height of three storeys and the proposed FSR is consistent with the Edgemont Village Centre: Plan and Design Guidelines.

Zoning

The subject properties are currently zoned Single Family Edgemont (RSE). A new Comprehensive Development Zone 92 (CD92) is required to accommodate the project.

Development Permit

If the OCP amendment is approved, the subject lots will be included in the following Development Permit Areas:

- Form and Character of Commercial, Industrial, and Multifamily Development
- Energy and Water Conservation and Greenhouse Gas Emission Reductions



A detailed development permit report, outlining the project’s compliance with the applicable development permit guidelines will be provided for Council’s consideration at the Development Permit stage should the OCP amendment and rezoning be approved.

SUBJECT: Bylaws 8159, 8160 and 8162: OCP Amendment, rezoning, and housing agreement for a 24 unit apartment project - 1103, 1109, 1123 Ridgewood Dr. and 3293 Edgemont Blvd.

January 6, 2016

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ANALYSIS:

Site and Surrounding Areas:

The development site is located on the southwest corner of Edgemont Blvd. and Ridgewood Dr. There are existing single family lots to the north and west of the site. The recently approved Grosvenor project (apartments over retail) is located east of the site, across Edgemont Blvd., Highland's United Church is located to the south of the site. The three residential properties to the west, along Ridgewood Dr., are identified for multiplex developments (triplex and four-plex), while the two properties to the south are identified as future duplex developments in the Edgemont Village: Plan and Design Guidelines.

Project Description:

Site Plan/Building Description

The project consists of 24 townhouse units in five three storey buildings. The proposal includes a 57.7 m² (621 sq. ft.) road dedication along Ridgewood Dr. which will enable the construction of a dedicated southbound right turn lane and dedicated bike lanes in both directions.



The townhomes are arranged in five buildings, framing a central courtyard, over one level of underground parking. Most units have basements at the parkade level, directly accessible from the parkade.

SUBJECT: Bylaws 8159, 8160 and 8162: OCP Amendment, rezoning, and housing agreement for a 24 unit apartment project - 1103, 1109, 1123 Ridgewood Dr. and 3293 Edgemont Blvd.

January 6, 2016

Page 5

The units are a mix of two and three bedroom layouts and range in size from 140 m² (1504 sq. ft.) to 198 m² (2,131 sq. ft.).

Parking

Parking is provided on one level of underground with access from Ridgewood Drive at the west end of the site. A total of 50 parking stalls are proposed which provides 2.1 parking stalls per unit, inclusive of six visitor stalls. This parking rate is higher than the District's 'Parking Principles for OCP Town and Village Centres'. This rate responds to the communities desire for increased parking of at least two stalls per unit due to the lack of on-street parking along Ridgewood Dr.

The proposal includes 24 Class 1 secure bicycle parking spaces (one per unit) in the underground parking and five Class 2 (short term) bicycle spaces at grade. Electrical outlets will be provided for all of the Class 1 bicycle spaces. Additional bike storage is available in individual unit garages and basement.



View looking east along Ridgewood Dr.

Landscaping

The proposed landscape plan features a variety of tree plantings to delineate the various areas of the project. Street trees are proposed along Ridgewood Dr. and Edgemont Blvd. and additional onsite trees and landscaping are provided. The project features a public plaza area at the corner of Edgemont Blvd. and Ridgewood Dr. The plaza responds to the plaza across Edgemont Blvd. on the Grosvenor site to create a 'green gateway' to the Village Centre.

The applicant is working with the neighbouring property owners to the south to finalize a landscape plan for the south property line which maximizes privacy.

Reduced copies of architectural and landscaping plans are included as Attachment A for Council's reference.

SUBJECT: Bylaws 8159, 8160 and 8162: OCP Amendment, rezoning, and housing agreement for a 24 unit apartment project - 1103, 1109, 1123 Ridgewood Dr. and 3293 Edgemont Blvd.

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Acoustic Regulations

Bylaw 8160 includes the District's residential acoustic regulations for maximum permitted noise levels in the bedrooms, living areas, and other areas of the units. As a condition of a development permit, the applicant will be required to provide a report from a qualified noise consultant, confirming compliance with acoustic regulations.

Accessibility

The proposal includes five units which meet the District's 'Basic Accessible Design' criteria under Council's adopted Accessible Design Policy for Multifamily; two of these units also meet the 'Enhanced Accessible Design' criteria. The project includes an elevator from the underground parkade to the courtyard; and, all five identified units have a ground level accessible entrance. Accessible design features proposed include: wider entry doors to allow for clear openings of 34"; lever-style handle; and, slip resistant flooring in bathroom and kitchen. Two of the units are proposed to meet the 'Enhanced Accessible Design' criteria and will include the following features: a personal elevator option from the underground parking to all levels of the units; larger master bedroom to provide a 60" turning space next to bed; a continuous counter between the sink and the stove; and, visual alarm wiring.



View looking east at corner of Edgemont Blvd. and Ridgewood Dr.

OFF-SITE IMPROVEMENTS:

The application includes a road dedication on Ridgewood Dr. to allow for the construction of a dedicated right turn lane; construction of both west and east bound dedicated bike lanes; upgrades to the sidewalks, street trees, curb, gutter, and lighting along the north and south sides of Ridgewood Dr. and the west side of Edgemont Blvd.

GREEN BUILDING MEASURES

Compliance with the Green Building Strategy is mandatory given the need for rezoning. The project is targeting an energy performance rating of Energuide 80 and will achieve a building performance equivalent to Built Green™ Gold. Details of green building features will be provided for Council review should the application proceed to the Development Permit stage.

COMMUNITY AMENITY CONTRIBUTION

The District's Community Amenity Contribution (CAC) Policy requires an amenity contribution for projects which result in an increase in residential density. A CAC of \$157,460 will be included in the proposed CD92 Zone. It is anticipated that the CACs from this development will be directed toward public art; park and trail improvements; the affordable housing fund; or, other public realm infrastructure improvements.

IMPLEMENTATION

Implementation of this project will require an OCP amendment bylaw, a rezoning, and a Housing Agreement, as well as issuance of a development permit and registration of legal agreements.

Bylaw 8160 (Attachment C) rezones the subject site from Single Family Residential Edgemont (RSE) to a new Comprehensive Development Zone 92 (CD92) which:

- Establishes the multifamily residential use;
- Allows home occupations as an accessory use;
- Establishes a base density of 0.45 FSR and four units;
- Establishes a maximum density of 3,761 m² (40,486 sq.ft.) and 24 units subject to payment of a \$157,460 Community Amenity Contribution;
- Provision of at least five units which meet the basic accessible design criteria, two of which must also meet the enhanced accessible design criteria;
- Incorporates acoustic requirements; and,
- Establishes parking and building regulations specific to this project.

Bylaw 8162, (Attachment E) authorizes the District to enter into a Housing Agreement to ensure that there will be no future restrictions on renting the units.

In addition, the following legal agreements are required and will be secured prior to zoning bylaw adoption:

- Green building covenant;
- Stormwater management covenant;
- A statutory right of way for the gateway plaza; and,
- A consolidation plan that shows the required dedications.

CONCURRENCE

Staff

The project has been reviewed by staff from the Environment, Permits, Parks, Engineering, Policy Planning, Urban Design, Transportation, the Fire Department and the Arts Office.

Advisory Design Panel

The application was considered by the Advisory Design Panel (ADP) on November 12, 2015; overall, the panel was pleased with the project. The Panel recommends approval of the project subject to resolution of the Panel comments. The applicant has addressed the Panel's comments by de-emphasizing the parkade entrance; adding increased colour and detail to the design, including changing all unit entry doors to red; and, providing better weather protection at Ridgewood Dr. entries.

Public Input:

Public Information Meeting

The applicant held a facilitated Public Information Meeting (PIM) on November 5, 2015. The meeting was attended by approximately 61 residents. Comments made included a mix of opinions including support for the development and for the opportunity it represents for young families to return to the North Shore.

A key issue for the neighbourhood was the number of proposed visitor parking stalls, particularly for residents of Brookridge Dr. In response to the concerns, the applicant increased the number of proposed visitor stalls from three to six.

An increase in traffic due to the new development was cited as a concern. It is anticipated that there will be a net increase of 9 vehicle trips in the morning peak hours and 12 vehicle trips in the afternoon peak hours. This is equivalent to approximately one additional vehicle every six minutes in the morning and one additional vehicle every five minutes in the afternoon. This level of increase in trips from the development is not expected to have a material effect on the operation of the intersection. The construction of a dedicated right hand turn lane as part of this development will help to improve traffic flow at this intersection which was raised as a concern by the community.

A copy of the facilitator's report of the Public Information Meeting is attached to this report (Attachment E).

CONSTRUCTION MANAGEMENT PLAN

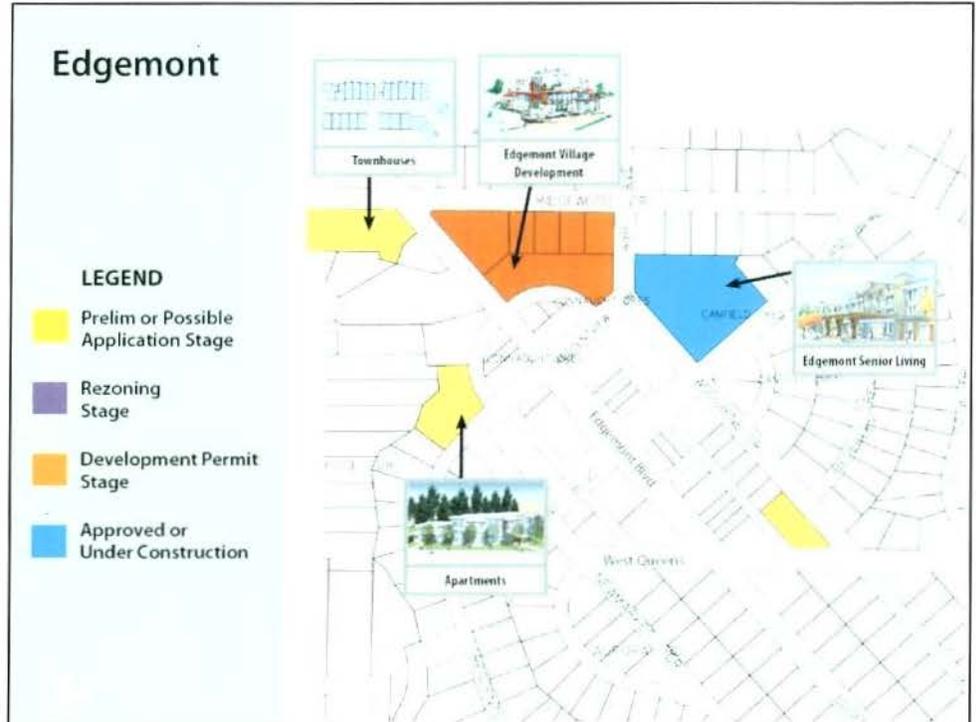
The site is shown in relation to other residential construction projects and potential development projects in the image to the right. If approved, the project will not commence construction until after the completion of the Capilano Water Main Project, anticipated for May 2016 and will be required to coordinate with other construction projects in the area.

SUBJECT: Bylaws 8159, 8160 and 8162: OCP Amendment, rezoning, and housing agreement for a 24 unit apartment project - 1103, 1109, 1123 Ridgewood Dr. and 3293 Edgemont Blvd.

January 6, 2016

Page 9

In order to address the goal to reduce development's impact on pedestrian and vehicular movements, the applicant will be required to provide a Construction Impact Mitigation Plan as a condition of a Development Permit. The Plan must outline how the applicant will coordinate with other projects in the area to minimize construction impacts on pedestrian and vehicle movement along Ridgewood Dr. and Edgemont Blvd. The plan is required to be approved by the District prior to issuance of a building permit.



In particular, the 'construction impact mitigation plan' must:

1. Provide safe passage for pedestrians, cyclists, and vehicle traffic;
2. Outline roadway efficiencies (i.e. location of traffic management signs and flaggers);
3. Make provisions for trade vehicle parking which is acceptable to the District and minimizes impacts to neighbourhoods;
4. Provide a point of contact for all calls and concerns;
5. Provide a sequence and schedule of construction activities;
6. Identify methods of sharing construction schedule with other developments in the area;
7. Ascertain a location for truck marshalling;
8. Address silt/dust control and cleaning up from adjacent streets;
9. Provide a plan for litter clean-up and street sweeping adjacent to site; and,
10. Include a communication plan to notify surrounding businesses and residents.

Edgemont Boulevard is expected to be closed at Capilano Road from January 2016 to April 2016 as part of the Capilano Water Main Replacement Project. Due to other development projects and associated major civil works potentially underway in Edgemont Village the excavation and demolition works for this project will not be permitted to take place until the intersection of Capilano Rd and Edgemont Blvd is re-opened.

Conclusion

This project is consistent with the directions established in the OCP and the Edgemont Village Centre: Plan and Design Guidelines. It addresses OCP housing policies related to the provision of a range of housing options. The project is now ready for Council's consideration.

Options

The following options are available for Council's consideration:

1. Introduce Bylaws 8159, 8160, and 8162 and refer Bylaw 8159 and 8160 to a Public Hearing (staff recommendation); or,
2. Defeat the bylaws at First Reading.

Natasha Letchford
Community Planner

Attachments:

- A. Architectural and Landscape Plans
- B. Bylaw 8159 – OCP Amendment
- C. Bylaw 8160 – Rezoning
- D. Bylaw 8162 – Housing Agreement
- E. Public Information Meeting Facilitator's Report

REVIEWED WITH:		
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<input type="checkbox"/> Development Services	_____	<input type="checkbox"/> Communications
<input type="checkbox"/> Utilities	_____	<input type="checkbox"/> Finance
<input type="checkbox"/> Engineering Operations	_____	<input type="checkbox"/> Fire Services
<input type="checkbox"/> Parks	_____	<input type="checkbox"/> ITS
<input type="checkbox"/> Environment	_____	<input type="checkbox"/> Solicitor
<input type="checkbox"/> Facilities	_____	<input type="checkbox"/> GIS
<input type="checkbox"/> Human Resources	_____	<input type="checkbox"/> Real Estate
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		<input type="checkbox"/> Museum & Arch.
		<input type="checkbox"/> Other:

EDGEMONT VILLAGE

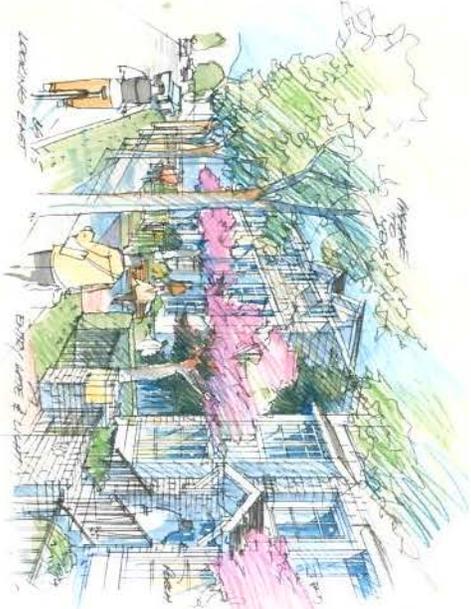
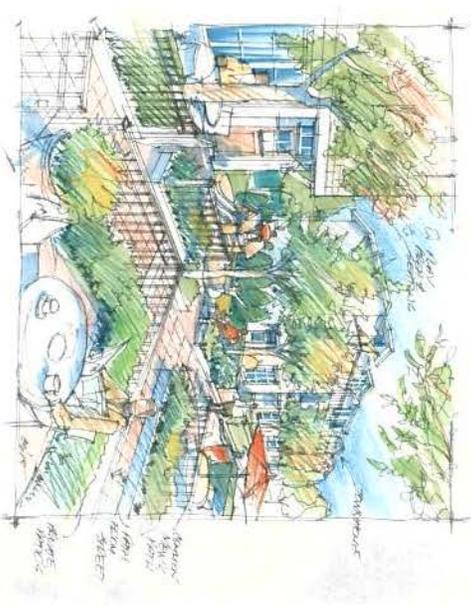
BOFFO PROPERTIES (EDGEMONT) INC.

DRAWING LIST

- A00 COVER SHEET
- A01 LAYOUT AND SITE PLAN
- A02 SECTION ELEVATION
- A03 SECTION ELEVATION
- A04 SECTION ELEVATION
- A05 SECTION ELEVATION
- A06 SECTION ELEVATION
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- A99 SECTION ELEVATION
- A00 BUILDING SECTION 1 - INC 1
- A01 BUILDING SECTION 2 - INC 2
- A02 BUILDING SECTION 3 - INC 3

CONSULTANT LIST

- OWNER:** BOFFO PROPERTIES (EDGEMONT) INC.
27 VANCOUVER STREET
VANCOUVER, B.C.
PHONE: 604-684-2828
- ARCHITECT:** SHIRLEY ADAMSON ARCHITECTS
VANCOUVER, B.C.
PHONE: 604-684-2828
- LANDSCAPE ARCHITECT:** CLARKE/BERNARD
VANCOUVER, B.C.
PHONE: 604-684-2828
- COMPLIANCE CONSULTANT:** LEANNE PEARL SERVICES
485 EAST 7TH AVENUE
VANCOUVER, B.C.
PHONE: 604-684-2828
- ARCHITECTURAL CONSULTANT:** BOFFO PROPERTIES (EDGEMONT) INC.
27 VANCOUVER STREET
VANCOUVER, B.C.
PHONE: 604-684-2828
- STRUCTURAL CONSULTANT:** BOFFO PROPERTIES (EDGEMONT) INC.
27 VANCOUVER STREET
VANCOUVER, B.C.
PHONE: 604-684-2828
- Mechanical Engineering Consultant:** TRINIC CONSULTING INC.
720 - 1010 WEST 10TH AVENUE
VANCOUVER, B.C.
PHONE: 604-684-2828
- Electrical Engineering Consultant:** TRINIC CONSULTING INC.
720 - 1010 WEST 10TH AVENUE
VANCOUVER, B.C.
PHONE: 604-684-2828
- TRINIC ARCHITECTURAL CONSULTANT:** TRINIC CONSULTING INC.
720 - 1010 WEST 10TH AVENUE
VANCOUVER, B.C.
PHONE: 604-684-2828
- ONLINE ENGINEERING CONSULTANT:** JENNIFER M. BROWN ENGINEERING
221 - 1262 CAMELOT WAY
VANCOUVER, B.C.
PHONE: 604-684-2828
- SOIL MECHANICS CONSULTANT:** DICKSON CONSULTANTS
485 EAST 7TH AVENUE
VANCOUVER, B.C.
PHONE: 604-684-2828

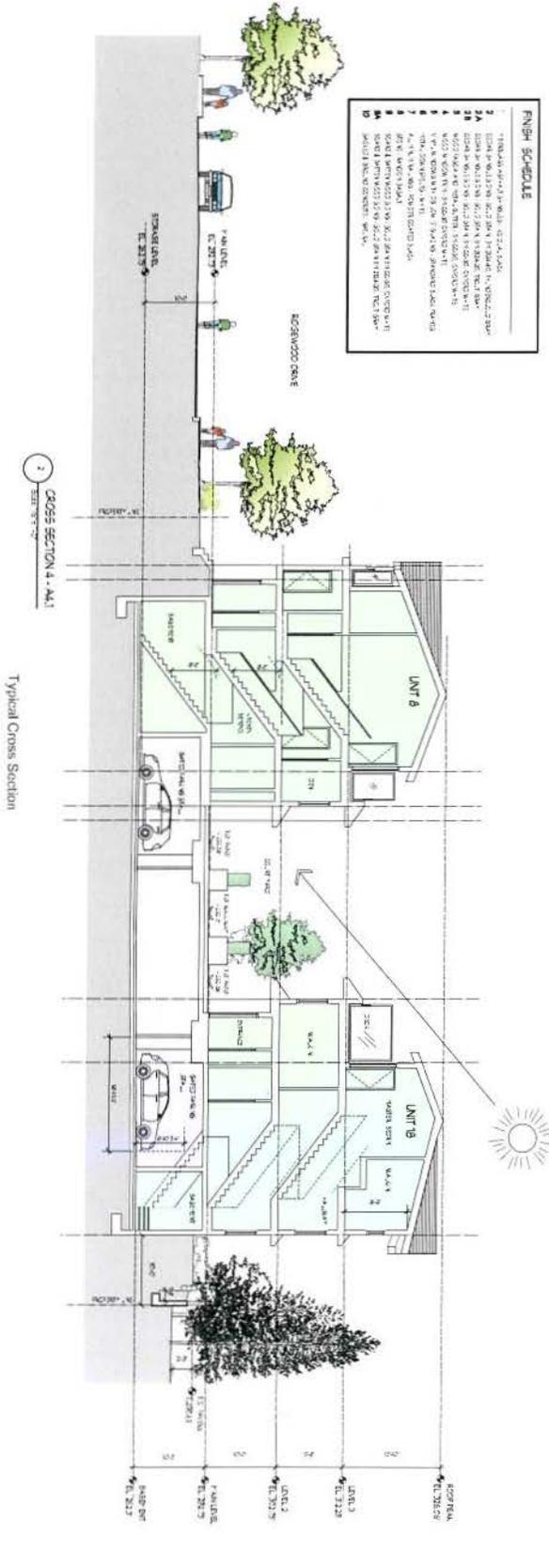


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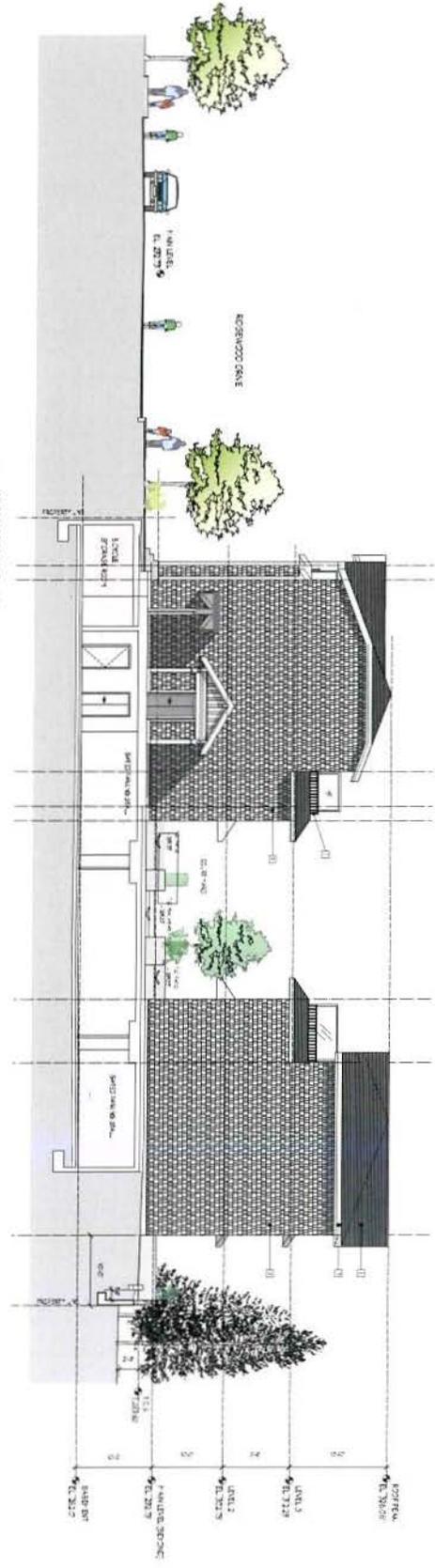
EDGEMONT VILLAGE
NORTH VANCOUVER DISTRICT, BC

DRAWINGS LIST
CONSULTANTS LIST

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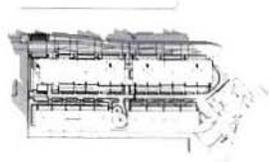


Typical Cross Section

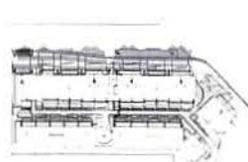


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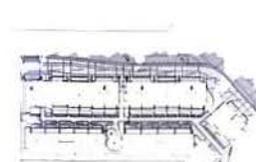
GATEWAY
 ARCHITECTURE
 1000 WEST 10TH AVENUE
 VANCOUVER, BC V6H 1A1
 TEL: 604-271-1111
 WWW.GATEWAYARCHITECTURE.COM



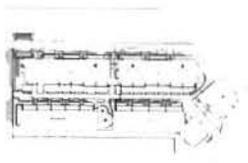
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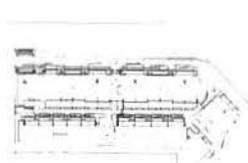
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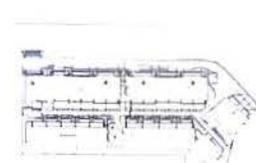
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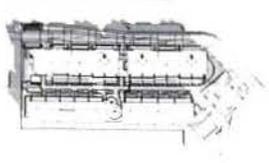
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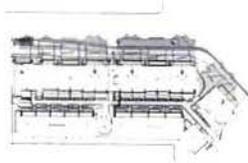
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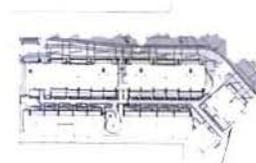
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↑ SEPT 21: 10AM



↑ SEPT 21: 12 NOON



↑ SEPT 21: 2 PM



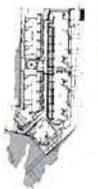
BOFFO

EDGE MOUNT VILLAGE
NEW AVENUE, PHASE 2
SHADOW ANALYSIS

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← MARCH 21: 6 PM



← JUNE 21: 6 PM



← SEPT 21: 6 PM

The Corporation of the District of North Vancouver

Bylaw 8159

A bylaw to amend District of North Vancouver Official Community Plan Bylaw 7900,
2011

The Council for The Corporation of the District of North Vancouver enacts as follows:

1. Citation

This bylaw may be cited as "District of North Vancouver Official Community Plan Bylaw 7900, 2011, Amendment Bylaw 8159, 2015 (Amendment 16)".

2. Amendments

2.1 District of North Vancouver Official Community Plan Bylaw 7900, 2011, is amended as follows:

- a) Map 2 Land Use: as illustrated on Schedule A, by changing the land use designation of the properties on Map 2 from Residential Level 2: Detached Residential to Residential Level 4: Transition Multifamily;
- b) Map 3.1 Form and character Development Permit Area: as illustrated on Schedule A, by adding the properties to Map 3.1, designating them as a Form and Character of Commercial, Industrial and Multifamily Development Development Permit Area; and,
- c) Map 4.1 Energy and Water Conservation and GHG Emission Reduction Development Permit Area: as illustrated on Schedule A, by adding the properties to Map 4.1, designating them as an Energy and Water Conservation and Greenhouse Gas Reduction Development Permit Area.

READ a first time by a majority of all Council members.

PUBLIC HEARING held

READ a second time by a majority of all Council members.

READ a third time by a majority of all Council members.

ADOPTED

by a majority of all Council members.

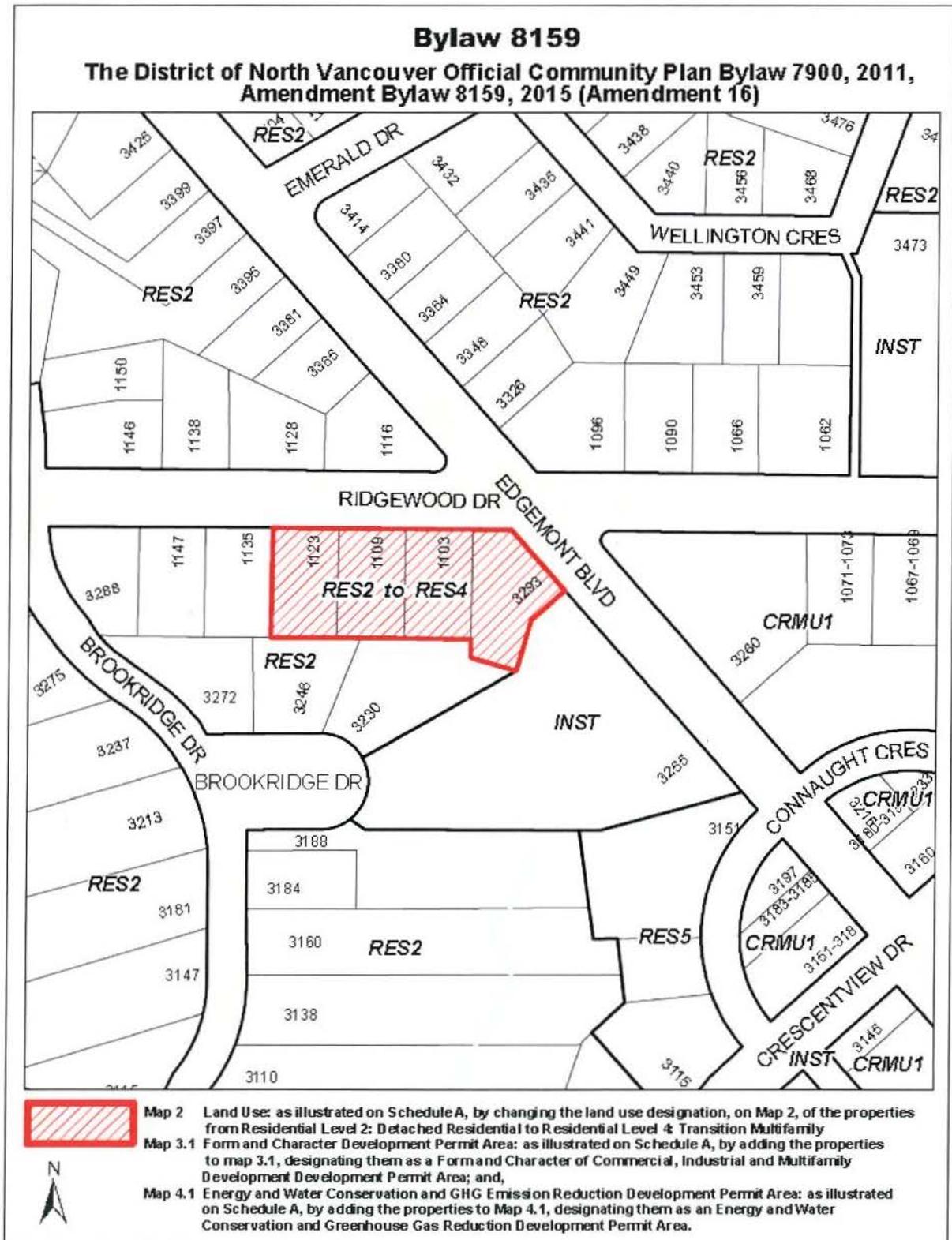
Mayor

Municipal Clerk

Certified a true copy

Municipal Clerk

Schedule A to Bylaw 8159



The Corporation of the District of North Vancouver

Bylaw 8160

A bylaw to amend District of North Vancouver Zoning Bylaw 3210, 1965

The Council for The Corporation of the District of North Vancouver enacts as follows:

1. Citation

This bylaw may be cited as "District of North Vancouver Rezoning Bylaw 3210 (Bylaw 8160)".

2. Amendments

2.1 District of North Vancouver Zoning Bylaw 3210, 1965 is amended as follows:

- i. Part 3, General Operative Clauses, Section 301 (2) by inserting the following zoning designation:

"Comprehensive Development Zone 92 CD 92"

- ii. Part 4B Comprehensive Development Zone Regulations by inserting the following:

"4B92 Comprehensive Development Zone 92 CD 92

The CD 92 zone is applied to:

1103, 1109, 1123 Ridgewood Drive and 3293 Edgemont Boulevard
Legally described as:

- a) Lot 4 Block 69 District Lots 598 to 601 Plan 6659, PID 010-823-522;
- b) Lot 5 Block 69 District Lots 598 to 601 Plan 6659, PID 010-823-531;
- c) Lot 6 Block 69 District Lots 598 to 601 Plan 6659, PID 010-823-549;
- d) Lot 7 Block 69 District Lots 598 to 601 Plan 6659, PID 010-823-557.

4B 92 – 1 Intent:

The purpose of the CD 92 Zone is to establish specific land use and development regulations for a 24 unit townhouse project.

4B 92 – 2 Permitted Uses:

The following *principal* uses are permitted in the CD 92 Zone:

- a) Uses Permitted Without Conditions:
 - i. Residential building, multifamily townhouse.

- b) Conditional Uses:

- i. Not applicable.

4B 92 – 3 Conditions of Use:

- a) Not applicable.

4B 92 – 4 Accessory Use:

- a) *Home occupations* are permitted in *residential* dwelling units.

4B 92 – 5 Density:

- a) The maximum permitted density in the CD92 Zone is limited to a floor space ratio (FSR) of 0.45 and four residential units, inclusive of any density bonus for energy performance.

4B 92 – 6 Amenities:

- a) Despite Subsection 4B92 – 5, permitted density in the CD 92 Zone may be increased to a maximum of 3,761 m² (40,486 sq. ft.) and 24 residential units, inclusive of any density bonus for energy performance, if the owner completes the following:
 - i. Contributes \$157,460 to the municipality to be used for any or all of the following amenities (with allocation and timing of expenditure to be determined by the municipality in its sole discretion):
 - a. Improvements to public parks, plazas, trails and greenways;
 - b. Municipal facilities and facility improvements;
 - c. Public art and other beautification projects; and,
 - d. Affordable or special needs housing.;
 - ii. Enters into a Housing Agreement prohibiting any restrictions preventing the owners in the project from renting their units; and,

- iii. Provides at least five units which meet the basic accessible design criteria; two of which must also meet the enhanced accessible design criteria as outlined in the District of North Vancouver Council Policy: 'Accessible Design Policy for Multi-Family Housing.'

4B 92 – 7 Height:

- a) The maximum permitted height for any building in the CD 92 Zone, is 11.6 m (38.0 ft.).

4B 92 – 8 Setbacks:

- a) Buildings must be set back from property lines to the closest building face, excluding any partially exposed underground parking structure and projecting window boxes which are not to exceed 0.15 m (0.5 ft), in accordance with the following regulations:

Setback	Minimum Required Setback
North (From Ridgewood Dr.)	2.08 m (6.8 ft.)
Northeast (From Edgemont Blvd)	5.02 m (16.5 ft)
Southeast	2.43 m (8.0 ft.)
South	3.04 m (10.0 ft.)
West	3.53 m (11. ft.)

4B 92 – 9 Coverage:

- a) Maximum building coverage is 50%; not including underground parking or patios;
- b) Maximum site coverage is 60%.

4B 92 – 10 Acoustic Requirements:

- a) In the case of residential purposes, a building permit application shall require evidence in the form of a report and recommendations prepared by persons trained in acoustics and current techniques of noise measurements, demonstrating that the noise levels expressed in decibels set opposite such portions of the dwelling units:

Portion of Dwelling Unit	Noise Level (Decibels)
Bedrooms	35
Living and Dining rooms	40
Kitchen, Bathrooms and Hallways	45

4B 92 – 11 Landscaping and Storm Water Management:

- a) All land areas not occupied by buildings must be landscaped in accordance with a landscape plan approved by the District of North Vancouver; and,
- b) All electrical kiosks and garbage and recycling container facilities not located underground or within a building must be screened.

4B 92 – 12 Parking, Loading and Servicing Regulations:

- a) A minimum of 44 parking spaces are required for the use of residents;
- b) A minimum of 6 parking spaces are required for designated visitor parking;
- c) All parking spaces must meet the minimum width and length standards established in Part 10 of the Zoning Bylaw, exclusive of building support columns; and,
- d) Resident bicycle storage must be provided on the basis of one space per unit;
- e) Visitor bicycle storage must be provided on the basis of a minimum of 5 class 2 bicycle parking spaces.”

2.2 The Zoning Map is amended in the case of the lands illustrated on the attached map (Schedule A) by rezoning the land from the Single Family Residential Edgemont Zone (RSE) to Comprehensive Development Zone 92 (CD 92).

READ a first time

PUBLIC HEARING held

READ a second time

READ a third time

Certified a true copy of

Municipal Clerk

APPROVED by the Ministry of Transportation and Infrastructure on

ADOPTED

Mayor

Municipal Clerk

Certified a true copy

Municipal Clerk

Bylaw 8160 Schedule A: Zoning Map

Bylaw 8160

The District of North Vancouver Rezoning Bylaw 3210 (Bylaw 8160)



The Corporation of the District of North Vancouver

Bylaw 8162

A bylaw to enter into a Housing Agreement (1103, 1109, 1123 Ridgewood Dr. and 3293 Edgemont Blvd.)

The Council for The Corporation of the District of North Vancouver enacts as follows:

1. Citation

This bylaw may be cited as “Housing Agreement Bylaw 8162, 2015 (1103, 1109, 1123 Ridgewood Dr. and 3293 Edgemont Blvd.)”.

2. Authorization to Enter into Agreement

2.1 The Council hereby authorizes a housing agreement between The Corporation of the District of North Vancouver and Boffo Properties (Edgemont) Inc. substantially in the form attached to this Bylaw as Schedule “A” with respect to the following lands:

- Lot 4 Block 69 District Lots 598 to 601 Plan 6659, PID 010-823-522;
- Lot 5 Block 69 District Lots 598 to 601 Plan 6659, PID 010-823-531;
- Lot 6 Block 69 District Lots 598 to 601 Plan 6659, PID 010-823-549; and,
- Lot 7 Block 69 District Lots 598 to 601 Plan 6659, PID 010-823-557.

3. Execution of Documents

The Mayor and Municipal Clerk are authorized to execute any documents required to give effect to the Housing Agreement.

READ a first time

READ a second time

READ a third time

ADOPTED

Mayor

Municipal Clerk

Certified a true copy

Municipal Clerk

SECTION 219 COVENANT – HOUSING AGREEMENT

This agreement is dated for reference the.

BETWEEN:

Boffo Properties (Edgemont) Inc.
1391 Venables Street
Vancouver, BC V5L 2G1

(the "Owner")

AND:

THE CORPORATION OF THE DISTRICT OF NORTH VANCOUVER, a municipality
incorporated under the *Local Government Act*, R.S.B.C. 1996, c.323 and having its office
at 355 West Queens Road, North Vancouver, BC V7N 4N5

(the "District")

WHEREAS:

- A. The Owner is the registered owner of the Lands (as hereinafter defined);
- B. The Owner wishes to obtain development permissions with respect to the Lands and wishes to create a condominium development which will contain residential strata units on the Lands;
- C. Section 905 of the *Local Government Act* authorises the District, by bylaw, to enter into a housing agreement to provide for the prevention of rental restrictions on housing, and provides for the contents of the agreement; and
- D. Section 219 of the *Land Title Act* (British Columbia) permits the registration in favour of the District of a covenant of a negative or positive nature relating to the use of land or a building thereon, or providing that land is to be built on in accordance with the covenant, or providing that land is not to be built on except in accordance with the covenant, or providing that land is not to be subdivided except in accordance with the covenant;

NOW THEREFORE in consideration of the mutual promises contained in it, and in consideration of the payment of \$1.00 by the District to the Owner (the receipt and sufficiency of which are hereby acknowledged by the Owner), the parties covenant and agree with each other as follows, as a housing agreement under Section 905 of the *Local Government Act*, as a contract and a deed under seal between the parties, and as a covenant under Section 219 of the *Land Title Act*, and the Owner hereby

further covenants and agrees that neither the Lands nor any building constructed thereon shall be used or built on except in accordance with this Agreement:

1. DEFINITIONS

1.01 Definitions

In this agreement:

- (a) “*Development Permit*” means development permit No. _____ issued by the District;
- (b) “*Lands*” means land described in Item 2 of the *Land Title Act* Form C to which this agreement is attached;
- (c) “*Proposed Development*” means the proposed development containing not more than 24 Units to be constructed on the Lands in accordance with the Development Permit;
- (d) “*Unit*” means a residential dwelling strata unit in the Proposed Development; and
- (e) “*Unit Owner*” means the registered owner of a Dwelling Unit in the Proposed Development.

2. TERM

This Agreement will commence upon adoption by District Council of Bylaw 8162 and remain in effect until terminated by the District as set out in this Agreement.

3. RENTAL ACCOMODATION

3.01 Rental Disclosure Statement

No Unit in the Proposed Development may be occupied unless the Owner has:

- (a) before the first Unit is offered for sale, or conveyed to a purchaser without being offered for sale, filed with the Superintendent of Real Estate a Rental Disclosure Statement designating all of the Units as rental strata lots and imposing at least a ninety-nine (99) year rental period in relation to all of the Units pursuant to the *Strata Property Act* (or any successor or replacement legislation); and
- (b) given a copy of the Rental Disclosure Statement to each prospective purchaser of any Unit before the prospective purchaser enters into an agreement to purchase in respect of the Unit.

3.02 Rental Accommodation

The Units constructed on the Lands from time to time may always be used to provide rental accommodation as the Owner or a Unit Owner may choose from time to time.

Binding on Strata Corporation

This agreement shall be binding upon all strata corporations created by the subdivision of the Lands or any part thereof (including the Units) pursuant to the *Strata Property Act*, and upon all Unit Owners.

3.03 Strata Bylaw Invalid

Any strata corporation bylaw which prevents, restricts or abridges the right to use any of the Units as rental accommodations shall have no force or effect.

3.04 No Bylaw

The strata corporation shall not pass any bylaws preventing, restricting or abridging the use of the Lands, the Proposed Development or the Units contained therein from time to time as rental accommodation.

3.05 Vote

No Unit Owner, nor any tenant or mortgagee thereof, shall vote for any strata corporation bylaw purporting to prevent, restrict or abridge the use of the Lands, the Proposed Development and the units contained therein from time to time as rental accommodation.

3.06 Notice

The Owner will provide notice of this Agreement to any person or persons intending to purchase a Unit prior to any such person entering into an agreement of purchase and sale, agreement for sale, or option or similar right to purchase as part of the Disclosure Statement for any part of the Proposed Development prepared by the Owner pursuant to the *Real Estate Development Marketing Act*.

3.07 Release of Covenant

The District agrees that if the District of North Vancouver Rezoning Bylaw 3210 (bylaw 8160), is not adopted by the District's Council before December 31, 2016, the Owner is entitled to require the District to execute and deliver to the Owner a discharge, in registrable form, of this Agreement from title to the Land. The Owner is responsible for the preparation of the discharge under this section and for the cost of registration at the Land Title Office.

4. DEFAULT AND REMEDIES4.01 Notice of Default

The District may, acting reasonably, give to the Owner written notice to cure a default under this Agreement within thirty (30) days of delivery of the notice. The notice must specify the nature of the default. The Owner must act with diligence to correct the default within the time specified.

4.02 Costs

The Owner will pay to the District upon demand all the District's costs of exercising its rights or remedies under this Agreement, on a full indemnity basis.

4.03 Damages an Inadequate Remedy

The Owner acknowledges and agrees that in the case of a breach of this Agreement which is not fully remediable by the mere payment of money and promptly so remedied, the harm sustained by the District and to the public interest will be irreparable and not susceptible of adequate monetary compensation.

4.04 Equitable Remedies

Each party to this Agreement, in addition to its rights under this Agreement or at law, will be entitled to all equitable remedies including specific performance, injunction and declaratory relief, or any of them, to enforce its rights under this Agreement.

4.05 No Penalty or Forfeiture

The Owner acknowledges and agrees that it is entering into this Agreement to benefit the public interest in providing rental accommodation, and that the District's rights and remedies under this Agreement are necessary to ensure that this purpose is carried out, and the District's rights and remedies under this Agreement are fair and reasonable and ought not to be construed as a penalty or forfeiture.

4.06 Cumulative Remedies

No reference to nor exercise of any specific right or remedy under this Agreement or at law or at equity by any party will prejudice, limit or preclude that party from exercising any other right or remedy. No right or remedy will be exclusive or dependent upon any other right to remedy, but any party, from time to time, may exercise any one or more of such rights or remedies independently, successively, or in combination. The Owner acknowledges that specific performance, injunctive relief (mandatory or otherwise) or other equitable relief may be the only adequate remedy for a default by the Owner under this Agreement.

5. LIABILITY**5.01** Indemnity

Except if arising directly from the negligence of the District or its employees, agents or contractors, the Owner will indemnify and save harmless each of the District and its board members, officers, directors, employees, agents, and elected or appointed officials,, and their heirs, executors, administrators, personal representatives, successors and assigns, from and against all claims, demands, actions, loss, damage, costs and liabilities that all or any of them will or may be liable for or suffer or incur or be put to any act or omission by the Owner or its officers, directors, employees, agents, contractors, or other persons for whom the Owner is at

law responsible, or by reason of or arising out of the Owner's ownership, operation, management or financing of the Proposed Development or any part thereof.

5.02 Release

The Owner hereby releases and forever discharges the District, its elected officials, board members, officers, directors, employees and agents, and its and their heirs, executors, administrators, personal representatives, successors and assigns from and against all claims, demands, damages, actions or causes of action by reason of or arising out of advice or direction respecting the ownership, operation or management of the Proposed Development or any part thereof which has been or hereafter may be given to the Owner by all or any of them.

5.03 Survival

The covenants of the Owner set out in Sections 5.01 and 5.02 will survive termination of this Agreement and continue to apply to any breach of the Agreement or claim arising under this Agreement during the ownership by the Owner of the Lands or any Unit therein, as applicable.

6. GENERAL PROVISIONS

6.01 District's Power Unaffected

Nothing in this Agreement:

- (a) affects or limits any discretion, rights, powers, duties or obligations of the District under any enactment or at common law, including in relation to the use or subdivision of land;
- (b) affects or limits any enactment relating to the use of the Lands or any condition contained in any approval including any development permit concerning the development of the Lands; or
- (c) relieves the Owner from complying with any enactment, including the District's bylaws in relation to the use of the Lands.

6.02 Agreement for Benefit of District Only

The Owner and District agree that:

- (a) this Agreement is entered into only for the benefit of the District:
- (b) this Agreement is not intended to protect the interests of the Owner, any Unit Owner, any Occupant or any future owner, occupier or user of any part of the Proposed Development, including any Unit, or the interests of any third party, and the District has no obligation to anyone to enforce the terms of this Agreement; and

- (c) The District may at any time terminate this Agreement, in whole or in part, and execute a release and discharge of this Agreement in respect of the Proposed Development or any Unit therein, without liability to anyone for doing so.

6.03 Agreement Runs With the Lands

This Agreement burdens and runs with the Lands and any part into which any of them may be subdivided or consolidated, by strata plan or otherwise. All of the covenants and agreements contained in this Agreement are made by the Owner for itself, its successors and assigns, and all persons who acquire an interest in the Lands or in any Unit after the date of this Agreement.

6.04 Release

The covenants and agreements on the part of the Owner and any Unit Owner and herein set forth in this Agreement have been made by the Owner and any Unit Owner as contractual obligations as well as being made pursuant to Section 905 of the *Local Government Act* (British Columbia) and as such will be binding on the Owner and any Unit Owner, except that neither the Owner nor any Unit Owner shall be liable for any default in the performance or observance of this Agreement occurring after such party ceases to own the Lands or a Unit as the case may be.

6.05 Priority of This Agreement

The Owner will, at its expense, do or cause to be done all acts reasonably necessary to ensure this Agreement is registered against the title to each Unit in the Proposed Development, including any amendments to this Agreement as may be required by the Land Title Office or the District to effect such registration.

6.06 Agreement to Have Effect as Deed

The District and the Owner each intend by execution and delivery of this Agreement to create both a contract and a deed under seal.

6.07 Waiver

An alleged waiver by a party of any breach by another party of its obligations under this Agreement will be effective only if it is an express waiver of the breach in writing. No waiver of a breach of this Agreement is deemed or construed to be a consent or waiver of any other breach of this Agreement.

6.08 Time

Time is of the essence in this Agreement. If any party waives this requirement, that party may reinstate it by delivering notice to another party.

6.09 Validity of Provisions

If a Court of competent jurisdiction finds that any part of this Agreement is invalid, illegal, or unenforceable, that part is to be considered to have been severed from the rest of this Agreement and the rest of this Agreement remains in force unaffected by that holding or by the severance of that part.

6.10 Extent of Obligations and Costs

Every obligation of a party which is set out in this Agreement will extend throughout the Term and, to the extent that any obligation ought to have been observed or performed prior to or upon the expiry or earlier termination of the Term, such obligation will survive the expiry or earlier termination of the Term until it has been observed or performed.

6.11 Notices

All notices, demands, or requests of any kind, which a party may be required or permitted to serve on another in connection with this Agreement, must be in writing and may be served on the other parties by registered mail or by personal service, to the following address for each party:

If to the District:

District Municipal Hall
355 West Queens Road
North Vancouver, BC V7N 4N5

Attention: Planning Department

If to the Owner:

Boffo Properties (Edgemont) Inc.
1391 Venables Street
Vancouver, BC V5L 2G1

Attention: Josh Anderson

If to the Unit Owner:

The address of the registered owner which appears on title to the Unit at the time of notice.

Service of any such notice, demand, or request will be deemed complete, if made by registered mail, 72 hours after the date and hour of mailing, except where there is a postal service disruption during such period, in which case service will be deemed to be complete only upon actual delivery of the notice, demand or request and if made by personal service, upon personal

service being effected. Any party, from time to time, by notice in writing served upon the other parties, may designate a different address or different or additional persons to which all notices, demands, or requests are to be addressed.

6.12 Further Assurances

Upon request by the District, the Owner will promptly do such acts and execute such documents as may be reasonably necessary, in the opinion of the District, to give effect to this Agreement.

6.13 Enuring Effect

This Agreement will enure to the benefit of and be binding upon each of the parties and their successors and permitted assigns.

7. INTERPRETATION

7.01 References

Gender specific terms include both genders and include corporations. Words in the singular include the plural, and words in the plural include the singular.

7.02 Construction

The division of this Agreement into sections and the use of headings are for convenience of reference only and are not intended to govern, limit or aid in the construction of any provision. In all cases, the language in this Agreement is to be construed simply according to its fair meaning, and not strictly for or against either party.

7.03 No Limitation

The word “including” when following any general statement or term is not to be construed to limit the general statement or term to the specific items which immediately follow the general statement or term similar items whether or not words such as “without limitation” or “but not limited to” are used, but rather the general statement or term is to be construed to refer to all other items that could reasonably fall within the broadest possible scope of the general statement or term.

7.04 Terms Mandatory

The words “must” and “will” and “shall” are to be construed as imperative.

7.05 Statutes

Any reference in this Agreement to any statute or bylaw includes any subsequent amendment, re-enactment, or replacement of that statute or bylaw.

7.06 Entire Agreement

- (d) This is the entire agreement between the District and the Owner concerning its subject, and there are no warranties, representations, conditions or collateral agreements relating to this Agreement, except as included in this Agreement.
- (e) This Agreement may be amended only by a document executed by the parties to this Agreement and by bylaw, such amendment to be effective only upon adoption by District Council of a bylaw to amend Bylaw 8160.

7.07 Governing Law

This Agreement is to be governed by and construed and enforced in accordance with the laws of British Columbia.

As evidence of their agreement to be bound by the terms of this instrument, the parties hereto have executed the *Land Title Act* Form C that is attached hereto and forms part of this Agreement.

GRANT OF PRIORITY

WHEREAS _____ (the “**Chargeholder**”) is the holder of the following charge which is registered in the Land Title Office:

(a) _____ (the “**Charge**”);

AND WHEREAS the Chargeholder agrees to allow the Section 219 Covenant herein to have priority over the Charge;

THIS PRIORITY AGREEMENT is evidence that in consideration of the sum of \$1.00 paid by THE CORPORATION OF THE DISTRICT OF NORTH VANCOUVER (the “**District**”) to the Chargeholder, the receipt and sufficiency of which are hereby acknowledged, the Chargeholder covenants and agrees to subordinate and postpone all its rights, title and interest in and to the lands described in the Form C to which this Agreement is attached (the “**Lands**”) with the intent and with the effect that the interests of the District rank ahead of the Charge as though the Section 219 Covenant herein had been executed, delivered and registered against title to the Lands before registration of the Charge.

As evidence of its Agreement to be bound by the above terms, as a contract and as a deed executed and delivered under seal, the Chargeholder has executed the Form C to which this Agreement is attached and which forms part of this Agreement.

END OF DOCUMENT

Boffo Properties: 1103 - 1123 Ridgewood Drive & 3293 Edgemont Blvd Rezoning**Public Information Meeting Summary Report**

Event Date: November 5, 2015
Time: 6pm – 8pm
Location: Highlands United Church, Edgemont Village
Attendance: 61 members of the public signed in
Comment Forms: 14 comment sheets and 3 emails were submitted.

Meeting Purpose:

- 1) To present development application materials to neighbours
- 2) To provide an opportunity for the public to ask questions about the development
- 3) To provide an opportunity for neighbours to comment on the proposal.

Notification:Invitation Brochures

Invitations with fact and comment sheets were delivered to 547 homes, including all homes within 100 metres of the site.

Site Sign

A sign was erected on the site to notify neighbours of the meeting. A copy of the sign is included in Appendix A: Notification.

Newspaper Ad

A newspaper ad was placed in the North Shore News on Wednesday, October 28th and Friday, October 30th. A copy of the ad is included in Appendix A: Notification.

Attendance:

61 members of the public attended and signed in for the meeting. A copy of the sign-in sheets are provided in Appendix B.

The following District staff and project team members were in attendance:

District of North Vancouver:

- Natasha Letchford, Development Planner, District of North Vancouver

Project Team:

- Jamie Wallace, Boffo Properties
- Josh Anderson, Boffo Properties
- Michael Cox, Gateway Architecture
- Jennifer Stamp, Durante Kreuk Landscape Architecture

- Lance Berelowitz, Urban Forum Associates
- Tyler Thompson, Bunt & Associates

Facilitator:

- Steven Petersson, Petersson Planning Consulting

Overview:

The Public Information Meeting was designed to provide several methods for the public to engage in the process.

The evening began with an Open House, which lasted approximately 45 minutes, where participants could browse display boards and have informal discussion with the Boffo Properties team.

The Open House was followed by a presentation by the Boffo Properties team.

The presentation was followed by a forty-five minute dialogue that provided the public with an opportunity to ask questions and discuss the project. To ensure an accurate record of the dialogue, the facilitator noted public comments and questions on flip chart paper on the wall.

The participants were invited to submit written comments to the facilitator or to the municipal planner.

The key themes of the evening were traffic, access, density and housing choice.

Public Dialogue:

(Q = Question, A = Answer, C=Comment, and the number is to track the dialogue)

Q1 How much will the road be widened?

A1 The DNV is re-designing the intersection.

Q2 Will public access to the courtyard be permitted?

A2 No.

C3 Trees could grow and block sunlight.

A3 Narrow, pyramidal trees have been selected to minimize blocking sunlight.

Q4 Will on-street parking be lost on Ridgewood?

A4 There will be no change in on-street parking on Ridgewood.

C5 Pedestrians will use a different entrance than drivers.

C6 The design is Georgian, rather than modern. It appears out of place in Edgemont Village.

A6 The forms have local precedents.

Q7 Will this project go to Advisory Design Panel?

A7 Yes: it will be reviewed by the Panel next week.

C8 The colours are not appropriate choices for Edgemont Village.
A8 Edgemont Village is eclectic.

Q9 Will the DNV require payment of a Community Amenity Contribution if the project is approved?
A9 Yes.

C10 There is no pedestrian walkway on the west side of Edgemont and Ridgewood.
A10 The DNV needs to follow up on this point.

Q11 Is there a delivery parking space, or a spot for moving vans?
A11 Yes, near the parkade entrance.

Q12 Will the townhouses be sold or rented?
A12 They will be sold as market units, but without rental restrictions.

C13 The full movement driveway entrance is near a busy intersection.
A13 The traffic analysis for this project is comprehensive, and factors in influences from other developments. The team is working on the intersections with the DNV.

Q14 Will the traffic lights remain after the water main project is complete?
A14 Our understanding is that the DNV plans to review the function of the traffic signals after the water main project is complete, and further data has been collected.

C15 I am unhappy with increased density in the Village because of traffic and construction impacts.

C16 We support increased density and housing choice for younger families and downsizers (re-iterated by four different participants)

Q17 How much will the units cost?
A17 We do not know yet: it will be set by the market.

Q18 Are you proposing any one-storey units for seniors?
A18 No one-storey units are proposed, but units in cluster "C" are designed to have space for a lift and include other adaptable design features.

C19 Three visitor parking stalls is not enough.
A19 The traffic analysis, which looked at other comparable projects, supports this number of visitor parking stalls. Some residential spots will not be used. Visitor parking demand peaks at night (not when trades are working).

C20 Grosvenor will supply extra visitor parking in the Village across the street.

Q21 Will the underground parking be secure?
A21 Yes.

C22 One parking entrance will not be enough.

A22 One parking stall for a project of this size is typical. If several people are coming and going simultaneously, they will take turns using the entrance.

Q23 How many trees will be removed for this development?

A23 Underground parking makes it hard to retain existing trees. Boffo Properties will replant many trees.

Q24 Will reduced front yard setbacks affect sight lines for traffic?

A24 No.

C25 Edgemont Village is changing rapidly with several concurrent projects that increase density. Three projects are happening at the same time within one block.

C26 The dialogue tonight reflected points raised during the Edgemont Village Plan Refresh process.

Comment Sheet and Email Summary

Fourteen comment sheets and 3 emails were submitted to the DNV after the meeting. Copies of the correspondence are attached as an Appendix C. Seven of the fourteen comment sheets and one of the emails explicitly expressed support for the project.

Below is a summary of the key themes communicated via comment sheets and emails after the meeting. They are clustered according to topic.

Affordability

- I currently live in a townhouse with my wife and two young boys but will require more space in the future. Single family homes in Edgemont are beyond my price range but this could be affordable and preferable as an option to live closer to my son's school at Cleveland Elementary.
- Who can afford to buy these homes? Neither singles over 50, nor young couples.

Architecture & Site Planning

- The living space in townhouses is too small: staircases take up too much room
- Stairs in multi-level townhouses make the units unsuitable for seniors and households with small children
- The pad-mounted transformer is proposed to be located on a prominent corner of the site. This could be integrated with the loading and services on the west side of the site, instead.
- The rooflines should have a steeper pitch.
- Project looks great.
- The project will have a positive impact on the community and businesses, but the architecture is very conservative. The architecture should incorporate design improvements that refer to the artistic side of the area.
- Nice mix of unit sizes.
- Very nice looking site!
- Keep the project on Ridgewood: don't go around the corner onto Edgemont Boulevard
- Skepticism about the artistic rendering of the project: where are the traffic lights and traffic?

- I find the bulky Georgian “shingle-style” architectural expression for this project to be very out of place in the Edgemont context.

Density

- I fully support this proposal. Edgemont needs more diverse housing types in this area and townhouses are appropriate and compatible with the existing area.
- I support townhouses in the village, but too many units are proposed
- The proposal is consistent with Edgemont Refresh vision and principles
- Land use and density will support Edgemont businesses and help animate the street
- New residents will make the neighbourhood even better.
- I have no problem with increased residential density at this location.

Parking

- Concern about amount of visitor parking, and a request for data from comparable projects
- Three visitor parking stalls is probably not enough, and will result in visitors parking on Brookridge Drive.

Traffic

- Concern about traffic impacts and pedestrian safety in Edgemont Village
- Traffic is an issue, but this density will support Edgemont as a walkable neighbourhood
- Worried about traffic!
- The mid-block parkade access/egress will be very problematic on the busy Ridgewood arterial. This should be right-in, right-out only. This intersection has become a serious problem, particularly during rush hour.

Environment

- More than 125 birds in this area depend on the existing older trees

Process

- What are the odds that this project will not be approved?
- Have the Edgemont Village Plan details been adopted into the existing OCP?
- Is the current RSE zoning FSR 0.55 or is it 0.35 + 350 sq ft?
- Does the project require an OCP amendment as well as rezoning in order to proceed?
- Have shadow studies been conducted?

General Comments

- Wonderful concept for young families!
- I'm so happy young families will be able to move here.
- Boffo is a quality developer.
- This project will serve the community well

Conclusion

The purpose of this public meeting was to present to neighbours the proposed development concept, and provide an opportunity to ask clarifying questions and comment on the proposal. Invitations were mailed to the community, a sign advertising the meeting was posted on the site, and two newspaper ads notified the community of the meeting.

The public could participate in this process in four ways:

- by browsing boards and talking to the project team and municipal Planner in an Open House
- listening to a presentation
- participating in a facilitated dialogue, and
- submitting written comments.

61 people signed in and participated in the meeting. Participants asked the development team and District planner a variety of specific questions.

The key themes raised at the meeting were parking, access and density, and support for increasing housing choice in Edgemont Village. The meeting length and format was sufficient to provide all participants an opportunity to learn more, ask questions, and make the comments they wished to provide that evening.

Fourteen comment sheets and three emails were submitted after the meeting. A relatively high proportion of the respondents supported the project: seven of the fourteen comment sheets and one of the emails explicitly expressed support for the project.

Appendix A: Notification

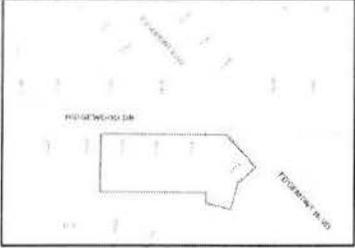
Newspaper Advertisement

PUBLIC INFORMATION MEETING

A redevelopment is being proposed for 1103, 1109, 1123 Ridgewood Drive and 3293 Edgemont Boulevard, to construct a townhouse project. You are invited to a meeting to discuss the project.

Date: Thursday, November 5, 2015
Time: 6:00 – 8:00 p.m.
Location of meeting: Highlands United Church
3255 Edgemont Blvd., North Vancouver

The applicant proposes to rezone the site from single-family zoning to a comprehensive development zone, to permit a 24-unit townhouse project. Homes range between approximately 1,536 and 2,452 square feet in size and the project includes 50 underground parking spaces.



Information packages are being distributed to residents within a 100m* meter radius of the site. If you would like to receive a copy or if you would like more information, please contact Natasha Letchford, Community Planner, District of North Vancouver at 604-990-2387 or Josh Anderson, Director, Development of Boffo Properties (Edgemont) Inc. at 604-648-0594.

***This is not a Public Hearing. DNV Council will receive a report from staff on issues raised at the meeting and will formally consider the proposal at a later date.**

Notification Sign

**Developer's
Public Information Meeting**

**Proposal:
3-Storey Townhouse Project**

**6 PM, Thursday, November 5
Highlands United Church
3255 Edgemont Boulevard**

**Boffo Properties (Edgemont) Inc.
604-648-0594**



**DISTRICT OF
NORTH
VANCOUVER**

This meeting has been required by the District of North Vancouver as part of the regulatory process.

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AGENDA INFORMATION	
<input checked="" type="checkbox"/> Regular Meeting	Date: <u>Jan 18, 2016</u>
<input type="checkbox"/> Committee of the Whole	Date: _____

9.5

 Dept. Manager	 GM/ Director	 CAO
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The District of North Vancouver REPORT TO COUNCIL

January 6, 2016
File: 3060/20/26.15

AUTHOR: Kathleen Larsen, Community Planner

SUBJECT: Bylaws 8147 and 8148: OCP Amendment and Rezoning for a Four Lot Single-Family Residential Development: 3600 Mahon Avenue (Braemar Elementary School)

RECOMMENDATION:

It is recommended THAT:

1. Bylaw 8147, amending the OCP for a portion of the subject site from "Institutional" to "Residential Level 2: Detached Residential (RES2)" to allow for a four lot detached residential development be given First Reading; and
2. Bylaw 8148, which rezones a portion of the subject site from "Public Assembly" (PA) to "Comprehensive Development Zone 93" (CD 93) to allow for a four lot detached residential development be given first reading; and
3. Bylaws 8147 and 8148 be referred to a Public Hearing; and
4. Pursuant to Section 879 of the Local Government Act, additional consultation is not required beyond that already undertaken with respect to Bylaw 8147; and
5. In accordance with Section 882 of the Local Government Act, Council has considered Bylaw 8147 in conjunction with its Financial Plan and applicable Waste Management Plans.

REASON FOR REPORT:

The proposed project requires Council's consideration of:

- Bylaw 8147 to amend the Official Community Plan (OCP) by changing the land use designation from Institutional to Detached Residential (RES2); and
- Bylaw 8148 to rezone a portion of the subject property from Public Assembly to CD93.



**SUBJECT: FOUR LOT SINGLE-FAMILY DEVELOPMENT: 3600 MAHON AVENUE
(BRAEMAR ELEMENTARY SCHOOL)**

January 6, 2016

Page 2

SUMMARY:

The applicant proposes to subdivide an undeveloped portion of land on the east side of the existing Braemar Elementary school site to create four single-family bare-land strata lots. The proposed houses range in size from 308m² (3315.9 sq ft) to 404.9m² (4,359 sq ft) and will be designed in keeping with the surrounding Residential Single-Family Norwood Queens (RSNQ) zoning. The proposal requires an amendment to the Official Community Plan and rezoning of the property. The Bylaws are recommended for introduction and for referral to a Public Hearing.

BACKGROUND:

On June 9, 2014 Council considered:

- Bylaw 8071 to amend the Official Community Plan; and
- Bylaw 8070 to rezone a portion of the subject property

Bylaws 8071 and 8070 would have allowed for a seven lot bare-land strata single-family development on the subject property.

The application was referred to a Public Hearing held on June 24 2014. Subsequently the bylaws were defeated at the Council meeting held on July 7, 2014.

The applicant has now revised the proposal by reducing the footprint of the development site from 5,414 m² (58,273 sq ft) to 3,767m² (40,544 sq ft) and decreasing the number of proposed lots from seven to four. The revised proposal will allow for a forested buffer area to be retained between the development site and the school as well as a 5.48m (18 ft) wide landscaped buffer area between the site and adjacent neighbours. The proposed lots will be accessed from a private, strata roadway, with public road dedication at the entrance of the site to allow a turn-around area for public and service vehicles.

EXISTING POLICY:

Official Community Plan:

The subject property is designated *Institutional* in both the District Official Community Plan (OCP) and the North Lonsdale-Delbrook OCP reference document, reflecting the existing school use on the site. Land designated *Institutional* is intended predominantly for a range of public assembly uses such as churches, schools, recreation centres, care facilities and public buildings.



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The District's OCP proposes to manage growth by creating a network of centres and respecting residential neighbourhood character in the single-family neighbourhoods by limiting growth in these areas. The proposal for single-family housing on this site maintains the single-family character of the area.

The proposed detached residential development addresses policy in Section 2.3 of the OCP (Policy 2.3.1) which supports maintaining ground-oriented detached housing as the predominant housing form in single-family neighbourhoods.

Public Assembly Lands Strategy:

In 2013 the District approved a Public Assembly Lands Strategy Policy to help guide decisions around land-use change proposals for public assembly lands. The proposed OCP amendment and rezoning application respond to the guiding principles of the Public Assembly Lands Strategy as well as public input received through the planning process.

A review of the guiding principles in the Public Assembly Lands Strategy with respect to this development demonstrates the following:

- The primary public assembly use on the site (Braemar School) will remain if the development is approved;
- The subject portion of the site has not been developed for any public assembly purpose nor does the School District have any plans for future institutional use of the subject portion of the site;
- The proposal will result in no loss of playing fields, school use, or community facilities associated with the school;
- Development of the subject portion of the lands for institutional use such as a church, school, recreation centre or care facility could be expected to generate more traffic than the proposed four detached homes;
- The proposal is not located in a town centre and therefore the land is not anticipated to be required for institutional use associated with growth in an OCP-designated growth centre;
- The proposal will allow the School District to utilize an undeveloped asset while increasing family-oriented housing options in a way that is complimentary to the surrounding neighbourhood;
- The proposal will result in no loss of municipal investment on this site or impact joint use agreements;
- Community Amenity Contributions will be used for the replacement of an existing staircase at the north end of Calder Avenue with the balance going to other area improvements, some of which may improve safe or alternate routes to schools (such as path upgrades and signage as appropriate).

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Development Permit Areas:

The site is located in Development Permit Areas for “Streamside Protection”, “Form and Character of Commercial, Multi-Family or Industrial Development”, “Protection of the Natural Environment” and “Water and Energy Conservation and Greenhouse Gas Reduction”.

To address the requirements of the “Protection of the Natural Environment” Development Permit Area, the applicant will work with staff and a qualified professional prior to issuance of the Development Permit to ensure that the project landscaping reflects the natural surroundings, incorporates new planting, and adds new diversity to the site. As part of the revised development proposal a report from a qualified environmental consultant was submitted. The report did not identify any significant ecological species at risk which would require specific responses or approvals.

A watercourse, Thain Creek, runs through the north-west portion of the school site separated from the development area by the existing school building. The new development is outside the streamside protected area and in accordance with OCP provisions is exempt from Streamside Development Permit area requirements.

As the proposal is for single-family housing it is exempt from requirements for a “Form and Character” development permit. In addition, OCP amendment Bylaw 8147 will remove this portion of the site from the “Form and Character” development permit area in the OCP.

As the proposal involves rezoning and subdivision, green building requirements will be secured by covenant and fulfilled as part of the subdivision process.

ANALYSIS:

Site and Surrounding Area:

The 3,767m² (40,544 sq ft) portion of the site proposed for development is located on the east side of the existing Braemar school property. This undeveloped portion of the school site shown in yellow on the adjacent map is not used by the school and slopes upwards from west to east.



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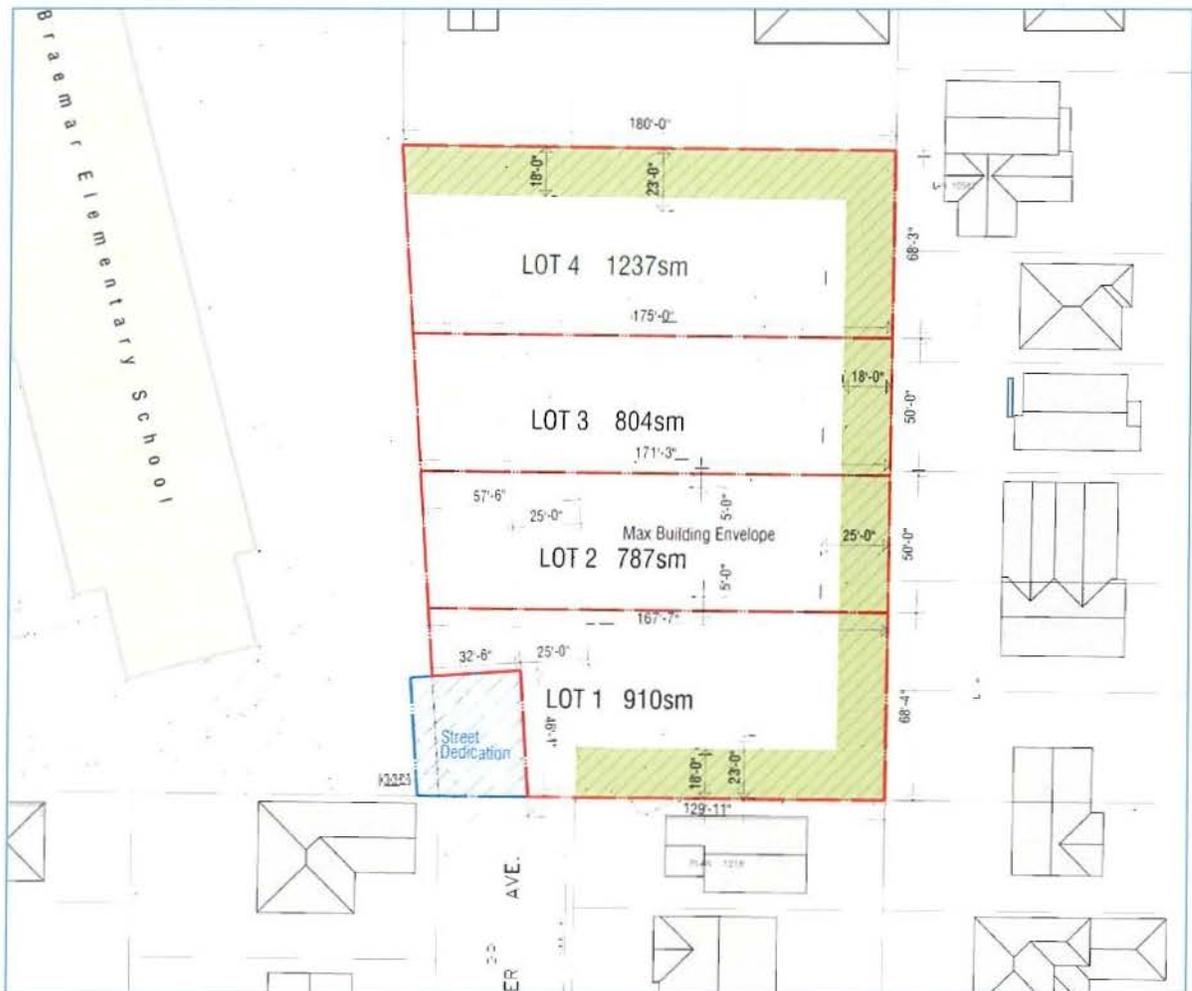
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Adjacent properties consist of single family lots (zoned RSNQ) to the north, east and south with Braemar Elementary School to the west. The proposed development site is accessed from Calder Avenue to the south and at the south end of the site a set of stairs extends down from Calder Avenue to the Braemar school site.

Proposal:

The proposal is for four bare-land strata single-family lots on a private road that extends north from the end of Calder Avenue as illustrated on the site plan below. The applicant has proposed a bare-land strata development approach to allow for the single-family homes to be located in a way to reduce impacts on surrounding neighbours.

As shown on the site plan the applicant is proposing a 5.48m (18 ft) wide buffer area planted with additional trees and hedging in order to provide privacy and screening. A 1.83m (6ft) high fence will be constructed around the perimeter of site and will be secured by covenant.



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Zoning:

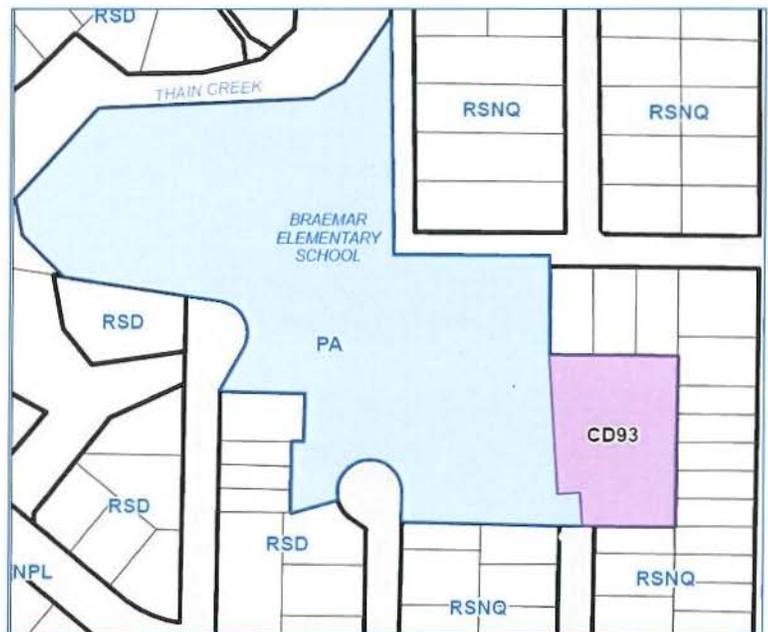
The subject site is currently zoned Public Assembly (PA) and rezoning is required to permit the proposed residential development. Bylaw 8148 proposes the establishment of a new Comprehensive Development Zone 93 (CD93) tailored specifically to this project. The CD93 zone is closely based on the RSNQ zone to reflect the single-family development in the surrounding area.

The proposed CD93 Zone reflects the RSNQ zoning in the surrounding neighbourhood. Height permitted in the new zone ranges from 6.7m (22 ft) to 8.5m (28 ft) depending on roof slope, with house sizes ranging from 308m² (3315 sq ft) to 405m² (4359 sq ft), depending on lot size.

The CD93 Zone provides for specific setbacks on each lot to ensure separation from and privacy for adjacent homes and Braemar School.

A 5.49m (18 ft) wide landscaped buffer area at the north, east, and south sides of the property will be protected by covenant and provide a generous setback and screening for surrounding neighbours.

The following chart compares the subdivision requirements for the four proposed CD93 lots to the RSNQ zone in the surrounding area:



Zone	Lot Area	Lot Width	Lot Depth
RSNQ Minimum	660m ² (7104 sq ft)	18m (59 ft)	34m (112 ft)
CD93 Minimum	787m ² (8471 sq ft)	15.24m (50 ft.)	39m (130 ft)
Range of proposed lots within CD93	787m ² (8472 sq ft) to 1,237m ² (13,315 sq ft)	15.24m (50 ft) to 20.8m (68.25 ft)	39.6m (130 ft) to 54.9m (180 ft)

The proposed minimum lot area and lot depth requirements of the CD93 zone are the same as the RSNQ zone. All of the four lots exceed the 660m² (7,104 sq ft) lot area and 34m (112 ft.) lot depth requirement as outlined in the table above. Average lot widths proposed exceed the minimum requirement in the RSNQ zone.

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The CD93 zone will establish specific building regulations for the proposed new houses. The regulations are based on the RSNQ zoning with the exception of the specific setback requirements which provide for the 5.48m (18 ft) landscaped buffer area at the north, east, and south sides of the property, and the generous 17.5 m (57.5 ft) setback from the school site at the west property line.

The proposed front-yard setback reflects the RSNQ requirement for a minimum of 7.6m (25 ft.) and is measured from the edge of the strata roadway.

Staff are supportive of the proposed zoning as it reflects the RSNQ zoning in the surrounding area and responds to the bare-land strata configuration of the site.

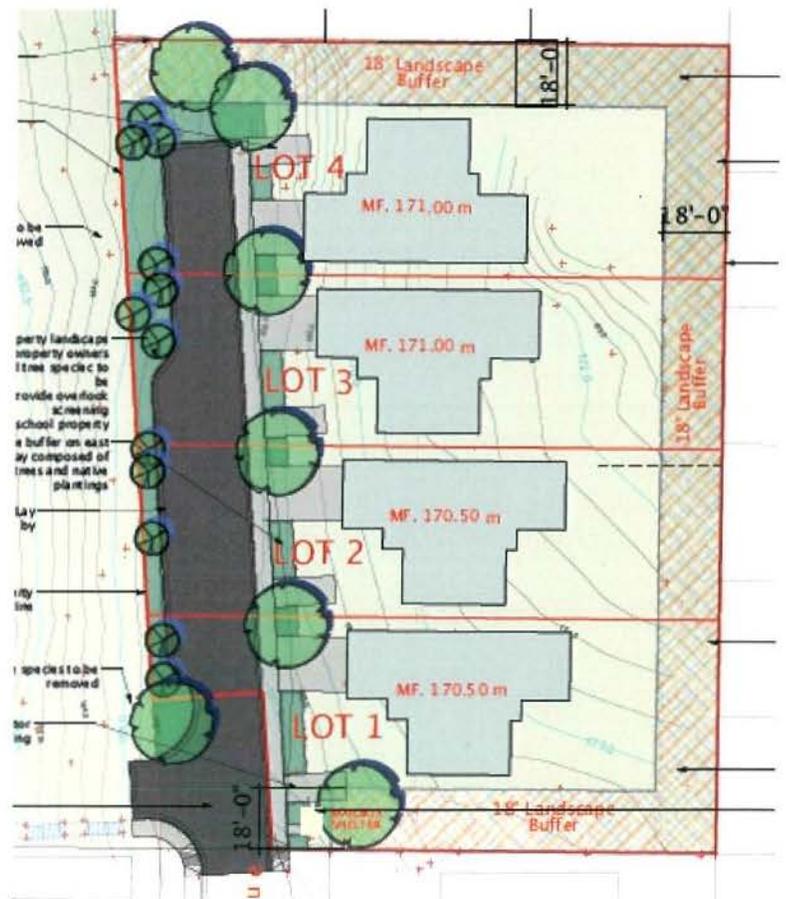
Parking and Access:

Vehicle access to the site is via a private road extending from the north end of Calder Avenue. Each lot will have two on-site parking spaces plus a third on-site space for either a suite or visitor parking. A parking bay along the west side of the private road will provide additional parking for four visitor vehicles.

The applicant is proposing to construct a hammerhead turn-around at the entrance to the site and dedicate this area as public road through the subdivision process. This dedication is intended to provide a turning area for both service and public vehicles at the termination of Calder Avenue. In keeping with existing restrictions no school drop-off or picks-ups will be permitted at the hammer-head entry to the private road.

Landscaping and Trees:

Landscaping has been proposed throughout the site and concentrated at the perimeter in the form of a covenanted 5.48m (18 ft) buffer area to minimize the impact of the proposed development on both the school and adjacent neighbours. Additional landscaping and street trees are shown along the private road. The area between the development and Braemar school will be retained by the school in its natural state with a mix of ground cover and a stand of Alder trees.



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Existing trees on the site consist primarily of Red Alder, Cottonwood and Cherry with many in poor condition and proposed to be replaced. One Western Cedar and one Hemlock in poor condition are also proposed to be removed. A final approved landscape plan and comprehensive replanting plan will be required prior to subdivision approval. These plans will ensure appropriate tree replacement will be completed in conjunction with the required "Protection of the Natural Environment" Development Permit process.

IMPLEMENTATION:

Implementation of this project requires adoption of an OCP amendment bylaw (Bylaw 8147) and rezoning bylaw (Bylaw 8148) and registration of a subdivision plan.

Bylaw 8148 (Attachment B) rezones a portion of the subject property from Public Assembly (PA) to a new Comprehensive Development Zone (CD93) which:

- establishes zoning regulations for the proposed detached residential use;
- allows secondary suites (provided three parking stalls are provided); and
- regulates setbacks specific to the proposal.

Registration of legal documents will be required in order to secure the following:

- registration of the subdivision plan including road dedication;
- green building covenant;
- unique house design covenant;
- preservation covenant to protect the buffer area between the development site and adjacent neighbours;
- a stormwater management plan; and
- a construction traffic management plan.

COMMUNITY AMENITY CONTRIBUTION AND BENEFITS:

The District's Community Amenity Contribution (CAC) Policy requires an amenity contribution for projects proposing an increase in residential density. The CAC for this proposal has been calculated at \$100,000. The CAC payment will be required prior to adoption of the zoning bylaw with the funds to be directed toward local improvements and public art.

In addition to the Community Amenity Contribution outlined above the applicant has also agreed to make additional key improvements that were identified in the 2011 School Transportation Review Study to improve walkability to Braemar school and reinforce the channelling of vehicles to a pick-up and drop-off zone on Evergreen Place.

These improvements include:

- Improvements to an existing staircase that currently serves as an access route from the north end of Calder Avenue to Braemar Elementary School;

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- Installation of curb bulges on the north and southwest legs of the intersection of Mahon Avenue and Evergreen Place;
- Installation of a raised crosswalk on the north leg of the intersection of Mahon Avenue and Evergreen Place. The raised crosswalk is expected to channel pedestrians between the pick-up/drop-off zone and the school and discourage driving into the Mahon cul-de-sac adjacent to the school;
- Signing of pick-up and drop-off zones on Evergreen Place.

Although not identified in the school transportation study the applicant has also agreed to install a sidewalk on the west side of Calder Avenue from Evergreen Place to the new turnaround to be constructed. As there are currently no sidewalks on either side of Calder Avenue this sidewalk will facilitate a safer pedestrian route to Braemar school. Existing signage to restrict school pick-ups and drop-offs will remain at the hammer-head entry to the private road.

Total cost of these works is in the range of \$225,000 to \$275,000, in addition to the CAC. A security deposit will be required prior to bylaw adoption to ensure the completion of the works.

GREEN BUILDING MEASURES:

Compliance with the Green Building Strategy is mandatory given the need for rezoning. The project is targeting an energy performance rating of Energuide 80 and will achieve a building performance equivalent to Built Green "Gold" with the commitment to be secured by covenant.

CONCURRENCE:

District Staff:

The project has been reviewed by staff from Environment, Building, Parks, Engineering, Policy Planning, Transportation Planning, and the Fire Department.

Outside Agencies:

School District 44 has identified these lands as surplus to school needs and supports the proposal. This neighbourhood is within the catchment area of Braemar Elementary School which can accommodate any students arising from the development.

Public Input:

The applicant held a facilitated Public Information Meeting on September 17, 2015. Prior to the meeting, notices were distributed to 90 adjacent residences within 75m of the subject site, A sign was installed on the property and notices placed in the newspaper. The meeting was attended by approximately 40 people, 33 of whom signed into the meeting.

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Topics raised at the meeting included:

- General school-related traffic congestion
- Loss of trees/green space and wildlife habitat
- Potential changes to site hydrology
- Parking/turn-around area and safety on Calder Avenue
- Width of the buffer between the school and the proposed road
- Allocation of proceeds from the sale of the property
- Potential for a protracted building window if the property is re-sold
- Desire for more details related to building design

Positive comments cited:

- The layout of the development
- The proposed buffer zone around the homes
- The decrease in density from previous proposal

Following the meeting a total of 13 comment sheets and 5 e-mails were received. Of these responses 16 were opposed and 2 were in favour.

New concerns focussed on the land sale and loss of school land required for any future Braemar School expansion.

Support for the project included an observation that the existing forested area is currently used as a “dumping ground” and the suggestion that the School District be involved in restoring any remnant parcels.

Petitions and Submissions:

The District received additional public input during the processing of the application in the form of petitions and letters.

Submissions in support are summarized in the table below. Comments noted benefits to the surrounding neighbourhood and the application of the sale proceeds to the funding of a full replacement of Argyle Secondary School.

Submissions in Support	Signatures	
Petition (on-line)	797	80 signatures were from outside the District or City of North Vancouver
Petition (hand-written signatures)	152	136 - District of North Vancouver/City of North Vancouver 7- Outside District/City of North Vancouver
Letters of Support	50	44 - District of North Vancouver 6 - City of North Vancouver

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Submissions in opposition are summarized in the table below and comments were similar to those noted above.

Submissions in Opposition	Signatures	
Petition (on-line)	133	27 signatures were from outside the District or City of North Vancouver
Petition (hand-written signatures)	622	547 - Area generally surrounding the subject site. 72 - District/City of North Vancouver 3 - Outside District/City

CONSTRUCTION MANAGEMENT:

A Construction Traffic Management Plan will be required which ensures all equipment and activities are staged on-site. To mitigate the impact of construction on the existing neighbourhood it has been recommended that the developer build the road extension to an interim condition in order to stage all construction activities and vehicles on-site.

A finalized Construction Traffic Management Plan will be required prior to the start of any construction at the site that must:

1. Provide safe passage for pedestrians, cyclists, and vehicle traffic;
2. Outline roadway efficiencies (i.e. location of traffic management signs and flaggers);
3. Make provisions for trade vehicle parking which is acceptable to the District and minimizes impacts to neighbourhoods;
4. Provide a point of contact for all calls and concerns;
5. Provide a sequence and schedule of construction activities;
6. Identify methods of sharing construction schedule with other developments in the area;
7. Ascertain a location for truck marshalling;
8. Address silt/dust control and cleaning up from adjacent streets;
9. Provide a plan for litter clean-up and street sweeping adjacent to site; and,
10. Include a communication plan to notify surrounding businesses and residents.

CONCLUSION:

The project is consistent with the guiding principles for potential changes to public assembly lands as outlined in Council's Public Assembly Lands Strategy Policy. The proposal has been modified since previously considered by Council with a generous buffer from the adjacent school and a reduction of the number of strata lots from seven to four. The applicant has proposed a site layout and house design format which reflects the surrounding RSNQ and respects existing neighbours.

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Options:

The following options are available for Council's consideration:

- 1) Introduce Bylaws 8147 and 8148 and refer them to a Public Hearing (staff recommendation); or
- 2) Defeat Bylaws 8147 and 8148 at First Reading.

KLarsen

Kathleen Larsen
Community Planner

- A - Bylaw 8147 (OCP Amendment)
- B - Bylaw 8148 (Rezoning)
- C - Facilitator Report: Public Information Meeting
- D - Reduced Site Plan and Landscape Plan

REVIEWED WITH:		
<input type="checkbox"/> Sustainable Community Dev. _____	<input type="checkbox"/> Clerk's Office _____	External Agencies:
<input type="checkbox"/> Development Services _____	<input type="checkbox"/> Communications _____	<input type="checkbox"/> Library Board _____
<input type="checkbox"/> Utilities _____	<input type="checkbox"/> Finance _____	<input type="checkbox"/> NS Health _____
<input type="checkbox"/> Engineering Operations _____	<input type="checkbox"/> Fire Services _____	<input type="checkbox"/> RCMP _____
<input type="checkbox"/> Parks _____	<input type="checkbox"/> ITS _____	<input type="checkbox"/> NVRC _____
<input type="checkbox"/> Environment _____	<input type="checkbox"/> Solicitor _____	<input type="checkbox"/> Museum & Arch. _____
<input type="checkbox"/> Facilities _____	<input type="checkbox"/> GIS _____	<input type="checkbox"/> Other: _____
<input type="checkbox"/> Human Resources _____	<input type="checkbox"/> Real Estate _____	

The Corporation of the District of North Vancouver

Bylaw 8147

A bylaw to amend the District of North Vancouver Official Community Plan Bylaw 7900, to designate the following property: A portion of 3600 Mahon Avenue legally described as Lot I, Block G, District Lots, 578, 617, 784 and 785, Plan 12849, PID: 008-796-033

The Council for The Corporation of the District of North Vancouver enacts as follows:

1. Citation

This bylaw may be cited as “The District of North Vancouver Official Community Plan Bylaw 7900, 2011, Amendment Bylaw 8147, 2015 (Amendment 15)”.

2. Amendments

2.1. The District of North Vancouver Official Community Plan Bylaw 7900, 2011 is amended as follows:

- i) Map 2 Land Use: by changing the land use designation of the portion of the property outlined from “**Institutional**” to “**Residential Level 2: Detached Residential**”; and
- ii) Map 3.1 Form and Character Development Permit Area: by removing from the area shown in red the portion of the property outlined, to remove the subject property from the Form and Character Development Permit area;

all as illustrated on Bylaw 8147 “Schedule A” attached.

READ a first time by a majority of all Council members.

PUBLIC HEARING held

READ a second time by a majority of all Council members.

READ a third time by a majority of all Council members.

ADOPTED by a majority of all Council members.

Mayor

Municipal Clerk

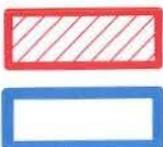
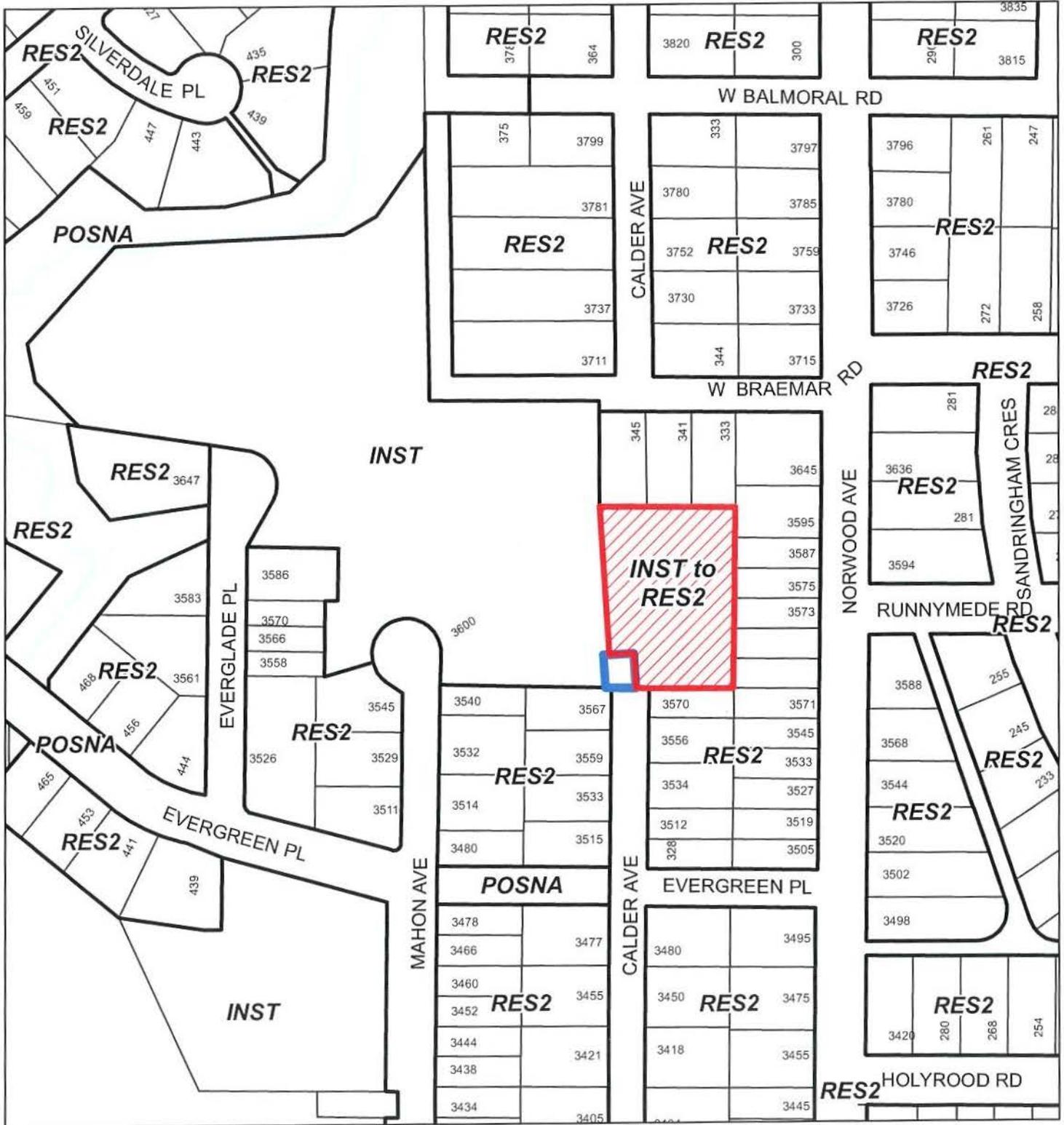
Certified a true copy

Municipal Clerk

Bylaw 8147 Schedule A

BYLAW 8147

The District of North Vancouver Official Community Plan Bylaw 7900 (2011)
Amendment Bylaw 8147 (2015)



**INSTITUTIONAL (INST)
TO RESIDENTIAL LEVEL 2: DETACHED RESIDENTIAL (RES2)**

ROAD DEDICATION



The Corporation of the District of North Vancouver

Bylaw 8148

A bylaw to amend the District of North Vancouver Zoning Bylaw 3210, 1965 to rezone the following property: A portion of 3600 Mahon Avenue legally described as Lot I, Block G, District Lots, 578, 617, 784 and 785, Plan 12849, PID: 008-796-033

The Council for The Corporation of the District of North Vancouver enacts as follows:

1. Citation

This bylaw may be cited as “The District of North Vancouver Rezoning Bylaw 1336, (Bylaw 8148)”.

2. Amendments

2.1 The District of North Vancouver Zoning Bylaw 3210,1965 is amended as follows:

a) Section 301(2) is amended by inserting the following zoning designation:

“Comprehensive Development Zone 93 (CD 93)”

b) Part 4B Comprehensive Development Zone Regulations is amended by inserting the following zone:

“4B93 Comprehensive Development Zone 93 (CD93)”

The CD 93 Zone is applied to:

A portion of 3600 Mahon Avenue legally described as Lot I, Block G, District Lots, 578, 617, 784 and 785, Plan 12849, PID: 008-796-033 as outlined on Schedule A

4B93-1 Intent

The purpose of the CD93 zone is to permit development of a four lot bare-land strata project of detached residential homes.

4B93-2 Permitted Uses

One single-family residential building per lot

4B93-3 Accessory Uses

Accessory uses are permitted as follows:

- a) home occupations;
- b) accommodation of not more than two boarders or lodgers in a single-family residential building;
- c) secondary suites subject to the following regulations:
 - i. secondary suites are permitted only in single-family residential zones;
 - ii. only one secondary suite is permitted on a single-family lot;
 - iii. the owner of a single-family residential building containing a secondary suite shall be a resident of either the secondary suite or the principal residential dwelling unit; and
 - iv. a single-family residential building containing more than one boarder or lodger may not have a secondary suite.
- d) bed and breakfast business submit to the regulations contained in Section 405A; and
- e) buildings and structures accessory to uses permitted in Section 4B93-2.

4B93-4 Size, Shape and Siting of Residential Buildings and Accessory Buildings and Structures in Single Family Residential Zones

- a) Location of Secondary Suites: secondary suites must be located within a single-family residential building; and
- b) Size of Secondary Suite: a secondary suite shall not exceed in total area the lesser of 90m² (968sq ft) or 40% of the residential floor space of the principal single-family residential building

4B93-5 Height

- a) Maximum Building Heights in the CD93 zone shall be as follows:

Roof Pitch	Maximum Height	Maximum Eave Height
Flat Roof	6.71m (22 ft)	6.71m (22 ft)
Minimum 3 in 12 pitch	7.32m (24 ft)	6.71m (22 ft)
Minimum 4 1/2 in 12 pitch or greater	7.92m (26 ft)	6.71m (22 ft)
Minimum 6 in 12 pitch or greater	8.53m (28 ft)	6.71m (22 ft)

- b) Maximum Building Height shall not extend above a line projected at a vertical angle of 45 degrees inward from the point of maximum eave height with the exception of a dormer of no more than 2.44m (8 ft) in width, and gable ends.

4B93-6 Density

a) Maximum permitted floorspace in the CD93 zone for each Lot 1 to 4 shown on Schedule B to this Bylaw shall be as follows:

Lot Area	Maximum Floor Space
Lot 1 (910m²)	351m ² (3778.3 sq ft)
Lot 2 (787m²)	308m ² (3315.0 sq ft)
Lot 3 (804m²)	313.9m ² (3378.0 sq ft)
Lot 4 (1237m²)	405.0m ² (4359.0 sq ft)

b) The following shall be excluded from maximum floor space calculation:

- i. Floor area contained within that part of buildings and structures having an adjacent exposed perimeter wall of less than 1.2m (4.0 ft) from the floor above to the lesser of natural grade and finished grade
- ii. Parking Structures not exceeding 37.2m² (400 sq ft)
- iii. Covered Balconies and Decks
- iv. Accessory buildings not exceeding 25m² (269 sq ft)

4B93-7 Maximum Principal Building Size

Maximum Principal Building Size shall be in accordance with Section 4B93-6 Density.

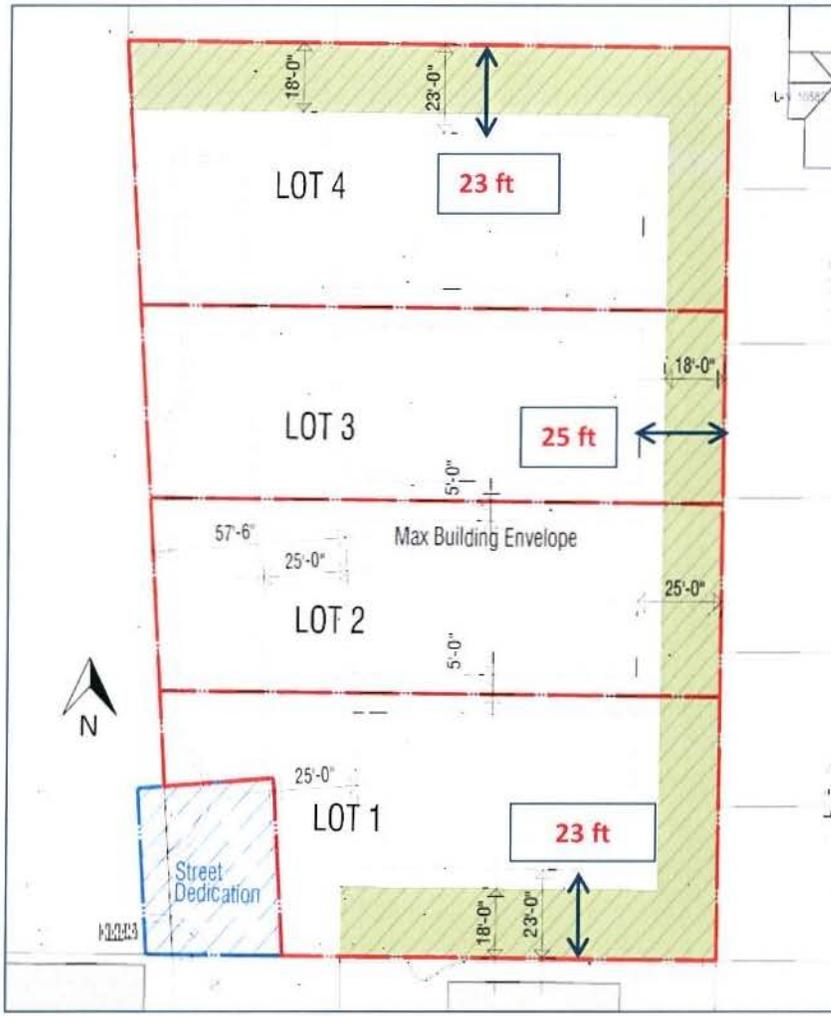
4B93-8 Setbacks

Minimum required setbacks in the CD93 zone shall be as follows:

Setback Location		Minimum Setback: Single-family building	Minimum Setback: Accessory Structure or Swimming Pool
Lot 1	from north lot line	1.5m (5 ft)	1.22 m (4 ft)
	from east lot line	7.6m (25 ft)	5.8m (19 ft)
	from south lot line	7m (23 ft)	5.8m (19 ft)
	from west lot line	17.5m (57.5 ft)	17.5m (57.5 ft)
Lot 2	from north lot line	1.5m (5 ft)	1.22 m (4 ft)
	from east lot line	7.6m (25 ft)	5.8m (19 ft)
	from south lot line	1.5m (5 ft)	1.22 m (4 ft)
	from west lot line	17.5m (57.5 ft)	17.5m (57.5 ft)
Lot 3	from north lot line	1.5m (5 ft)	1.22 m (4 ft)
	from east lot line	7.6m (25 ft)	5.8m (19 ft)
	from south lot line	1.5m (5 ft)	1.22 m (4 ft)
	from west lot line	17.5m (57.5 ft)	17.5m (57.5 ft)

Lot 4	from north lot line	7m (23 ft)	5.8m (19 ft)
	from east lot line	7.6m (25 ft)	5.8m (19 ft)
	from south lot line	1.5m (5 ft)	1.22 m (4 ft)
	from west lot line	17.5m (57.5 ft)	17.5m (57.5 ft)

All as illustrated on the following Site Plan showing setbacks to North, East, South and West for Single-Family Buildings:



4B93-9 Maximum Building Depth

Maximum permitted depth of a single-family residential building in the CD93 zone shall not exceed 19.8m (65 ft).

4B93-10 Maximum Upper Storey Floor Area

Maximum permitted upper storey floor area in the CD93 zone shall not exceed 92.9m² (1000 sq ft).

4B93-11 Coverage

Maximum permitted coverage in the CD93 zone shall be regulated as follows:

- a) Building Coverage for each of Lot 1 to 4 shown on Schedule B to this Bylaw shall not exceed a maximum of 35% (including parking and accessory structures).
- b) Site Coverage for each of Lot 1 to 4 shown on Schedule B to this Bylaw shall not exceed a maximum of 60%.

4B93-12 Accessory Buildings

Maximum permitted size of an accessory building in the CD93 zone shall not exceed 30m² (269 sq ft).

4B93-13 Landscaping

- a) All land areas not occupied by buildings, structures, common roadway area, parking spaces, loading spaces, driveways, manoeuvring aisles and sidewalks shall be landscaped.
- b) All electrical kiosks and garbage and recycling container pads not located within a building shall be screened with landscaping.

4B93-14 Subdivision Requirements

Subdivision layout shall be in accordance with Schedule B to this Bylaw.

4B93-15 Parking and Loading Regulations

Two parking stalls shall be provided per single-family residential building plus 1 parking stall for each secondary suite plus 4 visitor parking stalls serving the area zoned CD93."

3. The Zoning Map is amended in the case of the lands illustrated on the attached Schedule A to this Bylaw by rezoning a portion of the land from Public Assembly (PA) to Comprehensive Development Zone 93 (CD93)

READ a first time

PUBLIC HEARING held

READ a second time

READ a third time

ADOPTED

Mayor

Municipal Clerk

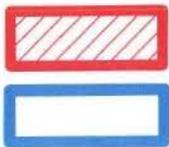
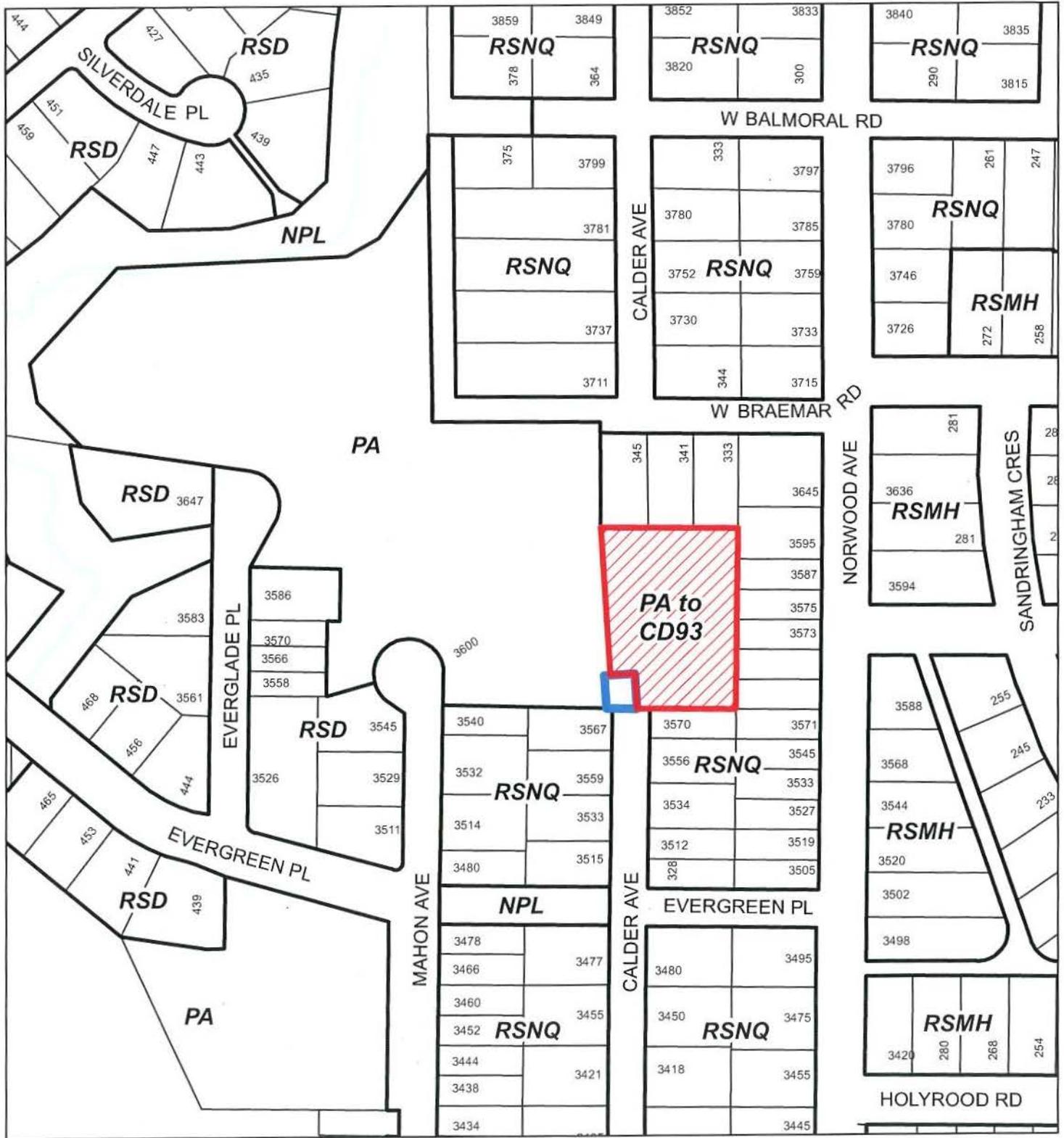
Certified a true copy

Municipal Clerk

Bylaw 8148 Schedule A: Zoning Map

BYLAW 8148

The District of North Vancouver Rezoning Bylaw 1336 (Bylaw 8148)



PUBLIC ASSEMBLY AREA (PA) TO COMPREHENSIVE DEVELOPMENT ZONE 93 (CD93)

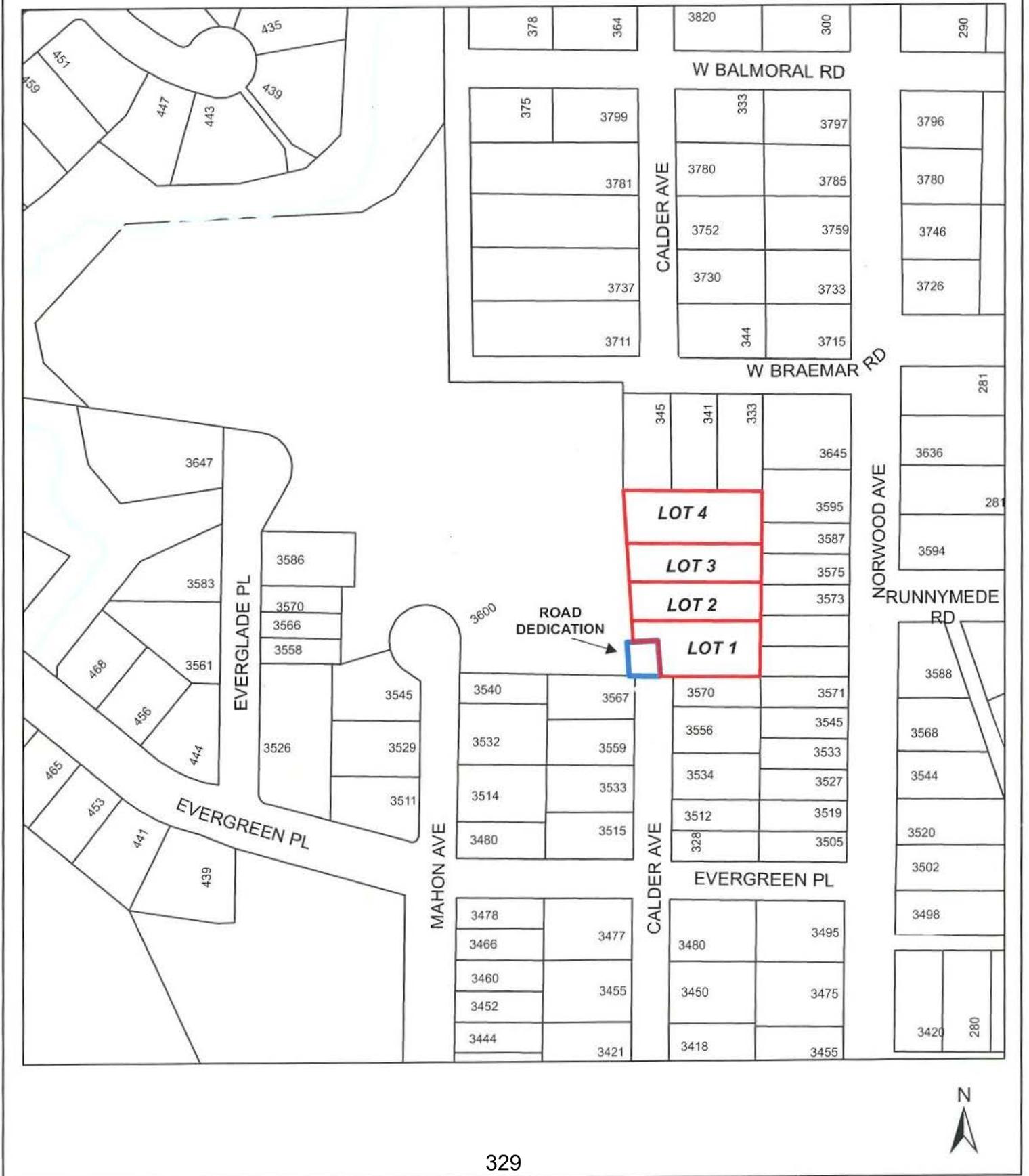
ROAD DEDICATION



Bylaw 8148 Schedule B: Lot Map

BYLAW 8148

The District of North Vancouver Rezoning Bylaw 1336 (Bylaw 8148)



Wedgewood Ventures Ltd. – Braemar Residential Subdivision Detailed Planning Application

September 17, 2015 Public Information Meeting Summary Report

Executive Summary

On September 17, 2015, Wedgewood Ventures Ltd. hosted a Public Information Meeting at Braemar Elementary School, 3600 Mahon Avenue, North Vancouver, regarding their revised Braemar Residential Subdivision detailed planning application. Approximately 40 members of the community were in attendance and thirty three individuals signed in.

The meeting objectives were to:

- Provide an overview of the Wedgewood Ventures Ltd. revised detailed planning application for the Braemar Residential Subdivision, and
- Provide an opportunity for community input and comment on this application

The Public Information Meeting was successful in achieving the above objectives. Many community members took the opportunity to discuss the proposal on a one-on-one basis with project team members during the 30 minute Open House before the meeting. James Fox, Wedgewood Ventures, then provided an overview of the project.

Many community members took the opportunity to pose questions, provide feedback or identify issues of concern during a Q&A session following the PowerPoint presentation. Issues and feedback raised during the Q&A session are included below in the Public Information Meeting Summary Report. Participants were also invited to complete comment sheets and submit them at the end of the meeting or send them in after the meeting. The comment sheets were collected by the District of North Vancouver representative.

Summary of Findings

Following the presentations, participants were invited to ask questions or offer comments on the project.

Many questions of clarification were posed and answered. Most community members who offered verbal feedback raised issues of concern related to: general school-related

traffic congestion; the loss of trees, green space and wildlife habitat, and the potential for changes to the site hydrology; and parking, turn-arounds and traffic safety on Calder Road. Additional issues of concern included the width of the buffer between the school and the road, allocation of proceeds from the sale of the property, the potential for a protracted building window if the property were to be re-sold, and a desire for more details related to the building design.

A summary of findings follows including positive feedback, issues of concern, and comments or requests. The next section documents each question, answer and comment.

Positive Feedback

- I like the development
- I appreciate the buffer zone and the decreased density

Issues of Concern

- Traffic congestion during pick-up and drop-off
 - Concern about potential for additional new traffic on Calder to exacerbate congestion during drop-off and pick-up periods
 - Concern about reference to parking on Evergreen for school drop-off and drop-off (Recommendation from 2011 Traffic Study)
 - Concern about the ongoing lack of regard for parking restrictions in and around the school
- Removal of large conifers and other trees on the site
 - Concern about the removal of two large hemlock trees from the site.
 - Concern about loss of green space and wildlife habitat
 - Concern about potential changes to the site hydrology
- Extension of Calder Road
 - Desire that the new portion of road be kept open to public use as with the rest of Calder
 - Concern about the absence of a proper cul-de-sac or a proper turn-around area at the current end of Calder
- Size of Buffer
 - Concern about buffer width between the school and the new extension of Calder Road
- Allocation of proceeds from sale of property
 - Concern that the Argyle community will benefit from the sale of this property but the local community will not benefit
- Resale of property

- Concern about the potential for resale of these properties and the potential for a protracted window of building over a period of years if the properties are developed one at a time.
- Project Details
 - Concern about lack of mock-up details regarding the built portion of the proposal.

Comments/ Requests

- Comment: Parking on Calder - Concern about potential for a sign indicating that parking on the new extension to Calder is for the exclusive use of those residents
- Comment: Desire to clarify that Wedgewood Ventures does not yet own the property and will only be able to purchase the property if the application is approved by the community and the District.
- Comment: Concern that the School District was not represented at the meeting and a reminder about the ongoing lack of support for the sale of this School District property by some members of the community.
- Comment: Reminder that the District Development Cost Charge bylaw does not provide for parks and that parks are needed in this community.
- Comment: Reminder that the large hemlock trees, found to be of poor health by the arborist and environmental assessment, withstood the big wind storm of a few weeks ago.
- Comment: Concern that the recommendation to remove the large hemlock trees is a result of the siting of the proposed development and not because they are inherently unhealthy.
- Comment: Acknowledgment that the School District would continue to be responsible for the health of the large trees if the property is not sold
- Comment: Concern that ongoing traffic problems on Mahon will be mirrored on Calder Road.
- Comment: Concern about the ongoing expectation that neighbours will continue to attend meetings and comment on this project. "It feels abusive." Desire to protect public assembly land.
- Comment and request: Concern about loss of forested area as green space for children to play in. Request that the application be reconsidered to retain a larger portion of the forest and only build two new houses.
- Request: Request for data that estimates total traffic levels after the development is completed and a comparison to current traffic levels.

Public Information Meeting Summary Report

Welcome and Project Presentation

James Fox, Wedgewood Ventures Ltd. introduced the project team including Ray Letkeman (Architect) and Michael Coulthard (Diamond Head Consulting Ltd.) and provided an overview of the revised detailed planning application.

Question and Answer Session

Following the presentation, participants were invited to ask questions or offer comments on the project. The following questions, comments and issues were raised:

- Q1** What does the 18 foot buffer mean?
- A1** The buffer is intended to be a planted area between the project and neighbours. Neighbours would be welcome to make requests or suggestions for species to be planted in that area.
- Q2** Are you purchasing the land for the same price as before?
- A2** Yes, the price is the same. As we know, the real estate market is hot right now. This means that we can pursue this current project with a reduced density and fewer number of homes than the previous application, on a smaller portion of the property. The School District still gets the money that they need for Argyle or wherever they are going to use it and we can make a go of this project.
- Q3** Can you please outline the dimensions of what you are buying and what will be included in this strata property?
- A3** The property is approximately 238 feet by 200 feet.
- Q4** So you are not buying as much property now as you were proposing before?
- A4** No, it is about two thirds of the previous size.
- Q5** So the School District retains the ownership of the remainder?
- A5** Yes, the School District maintains ownership of the remainder of the property. This includes the forested bank which is a good buffer.

- Q6** As a developer, are you in a position to sell those properties as they are right now? I know you aren't planning to sell the product. But right now, could you transfer the ownership of that property to another developer?
- A6** If I owned the properties, then yes, I could resell the properties. But I do not own them right now, and will not until it is rezoned and subdivided from the school. In addition, the zoning of the property will include details regarding the buffer, location of buildings, and landscaping, etc. However it is not our intention to sell. Sometimes, as a developer, you have to sell because of changes in the market. And you may even have to sell a property at a loss. But I need to have that option.
- Q7** You are talking about the buffer zone between the school and the road. How big is that buffer? If the school to the road is 50 feet (trees minus sidewalk), are you leaving 2 or 3 trees?
- A7** The school is 120 feet away from the nearest house, including the road. The landscape plan shows we have dedicated the distance between the edge of the road and the property line to be landscaped and buffered because we don't want to look at the school either. So we have 47 feet to the road and 120 feet to the nearest house.
- C8** There's only 10 feet of trees.
- A8** No, the bank is full of trees. The road is on the property of these homes.
- Q9** Why are you proposing that school drop off takes place on Evergreen? This is a really small road. Why are you proposing that drop off area?
- A9** The recommendation is referring to Evergreen at Mahon. That is where the 2011 Traffic Study recommendations call for road bulges and a raised cross walk. The engineers designed it in conjunction with the District. We are offering to pay for it.
- Q10** Will the road that goes into the property be for the exclusive use of the property owners?
- A10** It will read like an extension of Calder Road. The only reason it is bare land strata is that we didn't want to have to build a municipal 66 foot wide road. Anyone can drive up or down that road. There won't be a gate. It can be written into the zoning that gates are not allowed.

- C11** Will there be a sign that says "this is private property and don't park here"?
We can't put up a sign on our part of the street so they shouldn't be able to either.
- C12** You don't own the land yet. You have an option to purchase. The School District still owns the property. And the only way that you'll be able to purchase the property is if we're all happy and the District is happy.
- A12** Correct. I do not own it yet.
- Q13** Is Calder going to be extended right through the property?
- A13** No, the road will be extended to access the four homes but will still be a dead end.
- Q14** At the moment we have a quasi cul-de-sac. It is very difficult to come down and turn around. So people are backing into driveways or potentially backing into unsafe areas where kids may be. There is no cul-de-sac and the road is going to become longer and it will take longer for drivers to realize that there is nowhere to go. The road looks really narrow. What happens when people drive up there looking for a place to drop their kids off?
- C15** There will be a sign at Evergreen saying that you can't park there. And everyone already parks around there now.
- A15** There is already sign at Evergreen saying you can't drive up Mahon to drop off your children. This is an enforcement issue. In the previous proposal there was a cul-de-sac proposed and we had a lot of discussion about how people would turn around. Rightly or wrongly, District staff decided that the cul-de-sac was not wanted. Maybe this needs to be revisited or a hammerhead turnaround needs to be added to facilitate turning around.
- C16** I was at the last meeting when they were discussing the cul-de-sac. I thought it was a little strange because there were only 2 people that loudly said they didn't want it. It was disappointing when the District said that people didn't want the cul-de-sac. That cul-de-sac was important.
- A16** They need to be able to turn around there. We weren't trying to get out of building it. If there is a desire for a smaller cul-de-sac or a hammerhead then we should explore that. If you want this, let us know. (The facilitator

reminded participants to make their preferences known on the comment sheets.)

- Q17** You need to focus on this subject with the District. On Mahon there is a sign that indicates the times when you are not allowed to drive up to the school. But it is a continual runway all through the school day. Cars park up there and kids are picked up and dropped off regularly every hour. It won't work to say that cars can't go up Calder. As you know, parents are an unusual group of people. They'll do whatever they feel like. They'll drive right up.
- A17** Another comment I heard earlier this evening is that there are no sidewalks along Calder. The traffic study suggests that students should get dropped off and walk to the stairs. However, because there is no sidewalk, they have to walk up the middle of the road. And when it is garbage day, apparently there are trucks there too.
- Q18** When are we going to have some results of this feedback? We don't hear the feedback.
- A18** (Kathleen Larsen, Community Planner, DNV): All of your feedback is taken into consideration. After this public information meeting and when we have received all your comment sheets, we will go through them and highlight themes. We will also have the facilitator's summary report from this meeting documenting the questions and concerns raised here. Comments will also be received from the DNV Environment, Engineering, Fire and Traffic departments. Issues or comments that can be addressed will be communicated to Wedgewood Ventures Ltd. and may result in possible revisions to the plan. When that is done, I will prepare a report to Council introducing the project. In that report, I will summarize the concerns and document any changes that have been made to respond to different concerns. If Council feels there is merit, there may be a Public Hearing and you would all be notified and invited to attend, and there would be a sign on the property. The Public Hearing would give you a chance to hear about the report and speak to Council about the proposal.
- C19** It is a shame that the School District is not represented. There are a lot of people like myself that feel that this property should never have been deemed surplus. We need it for another purpose. This project does not solve the problem on Calder. It gives these properties an exclusive benefit that the rest of the street does not share. We will continue to put up with the traffic that the District has done nothing about in the last year

since there was a public meeting when it was brought to their attention. These properties get all the benefits and none of the negatives of living on Calder in the vicinity of the school. And this school is not a good neighbour to the community that lives around it.

Q20 Does that road include space for parking?

A20 (Ray Letkeman – Architect) Yes. This is a 6 meter wide road that provides access to each home. Each home has a 2 car garage under the home and an additional parking bay beside it. Each home has 3 parking spots. The road is too narrow to park on.

Q21 If there was a sidewalk would it be on the side closer to the school?

A22 I imagine it would be on that side.

C23 Traffic has been a problem at this school for 30 years. The only problem I have with this is that you have removed what is not a substantial enough cul-de-sac. Four houses aren't the issue. If you have a Silverdale sized proper sized cul-de-sac with an extension of a road off of it, that extension isn't the problem. The cars will have enough places to park. The problem is that you can't tell people not to park there. I was on the PAC for 6 years and the traffic problem at this school will never be resolved because all access is via cul-de-sacs. There is nothing you can do except having someone stand at every cul-de-sac with a sign saying you can't park there. You need to have a better sized cul-de-sac at the end of Calder. I like the development, I just don't like the fact that there isn't a cul-de-sac at the existing end of Calder.

Q24 You're saying these properties/ this property is strata title. I thought it would be fee simple.

A24 It's only called bare land strata because of the road. The only obligation for these houses is that they'll need to maintain their own road.

Q25 They'll also maintain their own landscaping?

A25 Yes, they'll also have a landscape plan and they'll have to maintain the buffer. These things will be written into the zoning. In terms of zoning, form, shape and height, it will be the same as elsewhere in this neighbourhood (RSNQ). We're proposing to use a CD zone to address some of these issues. A CD zone is something that is used to give specific direction re: what can be built. The specifics would be similar to

existing zoning with the addition of how to deal with the neighbours on issues like plantings. A CD zone (comprehensive development zone) is very specific in prescribing what you can do.

- Q26** Where is Argyle Secondary School? You mentioned money going there.
- A26** It is in Lynn Valley. In the big picture, the reason why the School District is considering selling surplus properties is because they have projects to be financed. At the moment, Argyle is the top priority. So money from this project will go to pay down debts and help them to finance building a new high school.
- Q27** And so we don't get anything out of it?
- A27** You get these other things that we are doing.
- C28** I live in Delbrook and our kids have gone to this school so we are as much a part of this discussion as those that live next to the school.
- A28** Many students from Braemar go to Argyle or Handsworth. Some of this school community are already driving here and they may possibly go to Argyle.
- C29** Most of them will go to Handsworth.
- Q30** Speaking of amenities, what were you thinking of? Not a statue I hope.
- A30** Amenities are something that the community should talk with the District about.
- C31** I am sure that you are aware that the District Development Cost Charge bylaw doesn't have anything in it for parks. They trashed that a couple of years ago. We need money for parks in this area because we don't have any.
- Q32** I am curious about the secondary suites. Is this area zoned for secondary suites?
- A32** (Kathleen Larsen, Community Planner, DNV) – Secondary suites are allowed throughout the District in every zone as long as they don't exceed the maximum size (square footage) and three parking spots are provided on-site (2 for the home plus 1 for the suite).

- Q33** You mentioned that the extension road will be 6 metres wide. How wide is the existing Calder Road? How much narrower will the extension be? Will it be possible for people to park on the side of the road?
- A33** (Ray – Architect) You can't park on a 6 metre road because of restrictions for firetrucks. Visitors will park on the driveway or the visitor parking stall but not on the street.
- A33** (James) It looks like Calder is about 5.5 metres wide (the paved part). Looking at the survey, it appears to be about the same size as the rest of Calder, but this needs to be confirmed.
- Q34** Are you going to totally clear cut the property or are you going to leave any of the big evergreens?
- A34** The big evergreens have been assessed to be not safe to retain adjacent to new homes, so they are going.
- Q35** Who determined that they were unhealthy?
- A35** An arborist and an environmental engineer.
- Q36** What did they determine was the invasive species?
- A36** It's not because of the invasive species.
- A37** (Mike Coulthard, Diamond Head Consulting Ltd.) – Most of the trees on the site are young deciduous trees. There are a two fairly significant sized hemlock trees which are the ones you may be talking about. They are proposed to be removed. When you assess trees for hazards which I do quite often and am certified to do, there are certain defects you look for. When you build a house or a structure near a tree, the tree needs to be healthy and sound so that, in the case of a big windstorm like a couple of weeks ago, there's no risk of them falling and hitting the house. One of the big hemlocks on this property has a big sweep at the base and poor rooting, unfortunately. The two large trees are both hemlocks which are not the best species as they are quite prone to getting decay. The second hemlock is growing on the property line and likely has some decay in the middle. However, it is also covered with ivy so it is hard to assess the real condition of the trunk.
- Q38** So on that whole site there are only two big evergreens? And everything else will be clearcut?

- A38** (Mike) There are only two big significant trees on site that will both have to be cut for the proposed project. All areas within the lots will be cleared, but in the landscape plan there will be mostly all native species planted. The buffer zone around the lots provides a good opportunity to replant native trees.
- Q39** So if this proposal doesn't go through and those trees are deemed a hazard, then I presume that the School District will come in and take them down, regardless.
- A39** (Mike) Yes, if the School District had them assessed and they are within striking distance of a target then yes, they have to take them down. I should add that there is a difference if a tree is in a yard or near a house. A house has more threat because people live there. If it is within striking distance of a house, then it would have to be removed.
- Q40** If the tree is healthy then it would not come down.
- A40** (Mike) Yes, park trees are kept unless they are deemed unsafe.
- C41** How would we know if this tree is healthy then?
- A41** (Mike) You would have someone like me assess it. But the risk level and acceptable threshold changes if you put a house closer to that tree. There is a higher risk of hitting that house.
- Q42** Aren't those trees the School Board's responsibility?
- A42** (Mike) Yes, they have been.
- Q43** What was the start date and end date of your assessment of this property?
- A43** (Mike) We did it two or three months ago (June or July). It was just two days on site; it was a small area. The report is available on Wedgewood Ventures' website. In a survey we take into consideration the vegetation and habitat features and wildlife we see on site. It is a habitat based assessment.
- Q40** Just to clarify, you made your assessment in the forest in two days.
- A40** (Mike) Yes, and there are limitations to doing assessments like this. You can't identify every species that lives in that area unless you sit there for four seasons of the year. It is very difficult to do a complete inventory

assessment of any natural area for that reason. You need to do it year after year.

Q41 Did you say that one of the bigger trees has ivy around it?

A41 (Mike) Yes, the one that is difficult to assess has really dense ivy wrapped around it.

C42 About 2 weeks ago on the North Shore we had a huge windstorm, probably one of the biggest windstorm I've seen in this neighbourhood in 20 years. Those trees are still there.

A42 (Mike) Yes, they are still there, that is true. Assessing trees is difficult. A lot of poor trees stay standing and some healthy ones come down. You can never call a tree totally safe either. Some totally structurally sound trees will come down in those storms and others that are really poor will stay standing. The assessment system is based on risk and requires that the tree owner identify their risk threshold.

Q43 If it is your business to assess trees, aren't you better to be safe in your assessment to cover yourself?

A43 (Mike) It's a risk system with a rating system of 1-12. The risk level that makes a tree unsafe is actually up to the owner of the tree. The system classifies trees in risk categories of low-medium-high according to an industry standard. Some people have a lower risk threshold, others have a higher risk threshold. Some people who own parks (like cities) keep lower rated trees because their priority is to retain trees on that site. Some people don't want risk on their property. So even a tree assessed to have a medium risk may be taken down because the owner doesn't want to take on that risk. On a site like this, in order to safely keep a big tree like that, you need to protect a big zone undisturbed around it. Those roots go out 8-10 metres. So when you are doing a proposal like this, excavation and foundations can't go into those zones. Therefore in developments like this, the only trees you can keep safely are usually on the outer edges.

Q44 If you clear cut that area, how is that going to affect the drainage in that area? Trees are so important for drainage. What will you do with the water running down that site?

- A44** (Mike) You address that all through the engineering and storm water systems.
- A44** (James) On our last couple of projects, we've installed rain gardens and infiltration systems. The objective here is to not throw water into pipes. The water that would be sucked up by trees we can infiltrate into the land in other ways. We would definitely do the same here.
- Q45** Going back to the big hemlocks that were there. If there was no development, there would not be a reason to remove those hemlocks. It is because there is going to be a development and you're going to be disturbing the soil around them, that the hemlocks are needing to be removed. Correct?
- A45** (Mike) Yes. Also, you're bringing a target and human life in and around those trees. You're increasing the presence of humans and structures. Most roots are in the top 1 metre of soil. As soon as you are digging into the top surface for all those buildings and roads, it's hard to keep trees safely.
- C46** So if someone proposing a development and hires your company because they are putting in a development, and they want to know how those trees are being affected, it ends up that the trees may need to be removed because that is where they want to build. If they weren't going to be building there, then they wouldn't need to be removed. This is an assessment from someone who is assessing the trees for a development coming into this site. If we were to hire someone to assess this area, they could do that and be unbiased.
- A46** (Mike) I have been doing this for 20 years. I am a member of two professional associations. I am unbiased. You have a natural green area. It is proposed to be cleared. We take an inventory of what is there. We want to document what you're losing and suggest what you can do to mitigate or compensate for it. No one is trying to say you aren't going to lose something. We try to document what is the value that is there and how can you compensate for it? If a tree is healthy and structurally stable, we make recommendations for design changes to accommodate that tree. For example if a garage comes into a root zone, we recommend to move the garage. But if the tree is in the middle of a lot and you're building a house there, the tree will have to come out.

A46 (James) We are talking about if trees are of good quality and value and not risky. We do design projects around trees. The purpose of our assessment was to look at how healthy these trees were. We already had an arborist assessment in addition to this broader environmental assessment. This was not a biased assessment. It was completely independent. In this case, we haven't been told that there is a really valuable tree here. We didn't hear that.

C47 I guess it depends on what you call valuable.

A47 Correct. And we have made changes in the past to accommodate that. We often replant way more than we take out. For example, I don't know if you've seen what we put in at Argyle. It is a jungle there. There are way more plants at Argyle than there were before. And it turned out really well for the water infiltration and the stream, etc. If the value isn't there to redesign than we ask how we can make it better when we rebuild. That is our approach here. Perhaps we need more detail on how we are going to deal with the buffer. Our intent is not to mow down excellent trees.

C48 I've lived in the neighbourhood for over 50 years. It's amazing how much of a problem we have with underground streams coming down. I live on Everglade (parallel to Mahon) and the neighbours have had troubles in their basements. They've all had to put sump pumps in. So I don't know what you'll be getting into above the school. This year the school storm sewer had trees growing into the sewer and we had 6 inches of water running across Evergreen. They had to sand bag it this winter when there were heavy rains. You'll have to be careful with the water coming from your end of Calder into the school area. The drainage will have to be looked at really carefully. Even when they put a big house next door to me on Everglade, the people on the Evergreen side had trouble with water running into their basement. They had to put a new drainage system in. So it affects surrounding areas more than you realize. That school (Braemar) was built over 50 years ago. So this will be a matter to be considered if you're going to put this development in.

Q49 Can you put the slide with the traffic numbers up please? The numbers aren't good without a reference. What is this traffic adding onto? I am concerned about how much the traffic will increase on that piece of Calder?

A49 I am not sure that the 2011 study has those numbers; whether they looked at that. We did a study on what our incremental increase would be. We

did not compare it to the existing. I can't tell you what the current numbers are on Calder, I can only tell you what the additional number of cars would be. At worst, if everyone was using a car, there would be an increase of 7 cars in a peak hour. This would be the worst case with everyone having a suite and two cars.

C50 We are concerned about a 50% increase on that street.

Q51 I would like to know who is Wedgewood Ventures and who is involved. There are seven School Board Trustees each of whom are paid a part time stipend. Are there any trustees who perhaps in their professional life might work in a development industry or related industry who might be working with or have worked for Wedgewood Ventures?

A51 No. And by the way, I am Wedgewood along with my partner who is sitting back there.

Q52 If you acquire this property, you said that you have the right to resell it. Would you sell it as a complete four houses?

A52 We would like to build them.

Q53 But if you decide that you can't or don't want to build it. Could you sell individual lots?

A53 Yes, I could sell individual lots (like a subdivision).

C54 So four different individuals could build those houses at their leisure. It is the timeline that I am interested in. Therefore, one could build one next year, and then the next one 3 years down the road, and so on. So we could have continuous building for years until they are all built and that would be completely unsatisfactory.

C55 When you proposed 7 units and applied for rezoning, it failed. To go back and get it approved, you would need to have changed something. I don't see anything changed here except that there are fewer cars (from 7 to 4 cars).

A55 There are a lot of other changes: positioning, location, mass, the School Board retaining a big piece of the land adjacent to the school, and the additional contributions that we are proposing.

C56 The reason it failed was because of traffic issues.

- A56** That is why we are trying to address traffic issues. Will it solve Calder's problems? No. But will it improve the neighbourhood in a broader sense? Yes.
- C57** They have been trying to fix Calder (Mahon?) for 12 years. And you're going to try to do the same thing on Calder for another 12 years.
- A57** There was a big traffic study done in 2011 partly in response to the repurposing of Balmoral School. Since this project started, I have heard people talking about issues on Calder. We had a cul-de-sac in the previous proposal. Then it was taken out. I am hearing that we need to address that again. Maybe it was a vocal minority that wanted it taken out. We are trying to improve things. We are willing to put something in to improve the situation.
- C58** The cul-de-sac doesn't solve the problem. The problem is parking and people waiting for those students. If we are going to have to lose our property, the only benefit would be if this parking problem could be eliminated. I don't see anything here that gives us any benefit. We are losing the forest and we are not having the parking problem eliminated. We don't have any benefit.
- C59** I appreciate the buffer zone and the decreased density, etc. But the thing I am opposed to is the loss of forest space and not just for me. I love being near the forest. I will really miss it. I am more concerned in the long term that a school property is making this decision. We have increased density of housing all over North Vancouver and less and less wild spaces. It is critical that kids have access to green spaces that are not purified. The way it stands now, this forest is a mess and I see a lot of invasive species. I need tidying up and I don't see the School District spending money on this. But I am going to suggest something a little narrower than that and still having the development. For example, cutting it down by half, like only two houses but with more green space. The green space would be naturalized but still usable.
- Q60** Over the years, we have had good mock-up details of the houses with square footage and layout and so on. Why is that absent tonight?
- A60** This application is a little different because it is a subdivision. We wanted to focus on the zoning control and the look. Those drawings are still the flavor of what we want to build. We are focusing on the zoning so everyone knows exactly what it will involve (shape, height, pitches, buffer).

It is little different than a town house project where everything is laid out and detailed. In the last project there were seven smaller houses which was pretty unique. We had to show details to show how it was going to look and how it would work. This time we are focusing on how the land will be used and, more importantly, how to use other parcels of land to maintain buffer zones, etc. That 18 feet is a pretty wide swath of land.

Q61 What is the total house size?

A61 These houses, per the zoning, could be 3800 to 4000 square feet. In the previous proposal they were smaller, but now they can be built bigger.

Q62 I am having a real difficulty with this meeting because one of the major players is not here (the School Board). Why are they not here?

A62 I wanted to talk about the proposal. I know that a lot of you have ideological issues about the sale of land. But I needed you to hear about the proposal. If they were here, we wouldn't even get through this presentation.

C63 We went last year. This has been going on for years now. It feels abusive. We all collected up and went down there and we have spoken. We want to protect public assembly land. It is not about Mr. Fox's development. Once it is gone it is gone. That way, many people will benefit from that property. This way, only one person will benefit. We have said this at District Hall. At the end of the day, it will be the District that will decide on the zoning but I don't know how many times we will have to say this again. How many people need to hear this? That is why the majority of people are here and it is because they feel very strongly about their community. Nothing can change that.

C64 There is nothing you can build that will compensate for the loss of the beauty of forest and the trees. Nothing.

Q65 Who makes money out of this? Repurposing of the land and rezoning it makes the land much more valuable. Who gets that? The District, the School Board or you?

A66 I make money because I am the developer. The School Board liquidates cash to redeploy in a project that they have earmarked. And the District and community should get some money out of it too. And that's what we're trying to work out here, tangible things like addressing other traffic calming things, a turn-around, etc. The School Board is committed to

doing something because they need to. I am here to come up with a proposal that works. It doesn't work for everybody obviously. Over the years, we have come back and this is a significantly different proposal than last time. It is an opportunity. We can fine tune things and address some of these issues you have raised. There is a philosophical question about selling School Board land, but that is not my conversation. We have done lots of infill projects. You can look at what we did at Argyle. I am here to propose a use so that the School District can get their money out of this property to put to use.

Next Steps and Closing Comments

Kathleen Larson, Community Planner with the DNV, outlined the next steps in this process. The District is open to hearing all of your responses. The next step for this project is for the facilitators meeting summary report and comment sheets to be received at the District Office. Comments will also be received from the District Environmental, Engineering, Traffic and Fire departments. The proponent may then be asked to make revisions. A report will then be prepared for Council. This could be as early as November. This would be followed by a Public Hearing at which time residents will be invited to come and speak before Council about the application.

Participants were encouraged to submit comments via the written comment forms, email or fax to Kathleen Larsen at the District of North Vancouver (klarsen@dnv.org) by October 2, 2015.

Braemar Elementary School

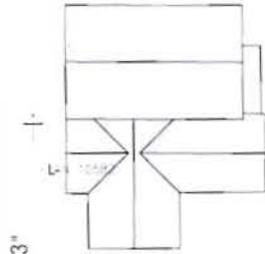
E



180'-0"

18'-0"

23'-0"



LOT 4 1237sm

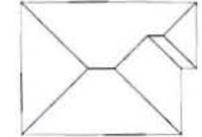
175'-0"

68'-3"

LOT 3 804sm

171'-3"

50'-0"



LOT 2 787sm

167'-7"

50'-0"

Max Building Envelope

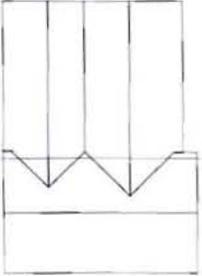
57'-6"

25'-0"

5'-0"

5'-0"

25'-0"



32'-6"

25'-0"

LOT 1 910sm

18'-0"

23'-0"

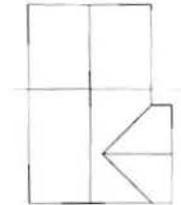
68'-4"

Street Dedication

46'-1"

129'-11"

PLAN 1218

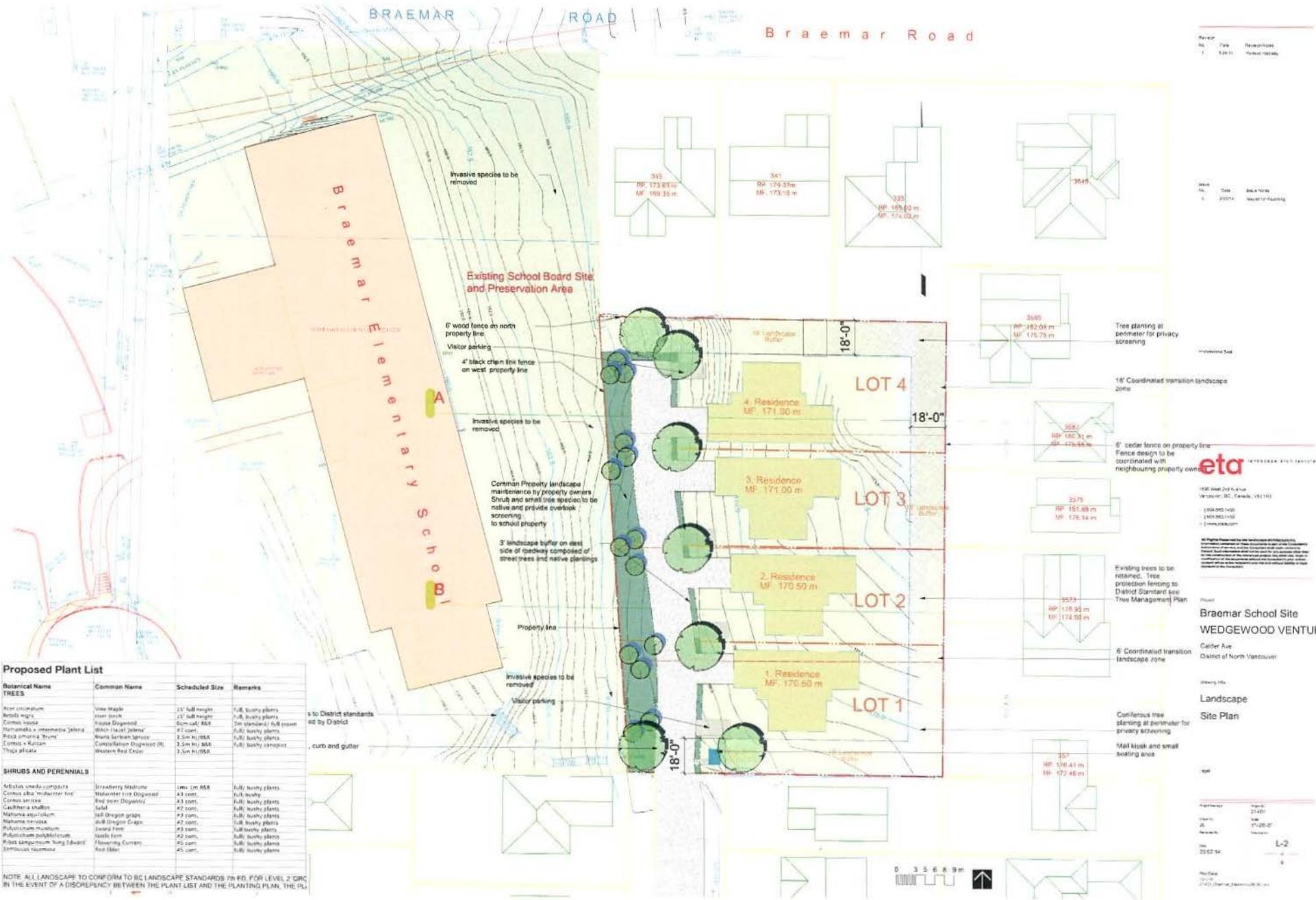


12.1.13

CALDER AVE.

348

ATTACHMENT D



Proposed Plant List

Botanical Name	Common Name	Scheduled Size	Remarks
TREES			
<i>Acer circinatum</i>	Wet Maple	15' full height	Full, bushy plants
<i>Azalea indica</i>	Wax Tree	15' full height	Full, bushy plants
<i>Cornus lucida</i>	Flower Dogwood	60cm cal. 85H	5m standard full round
<i>Hamamelis x intermedia</i> (Hem)	Witch Hazel (Hem)	42 cont.	Full, bushy plants
<i>Prunella americana</i> (Hem)	Black Cherry (Hem)	3.5m H/100A	Full, bushy plants
<i>Quercus + Rubus</i>	Constellation Dogwood (H)	3.5m H/100A	Full, bushy plants
<i>Thuja plicata</i>	Western Red Cedar	3.5m H/100A	Full, bushy plants
SHRUBS AND PERENNIALS			
<i>Amelanchier alnifolia</i>	Strawberry Madrone	1m, 2m 85H	Full, bushy plants
<i>Cornus alba</i> 'Madrone' (H)	Madrone Tree Dogwood	43 cont.	Full, bushy
<i>Cornus sericea</i>	Red osier Dogwood	43 cont.	Full, bushy plants
<i>Geukensia scabra</i>	Salal	42 cont.	Full, bushy plants
<i>Malva sylvestris</i>	Red Dogwood	42 cont.	Full, bushy plants
<i>Alchemilla nemoralis</i>	Red Dogwood	42 cont.	Full, bushy plants
<i>Polystichum munitum</i>	Woolly Fern	43 cont.	Full, bushy plants
<i>Polygonatum poliflorum</i>	Woolly Fern	43 cont.	Full, bushy plants
<i>Ribes sanguineum</i> 'King Edward'	Flowering Currant	45 cont.	Full, bushy plants
<i>Sambucus racemosa</i>	Red Elder	45 cont.	Full, bushy plants

NOTE: ALL LANDSCAPE TO CONFORM TO BC LANDSCAPE STANDARDS 7th ED. FOR LEVEL 2 GRC IN THE EVENT OF A DISCREPANCY BETWEEN THE PLANT LIST AND THE PLANTING PLAN, THE PLANT LIST SHALL PREVAIL.

Rev	No.	Date	Description
1	1	2024.11	Issue for Review

Sheet No.	Date	Scale	Notes
A	2024.11	1:1000	Issue for Review

eta ENVIRONMENTAL TERRACE

1000 West 2nd Avenue
Vancouver, BC, Canada V6B 1H2
[604] 682-1400
[604] 682-1402
www.eta.com

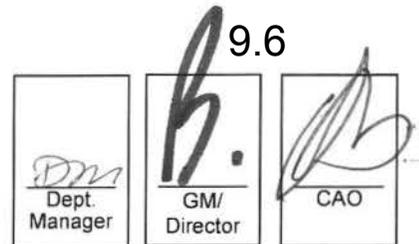
Project:
Braemar School Site
WEDGEWOOD VENTURES
Calder Ave
District of North Vancouver

Drawing Title:
Landscape
Site Plan

Scale:
1:1000
Date:
17-02-2024
Sheet:
L-2

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AGENDA INFORMATION	
<input checked="" type="checkbox"/> Regular Meeting	Date: <u>Jan 18, 2016</u>
<input type="checkbox"/> Committee of the Whole	Date: _____



The District of North Vancouver REPORT TO COUNCIL

January 7, 2016
File: 3060/20.00.0

AUTHOR: Dan Milburn

SUBJECT: Community Amenity Contribution Policy Update

RECOMMENDATION:

- 1) THAT the Community Amenity Contribution Policy (Attachment 1) is endorsed for approval by the Chief Administrative Officer.

REASON FOR REPORT:

The current Community Amenity Policy (Attachment 2) has not been comprehensively reviewed since it was approved in 2010. Staff anticipate the need to amend the policy to keep pace with changing market conditions and pricing of land for residential developments. In addition, the Province published a guideline for Community Amenity Contributions in 2014 titled *Community Amenity Contributions: Balancing Community Planning, Public Benefits and Housing Affordability*, which is intended to “help local governments understand the risks, challenges and recommended practices related to obtaining community amenity contributions (CACs).” Finally, members of the real estate and development community have encouraged the District to review how CACs are collected with a mind to increasing certainty prior to land assembly. For these reasons staff sought advice from Coriolis Consulting Group Corp. to complete the necessary market and financial analysis. A summary of the analysis results are contained in the Analysis section of this report.

This report includes the proposed Community Amenity Contribution Policy (Attachment 1), the current Community Amenity Policy (Attachment 2) and a red-lined version of the current Community Amenity Policy (Attachment 3) to assist in the comparison between the two policies.

SUMMARY:

The Community Amenity Policy was adopted in 2010 and requires updating. Based on a comprehensive review by staff and Coriolis Consulting Corp. staff recommend increasing the rates for CACs outside of centres, and establishing new fixed rate areas within centres. Other areas within the centres would continue to be negotiated on a case-by-case basis. In addition, the draft updated policy clarifies that fixed rates are “targets” only. The option for the developer to negotiate these rates as applicable to the site and project remains in the policy.

This report includes a summary of the proposed changes to the policy, and a summary of the stakeholder input received from members of the development community.

BACKGROUND:

The District of North Vancouver’s current Community Amenity Policy was approved in December 2010 to provide direction on obtaining amenity contributions from privately initiated rezonings.

The following Table 2.0 summarizes the amount of CACs collected and spent from the adoption of the Community Amenity Policy in December 2010 until December 2015. In addition to the amount of CACs collected below an additional \$36.3 million in CACs committed by developers.

Table 2.0

Collected	CAC \$	CACs Spent	Interest	Balance
Total	4,885,196	(761,800)	85,322	4,208,718

CACs Spent	Community	Park	Public Art	Total
Lynn Valley	(18,033)			(18,033)
Lynnmour Inter River	(10,686)	(85,000)	(30,000)	(125,686)
Seymour	(429,303)	(9,531)		(438,834)
Lower Lynn		(135,000)		(135,000)
Community Art			(44,246)	(44,246)
Total	(458,022)	(229,531)	(74,246)	(761,800)

CACs may be provided by developers when Council approves applications for increased density at rezoning to address increased demand on community facilities and services due to growth and redevelopment. CACs can be used towards the development of site specific community amenities or provided as a cash-in-lieu contribution to deliver off-site amenities. CACs help deliver amenities that would otherwise have to be provided through increased property taxes.

EXISTING POLICY:

The District of North Vancouver's existing CAC policy includes two different approaches to determine the appropriate value of a CAC, depending on the location of the rezoning:

- In the designated Town and Village Centres (growth centres), the value of the CAC is determined through a negotiated approach, equivalent to 75% of the estimated increase in the market value of the property due to the rezoning. The reference to 75% of the increase in property value is to ensure that the CAC does not exceed the amount that is financially viable for the development project.
- Outside the Centres, the CAC value is based on a target fixed rate per square foot of additional residential floorspace approved by the rezoning. Outside of centres, the CAC can be negotiated if the developer thinks the fixed rate is not appropriate or the rezoning exceeds the density identified in the OCP.

It has been over four years since existing CAC policy was last examined. Over this time period, market conditions have changed, development regulations (such as DCCs) have changed and more detailed planning work has been completed on the District's growth centres. Therefore, staff determined that it was timely to review the existing CAC policy and retained Coriolis Consulting Corp. to review the existing policy and recommend any changes that should be considered.

ANALYSIS:

Recommended CAC Approach Outside Centres:

Staff's recommended approach to CACs outside of the Centres is:

1. Establish three separate fixed rate CAC categories outside the Centres with fixed rate targets as follows:
 - (a) \$6 per square foot (current rate \$5) of increased permitted residential gross floor area for any project with an FSR less than or equal to 0.8 FSR;
 - (b) \$13 per square foot (current rate \$5) of increased permitted residential gross floor area for any project with an FSR greater than 0.8 but less than or equal to 1.0 FSR
 - (c) \$20 per square foot (current rate \$15) of increased permitted residential gross floor area for any project with an FSR greater than 1.0
2. Negotiate the CAC for the rezoning of any properties that are currently improved with rental housing to take into account the specific details of any rental replacement requirement. The target for negotiations should be at most 75% of the increased value due to the rezoning.
3. Continue to allow negotiated CACs in the specific circumstances currently identified in the District's policy, but change the target negotiated CAC to be a maximum of 75% of the increased value due to the rezoning, rather than "50% to 75%" of the increased

value due to the rezoning as currently written. This will make it consistent with the negotiated approach inside the Centres.

Recommended CAC Approach Inside Centres:

Staff's recommended approach to CACs in the Centres is:

1. Negotiate CACs for major, complex rezonings where it is difficult to determine an appropriate CAC rate in advance of a development application, including:
 - Large sites that have significant land dedications and on-site infrastructure requirements.
 - Sites which include existing rental housing that the District would like to see replaced as part of any redevelopment.
 - Higher density mixed-use sites, such sites in the CRMU 2 and CRMU 3 OCP designations (i.e., mixed-use projects over 1.75 FSR).
 - Highrise residential projects (over 6-storeys in the RES Level 6 designation).
 - Sites identified for a significant on-site amenity.
 - Sites currently zoned for industrial use.
 - Applications that require an OCP amendment.

The target for negotiations should be a maximum of 75% of the increased property value due to the rezoning.

2. Establish new fixed rate area inside the five Centres with a fixed rate target of \$20 per square foot (presently negotiated) of additional permitted residential floorspace. The fixed rate areas for inside centres may be found in Schedule 2 of the Draft CAC Policy attached to this report.

Timing/Approval Process:

The draft CAC policy includes transitional provisions to assist in the orderly implementation of the policy. The transitional provisions are intended to avoid a surge of applications to the District by providing a reasonable amount of time for developers, in the midst of land negotiations, to finalize those negotiations and prepare complete preliminary application submissions.

Concurrence:

Financial Planning has reviewed this report.

Financial Impacts:

Staff acknowledges that market conditions, site specific conditions, and other factors will affect the ability of specific projects to contribute towards infrastructure and amenities.

However, the rates should be adjusted annually based on a publicly available indicator of construction cost inflation, such as the Statistics Canada non-residential construction cost

index. Periodically (e.g., every three years), the fixed rates should be reviewed to account for changes in the market value of developments sites and the market value of additional density.

In addition, staff will be completing an analysis of the anticipated increased CAC revenue as a result of these increased CAC rates and any updated amenity needs identified in the OCP and subsequent town and village centre plans, and will report back to Council.

Liability/Risk:

The proposed policy is not anticipated to create liability for the District.

Social Policy Implications:

No significant social impacts are anticipated as a result of the Draft CAC Policy Update

Environmental Impact:

No significant environmental impacts are anticipated as a result of the Draft CAC Policy Update.

Stakeholder Input:

Staff presented a draft of the Community Amenity Contribution Policy to members of the real estate and development community from the North Vancouver Chamber of Commerce (Meeting November 25th, 2015, with 4 participants) and the Urban Development Institute (Meeting December 2, 2015 with 13 participants).

The following Table 2.0 summarizes the comments raised by the meeting attendees related to the Draft CAC changes and how these comments were addressed (or not) the revised draft attached to this report as Attachment 2 (not in priority order):

Table 2.0: Stakeholder Input

Input Received

Proposed Response

Flat-rate CACs are easier to work with.

Flat rates continue to be used for outside of centres and are expanded to include certain areas within key town and village centres.

Development costs might increase after CAC negotiations are finalized (i.e. costs for off-site services) which unfairly penalizes developers.

Minimum information submission standards are provided by the District to assist in the District's evaluation of the application, but the amount of costing due diligence beyond these minimum requirements is determined by the developer.

A developer should be able to initiate negotiations where a flat rate exists, but the District should not be able to negotiate beyond in established flat rate areas (except under limited and pre-defined circumstances).

The revised policy now includes an indication of when District initiated negotiation may be appropriate.

Calculations for FSR should be consistent.

FSR calculations are consistently applied. Staff will continue to work with developers to communicate the calculation methodology. A definition for Gross Floor Area has been added to the definitions to improve interpretation.

Some other jurisdictions are very difficult to negotiate with, while others are easy to negotiate with.

Alternative CAC calculation methods were evaluated and were not found to be consistent with the District's OCP vision.

Amenities should be clearly defined for the developer's certainty.

Amenities are described in the OCP and area implementation plans. Where the amenity is to be located on the development site, its scope and costing is clearly defined in the negotiation and final CAC rate.

Contributions other than cash should be acknowledged as equivalent contributions (i.e. on-site amenities or rental replacement)

Staff reports indicate the form of amenity contribution which is ultimately determined to be in the best interest of the community.

The reference to amenity strategies in Sec. 3.4 is no longer relevant.

The reference has been removed from the revised policy

The term "equivalent" should be replaced with "maximum" in Sec. 3.5.

The language of the policy has been amended to reflect this intent

The list of potential amenities in Sec. 3.8 should not include "amenity maintenance"

"amenity maintenance" has been removed from the amenity list

The draft policy language regarding exemptions for non-profits (e.g. affordable

The draft exemption language has been removed and deferred to the ongoing

housing) should be amended to be similar to the Metro Vancouver Development Cost Charge Policy exemptions.

affordable housing discussion

The current flat rates should apply during the proposed transitional period before the new rates are implemented. The 6 month implementation notice is sufficient.

The transitional provisions have been amended so they are consistent with this request. The transitional period is intended to provide developers with a reasonable time period to complete land negotiations and prepare a preliminary application.

Conclusion:

The Community Amenity Contribution (CAC) Policy has not been comprehensively reviewed since it was approved in 2010. Staff have sought the advice of Coriolis Consulting Corp. to analyse market conditions and recommend updated CAC rates. Staff have also consulted with the development community through meetings with the Chamber of Commerce and Urban Development Institute. Staff have prepared a revised Community Amenity Contribution Policy for Council's consideration.

Options:

- 1) THAT the Community Amenity Contribution Policy (Attachment 1) is endorsed for approval by the Chief Administrative Officer.
- 2) That Council endorse the retention of the current Community Amenity Policy.
- 3) That Council direct staff to complete additional CAC analysis, or consultation and report those findings to Council.

Respectfully submitted,



Dan Milburn
Deputy General Manager Planning & Permits

Attachments:

- Attachment 1: Community Amenity Contribution Policy (Draft 2016)
- Attachment 2: Community Amenity Policy (2010)
- Attachment 3: Red-lined version of Community Amenity Policy (2010)

REVIEWED WITH:		
<input type="checkbox"/> Sustainable Community Dev. _____	<input type="checkbox"/> Clerk's Office _____	External Agencies:
<input type="checkbox"/> Development Services _____	<input type="checkbox"/> Communications _____	<input type="checkbox"/> Library Board _____
<input type="checkbox"/> Utilities _____	<input type="checkbox"/> Finance _____	<input type="checkbox"/> NS Health _____
<input type="checkbox"/> Engineering Operations _____	<input type="checkbox"/> Fire Services _____	<input type="checkbox"/> RCMP _____
<input type="checkbox"/> Parks _____	<input type="checkbox"/> ITS _____	<input type="checkbox"/> NVRC _____
<input type="checkbox"/> Environment _____	<input type="checkbox"/> Solicitor _____	<input type="checkbox"/> Museum & Arch. _____
<input type="checkbox"/> Facilities _____	<input type="checkbox"/> GIS _____	<input type="checkbox"/> Other: _____
<input type="checkbox"/> Human Resources _____	<input type="checkbox"/> Real Estate _____	



The Corporation of the District of North Vancouver

ADMINISTRATIVE POLICY MANUAL

Section:	Land Administration	8
Sub-Section:	Development	3060
Title:	Community Amenity Contribution Policy	2

1. OBJECTIVE:

To create policy guidance for the provision of community benefits and community amenities achieved through new development.

2. INTERPRETATION:

“Community Benefit” means the overall contribution or improvement that a new development could make to the community.

“Community Amenity” means any public amenity or benefit that improves the quality of life of a community, over and above the new development itself and over and above the Community Benefits listed in Section 3.1 (a) to (e) of this Policy, and may include any of the amenities listed in 3.8 of this Policy.

“Community Amenity Contribution” or **“CAC”** means a community amenity contribution as set out in Sections 3.4 through 3.7 of this policy.

“Gross Floor Area” is the total floor area permissible on a site inclusive of areas exempted from the FSR calculation for zoning purposes, but excluding underground parking.

“OCP” means the District Official Community Plan.

“OCP Designated Centres” means areas as shown in Schedule 2 to this policy.

3. POLICY:

PROVISION OF COMMUNITY BENEFITS:

3.1 New development must meet the goals set out in the District of North Vancouver’s Official Community Plan and provide the following community benefits:

- a) Works and services required to accommodate the development and works and services to the centreline of abutting streets;
- b) Development Cost Charges as specified in the District of North Vancouver Development Cost Charge Bylaw;
- c) Mitigation actions, works and measures to address negative impacts on the community;
- d) Subdivision requirements as applicable;
- e) Any required road dedications;
- f) In certain circumstances, as outlined in Section 3.3 below, new development should provide community amenities or a financial contribution to cover the cost of a specified Community Amenity or a portion thereof sufficient that the District can make provision toward the specified Community Amenity.

- 3.2 The requirements for basic works and services, development cost charges and mitigation measures will be determined through the development review process and in accordance with any applicable statutory requirements and District bylaws and policies. The provision of Community Amenity Contributions should be determined as specified in the balance of this policy. Council supports the view that residents expect new development to pay its own way and make a positive contribution to the community.

PROVISION OF COMMUNITY AMENITY CONTRIBUTIONS:

- 3.3 Community Amenity Contributions should be provided for rezonings that involve an increase in density in accordance with:
- (a) Section 3.4 for sites within the OCP Designated Centres (as defined in Part 2);
 - (b) Section 3.5 for residential development sites outside a designated town centre or village centre for which the OCP contemplates an increase in density;
 - (c) Section 3.6 for sites where the increase in density is not contemplated in the OCP.
- 3.4 The OCP or other adopted town or village centre policy will guide decisions related to Community Amenity Contributions for sites within an OCP designated town centre or village centre. Where a developer is seeking an increase in density for a site within a town centre or village centre, the CACs should be negotiated on a case specific basis, except for the areas specified within the OCP Designated Centres (as defined in Part 2) where the recommended CAC target is \$20 per square foot of increased residential gross floor area calculated on the basis of the proposed gross floor area for the subject site requested by the developer minus the gross floor area under the "base density", where "base density" means the floor space ratio for the site calculated in accordance with "Schedule 1"
- 3.5 For sites within an area contemplated for increased density in the OCP but outside of a town centre or village centre, the recommended CAC target should be calculated as follows:
- (a) \$6 per square foot of increased residential gross floor area for any project with an FSR less than or equal to 0.8 FSR;
 - (b) \$13 per square foot of increased residential gross floor area for any project with an FSR greater than 0.8 but less than or equal to 1.0 FSR
 - (c) \$20 per square foot of increased residential gross floor area for any project with an FSR greater than 1.0

For the purpose of this section 3.5, the increase in gross floor area should be calculated on the basis of the proposed gross floor area for the subject site requested by the developer minus the gross floor area on the site under the "base density", where "base density" means the floor space ratio for the site calculated in accordance with "Schedule 1".

While the above formula is intended to be applicable in the majority of circumstances, there may be rezoning applications where the District or the developer identifies the formula to be inappropriate and in those cases, the CACs should be negotiated outside the above formula. Where the CACs are negotiated outside the above formula, the total value of the CAC should be a maximum of 75% of the estimated increase in the market value of the land attributable to the density increase.

- 3.6 For sites that are being rezoned to permit an increase in gross floor area over and above that which is contemplated in the OCP, CACs should be negotiated on a case by case basis and the value of the CACs should be a maximum of 75% of the estimated increase in the market value of the land attributable to the density increase.
- 3.7 For the purpose of estimating the increase in the market value of land attributable to a proposed density increase, the base market value of the subject land (not necessarily equal to acquisition cost) should be calculated on the basis of the applicable zoning as at the date of the complete preliminary application for the increased density.
- 3.8 When considering the inclusion of a specific amenity rather than cash-in-lieu, the District's OCP or other Council direction or policy may provide guidance as to the type of Community Amenity Contributions that may be provided and if there are no such policies applicable to a proposed new

development, then the following list should be used as a guide for determining the type of Community Amenity Contribution(s) (in no particular order of priority):

- Land for, or provision of, affordable, rental or special needs housing;
- Community, cultural, school, library or recreation facility or facility improvements;
- Seniors care, seniors day care or seniors wellness facility or facility improvements;
- Child care facility or facility improvements;
- Youth, children or family facility or facility improvements;
- Heritage conservation;
- Public Art in accordance with established policy;
- Provision of park land or park improvements;
- Extraordinary pedestrian, cycling, streetscape, public plaza or other public-realm linkages and improvements beyond those required by District bylaws and design guidelines;
- Environmental, or sustainability measures beyond the normal environmental development permit requirements;
- Other Community Amenities as identified by the District of North Vancouver to meet established community goals, policies or needs.

- 3.9 Negotiation of CAC's, as above, will be directed by senior staff and reported to Council.
- 3.10 CAC's should be a cash payment in-lieu of specific Community Amenities unless the District requires the inclusion of a specific Community Amenity. If specific Community Amenities are required, they should be valued in accordance with Section 3.4, 3.5 or 3.6 as applicable.
- 3.11 Where CAC's are being negotiated, the developer should be required to pay the District's costs of the negotiation including the District's cost to engage a qualified consultant for the purposes of such negotiation.
- 3.12 In the case where policy objectives in the OCP or other established policies are inconsistent with this policy, the CAC should be negotiated with the aim of achieving Council's priority objectives in relation to the particular rezoning proposal being applied for.

4.0 SECURING COMMUNITY AMENITY CONTRIBUTIONS:

- 4.1 The provision of CACs may be secured through one or more of the following methods:
- A phased development agreement, under Section 905.1 of the *Local Government Act*, as may be amended from time to time;
 - Zoning for amenities and affordable housing, under Section 904 of the *Local Government Act* as may be amended from time to time;
 - A housing agreement for affordable and special needs housing, under Section 904 and/or 905 of the *Local Government Act* as may be amended from time to time;
 - As articulated in the terms of a sale agreement for projects involving the disposition of an interest in land owned by the District; or
 - Other methods as recommended by the Municipal Solicitor.

5.0 REASON FOR POLICY

To ensure that the community obtains benefits from new development through a fair and equitable approach and to provide opportunities to achieve community improvements and innovation through development.

6.0 PROCEDURE

Planning staff are directed to implement the community amenity policy as part of development application processing and to include a summary of the community benefits when new development requires a report to Council.

This policy should be reviewed at the staff level every two years.

7.0 AUTHORITY TO ACT

Senior staff will negotiate appropriate amenities which will then be referred to Council for final approval.

8.0 TRANSITIONAL PROVISIONS

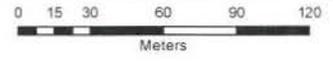
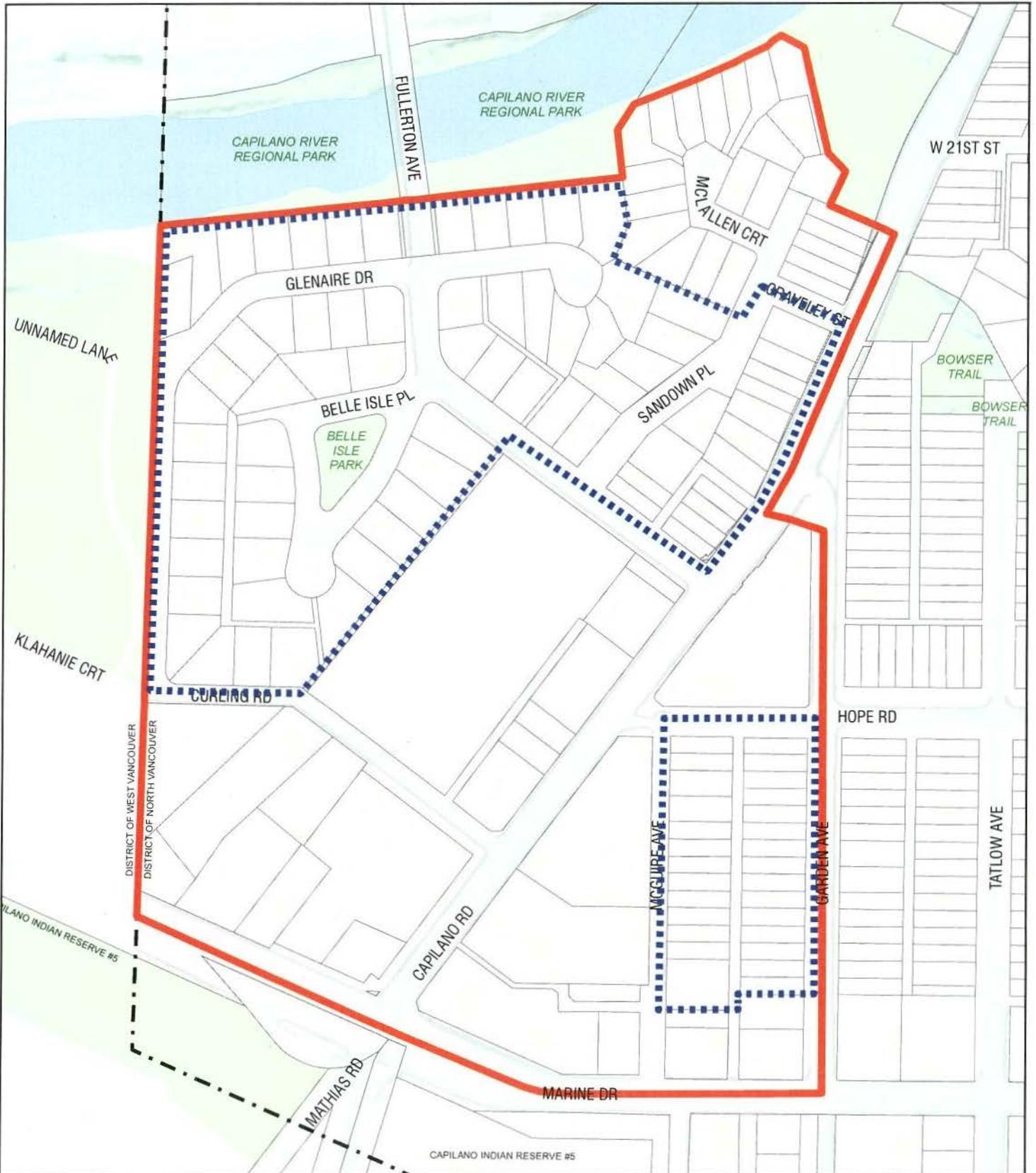
8.1 The Community Amenity Charges in this policy do not apply until August 1, 2016, unless the applicant agrees in writing that this policy should have effect. During this transitional period all CACs should continue to be determined through the Community Amenity Policy adopted in December 2010.

<i>Approval Date:</i>	December 13, 2010	<i>Approved by:</i>	Chief Administrative Officer
<i>1. Amendment Date:</i>		<i>Approved by:</i>	
<i>2. Amendment Date:</i>		<i>Approved by:</i>	
<i>3. Amendment Date:</i>		<i>Approved by:</i>	

SCHEDULE 1: DEEMED DENSITIES IN DNV ZONING DISTRICTS

ZONING CATEGORY			
RESIDENTIAL	ZONE	Building Form	Deemed Density
Single Family Residential (RS)			
	RS1	Single Family	0.45
	RS2	Single Family	0.45
	RS3	Single Family	0.45
	RS4	Single Family	0.45
	RS5	Single Family	0.45
	Neighbourhood Zones		0.45 – 0.55 As specified in zone
Multi-Family Residential (RM)			
	RM1	Single Family	0.45
	RM1	Townhouse	0.45
	RM2	Single Family	0.45
	RM2	Townhouse	0.6
	RM2	Low-rise Apartment	0.6
	RM3	Townhouse	0.75
	RM3	Low-rise Apartment	0.75
	RM5	Townhouse	0.45
	RM6	Townhouse or Low-rise Apt.	0.80
	RM7	Townhouse or Low-rise Apartment	0.80
Low-rise Residential (RL)			
	RL1	Townhouse	1.0
	RL1	Low-rise Apartment	1.0
	RL2	Low-rise Apartment	1.2
	RL3	Low-rise Apartment	1.25
	RL4	Low-rise Apartment	1.3 FSR
High-rise Residential (RH)			
	RH2	High-rise Apartment	1.75
COMMERCIAL			
	ZONE	Building Form	Deemed Density
General Commercial Zone 1	C 1		1.75
Local Commercial Zone 1A	C 1A		0.55
Corner Store Commercial Zone 1B	C 1B		0.55

General Commercial Zone 1L	C 1L		1.75
General Commercial Zone 2	C 2		1.75 except 1.0 in Edgemont Village
General Commercial Zone 3	C 3		1.75 except 1.0 in Edgemont Village
General Commercial Zone 3A	C 3A		1.75 except 1.0 in Edgemont Village
Tourist Commercial Zone	C 4	Tourist Accommodation	1.2 for commercial use (Change in use would trigger negotiated process)
Entertainment Commercial Zone	C 5		2.4 for commercial use (Change in use would trigger negotiated process)
Entertainment/Outdoor Tourist Attraction	C 5A	Suspension Bridge	0.35 for commercial use (Change in use would trigger negotiated process)
Public House Commercial Zone 6	C 6	Neighbourhood Pub	1.75 for commercial use (Change in use would trigger negotiated process)
General Commercial Zone 7	C 7	Garden Centre	0.5 (Change in use would trigger negotiated process)
Commercial Business Zone 8	C 8		1.0 (Change in use would trigger negotiated process)
Marine Drive Commercial Zone	C 9		1.0 for lots less than 1,100 m ² 1.75 for lots = to or greater than 1,100 m ²
Commercial Business Zone 10	C 10		1.75 (Change in use would trigger negotiated process)
COMPREHENSIVE DEVELOPMENT ZONES (CD)	ZONE	Building Form	Deemed Density
	CD-1 through CD-61+	Varies by zone	Refer to Zone

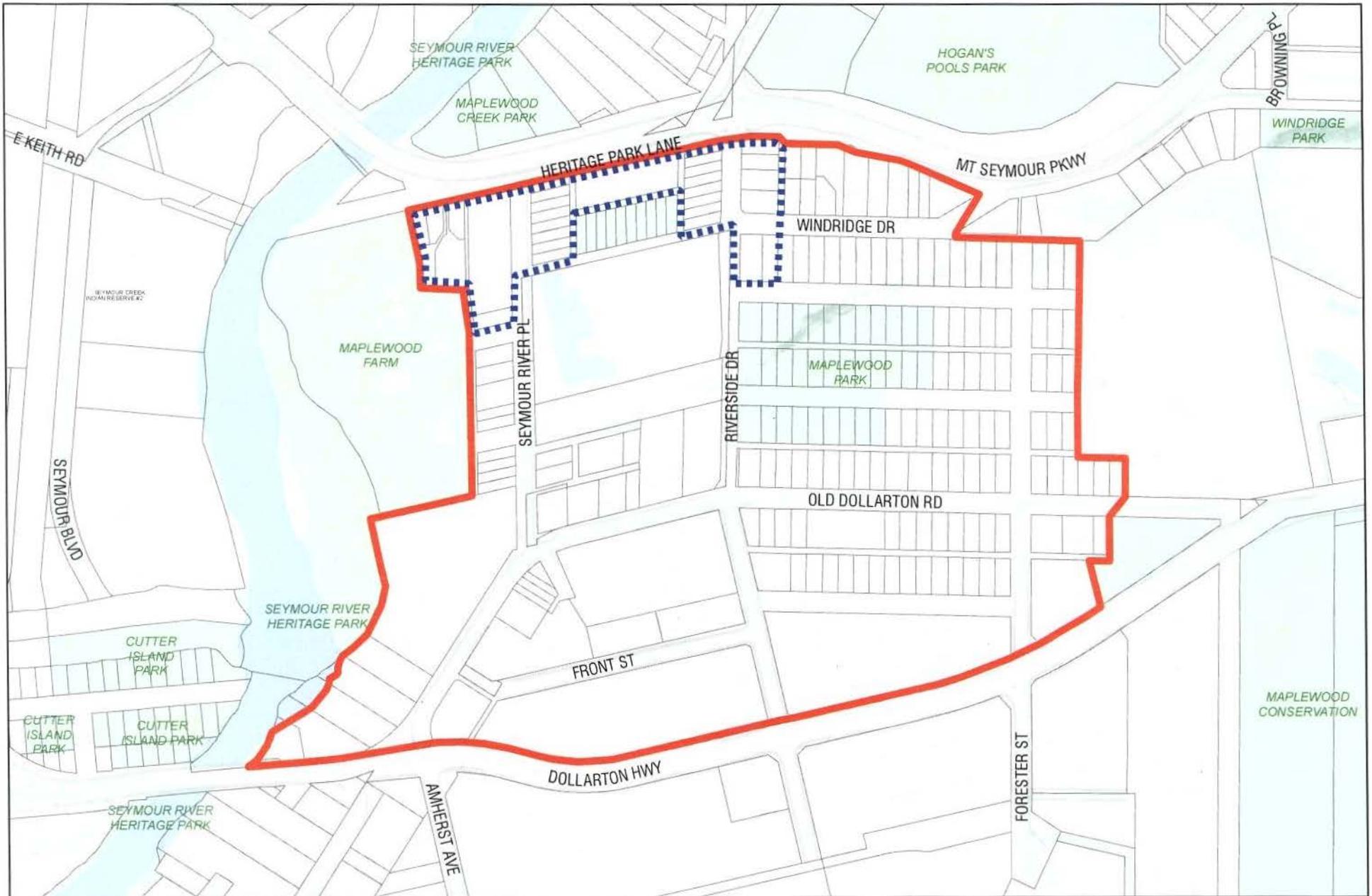


Scale: 1:3,000

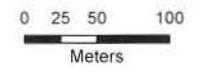
- OCP Designated Centre
- - - - - Fixed Rate Boundary



LIONS GATE VILLAGE

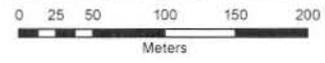
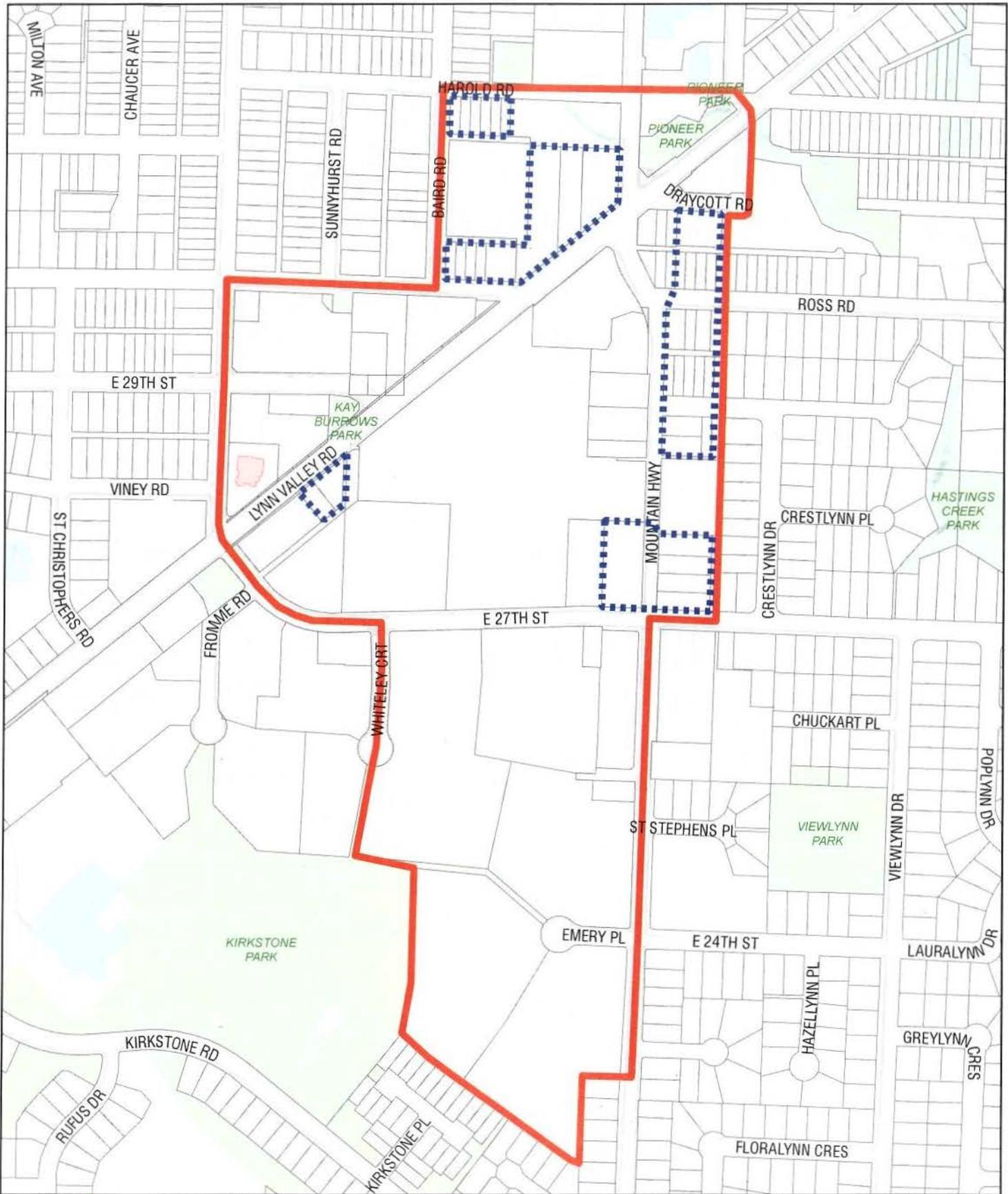


MAPLEWOOD VILLAGE CENTRE



Scale: 1:5,000

- OCP Designated Centre
- - - - - Fixed Rate Boundary



Scale: 1:5,000

- OCP Designated Centre
- Fixed Rate Boundary

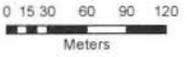


**Planning
Department**

LYNN VALLEY TOWN CENTRE

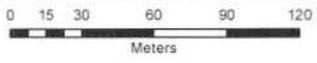
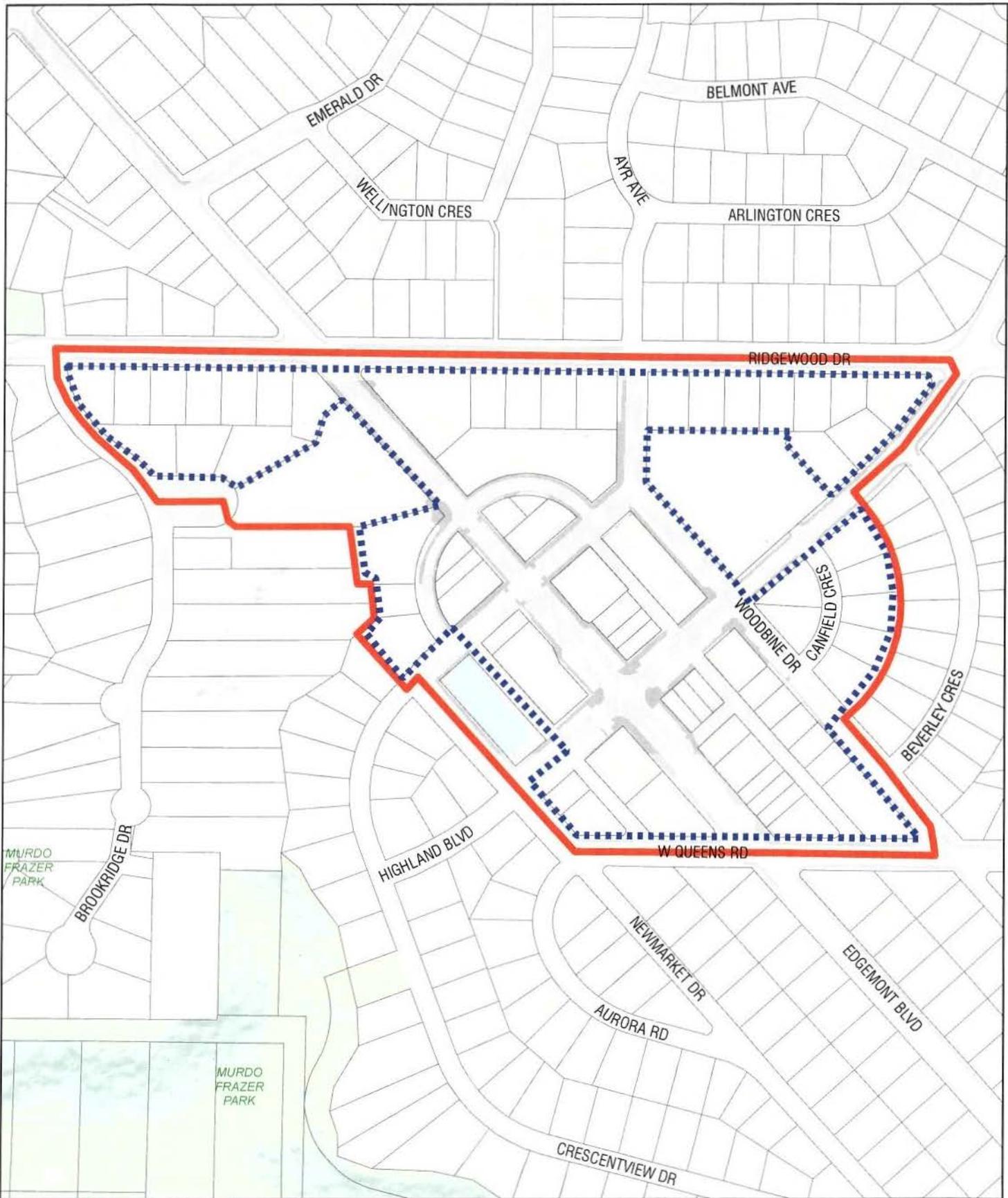


LYNN CREEK TOWN CENTRE



Scale: 1:5,500

- OCP Designated Centre
- - - - Fixed Rate Boundary



Scale: 1:3,000

- OCP Designated Centre
- Fixed Rate Boundary



**Planning
Department**

EDGEMONT VILLAGE CENTRE



The Corporation of the District of North Vancouver

ADMINISTRATIVE POLICY MANUAL

Section:	Land Administration	8
Sub-Section:	Development	3060
Title:	Community Amenity Policy	2

1. OBJECTIVE:

To create policy guidance for the provision of community benefits and community amenities achieved through new development.

2. INTERPRETATION:

“Community Benefit” means the overall contribution or improvement that a new development could make to the community.

“Community Amenity” means any public amenity or benefit that improves the quality of life of a community, over and above the new development itself and over and above the Community Benefits listed in Section 3.1 (a) to (e) of this Policy, and may include any of the amenities listed in 3.8 of this Policy.

“Community Amenity Contribution” or **“CAC”** means a community amenity contribution as set out in Sections 3.4 through 3.7 of this policy.

“OCP” means the District Official Community Plan.

3. POLICY:**PROVISION OF COMMUNITY BENEFITS:**

- 3.1 New development must meet the goals set out in the District of North Vancouver's Official Community Plan and provide the following community benefits:
- a) Works and services required to accommodate the development and works and services to the centreline of abutting streets;
 - b) Development Cost Charges as specified in the District of North Vancouver Development Cost Charge Bylaw;
 - c) Mitigation actions, works and measures to address negative impacts on the community;
 - d) Subdivision requirements as applicable;
 - e) Any required road dedications;
 - f) In certain circumstances, as outlined in Section 3.3 below, new development should provide community amenities or a financial contribution to cover the cost of a specified Community Amenity or a portion thereof sufficient that the District can make provision toward the specified Community Amenity.
- 3.2 The requirements for basic works and services, development cost charges and mitigation measures will be determined through the development review process and in accordance with any applicable statutory requirements and District bylaws and policies. The requirement for Community Amenity Contributions should be determined as specified in the balance of this policy.

PROVISION OF COMMUNITY AMENITY CONTRIBUTIONS:

- 3.3 Community Amenity Contributions should be required for rezonings that involve an increase in density in accordance with:
- Section 3.4 for sites within the Lower Lynn, Lynn Valley, Lower Capilano or Maplewood OCP designated town centres or village centres;
 - Section 3.5 for residential development sites outside a designated town centre or village centre for which the OCP contemplates an increase in density;
 - Section 3.6 for sites where the increase in density is not contemplated in the OCP.

3.4 The OCP or other adopted town or village centre policy will guide decisions related to Community Amenity Contributions for sites within an OCP designated town centre or village centre. Where a developer is seeking an increase in density for a site within an existing or a proposed town centre or village centre prior to adoption of an amenity strategy for the town centre or village centre, the CACs should be negotiated on a case specific basis.

3.5 For sites within an area contemplated for increased density in the OCP but outside of a town centre or village centre, CACs should be required and should be calculated as follows: \$5 per square foot of increased residential gross floor area for townhouse, duplex, triplex or similar development; and \$15 per square foot of increased residential gross floor area for apartment development. For the purpose of this section 3.5, the increase in gross floor area should be calculated on the basis of the proposed gross floor area for the subject site requested by the developer minus the total gross floor area permitted on the site under the "base density", where "base density" means the floor area ratio for the site calculated in accordance with "Schedule 1".

While the above formula is intended to be applicable in the majority of circumstances, there may be rezoning applications where the District or the Developer identifies the formula to be inappropriate and in those cases, the CACs should be negotiated outside the above formula. Where the CACs are negotiated outside the above formula, the total value of the CAC should be equivalent to 50% of the estimated increase in the market value of the land attributable to the density increase.

3.6 For sites that are being rezoned to permit an increase in gross floor area over and above that which is contemplated in the OCP, CACs should be negotiated on a case by case basis and the value of the CACs should be equivalent to 50% to 75% of the estimated increase in the market value of the land attributable to the density increase.

3.7 For the purpose of estimating the increase in the market value of land attributable to a proposed density increase, the base market value of the subject land (not necessarily equal to acquisition cost) should be calculated on the basis of the applicable zoning as at the date of the formal application for the increased density.

3.8 When considering the inclusion of a specific amenity rather than cash-in-lieu, the District's OCP or other Council direction or policy may provide guidance as to the type of Community Amenity Contributions that may be provided and if there are no such policies applicable to a proposed new development, then the following list should be used as a guide for determining the type of Community Amenity Contribution(s) (in no particular order of priority):

- Land for, or provision of, affordable, rental or special needs housing;
- Community, cultural, school, library or recreation facility or facility improvements;
- Seniors care, seniors day care or seniors wellness facility or facility improvements;
- Child care facility or facility improvements;
- Youth, children or family facility or facility improvements;
- Heritage conservation;
- Public Art in accordance with established policy;
- Provision of park land or park improvements;
- Extraordinary pedestrian, cycling, streetscape, public plaza or other public-realm linkages and improvements beyond those required by District bylaws and design guidelines;

- Environmental, or sustainability measures beyond the normal environmental development permit requirements;
- Contribution toward amenity maintenance, for example contribution into a long term maintenance fund to offset future maintenance costs of community amenities;
- Other Community Amenities as identified by the District of North Vancouver to meet established community goals, policies or needs.

3.9 Negotiation of CAC's, as above, will be directed by senior staff and reported to Council.

3.10 CAC's should be a cash payment in-lieu of specific Community Amenities unless the District requires the inclusion of a specific Community Amenity. If specific Community Amenities are required, they should be valued in accordance with Section 3.4, 3.5 or 3.6 as applicable.

3.11 Where CAC's are being negotiated, the developer should be required to pay the District's costs of the negotiation including the District's cost to engage a qualified consultant for the purposes of such negotiation.

3.12 In the case where policy objectives in the OCP or other established policies are inconsistent with this policy, the CAC should be negotiated with the aim of achieving Council's priority objectives in relation to the particular rezoning proposal being applied for.

4.0 SECURING COMMUNITY AMENITY CONTRIBUTIONS:

4.1 The provision of Community Amenities Contributions may be secured through one or more of the following methods:

- A phased development agreement, under Section 905.1 of the *Local Government Act*, as may be amended from time to time;
- Zoning for amenities and affordable housing, under Section 904 of the *Local Government Act* as may be amended from time to time;
- A housing agreement for affordable and special needs housing, under Section 904 and/or 905 of the *Local Government Act* as may be amended from time to time;
- As articulated in the terms of a sale agreement for projects involving the disposition of an interest in land owned by the District; or
- Other methods as recommended by the Municipal Solicitor.

5.0 REASON FOR POLICY

To ensure that the community obtains benefits from new development through a fair and equitable approach and to provide opportunities to achieve community improvements and innovation through development.

6.0 PROCEDURE

Planning staff are directed to implement the community amenity policy as part of development application processing and to include a summary of the community benefits when new development requires a report to Council.

This policy should be reviewed at the staff level every two years.

7.0 AUTHORITY TO ACT

Senior staff will negotiate appropriate amenities which will then be referred to Council for final approval.

Approval Date:	<u>December 13, 2010</u>	Approved by:	<u>Chief Administrative Officer</u>
1. Amendment Date:		Approved by:	
2. Amendment Date:		Approved by:	
3. Amendment Date:		Approved by:	

SCHEDULE 1: DEEMED DENSITIES IN DNV ZONING DISTRICTS

ZONING CATEGORY			
RESIDENTIAL	ZONE	Building Form	Deemed Density
Single Family Residential (RS)			
	RS1	Single Family	0.45
	RS2	Single Family	0.45
	RS3	Single Family	0.45
	RS4	Single Family	0.45
	RS5	Single Family	0.45
	Neighbourhood Zones		0.45 – 0.55 As specified in zone
Multi-Family Residential (RM)			
	RM1	Single Family	0.45
	RM1	Townhouse	0.45
	RM2	Single Family	0.45
	RM2	Townhouse	0.6
	RM2	Low-rise Apartment	0.6
	RM3	Townhouse	0.75
	RM3	Low-rise Apartment	0.75
	RM5	Townhouse	0.45
	RM6	Townhouse or Low-rise Apt.	0.80
	RM7	Townhouse or Low-rise Apartment	0.80
Low-rise Residential (RL)			
	RL1	Townhouse	1.0
	RL1	Low-rise Apartment	1.0
	RL2	Low-rise Apartment	1.2
	RL3	Low-rise Apartment	1.25
	RL4	Low-rise Apartment	1.3 FSR
High-rise Residential (RH)			
	RH2	High-rise Apartment	1.75
COMMERCIAL	ZONE	Building Form	Deemed Density
General Commercial Zone 1	C 1		1.75
Local Commercial Zone 1A	C 1A		0.55
Corner Store Commercial Zone 1B	C 1B		0.55
General Commercial Zone 1L	C 1L		1.75
General Commercial Zone 2	C 2		1.75 except 1.0 in Edgemont Village
General Commercial Zone 3	C 3		1.75 except 1.0 in Edgemont Village
General Commercial Zone 3A	C 3A		1.75 except 1.0 in Edgemont Village
Tourist Commercial Zone	C 4	Tourist Accommodation	1.2 for commercial use (Change in use would trigger negotiated process)
Entertainment Commercial Zone	C 5		2.4 for commercial use (Change in use would trigger negotiated process)
Entertainment/Outdoor Tourist Attraction	C 5A	Suspension Bridge	0.35 for commercial use (Change in use would trigger negotiated process)
Public House Commercial Zone 6	C 6	Neighbourhood Pub	1.75 for commercial use (Change in use would trigger negotiated process)
General Commercial Zone 7	C 7	Garden Centre	0.5 (Change in use would trigger negotiated process)
Commercial Business Zone 8	C 8		1.0 (Change in use would

			trigger negotiated process)
Marine Drive Commercial Zone	C 9		1.0 for lots less than 1,100 m ² 1.75 for lots = to or greater than 1,100 m ²
Commercial Business Zone 10	C 10		1.75 (Change in use would trigger negotiated process)
COMPREHENSIVE DEVELOPMENT ZONES (CD)	ZONE	Building Form	Deemed Density
	CD-1 through CD-61+	Varies by zone	Refer to Zone



The Corporation of the District of North Vancouver

ADMINISTRATIVE POLICY MANUAL

Section:	Land Administration	8
Sub-Section:	Development	3060
Title:	(DRAFT) Community Amenity Contribution Policy	2

1. OBJECTIVE:

To create policy guidance for the provision of community benefits and community amenities achieved through new development.

2. INTERPRETATION:

“**Community Benefit**” means the overall contribution or improvement that a new development could make to the community.

“**Community Amenity**” means any public amenity or benefit that improves the quality of life of a community, over and above the new development itself and over and above the Community Benefits listed in Section 3.1 (a) to (e) of this Policy, and may include any of the amenities listed in 3.8 of this Policy.

“**Community Amenity Contribution**” or “**CAC**” means a community amenity contribution as set out in Sections 3.4 through 3.7 of this policy.

“Gross Floor Area” is the total floor area permissible on a site inclusive of areas exempted from the FSR calculation for zoning purposes, but excluding underground parking.

“**OCP**” means the District Official Community Plan.

“OCP Designated Centres” means areas as shown in Schedule 2 to this policy.

3. POLICY:**PROVISION OF COMMUNITY BENEFITS:**

3.1 New development must meet the goals set out in the District of North Vancouver’s Official Community Plan and provide the following community benefits:

- a) Works and services required to accommodate the development and works and services to the centreline of abutting streets;
- b) Development Cost Charges as specified in the District of North Vancouver Development Cost Charge Bylaw;
- c) Mitigation actions, works and measures to address negative impacts on the community;
- d) Subdivision requirements as applicable;
- e) Any required road dedications;
- f) In certain circumstances, as outlined in Section 3.3 below, new development should provide community amenities or a financial contribution to cover the cost of a specified Community Amenity or a portion thereof sufficient that the District can make provision toward the specified Community Amenity.

- 3.2 The requirements for basic works and services, development cost charges and mitigation measures will be determined through the development review process and in accordance with any applicable statutory requirements and District bylaws and policies. The ~~requirement provision for of~~ Community Amenity Contributions should be determined as specified in the balance of this policy. Council supports the view that residents expect new development to pay its own way and make a positive contribution to the community.

PROVISION OF COMMUNITY AMENITY CONTRIBUTIONS:

- 3.3 Community Amenity Contributions should be ~~required provided~~ for rezonings that involve an increase in density in accordance with:
- (a) Section 3.4 for sites within the ~~OCP Designated Centres (as defined in Part 2) Lower Lynn, Lynn Valley, Lower Capilano or Maplewood OCP designated town centres or village centres;~~
 - (b) Section 3.5 for residential development sites outside a designated town centre or village centre for which the OCP contemplates an increase in density;
 - (c) Section 3.6 for sites where the increase in density is not contemplated in the OCP.
- 3.4 The OCP or other adopted town or village centre policy will guide decisions related to Community Amenity Contributions for sites within an OCP designated town centre or village centre. Where a developer is seeking an increase in density for a site within an ~~existing or a proposed town centre or village centre prior to adoption of an amenity strategy for the~~ town centre or village centre, the CACs should be negotiated on a case specific basis, ~~except for the areas specified within the OCP Designated Centres (as defined in Part 2) where the recommended CAC target is \$20 per square foot of increased residential gross floor area calculated on the basis of the proposed gross floor area for the subject site requested by the developer minus the gross floor area under the "base density", where "base density" means the floor space ratio for the site calculated in accordance with "Schedule 1".~~
- 3.5 For sites within an area contemplated for increased density in the OCP but outside of a town centre or village centre, ~~the recommended CAC targets should be required and~~ should be calculated as follows:
- (a) ~~\$5-6~~ per square foot of increased residential gross floor area ~~for any project with an FSR less than or equal to 0.8 FSR;~~
 - (b) ~~\$13~~ per square foot of increased residential gross floor area for any project with an FSR greater than 0.8 but less than or equal to 1.0 FSR
 - (b)(c) ~~\$20~~ per square foot of increased residential gross floor area for any project with an FSR greater than 1.0 ~~for townhouse, duplex, triplex or similar development, and \$15 per square foot of increased residential gross floor area for apartment development.~~

For the purpose of this section 3.5, the increase in gross floor area should be calculated on the basis of the proposed gross floor area for the subject site requested by the developer minus the ~~total~~ gross floor area ~~permitted~~ on the site under the "base density", where "base density" means the floor ~~area~~ space ratio for the site calculated in accordance with "Schedule 1".

While the above formula is intended to be applicable in the majority of circumstances, there may be rezoning applications where the District or the ~~d~~Developer identifies the formula to be inappropriate and in those cases, the CACs should be negotiated outside the above formula. Where the CACs are negotiated outside the above formula, the total value of the CAC should be ~~equivalent to a maximum of 50~~ equivalent to a maximum of 75% of the estimated increase in the market value of the land attributable to the density increase.

- 3.6 For sites that are being rezoned to permit an increase in gross floor area over and above that which is contemplated in the OCP, CACs should be negotiated on a case by case basis and the value of the CACs should be ~~equivalent to a maximum of 50% to~~ 75% of the estimated increase in the market value of the land attributable to the density increase.
- 3.7 For the purpose of estimating the increase in the market value of land attributable to a proposed density increase, the base market value of the subject land (not necessarily equal to acquisition cost) should be calculated on the basis of the applicable zoning as at the date of the ~~formal complete preliminary~~ application for the increased density.

3.8 When considering the inclusion of a specific amenity rather than cash-in-lieu, the District's OCP or other Council direction or policy may provide guidance as to the type of Community Amenity Contributions that may be provided and if there are no such policies applicable to a proposed new development, then the following list should be used as a guide for determining the type of Community Amenity Contribution(s) (in no particular order of priority):

- Land for, or provision of, affordable, rental or special needs housing;
- Community, cultural, school, library or recreation facility or facility improvements;
- Seniors care, seniors day care or seniors wellness facility or facility improvements;
- Child care facility or facility improvements;
- Youth, children or family facility or facility improvements;
- Heritage conservation;
- Public Art in accordance with established policy;
- Provision of park land or park improvements;
- Extraordinary pedestrian, cycling, streetscape, public plaza or other public-realm linkages and improvements beyond those required by District bylaws and design guidelines;
- Environmental, or sustainability measures beyond the normal environmental development permit requirements;
- ~~Contribution toward amenity maintenance, for example contribution into a long term maintenance fund to offset future maintenance costs of community amenities;~~
- Other Community Amenities as identified by the District of North Vancouver to meet established community goals, policies or needs.

3.9 Negotiation of CAC's, as above, will be directed by senior staff and reported to Council.

3.10 CAC's should be a cash payment in-lieu of specific Community Amenities unless the District requires the inclusion of a specific Community Amenity. If specific Community Amenities are required, they should be valued in accordance with Section 3.4, 3.5 or 3.6 as applicable.

3.11 Where CAC's are being negotiated, the developer should be required to pay the District's costs of the negotiation including the District's cost to engage a qualified consultant for the purposes of such negotiation.

3.12 In the case where policy objectives in the OCP or other established policies are inconsistent with this policy, the CAC should be negotiated with the aim of achieving Council's priority objectives in relation to the particular rezoning proposal being applied for.

4.0 SECURING COMMUNITY AMENITY CONTRIBUTIONS:

4.1 The provision of ~~Community Amenities Contributions~~CACs may be secured through one or more of the following methods:

- A phased development agreement, under Section 905.1 of the *Local Government Act*, as may be amended from time to time;
- Zoning for amenities and affordable housing, under Section 904 of the *Local Government Act* as may be amended from time to time;
- A housing agreement for affordable and special needs housing, under Section 904 and/or 905 of the *Local Government Act* as may be amended from time to time;
- As articulated in the terms of a sale agreement for projects involving the disposition of an interest in land owned by the District; or
- Other methods as recommended by the Municipal Solicitor.

5.0 REASON FOR POLICY

To ensure that the community obtains benefits from new development through a fair and equitable approach and to provide opportunities to achieve community improvements and innovation through development.

6.0 PROCEDURE

Planning staff are directed to implement the community amenity policy as part of development application processing and to include a summary of the community benefits when new development requires a report to Council.

This policy should be reviewed at the staff level every two years.

7.0 AUTHORITY TO ACT

Senior staff will negotiate appropriate amenities which will then be referred to Council for final approval.

8.0 TRANSITIONAL PROVISIONS

8.1 The Community Amenity Charges in this policy do not apply until August 1, 2016, unless the applicant agrees in writing that this policy should have effect. During this transitional period all CACs should continue to be determined through the Community Amenity Policy adopted in December 2010.

Approval Date:	December 13, 2010 January 18, 2016	Approved by:	Chief Administrative Officer
1. Amendment Date:		Approved by:	
2. Amendment Date:		Approved by:	
3. Amendment Date:		Approved by:	

SCHEDULE 1: DEEMED DENSITIES IN DNV ZONING DISTRICTS

ZONING CATEGORY	ZONE	Building Form	Deemed Density
RESIDENTIAL			
Single Family Residential (RS)			
	RS1	Single Family	0.45
	RS2	Single Family	0.45
	RS3	Single Family	0.45
	RS4	Single Family	0.45
	RS5	Single Family	0.45
	Neighbourhood Zones		0.45 – 0.55 As specified in zone
Multi-Family Residential (RM)			
	RM1	Single Family	0.45
	RM1	Townhouse	0.45
	RM2	Single Family	0.45
	RM2	Townhouse	0.6
	RM2	Low-rise Apartment	0.6
	RM3	Townhouse	0.75
	RM3	Low-rise Apartment	0.75
	RM5	Townhouse	0.45
	RM6	Townhouse or Low-rise Apt.	0.80
	RM7	Townhouse or Low-rise Apartment	0.80
Low-rise Residential (RL)			
	RL1	Townhouse	1.0
	RL1	Low-rise Apartment	1.0
	RL2	Low-rise Apartment	1.2
	RL3	Low-rise Apartment	1.25
	RL4	Low-rise Apartment	1.3 FSR

High-rise Residential (RH)			
	RH2	High-rise Apartment	1.75
COMMERCIAL	ZONE	Building Form	Deemed Density
General Commercial Zone 1	C 1		1.75
Local Commercial Zone 1A	C 1A		0.55
Corner Store Commercial Zone 1B	C 1B		0.55
General Commercial Zone 1L	C 1L		1.75
General Commercial Zone 2	C 2		1.75 except 1.0 in Edgemont Village
General Commercial Zone 3	C 3		1.75 except 1.0 in Edgemont Village
General Commercial Zone 3A	C 3A		1.75 except 1.0 in Edgemont Village
Tourist Commercial Zone	C 4	Tourist Accommodation	1.2 for commercial use (Change in use would trigger negotiated process)
Entertainment Commercial Zone	C 5		2.4 for commercial use (Change in use would trigger negotiated process)
Entertainment/Outdoor Tourist Attraction	C 5A	Suspension Bridge	0.35 for commercial use (Change in use would trigger negotiated process)
Public House Commercial Zone 6	C 6	Neighbourhood Pub	1.75 for commercial use (Change in use would trigger negotiated process)
General Commercial Zone 7	C 7	Garden Centre	0.5 (Change in use would trigger negotiated process)
Commercial Business Zone 8	C 8		1.0 (Change in use would trigger negotiated process)
Marine Drive Commercial Zone	C 9		1.0 for lots less than 1,100 m ² 1.75 for lots = to or greater than 1,100 m ²
Commercial Business Zone 10	C 10		1.75 (Change in use would trigger negotiated process)
COMPREHENSIVE DEVELOPMENT ZONES (CD)	ZONE	Building Form	Deemed Density
	CD-1 through CD-61+	Varies by zone	Refer to Zone

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Clerks
District of North Vancouver

North Vancouver Recreation & Culture Commission

December 21, 2015

Mayor Richard Walton and District Council
 District of North Vancouver
 355 West Queens Road
 North Vancouver, BC V7N 4N5

Dear Mayor Walton and Councillors:

Re: Motion Passed by the North Vancouver Recreation & Culture Commission

The North Vancouver Recreation & Culture Commission passed the following motion at its November 19, 2015 meeting:

THAT the Commission recommends that the North Vancouver District Council allocate a \$200 individual travel grant to [District of North Vancouver resident] Brendan Artley and a \$1,000 grant to the Capilano University Men's Soccer Team.

The staff report upon which the motion was based is attached for your information.

We hereby request that District Council consider the Commission's recommendation to award the above applicants with Sport and Recreation Travel Grants as noted.

If further information or clarification is required, please do not hesitate to contact me.

Sincerely,

Heather A. Turner
 Director of Recreation & Culture

Attachment

cc. Ron Holbrook, Coordinator



North Vancouver Recreation & Culture Commission

Commission Meeting Date:

November 19, 2015

Subject: Sport and Recreation Travel Grants

RECOMMENDATION:

THAT the Commission recommends:

1. That District Council allocate a \$200 individual travel grant to Brendan Artley and a \$1,000 grant to the Capilano University Mens Soccer Team;
2. That City Council allocate \$100 individual travel grants to Kristin Schulz and Alana Mussatto and a \$500 grant to the Capilano University Mens Soccer Team; and
3. That City Council considers increasing the maximum amounts for eligible individual and team grants to be the same as the District of North Vancouver (\$200 per individual to a maximum of \$1,000 team), effective January 2016.

BACKGROUND:

Since the early 1990s, the City and District of North Vancouver have been allocating Sport and Recreation Travel Grants to eligible individuals and teams who have achieved championship status and are advancing to interprovincial, national or international championship events that are held outside of British Columbia. The North Vancouver Recreation & Culture Commission reviews applications and make recommendations to Council in accordance with the approved municipal guidelines.

DISCUSSION:

Sport and Recreation Travel Grant applications were received for:

- District resident Brendan Artley of the NorWesters Track and Field Club, for attendance at the Legion Midget & Youth Canadian Championships in St. Therese, Quebec (August 7-9, 2015)
- City resident Kristin Schulz of the NorWesters Track and Field Club, for attendance at the Legion Midget & Youth Canadian Championships in St. Therese, Quebec (August 7-9, 2015)
- City resident Alana Mussatto of the Simon Fraser University Track and Field team for attendance at the Canadian Track and Field Championships in Edmonton, Alberta (July 2-5, 2015)
- The Capilano University Mens Soccer Team (with eight City resident athletes and six District resident athletes) for attendance at the Canadian Collegiate Athletic Association Soccer Championships in Longueuil, Quebec (November 9-15, 2015) - see Attachment 1.

The eligibility and criteria for these grants is as follows:

- The team or individual must have achieved regional or higher championship status recognized by an authorized sport or recreation organization and be advancing to finals competition at a Western Canada, national or international championship (out of province only).
- City residents are eligible for City funding and District residents are eligible for District funding. Funding is for athletes only.
- For teams with residents from outside of North Vancouver, only those who reside in the City or District will be eligible for a grant.
- Applicant organizations must be not for profit.
- Applications are to be submitted by the organization through which the team or individual competes, are to be received prior to the championship event taking place and must identify funding from other sources.

All applications in this report meet the funding criteria for the grant program and therefore all are being recommended for approval. Not all applications were received prior to the applicable championship event, but there is past precedent for consideration of these applications so it is recommended that they be approved.

In 1992, responsibility to administer the Sport and Recreation Travel Grant programs of the City and District was delegated to the Commission. Each Council maintained separate authority for determining their grant amounts and the ultimate authority for approval of the grants. At that time, the maximum amounts provided for individuals and teams allowed by the District and City were the same.

On September 8, 1997, District Council increased their maximum grant amounts to \$200/individual to a maximum of \$1,000 per team and the City's remained at \$100/individual to a maximum of \$500 per team. In order to keep these grants consistent between the two municipalities and fair to all North Vancouver residents, it is recommended that the City of North Vancouver increase the maximum grant amount for City residents up to \$200/individual to a maximum amount of \$1,000 per team, effective January 1, 2016.

BUDGET IMPLICATIONS:

The City and District of North Vancouver have budgets for this grant program. If City Council decided to provide the same amount as the District, the annual budget may need to be doubled from \$1,000 to \$2,000.

CONCURRENCE & COMMUNICATION PLAN:

The Commission's recommendation will be communicated to the City and District of North Vancouver Councils.

POLICY/AUTHORITY:

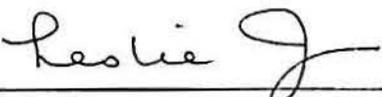
In 1992, the City and District Councils authorized the Commission to review and make recommendations to the Councils on the merits of applications for Sport and Recreation Travel Grants.

CONCLUSION:

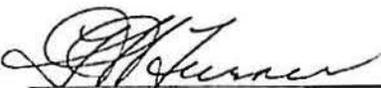
The recommended awards are the maximum allowable according to the current Travel Grant Guidelines of the District and City of North Vancouver.



Ron Holbrook
Recreation Coordinator



Leslie June
Manager of Recreation & Culture



Heather A. Turner
Director of Recreation & Culture

November 6, 2015

City of North Vancouver
141 West 14th Street
North Vancouver, BC V7M 1H9

Attention: Counsel Members

On October 30-31st, 2015 the Capilano University Blues Men's Soccer Team won GOLD at the PACWEST Men's & Women's Provincial Championship last weekend. As a result, this entitles them to compete in the CCAA Men's Soccer National Championship - November 9-15, 2015 in Longueuil, QC.

We are very proud of our Men's Soccer team and look forward to next week's competition. As you know sending a team to nationals is very costly venture. We are applying for the Sport and Recreation Travel Grant on behalf of our Men's Soccer team to assist us in offsetting some of these costs.

Many of our Athletes reside in the City of North Vancouver and the District of North Vancouver. We respectfully ask if the City Council would consider awarding our Men's Soccer program \$200 per person in grant funding to match that of the District of North Vancouver.

If you have any questions regarding the upcoming Nationals competition next week please do not hesitate to contact me for more information. I can be reached by phone at 604-984-4977 or by email at mwilliam@capilanou.ca.

Kind regards,



Milt Williams
Manager, Athletics & Recreation

MW/ro

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